

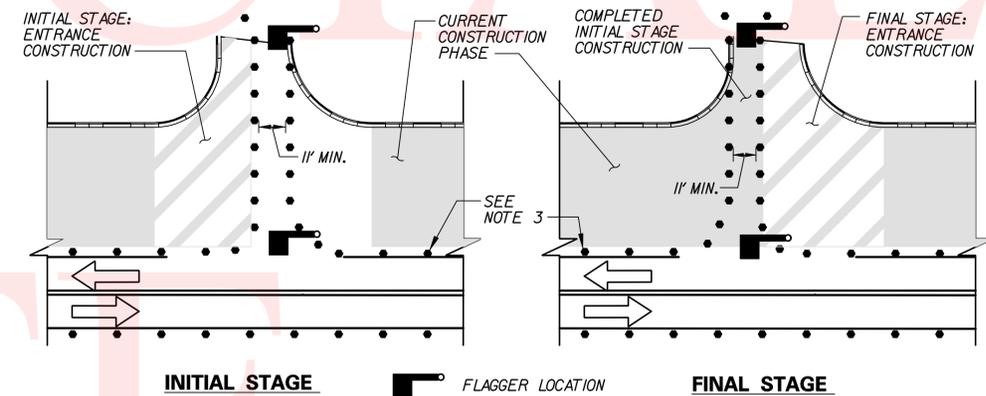
**GENERAL M.O.T  
TRAFFIC CONTROL PLAN**

SCALE: NONE

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	59	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**GENERAL TRAFFIC CONTROL NOTES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS WORK WITH OTHER CONTRACTORS IN THE AREA. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES SHALL BE IN ACCORDANCE WITH TYPICAL APPLICATION 10 OR 33 OF THE TRAFFIC CONTROL MANUAL.
2. GRADING AND MAINTAINING BASE COURSE THAT IS BEING USED AS A TRAVELWAY, DRIVEWAY, ACCESS RAMP, ETC. SHALL BE INCIDENTAL TO ITEM 763500 - MAINTENANCE OF TRAFFIC. EXCESS BASE COURSE MATERIAL SHALL BE PUSHED AHEAD AND USED IN THE NEXT SEGMENT AND SHALL BE INCIDENTAL TO THE PARTICULAR BASE COURSE PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR TEMPORARY ROADWAY MATERIAL (TRM) USED TO PROTECT EDGE DROP-OFFS, UNLESS THE MATERIAL IS EVENTUALLY UTILIZED AS PART OF A PERMANENT ROADWAY AT WHICH TIME THE MATERIAL WOULD BE PAID FOR UNDER THE RESPECTIVE CONTRACT MATERIAL ITEM. CONSTRUCTION OF A PLANNED RUNAROUND OR DETOUR WOULD BE ELIGIBLE FOR PAYMENT AS SPECIFIED IN THE CONTRACT.
3. THE CONTRACTOR SHALL SUPPLY MESSAGE BOARDS UNDER ITEM 743004 - FURNISH AND MAINTAIN MESSAGE BOARD. THE CONTRACTOR MUST DESIGNATE WHO SHALL BE RESPONSIBLE FOR THESE MESSAGE BOARDS. MOVING THE MESSAGE BOARDS TO DIFFERENT LOCATIONS DURING CONSTRUCTION WILL BE INCIDENTAL TO ITEM 743004 - FURNISH AND MAINTAIN MESSAGE BOARD. AT A MINIMUM, MESSAGE BOARDS SHALL BE POSTED TWO WEEKS PRIOR TO THE START OF CONSTRUCTION AND CHANGES IN TRAFFIC PATTERNS. SIX PLASTIC DRUMS SHALL BE PLACED IN FRONT OF THE MESSAGE BOARD WITH ONE TYPE B WARNING LIGHT.
4. A TYPE II TRUCK MOUNTED ATTENUATOR (TMA) SHALL BE REQUIRED ON THIS PROJECT DURING THE FOLLOWING OPERATIONS: TEMPORARY/PERMANENT PAVEMENT MARKINGS, ROADSIDE SPRAYING, PATCHING, MILLING, SWEEPING, TEMPORARY TRAFFIC BARRIER PLACEMENT OR AS DIRECTED BY THE ENGINEER. THE ROLL AHEAD DISTANCE SHALL BE AS PER THE MANUFACTURER'S RECOMMENDATIONS. THE TMA SHALL CONFORM TO THE REQUIREMENTS OF SECTION 6F.06 OF THE DELAWARE MUTCD.
5. ROADWAY AND SHOULDER WORK SHALL BE PERFORMED IN A MANNER SUCH THAT POSITIVE DRAINAGE OF THE ROADWAY SURFACE IS MAINTAINED THROUGHOUT. ALL WORK AND MATERIALS TO PROVIDE AND MAINTAIN THIS DRAINAGE SHALL BE INCIDENTAL TO ITEM 763500 - MAINTENANCE OF TRAFFIC.
6. CONSTRUCTION OF ANY PROPOSED DRAINAGE, TEMPORARY DRAINAGE OR INSTALLATION OF EROSION CONTROL MEASURES SHALL START AT THE OUTFALL AND PROGRESS UPSTREAM FOR ALL CONSTRUCTION PHASES UNLESS OTHERWISE INDICATED ON PLANS. TEMPORARY PATCHING OF EXISTING ENTRANCES FOR STORM DRAIN CONSTRUCTION SHALL CONSIST OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B (10' DEPTH).
7. DURING INSTALLATION OF PIPE WITHIN INTERSECTION OF RIVERS END DRIVE, CHRISTIANA MEADOWS AND SCHOOL BELL ROAD, CONTRACTOR SHALL MAINTAIN TRAFFIC WITH INTERSECTION FLAGGING OPERATION AND TRAFFIC OFFICER ASSISTANCE USING MANUAL SIGNAL OPERATION.
8. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES ON MULTI-LANE, DIVIDED ROADWAYS FOR PAVING OPERATIONS, TEMPORARY/PERMANENT PAVEMENT MARKINGS, PATCHING, OR FOR OTHER OPERATIONS AS DIRECTED BY THE ENGINEER SHALL CONFORM TO TYPICAL APPLICATION 33, 35 OR 35B OF THE TRAFFIC CONTROL MANUAL.
9. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES ON TWO-LANE ROADWAYS FOR PAVING OPERATIONS, TEMPORARY/PERMANENT PAVEMENT MARKINGS, PATCHING, OR OTHER OPERATIONS AS DIRECTED BY THE ENGINEER SHALL CONFORM TO TYPICAL APPLICATION 10, 17 OR 17A OF THE TRAFFIC CONTROL MANUAL.
10. THE CONTRACTOR SHALL PLACE A WEDGE OF HOT-MIX AT 20' AROUND ADJUSTED DRAINAGE INLETS DURING PHASE 1A, 1B, 3A AND 3B PRIOR TO PLACEMENT OF FINAL TYPE C HOT-MIX PAVING. THE CONTRACTOR SHALL NOTCH ADJUSTED DRAINAGE INLETS IN SUMP LOCATIONS TO ACHIEVE POSITIVE DRAINAGE CONDITIONS PRIOR TO PLACEMENT OF FINAL TYPE C HOT-MIX PAVING. NOTCHES SHALL BE REPAIRED TO PROPER CONDITION PRIOR TO OVERLAY. COST OF THIS WORK SHALL BE INCIDENTAL TO ITEM 71001 - ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET.
11. ALL CONSTRUCTION ACTIVITY THAT REQUIRES A TEMPORARY LANE CLOSURE OR FLAGGING OPERATIONS CAN OCCUR FROM 9 AM TO 3 PM. NIGHT WORK SHALL ONLY BE PERMITTED FOR CONSTRUCTION AT THE INTERSECTION OF SR 7 AND SR 273.
12. THE CONTRACTOR SHALL LOCATE THE EXISTING DELDOT FIBER BEFORE STARTING CONSTRUCTION OF PHASE 1A. DELDOT FIBER SERVICE SHALL BE MAINTAINED AT ALL TIMES.
13. ALL WORK OCCURRING DURING PHASES 1A, 1B AND 1C SHALL BE COMPLETED BY MARCH XX 2013, DUE TO WETLAND PERMIT EXPIRATION.



**TYPICAL ENTRANCE RECONSTRUCTION DETAIL**

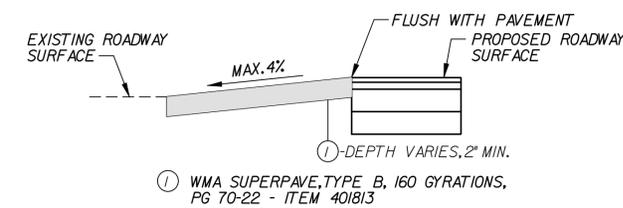
NOT TO SCALE

**NOTES:**

1. THIS DETAIL PROVIDES THE PROPER TRAFFIC CONTROL DEVICES AT ENTRANCES THAT ARE TO BE BUILT IN HALF-SECTION UNDER FLAGGER CONTROL. APPLICATION OF THIS DETAIL SHALL BE AS INDICATED ON THE PLANS, DURING DAYLIGHT HOURS ONLY, OR AS DIRECTED BY THE ENGINEER. TRAFFIC CONTROL DEVICES AT ENTRANCES SHALL BE RESET TO MAIN PHASE AT END OF EACH WORKING DAY.
2. CHANNELIZATION THROUGH ENTRANCE AREA SHALL BE ACCOMPLISHED WITH DRUMS PLACED AT 10' ON CENTER UNLESS OTHERWISE DIRECTED BY THE ENGINEER. DRUM LAYOUT SHALL ACCOMMODATE THE LARGEST DESIGN VEHICLE EXPECTED TO USE THE ENTRANCE.
3. TRAFFIC CONTROL DEVICES ALONG MAINLINE ROADWAY SHALL BE AS SHOWN ON CONSTRUCTION PHASING PLANS.
4. DEPENDING ON TRAFFIC VOLUME UTILIZING ENTRANCE AND OTHER SITE-SPECIFIC CONDITIONS, THE NUMBER OF FLAGGERS USED DURING ENTRANCE CONSTRUCTION MAY BE REDUCED FROM 2 TO 1, SUBJECT TO APPROVAL OF THE ENGINEER.
5. THIS DETAIL DOES NOT APPLY TO RESIDENTIAL DRIVEWAYS.

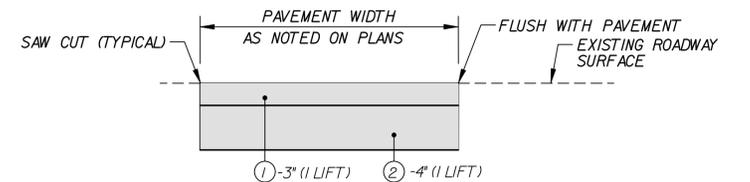
**SEQUENCE OF CONSTRUCTION - PHASE 4**

- 1 UPON COMPLETION OF ALL RAISED MEDIANS AND PAVEMENT, REMOVE ALL PERTINENT TRAFFIC CONTROL DEVICES AND PLACE FINAL SURFACE COURSE THROUGHOUT PROJECT USING TYPICAL APPLICATION 33.
- 2 PLACE PERMANENT PAVEMENT MARKINGS USING TYPICAL APPLICATION 35B.
- 3 FOLLOWING PERMANENT STABILIZATION OF ALL DISTURBED AREAS AND WITH THE APPROVAL OF THE ENGINEER, REMOVE ALL EROSION AND SEDIMENT CONTROL MEASURES.



**TEMPORARY OVERLAY SECTION**

NOT TO SCALE



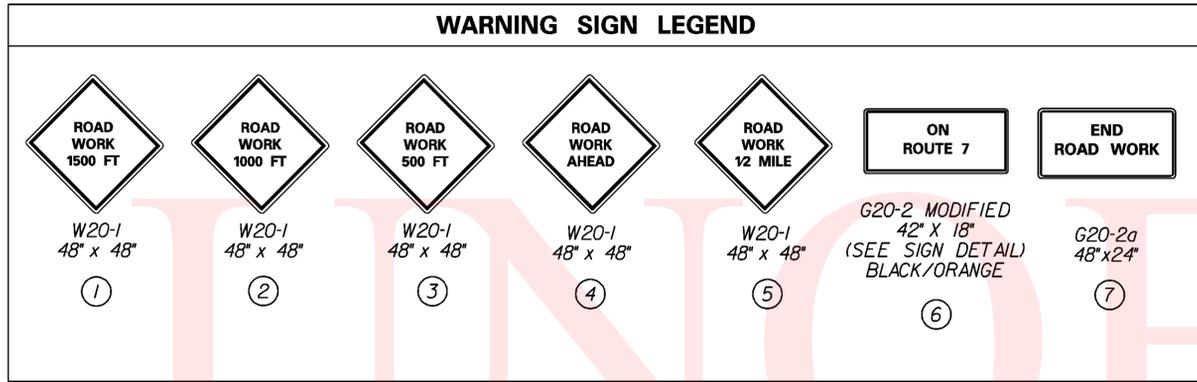
**TEMPORARY PAVEMENT SECTION**

NOT TO SCALE

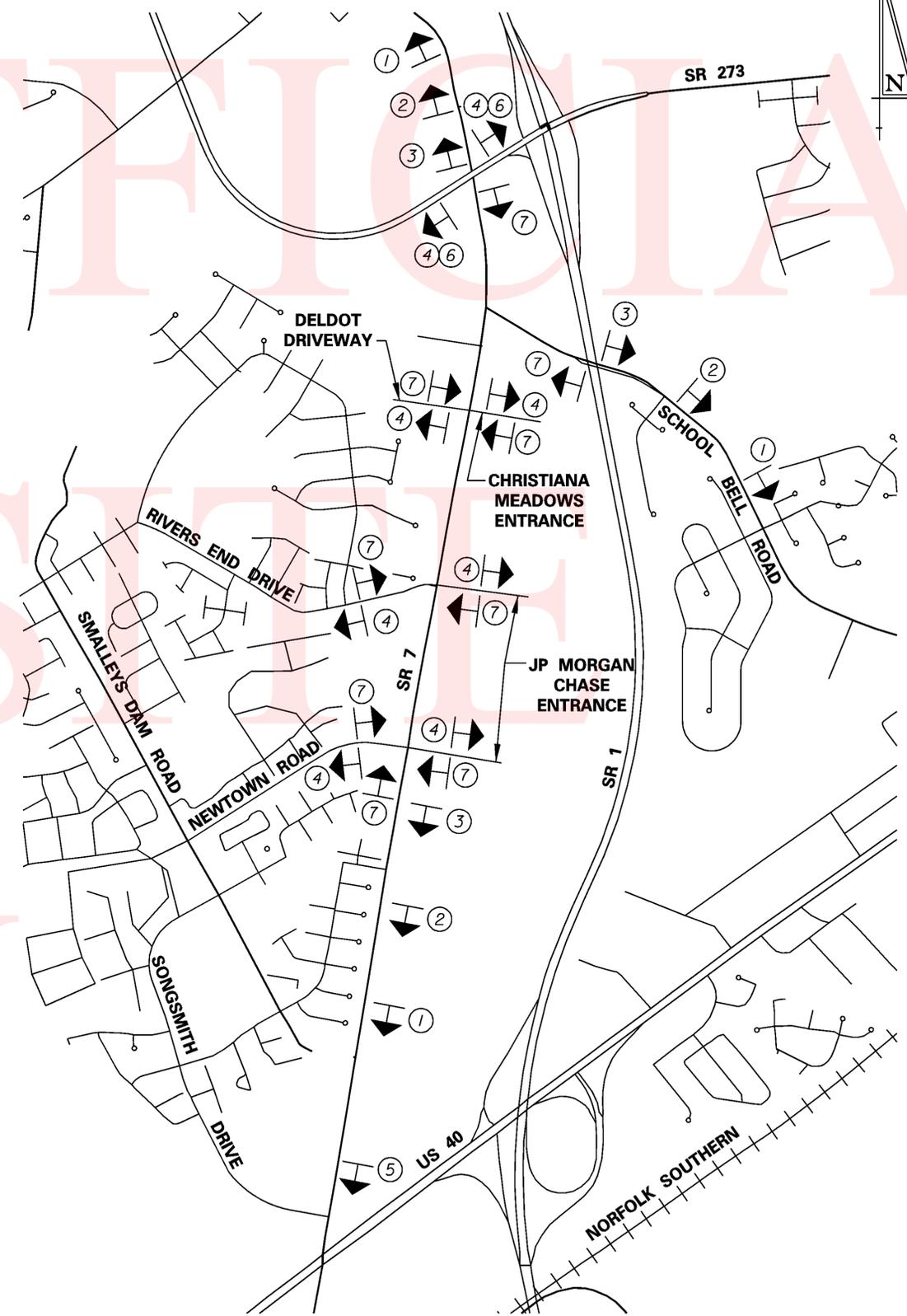
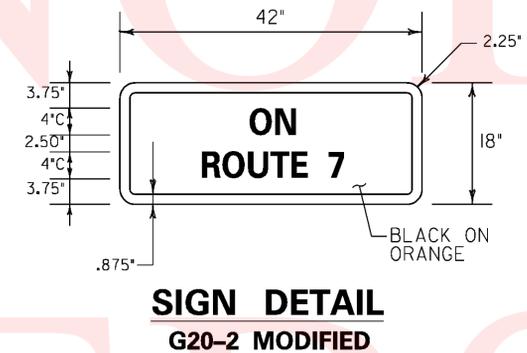
PREL. TRACING SUB DESIGN KOM-A CHKD. JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	60	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**MAINTENANCE OF TRAFFIC  
PERMANENT WARNING  
SIGN LOCATIONS**  
NOT TO SCALE



- NOTES:**
- PLACE ALL PERMANENT WARNING SIGNS PRIOR TO THE BEGINNING OF CONSTRUCTION. ALL PERMANENT WARNING SIGNS SHALL BE PAID FOR UNDER ITEM 743504 - WARNING SIGNS.
  - SEE INDIVIDUAL PHASING PLANS FOR PLACEMENT OF TEMPORARY WARNING SIGNS. LOCATION OF ALL WARNING SIGNS SHALL BE COORDINATED WITH THE ENGINEER.
  - WHERE POSSIBLE, WARNING SIGNS SHALL BE PLACED A MINIMUM OF 500 FEET FROM THE INTERSECTION. ANY LOCATIONS WHERE THEY ARE SPACED LESS THAN 500 FEET SHALL BE APPROVED BY THE ENGINEER.



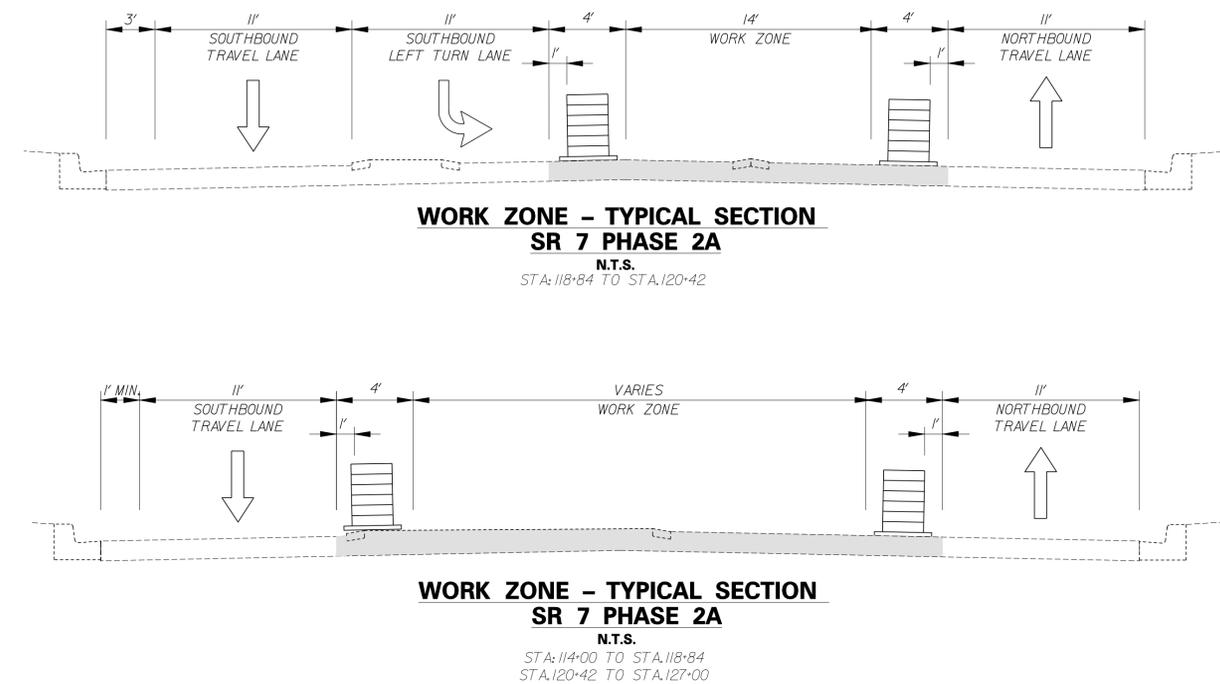
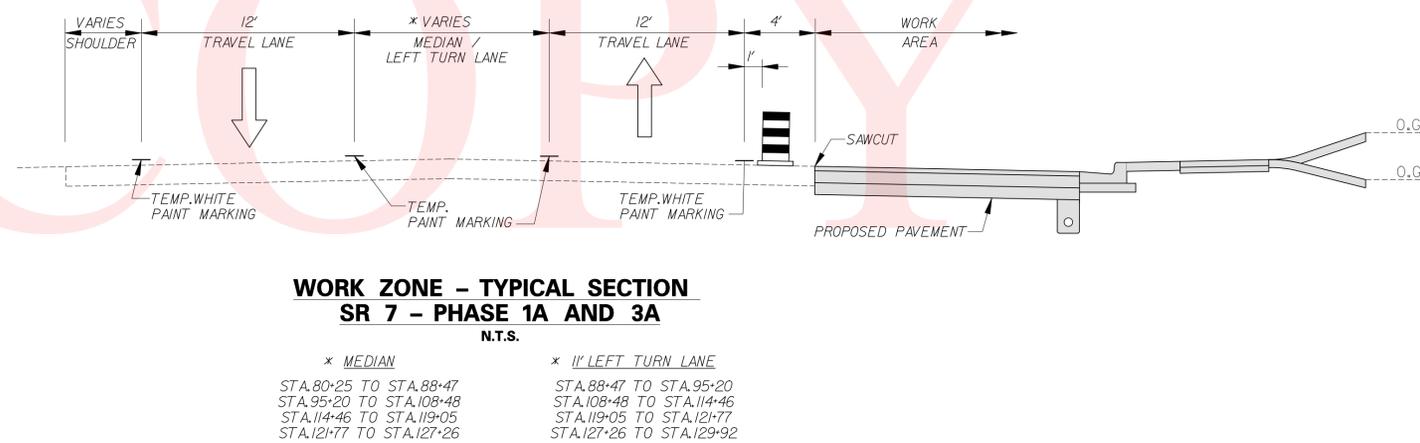
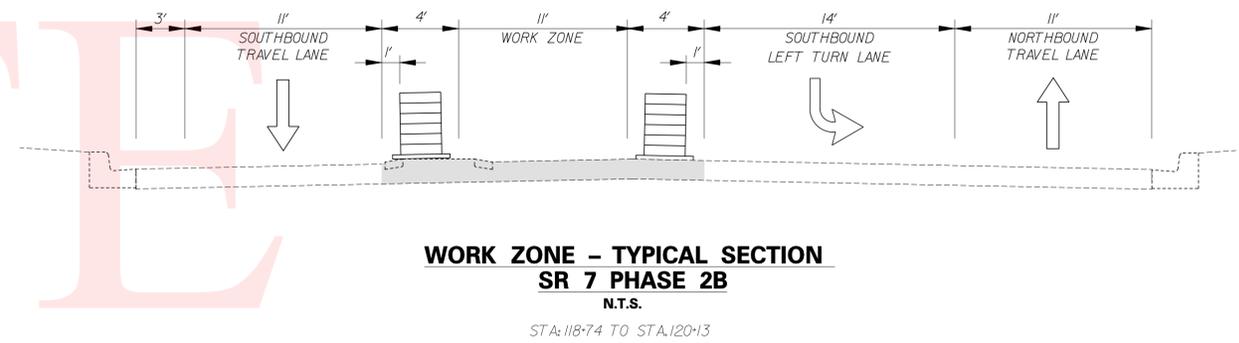
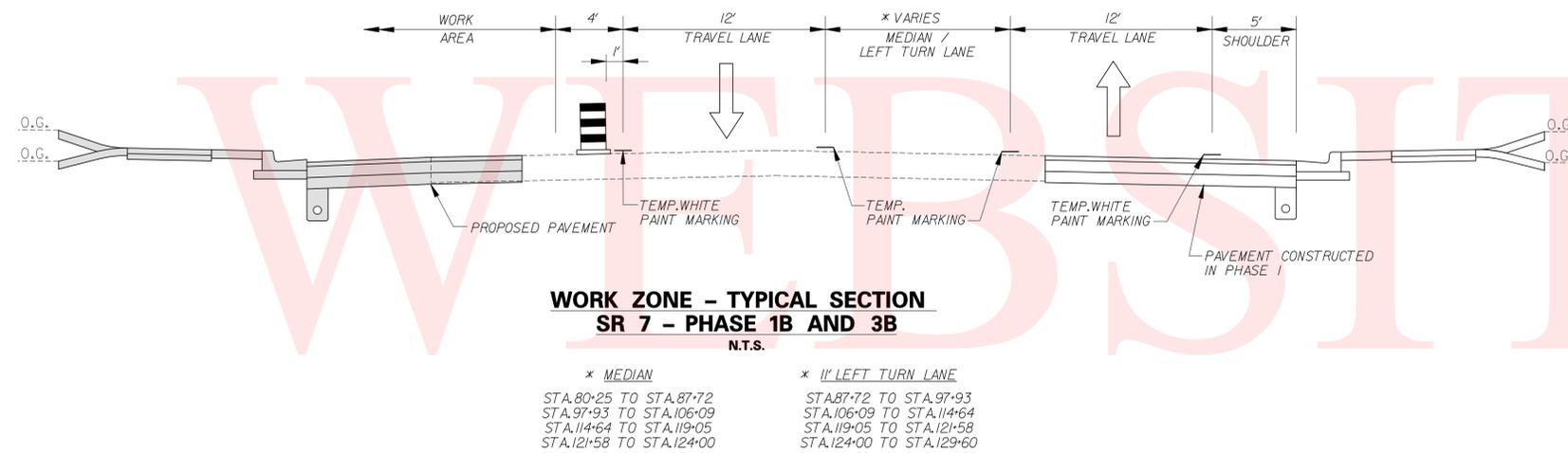
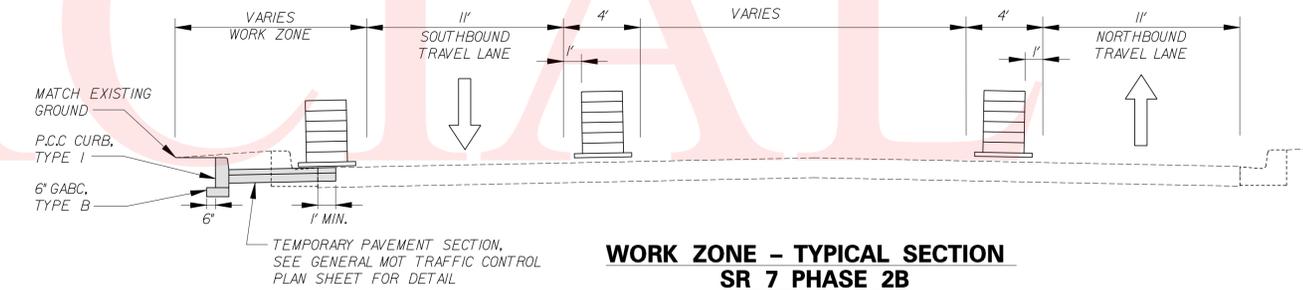
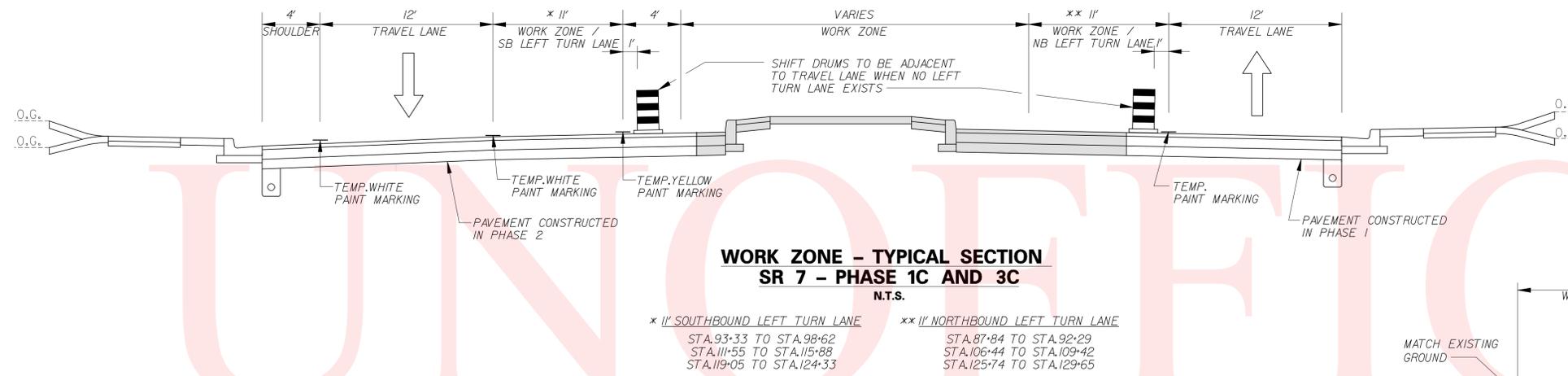
PREL. TRACING \_\_\_\_\_ SUB \_\_\_\_\_ DESIGN \_\_\_\_\_ KGM-A \_\_\_\_\_ CHKD. \_\_\_\_\_ JRR

**MAINTENANCE OF TRAFFIC  
TYPICAL SECTIONS**

SCALE: NONE

**SR 7,  
NEWTOWN ROAD TO SR 273**

REVISIONS

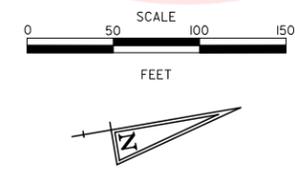


PREL. TRACING SUB DESIGN CHKD. JRR

**COPY**

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	62	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

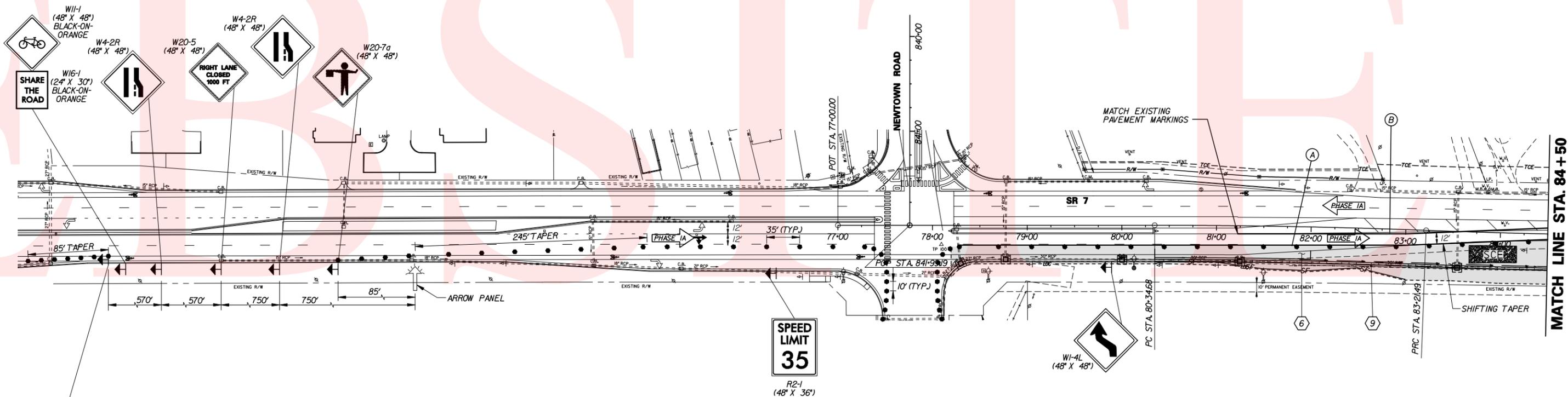
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1A**



- SEQUENCE OF CONSTRUCTION - PHASE 1A**
- 1 PLACE PORTABLE MESSAGE BOARDS TWO WEEKS PRIOR TO BEGINNING PHASE 1A ON SR 7 NORTHBOUND.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1A.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE NORTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 7 WHERE SHOWN.
  - 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. UTILITY COORDINATION AND RELOCATIONS SHALL OCCUR CONCURRENTLY WITH PHASE 1A CONSTRUCTION.



NEW	ON SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

PORTABLE MESSAGE BOARDS SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 1A AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION BEGINS.

LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

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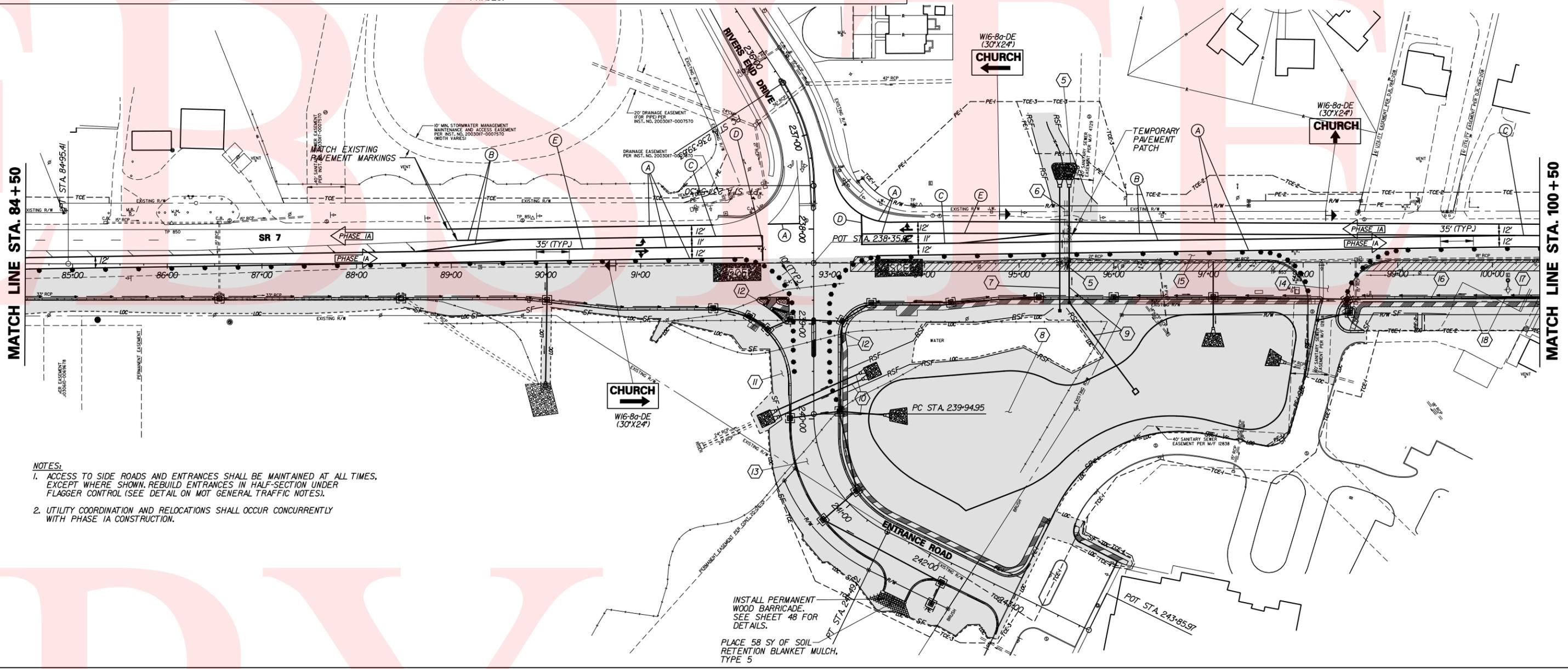
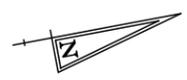
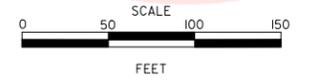
**SEQUENCE OF CONSTRUCTION - PHASE 1A**

- 1 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
- 2 USING TYPICAL APPLICATION IIB OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1A. MODIFY EXISTING TRAFFIC SIGNAL AT RIVERS END DRIVE. SEE TRAFFIC SIGNAL PLAN, PHASE 1.
- 3 USING TYPICAL APPLICATION IIB OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE NORTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
- 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
- 5 BEGINNING AT THE OUTFALL AND WORKING UPSTREAM CONSTRUCT THE CROSS ROAD PIPE P106, JB10 AND 6 LF OF TEMPORARY PIPE, 24" RCP CLASS IV @ 2.5% INT. INV. 58.27 DIS. INV. 58.12. CONSTRUCT P106 IN SECTIONS ACROSS THE EXISTING TRAVEL LANES USING ONE LANE FLAGGING OPERATION USING CASE 6. USE TEMPORARY PAVEMENT PATCH ON EXISTING TRAVEL LANES. OPEN ROADWAY TO TWO LANE OPERATION AT THE END OF EACH WORK DAY.
- 6 REMOVE EXISTING 30" RCP, EXISTING MANHOLE AND EXISTING 24" RCP ADJACENT TO P106 AT STA. 95+53. MAINTAIN 24" RCP CONNECTION TO PROPOSED JUNCTION BOX.
- 7 BEGINNING AT THE OUTFALL AND WORKING UPSTREAM, CONSTRUCT THE CROSS ROAD PIPE P20. USE SAME MOT OPERATION AS REFERENCED IN NOTE 6. FOR CONSTRUCTION OF P20, AT COMPLETION REMOVE TEMPORARY 24" RCP PIPE.
- 8 CONSTRUCT PROPOSED STORMWATER MANAGEMENT FACILITY \*278. SEE STORMWATER MANAGEMENT DETAILS FOR POND SEQUENCE OF CONSTRUCTION NOTES.
- 9 CONSTRUCT DRAINAGE SYSTEM P34, MH8, P33 AND POND OUTLET STRUCTURE.
- 10 CONSTRUCT DRAINAGE SYSTEM ALONG THE ST. ELIZABETH ENTRANCE INCLUDING P19 OUTFALLING TO SWM 278 AND P104 AND P105.
- 11 GRADE AND EXCAVATE FOR PROPOSED ST. ELIZABETH CHURCH ENTRANCE.
- 12 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALK FOR ST. ELIZABETH CHURCH ENTRANCE, INCLUDING ISLAND AT THE INTERSECTION OF SR7 AND ST. ELIZABETH CHURCH ENTRANCE.
- 13 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE C FOR THE PROPOSED ST. ELIZABETH CHURCH ENTRANCE TO THE LIMITS OF EXISTING SR 7. PLACE FINAL STRIPING ALONG THE ST. ELIZABETH CHURCH ENTRANCE.
- 14 THE EXISTING ST. ELIZABETH CHURCH ENTRANCE MUST REMAIN OPEN AT ALL TIMES UNTIL THE CONSTRUCTION OF THE NEW ENTRANCE IS COMPLETE AND THE TRAFFIC SIGNAL IS OPERATING FOR THE NEW ST. ELIZABETH CHURCH ENTRANCE.
- 15 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 7 AS SHOWN.
- 16 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINTS ALONG THE NORTHBOUND SIDE OF SR 7 AS SHOWN.
- 17 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
- 18 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
- 19 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER, AND TEMPORARY PAVEMENT WITHIN LIMITS SHOWN.
- 20 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

**MOT LEGEND**

- TEMPORARY PAVEMENT
- PROPOSED CONSTRUCTION THIS PHASE
- SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- EXISTING DIRECTION OF TRAVEL
- PROPOSED DIRECTION OF TRAVEL
- TEMPORARY PAVEMENT MARKING
- WHITE TEMPORARY PAVEMENT MARKINGS
- YELLOW TEMPORARY PAVEMENT MARKINGS
- DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
- 16" WHITE TEMPORARY PAVEMENT MARKINGS
- SKIP WHITE REMOVABLE PAVEMENT MARKINGS
- TYPE C ARROW PANEL

**CONSTRUCTION PHASING, MOT AND EROSION CONTROL PHASE 1A**

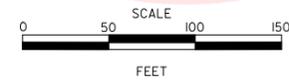


- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN, REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. UTILITY COORDINATION AND RELOCATIONS SHALL OCCUR CONCURRENTLY WITH PHASE 1A CONSTRUCTION.

PREL. TRACING  
 S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

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REVISIONS				

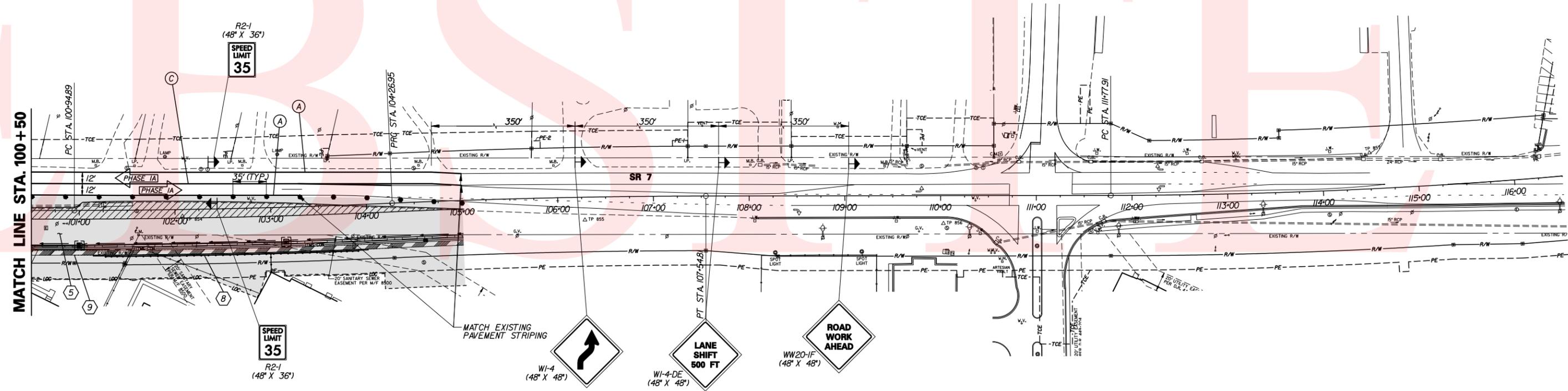
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1A**



- SEQUENCE OF CONSTRUCTION - PHASE 1A**
- 1 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 2 USING TYPICAL APPLICATION IIB OR IT OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1A.
  - 3 USING TYPICAL APPLICATION IIB OR IT OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE NORTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 5 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 7 WHERE SHOWN.
  - 6 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 7 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 8 CONSTRUCT RETAINING WALLS FROM STA. 104+15 TO STA. 104+00 RT.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
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  - TYPE C ARROW PANEL

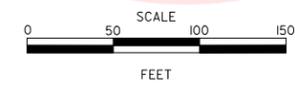
- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. UTILITY COORDINATION AND RELOCATIONS SHALL OCCUR CONCURRENTLY WITH PHASE 1A CONSTRUCTION.



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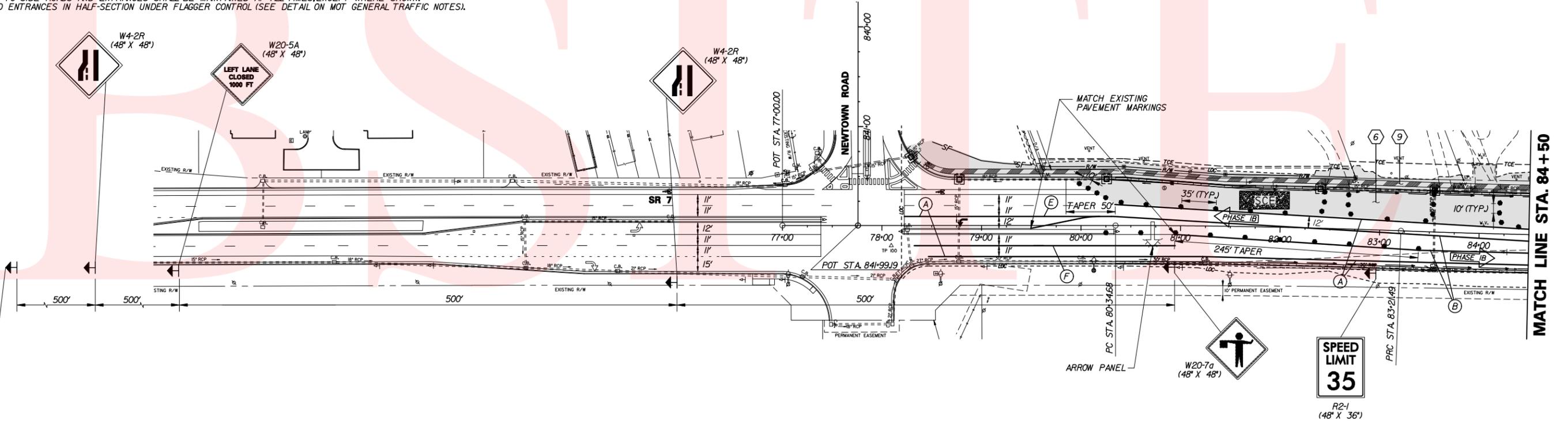
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1B**



- SEQUENCE OF CONSTRUCTION - PHASE 1B**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1A.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY. USING CASE 6 OR 23A OF THE TRAFFIC CONTROL MANUAL, PLACE TEMPORARY STRIPING WHERE SHOWN FOR PHASE 1B.
  - 4 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE SOUTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A.
  - 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE SOUTHBOUND SIDE OF SR 7 WHERE SHOWN.
  - 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES. IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS, REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - WHITE TEMPORARY PAVEMENT MARKINGS
  - YELLOW TEMPORARY PAVEMENT MARKINGS
  - DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - SKIP (2' LINE 6' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - SKIP (10' LINE 30' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



NEW	ON SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

PORTABLE MESSAGE BOARD SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 2 AND SHALL REMAIN IN PLACE UNTIL PHASE 2 CONSTRUCTION BEGINS.  
LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

PREL. TRACING  
 S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	66	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**SEQUENCE OF CONSTRUCTION - PHASE 1B**

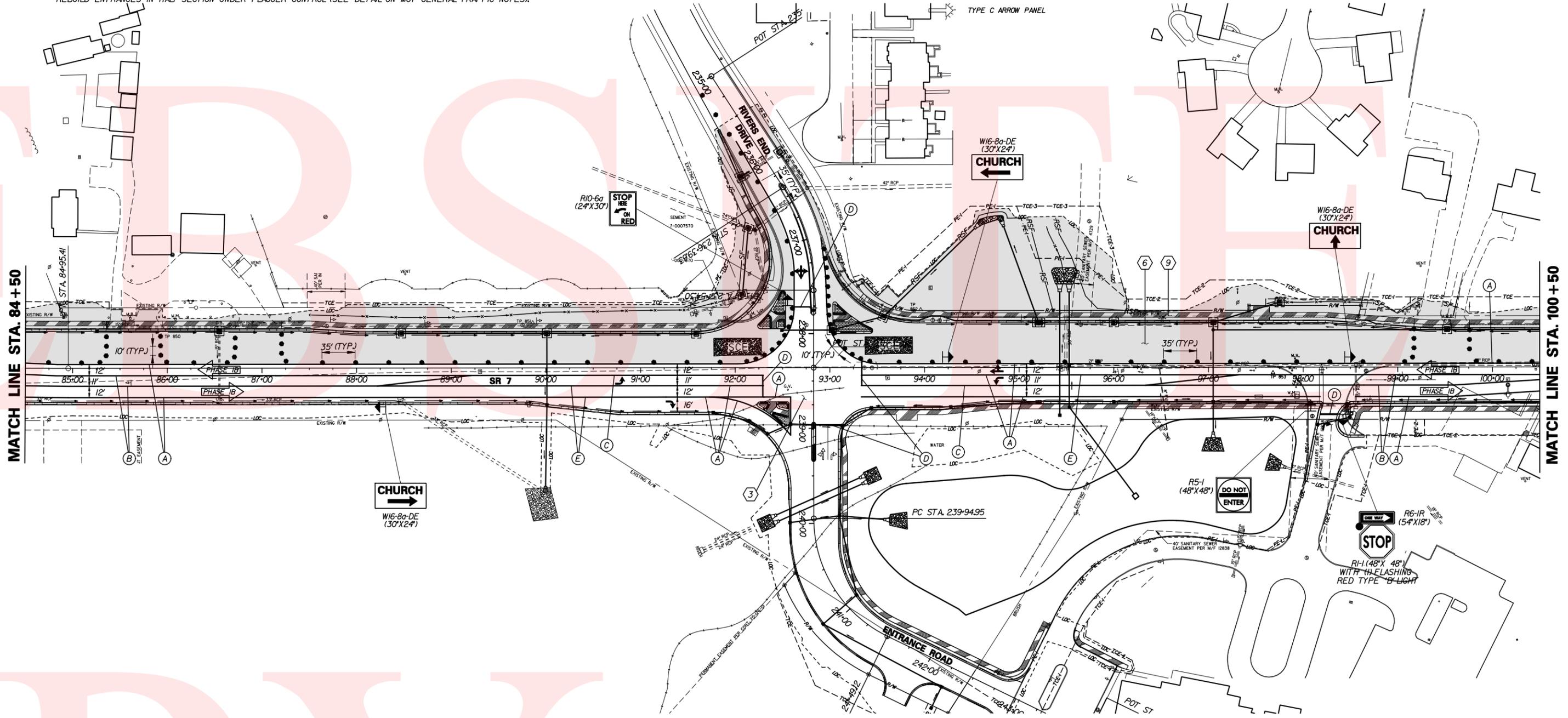
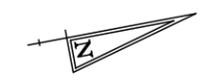
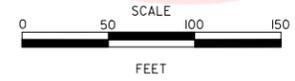
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1A.
- 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
- 3 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1B. MODIFY EXISTING TRAFFIC SIGNAL AT RIVERS END DRIVE. SEE TRAFFIC SIGNAL PLAN, PHASE 2.
- 4 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE SOUTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
- 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A.
- 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE SOUTHBOUND SIDE OF SR 7 WHERE SHOWN.
- 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
- 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
- 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS AS SHOWN.
- 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
- 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).

**MOT LEGEND**

- TEMPORARY PAVEMENT
- PROPOSED CONSTRUCTION THIS PHASE
- SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- EXISTING DIRECTION OF TRAVEL
- PROPOSED DIRECTION OF TRAVEL
- TEMPORARY PAVEMENT MARKING
- 4" WHITE TEMPORARY PAVEMENT MARKINGS
- 4" YELLOW TEMPORARY PAVEMENT MARKINGS
- 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
- 16" WHITE TEMPORARY PAVEMENT MARKINGS
- 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
- TYPE C ARROW PANEL

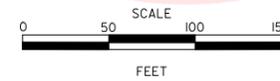
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1B**



PREL. TRACING  
 S.S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	67	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

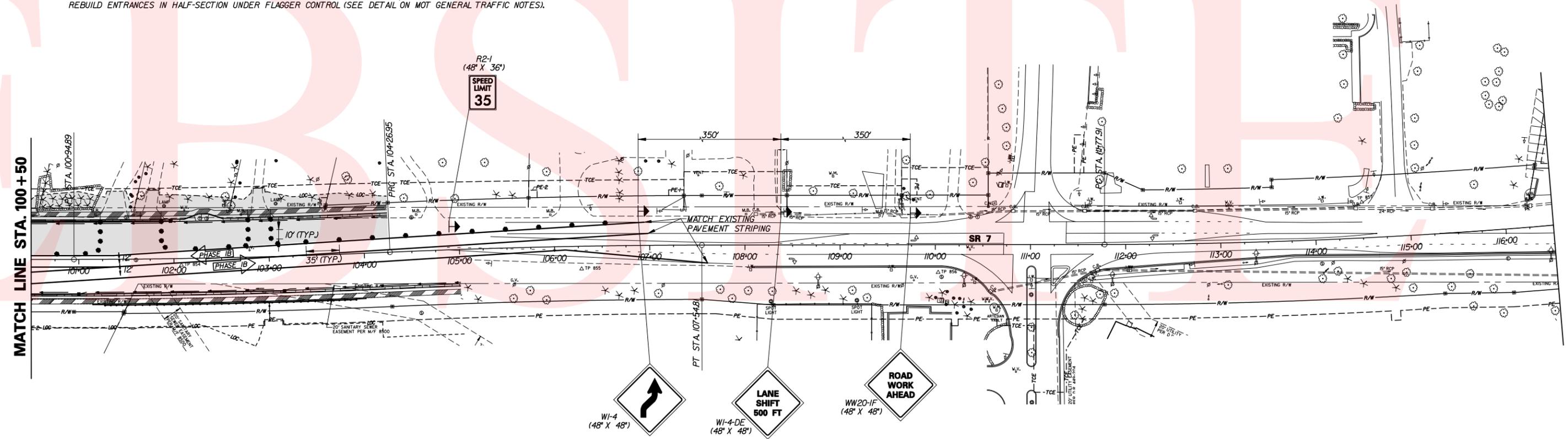
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1B**



- SEQUENCE OF CONSTRUCTION - PHASE 1B**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1A.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1B.
  - 4 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE SOUTHBOUND SIDE OF SR 7, ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS AS SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A.
  - 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE SOUTHBOUND SIDE OF SR 7 AS SHOWN.
  - 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

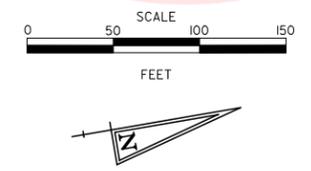
**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



PREL. TRACING  
 S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	68	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

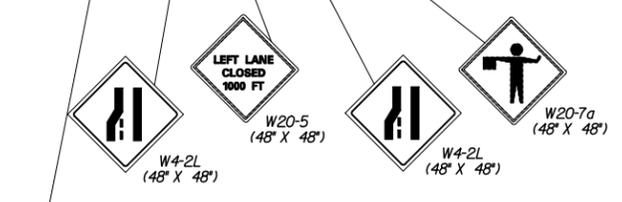
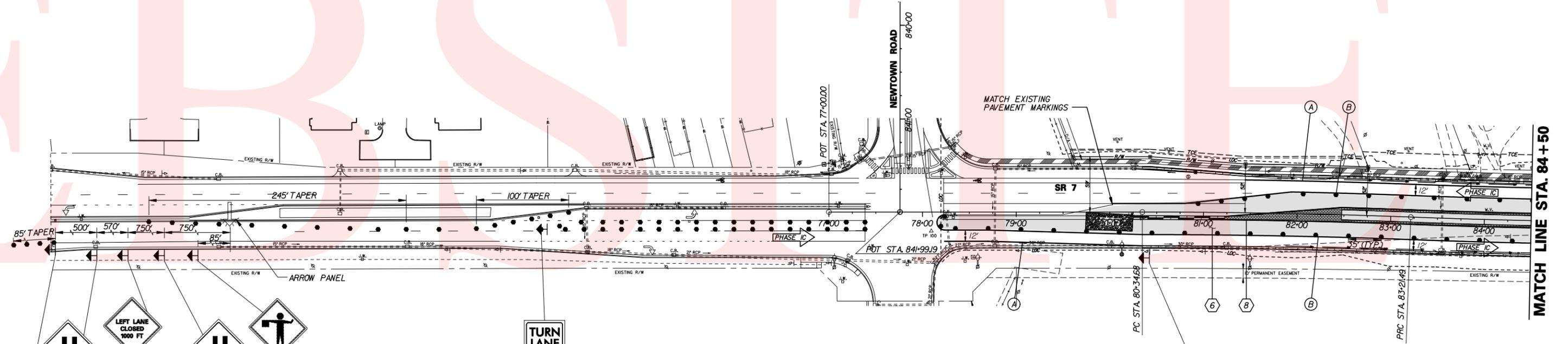
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1C**



- SEQUENCE OF CONSTRUCTION - PHASE 1C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1C.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A AND PHASE 1B.
  - 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
  - 7 COMPLETE DRAINAGE SYSTEM.
  - 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE WHERE SHOWN.
  - 9 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # YELLOW TEMPORARY PAVEMENT MARKINGS
  - # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - # SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



NEW	ON SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

PORTABLE MESSAGE BOARD SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 3 AND SHALL REMAIN IN PLACE UNTIL PHASE 3 CONSTRUCTION BEGINS.  
LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

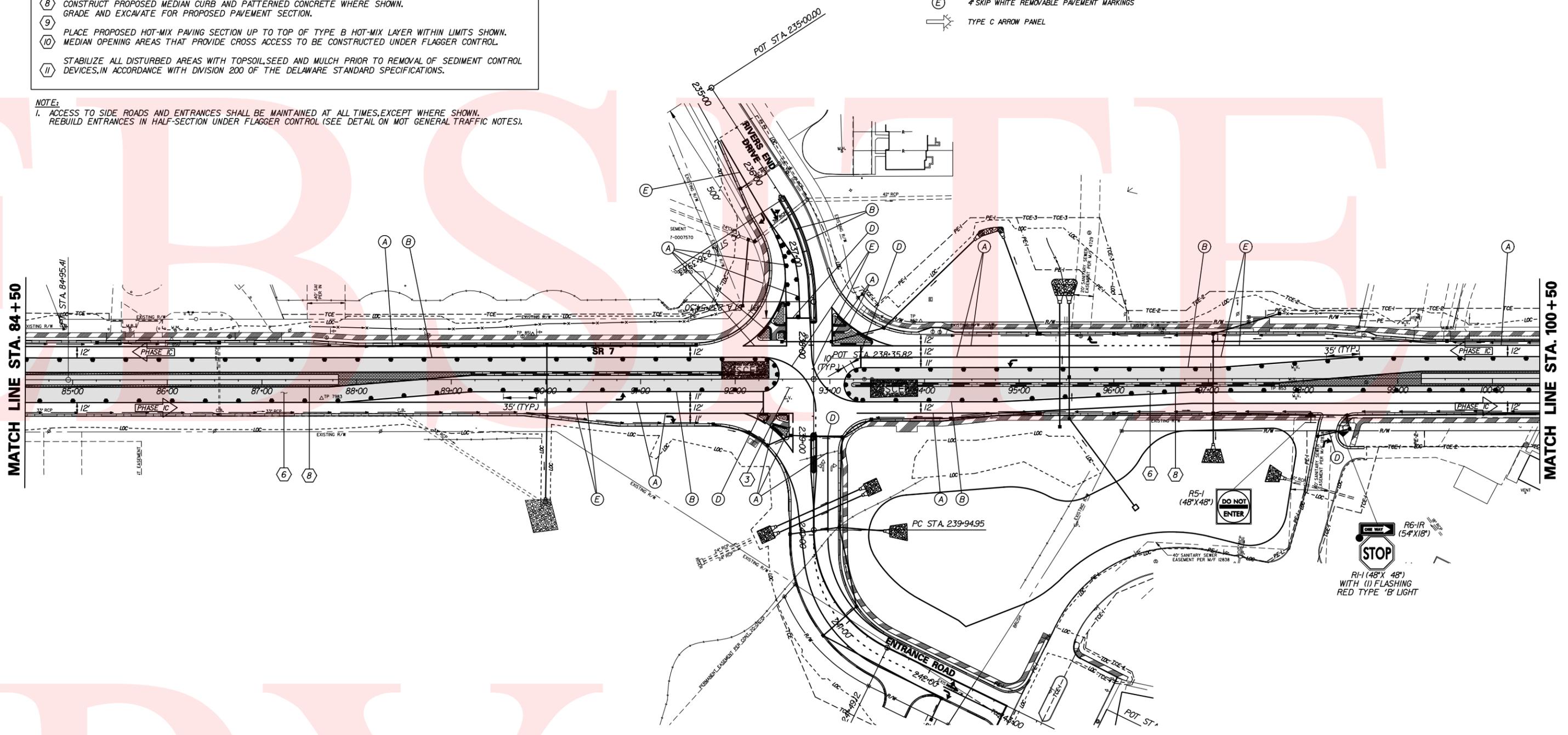
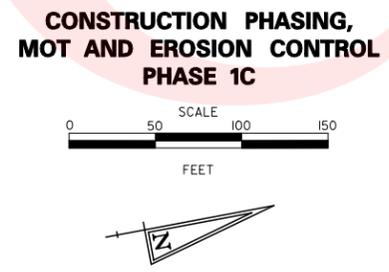
JUR  
 CHKD.  
 KGM-A  
 DESIGN  
 SJB  
 PREL. TRACING

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	69	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

- SEQUENCE OF CONSTRUCTION - PHASE 1C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE DELAWARE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1C. MODIFY EXISTING TRAFFIC SIGNAL AT RIVERS END DRIVE ENTRANCE, SEE TRAFFIC SIGNAL PLAN PHASE 3.
  - 4 USING TYPICAL APPLICATION 11B OR 17 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A AND PHASE 1B.
  - 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
  - 7 COMPLETE DRAINAGE SYSTEM.
  - 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE WHERE SHOWN. GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 9 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
  - 10 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

**NOTE:**  
 1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).

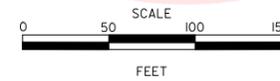
- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - WHITE TEMPORARY PAVEMENT MARKINGS
  - YELLOW TEMPORARY PAVEMENT MARKINGS
  - DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL



PREL. TRACING  
 S.S.B.  
 DESIGN  
 K.G.M.-A.  
 CHKD.  
 J.R.R.

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	70	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

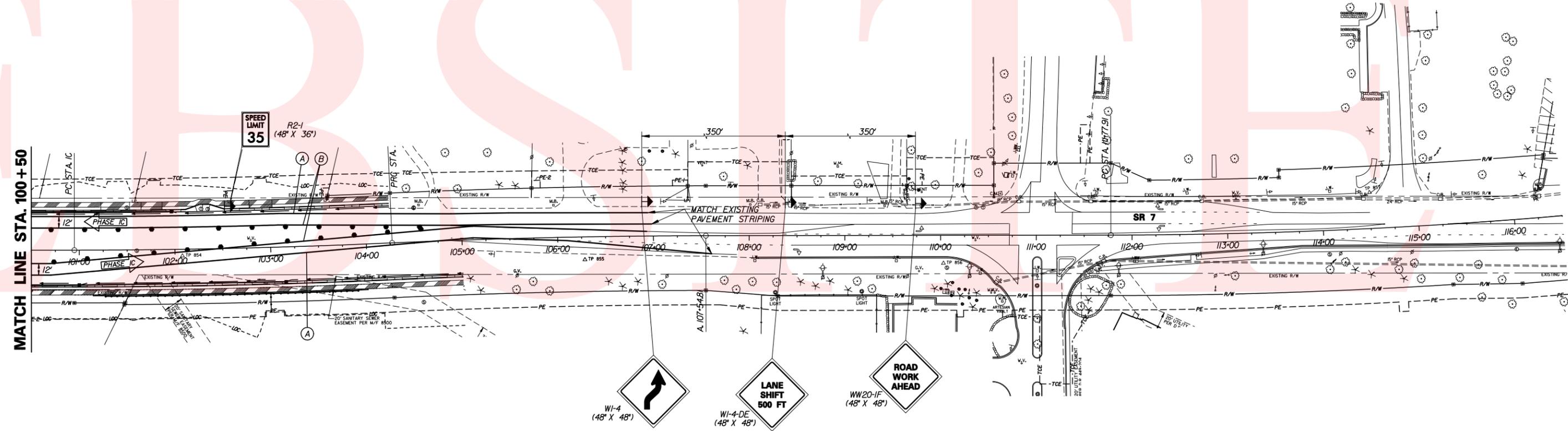
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 1C**



- SEQUENCE OF CONSTRUCTION - PHASE 1C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 1B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1C.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 1A AND PHASE 1B.
  - 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
  - 7 COMPLETE DRAINAGE SYSTEM.
  - 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE AS SHOWN. GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 9 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
  - 10 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # YELLOW TEMPORARY PAVEMENT MARKINGS
  - # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - # SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

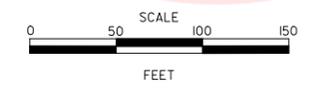
**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



PREL. TRACING  
 S.S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

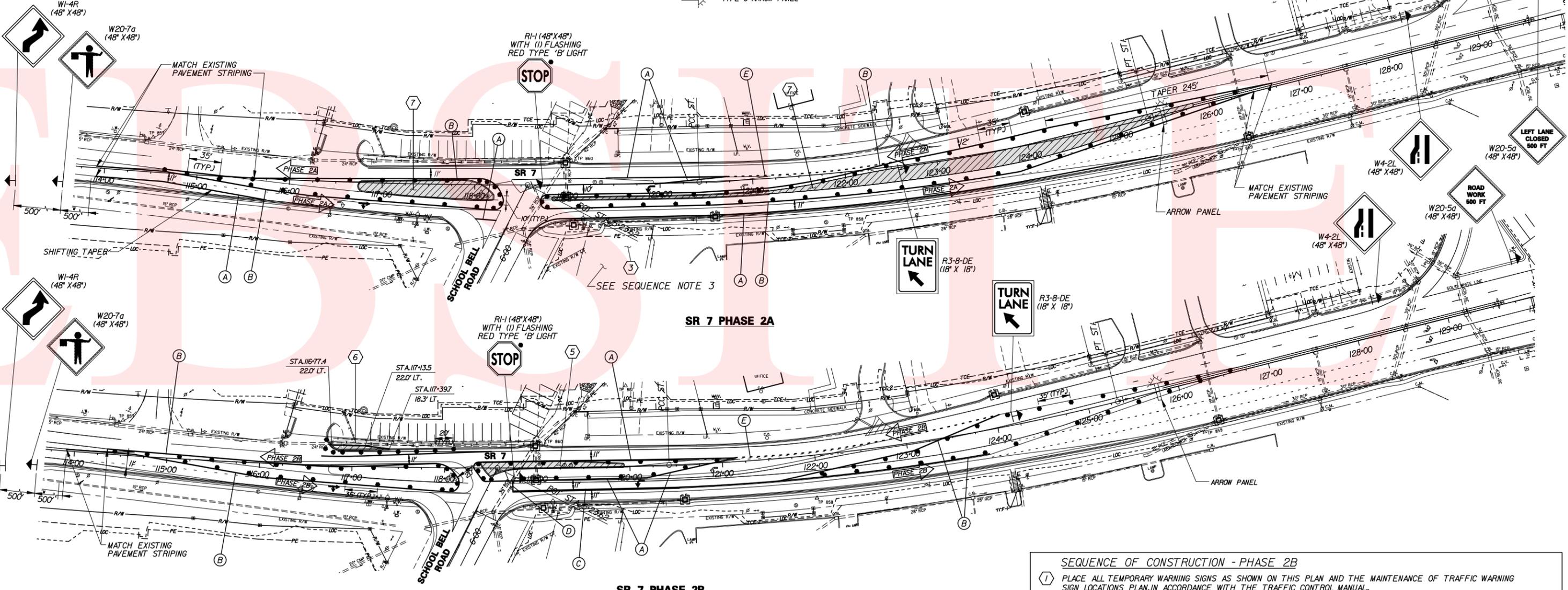
CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	71	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 2A AND 2B**



- SEQUENCE OF CONSTRUCTION - PHASE 2A**
- PLACE ALL PERMANENT AND TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - PLACE PORTABLE MESSAGE BOARDS TWO WEEKS PRIOR TO BEGINNING OF PHASE 2A ON SR 7 SOUTHBOUND AND SR 273 EASTBOUND, SEE SHEET 60.
  - INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - INSTALL CONSTRUCTION SAFETY FENCE ALONG THE PROPERTY LINE OF PARCEL 5-R - 10-028.00-029.00, OLD FORT U.A.M.E. CHURCH. CONSTRUCTION SAFETY FENCE SHALL REMAIN IN PLACE FOR THE DURATION OF ALL CONSTRUCTION ACTIVITY ASSOCIATED WITH THE PROJECT. SEE PROJECT NOTES.
  - USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 2A.
  - USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS AS SHOWN. MAINTAIN ACCESS TO SCHOOL BELL ROAD AT ALL TIMES.
  - REMOVE EXISTING CONCRETE MEDIAN FROM STA. 116+75 TO STA. 118+20 AND FROM STA. 118+84 TO STA. 125+84. PLACE TEMPORARY PAVEMENT SECTION.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 18" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL



**SR 7 PHASE 2A**

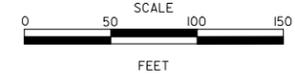
**SR 7 PHASE 2B**

- SEQUENCE OF CONSTRUCTION - PHASE 2B**
- PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - MAINTAIN EROSION AND SEDIMENT CONTROL DEVICES FROM PHASE 2A.
  - USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 1B.
  - USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS AS SHOWN. MAINTAIN ACCESS TO SCHOOL BELL ROAD AT ALL TIMES.
  - REMOVE EXISTING CONCRETE MEDIAN FROM STA. 118+40 TO STA. 119+93. PLACE TEMPORARY PAVEMENT SECTION.
  - SAW CUT AND REMOVE EXISTING CURB AND GUTTER FROM STA. 116+75 LT. TO STA. 117+40 LT. PLACE 65' OF PROPOSED P.C.C. CURB, TYPE I AS SHOWN. PLACE TEMPORARY PAVEMENT SECTION.

JUR  
 CHKD.  
 KGM-A  
 DESIGN  
 SJB  
 PREL. TRACING

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	72	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

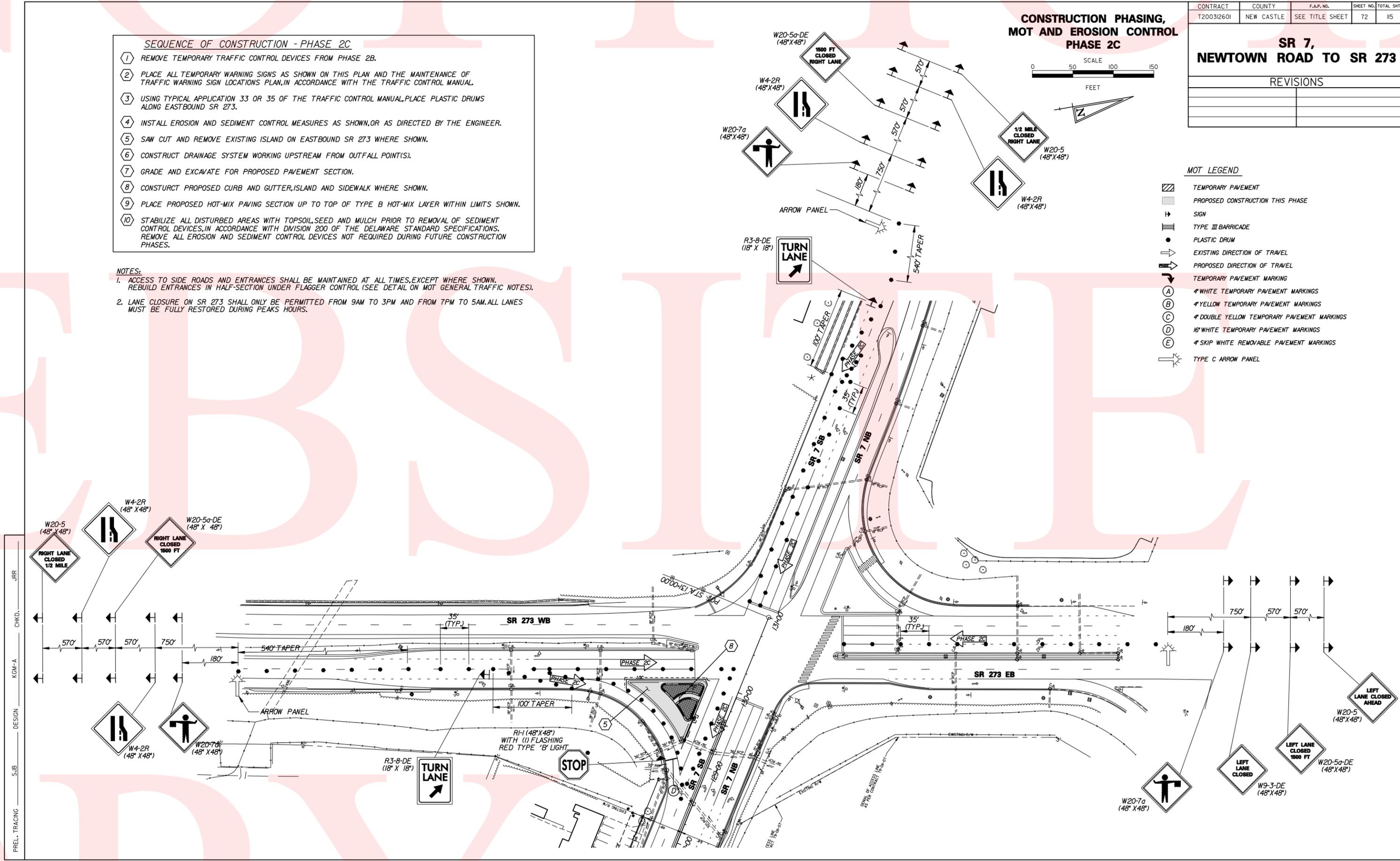
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 2C**



- SEQUENCE OF CONSTRUCTION - PHASE 2C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 2B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION 33 OR 35 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG EASTBOUND SR 273.
  - 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 5 SAW CUT AND REMOVE EXISTING ISLAND ON EASTBOUND SR 273 WHERE SHOWN.
  - 6 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 7 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 8 CONSTRUCT PROPOSED CURB AND GUTTER, ISLAND AND SIDEWALK WHERE SHOWN.
  - 9 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 10 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

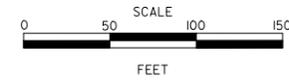
- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. LANE CLOSURE ON SR 273 SHALL ONLY BE PERMITTED FROM 9AM TO 3PM AND FROM 7PM TO 5AM. ALL LANES MUST BE FULLY RESTORED DURING PEAKS HOURS.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 18" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL



PREL. TRACING  
 S.S.B.  
 DESIGN  
 K.G.M.-A.  
 CHKD.  
 J.R.R.

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 2D**

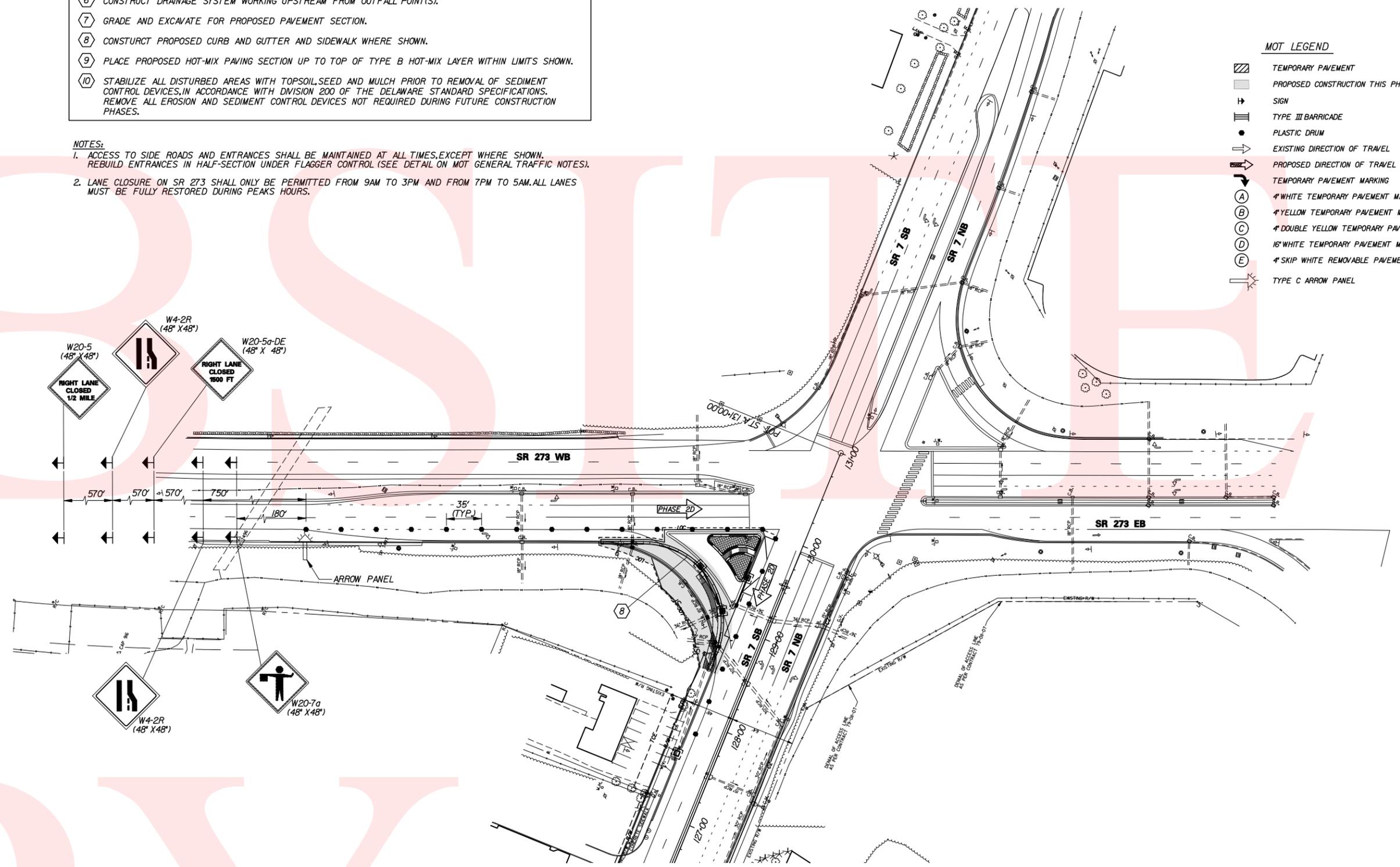


CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	73	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

- SEQUENCE OF CONSTRUCTION - PHASE 2D**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 2C.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION 33 OR 35 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG EASTBOUND SR 273.
  - 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 5 SAW CUT AND REMOVE EXISTING PAVEMENT AS SHOWN FOR THIS PHASE.
  - 6 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 7 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 8 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALK WHERE SHOWN.
  - 9 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 10 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. LANE CLOSURE ON SR 273 SHALL ONLY BE PERMITTED FROM 9AM TO 3PM AND FROM 7PM TO 5AM. ALL LANES MUST BE FULLY RESTORED DURING PEAKS HOURS.

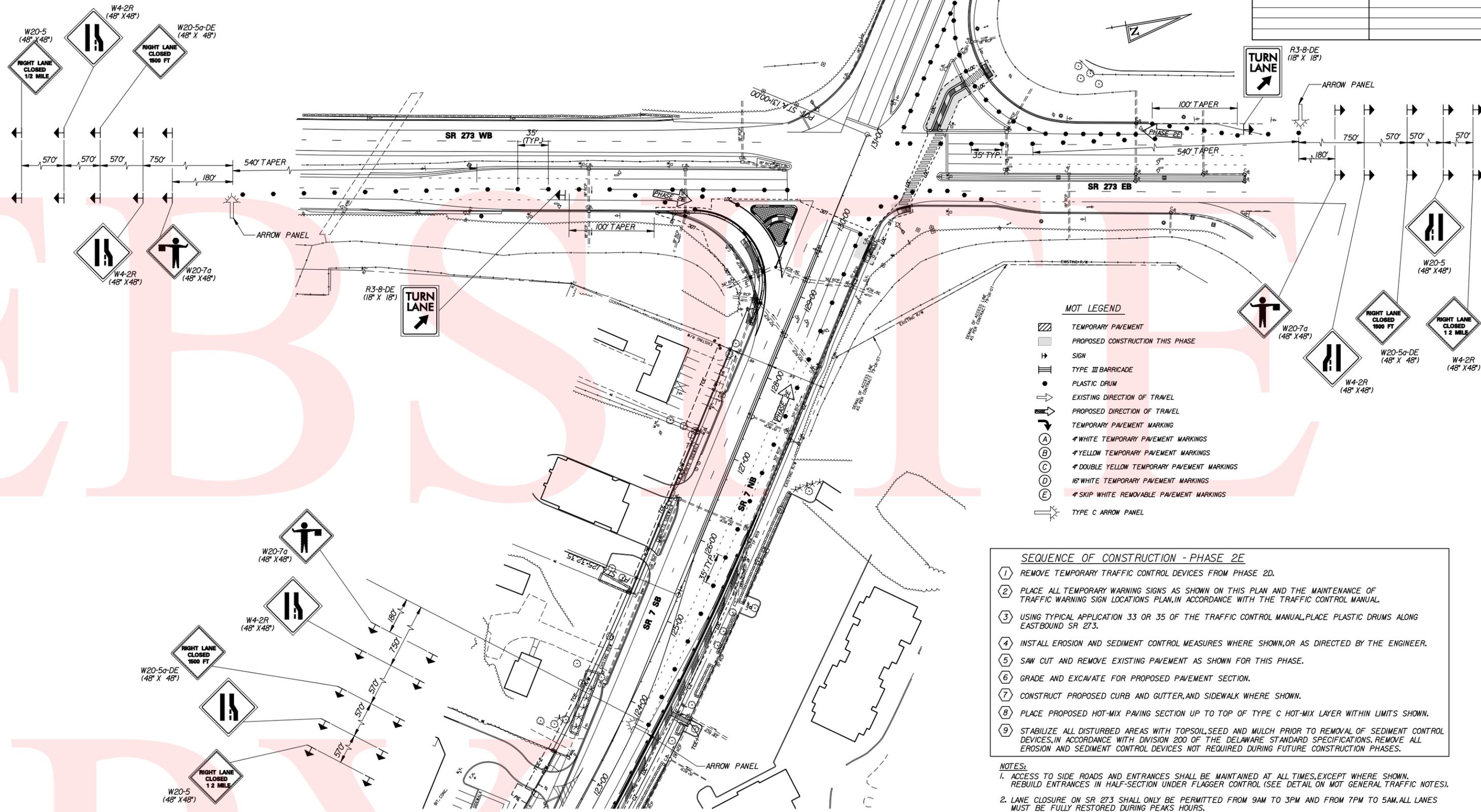
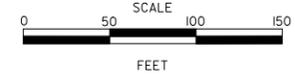
- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # YELLOW TEMPORARY PAVEMENT MARKINGS
  - # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL



PREL. TRACING — S.S.B — DESIGN — KGM-A — CHKD. — JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	74	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 2E**



**MOT LEGEND**

- TEMPORARY PAVEMENT
- PROPOSED CONSTRUCTION THIS PHASE
- SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- EXISTING DIRECTION OF TRAVEL
- PROPOSED DIRECTION OF TRAVEL
- TEMPORARY PAVEMENT MARKING**
- # WHITE TEMPORARY PAVEMENT MARKINGS
- # YELLOW TEMPORARY PAVEMENT MARKINGS
- # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
- 16" WHITE TEMPORARY PAVEMENT MARKINGS
- # SKIP WHITE REMOVABLE PAVEMENT MARKINGS
- TYPE C ARROW PANEL

**SEQUENCE OF CONSTRUCTION - PHASE 2E**

- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 2D.
- 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
- 3 USING TYPICAL APPLICATION 33 OR 35 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG EASTBOUND SR 273.
- 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES WHERE SHOWN, OR AS DIRECTED BY THE ENGINEER.
- 5 SAW CUT AND REMOVE EXISTING PAVEMENT AS SHOWN FOR THIS PHASE.
- 6 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
- 7 CONSTRUCT PROPOSED CURB AND GUTTER, AND SIDEWALK WHERE SHOWN.
- 8 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE C HOT-MIX LAYER WITHIN LIMITS SHOWN.
- 9 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

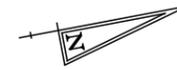
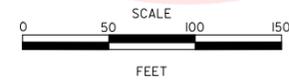
**NOTES:**

1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
2. LANE CLOSURE ON SR 273 SHALL ONLY BE PERMITTED FROM 9AM TO 3PM AND FROM 7PM TO 5AM. ALL LANES MUST BE FULLY RESTORED DURING PEAKS HOURS.

PREL. TRACING  
 S.S.B. DESIGN  
 K.G.M.-A. CHKD.  
 J.R.R.

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	75	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

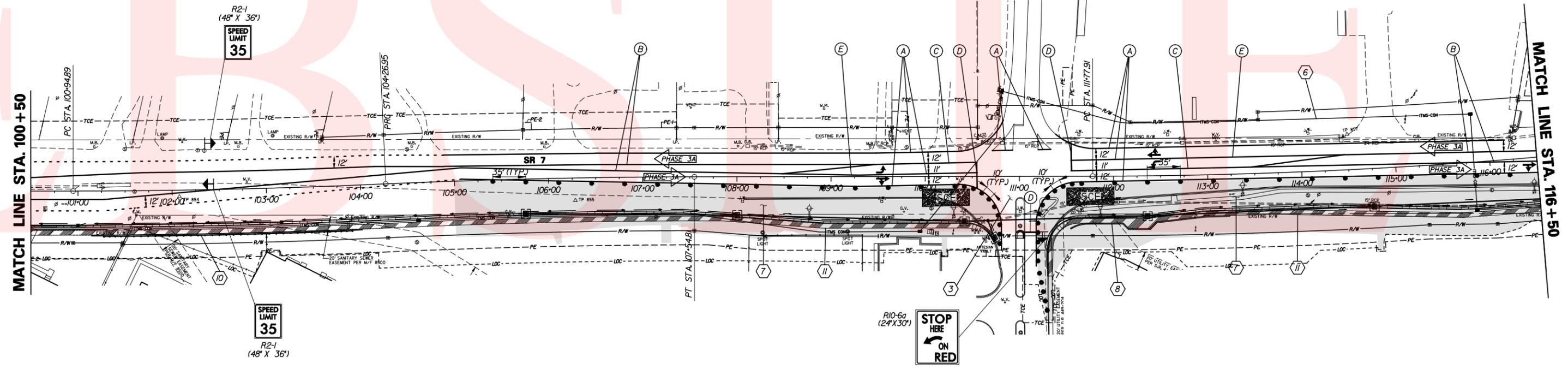
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3A**



- SEQUENCE OF CONSTRUCTION - PHASE 3A**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 2E.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3A. MODIFY EXISTING TRAFFIC SIGNAL AT DELDOT/CHRISTIANA MEADOWS ENTRANCE. SEE TRAFFIC SIGNAL PLAN, PHASE 1.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE NORTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 6 INSTALL ITMS JUNCTION WELLS AND CONDUIT FROM THE DELDOT ENTRANCE TO SCHOOL BELL ROAD.
  - 7 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 7 WHERE SHOWN.
  - 8 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 9 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 10 CONSTRUCT RETAINING WALLS FROM STA. 111+55 TO STA. 113+22 RT.
  - 11 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 12 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 13 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

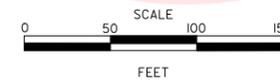
- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. UTILITY COORDINATION AND RELOCATIONS SHALL OCCUR CONCURRENTLY WITH PHASE 1C CONSTRUCTION.



PREL. TRACING  
 S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	76	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3A**



PORTABLE MESSAGE BOARDS SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 1A AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION BEGINS.

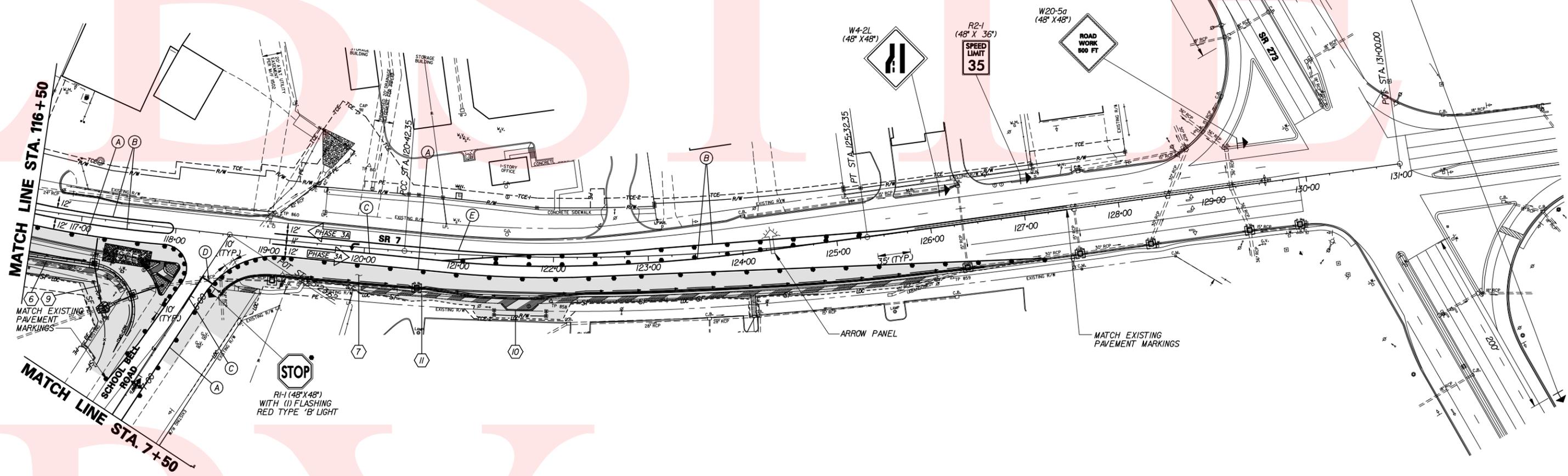
LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

NEW	SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

- SEQUENCE OF CONSTRUCTION - PHASE 3A**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 2E.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3A.
  - 4 USING TYPICAL APPLICATION I1B OR I7B OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE NORTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 6 INSTALL ITMS JUNCTION WELLS AND CONDUIT FROM THE DELDOT ENTRANCE TO SCHOOL BELL ROAD.
  - 7 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 7 AS SHOWN.
  - 8 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 9 INSTALL 42 LF OF TEMPORARY 18" RCP CL/V PIPE AT 0.5% INT. INV. 37.77, DIS. INV. 37.56.
  - 10 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 11 CONSTRUCT RETAINING WALL FROM STA. 121+46 TO STA. 121+85 RT.
  - 12 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 13 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 14 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # YELLOW TEMPORARY PAVEMENT MARKINGS
  - # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - # SKIP (2' LINE 6' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - # SKIP (10' LINE 30' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

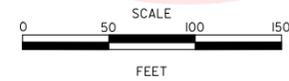
- NOTES:**
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).
  2. UTILITY COORDINATION AND RELOCATIONS SHALL OCCUR CONCURRENTLY WITH PHASE 1C CONSTRUCTION.



JUR  
 CHKD.  
 KGM-A  
 DESIGN  
 SJB  
 PREL. TRACING

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	77	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

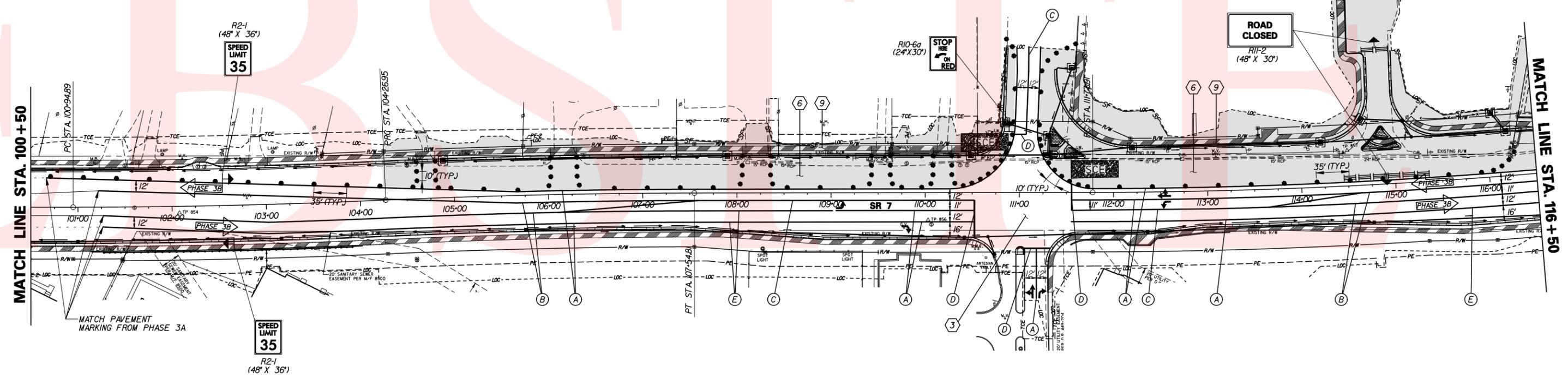
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3B**



- SEQUENCE OF CONSTRUCTION - PHASE 3B**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 3A.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 2D. MODIFY EXISTING TRAFFIC SIGNAL AT DELDOT/CHRISTIANA MEADOWS ENTRANCE. SEE TRAFFIC SIGNAL PLAN, PHASE 2.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE SOUTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS AS SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 3A.
  - 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE SOUTHBOUND SIDE OF SR 7 AS SHOWN.
  - 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS WHERE SHOWN.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

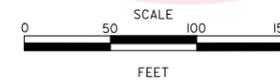
**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



PREL. TRACING  
 S.S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	78	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3B**



- SEQUENCE OF CONSTRUCTION - PHASE 3B**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 3A.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION IIB OR IT OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3B.
  - 4 USING TYPICAL APPLICATION IIB OR IT OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE SOUTHBOUND SIDE OF SR 7. ALLOW FOR DRUM OPENINGS AT EXISTING ENTRANCES AND DRIVEWAYS WHERE SHOWN.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 3A.
  - 6 SAW CUT AND REMOVE EXISTING PAVEMENT ALONG THE SOUTHBOUND SIDE OF SR 7 AS SHOWN.
  - 7 CONSTRUCT DRAINAGE SYSTEM WORKING UPSTREAM FROM OUTFALL POINT(S).
  - 8 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION. INSTALL UNDERDRAINS AS SHOWN.
  - 9 CONSTRUCT PROPOSED CURB AND GUTTER AND SIDEWALKS WHERE SHOWN.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN.
  - 11 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES NOT REQUIRED DURING FUTURE CONSTRUCTION PHASES.

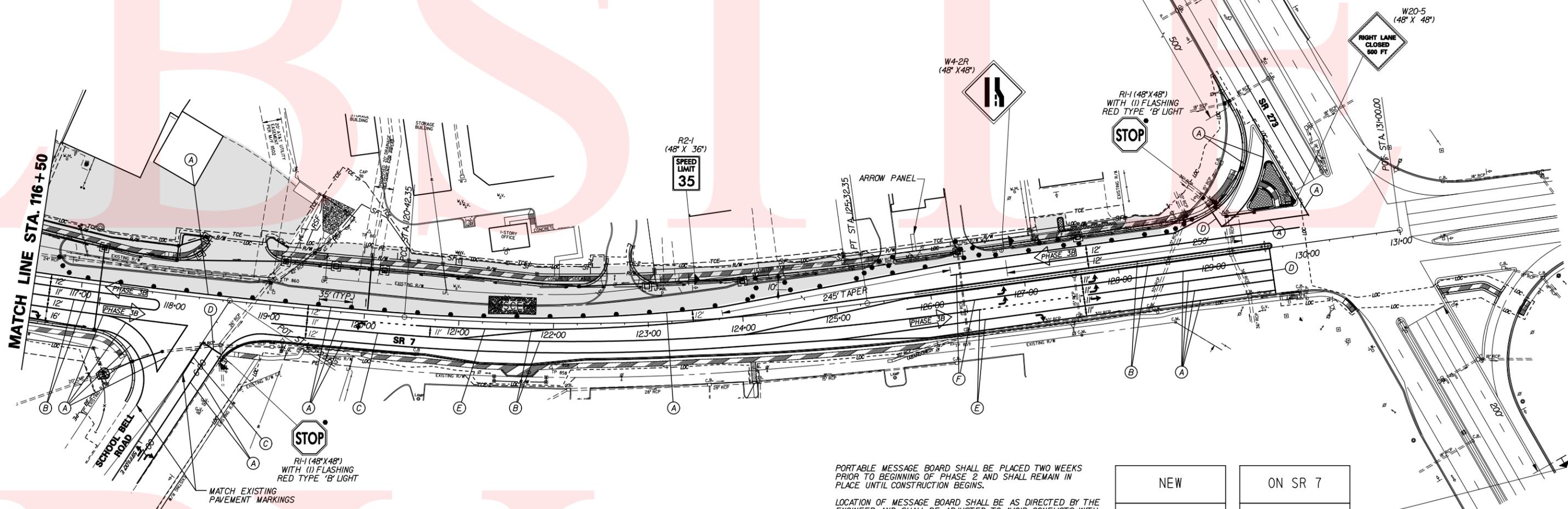
- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - # WHITE TEMPORARY PAVEMENT MARKINGS
  - # YELLOW TEMPORARY PAVEMENT MARKINGS
  - # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - # SKIP (2' LINE 6' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - # SKIP (10' LINE 30' GAP) WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

PORTABLE MESSAGE BOARD SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 2 AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION BEGINS.

LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

NEW	ON SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



PORTABLE MESSAGE BOARD SHALL BE PLACED TWO WEEKS PRIOR TO BEGINNING OF PHASE 2 AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION BEGINS.

LOCATION OF MESSAGE BOARD SHALL BE AS DIRECTED BY THE ENGINEER AND SHALL BE ADJUSTED TO AVOID CONFLICTS WITH OTHER WORK ZONE SIGNS.

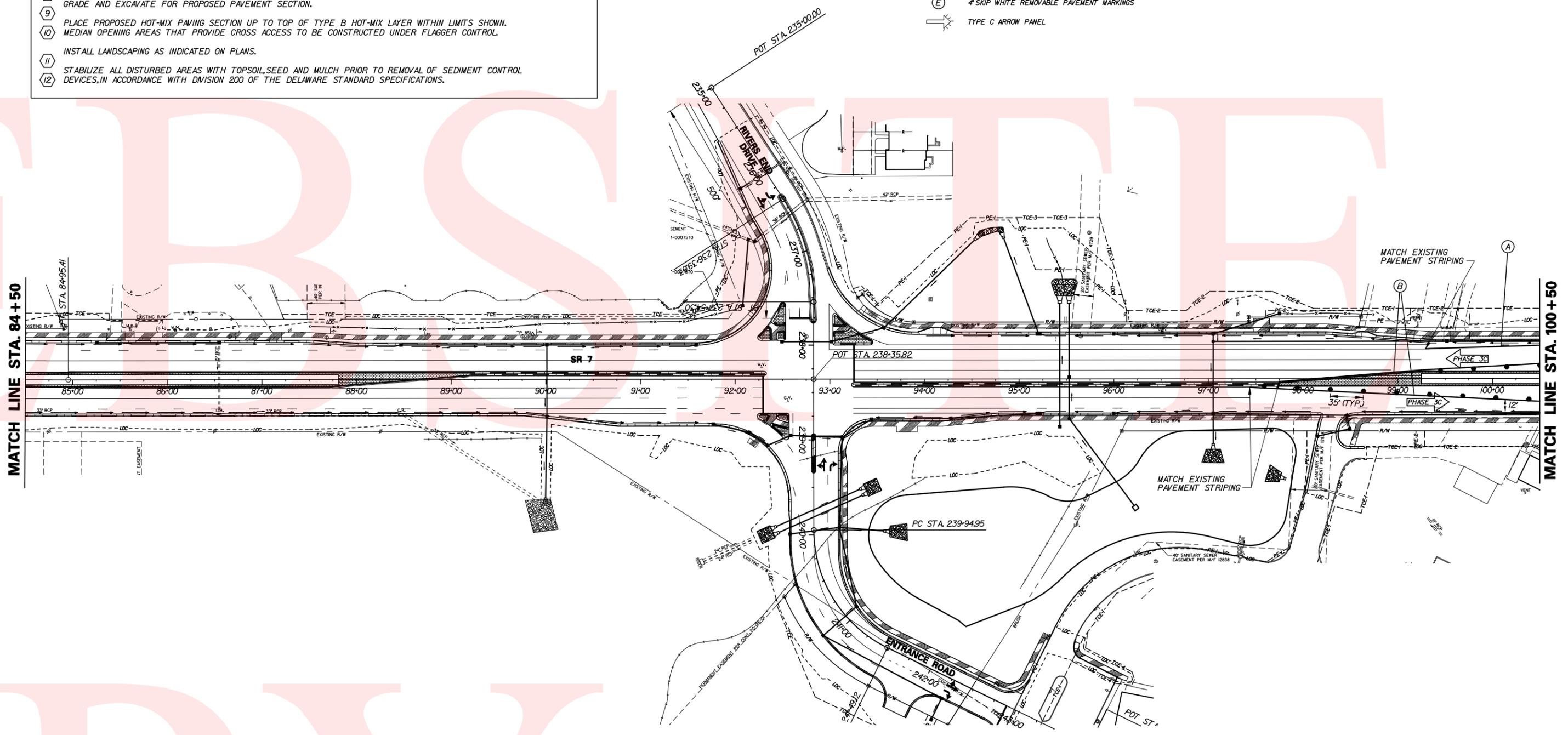
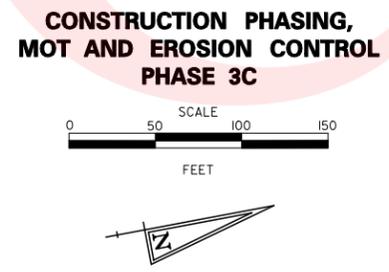
NEW	ON SR 7
TRAFFIC	BEGIN
PATTERN	MM/DD/YY

PREL. TRACING  
 S.S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	79	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

- SEQUENCE OF CONSTRUCTION - PHASE 3C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 3B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE DELAWARE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3C.  
USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
  - 4 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER.
  - 5 MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 3A AND PHASE 3B.
  - 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
  - 7 COMPLETE DRAINAGE SYSTEM.
  - 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE WHERE SHOWN.
  - 9 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
  - 11 INSTALL LANDSCAPING AS INDICATED ON PLANS.
  - 12 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL



PREL. TRACING  
 S.S.B.  
 DESIGN  
 K.G.M.-A.  
 CHKD.  
 J.R.R.

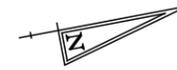
**SEQUENCE OF CONSTRUCTION - PHASE 3C**

- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 3B.
- 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
- 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3C. MODIFY EXISTING TRAFFIC SIGNAL AT DELDOT/CHRISTIANA MEADOWS ENTRANCE. SEE TRAFFIC SIGNAL PLAN, PHASE 3.
- 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
- 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 3A AND PHASE 3B.
- 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
- 7 COMPLETE DRAINAGE SYSTEM.
- 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE AS SHOWN.
- 9 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
- 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
- 11 INSTALL LANDSCAPING AS INDICATED ON PLANS.
- 12 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

**MOT LEGEND**

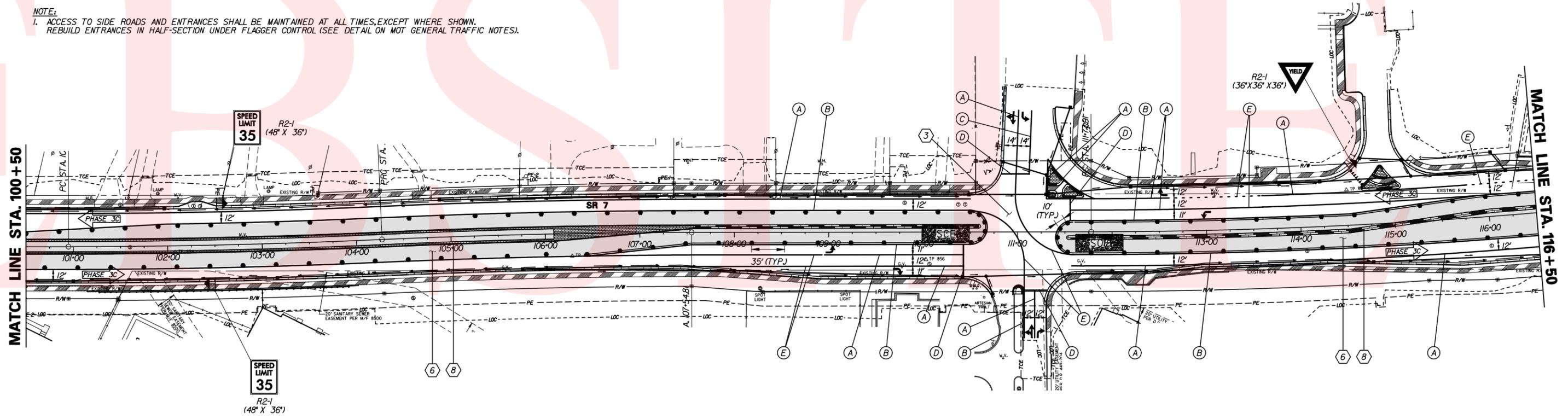
- TEMPORARY PAVEMENT
- PROPOSED CONSTRUCTION THIS PHASE
- SIGN
- TYPE III BARRICADE
- PLASTIC DRUM
- EXISTING DIRECTION OF TRAVEL
- PROPOSED DIRECTION OF TRAVEL
- TEMPORARY PAVEMENT MARKING
- # WHITE TEMPORARY PAVEMENT MARKINGS
- # YELLOW TEMPORARY PAVEMENT MARKINGS
- # DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
- 16" WHITE TEMPORARY PAVEMENT MARKINGS
- # SKIP WHITE REMOVABLE PAVEMENT MARKINGS
- TYPE C ARROW PANEL

**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3C**



CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	79A	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

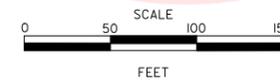
**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



PREL. TRACING  
 S.S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	79B	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

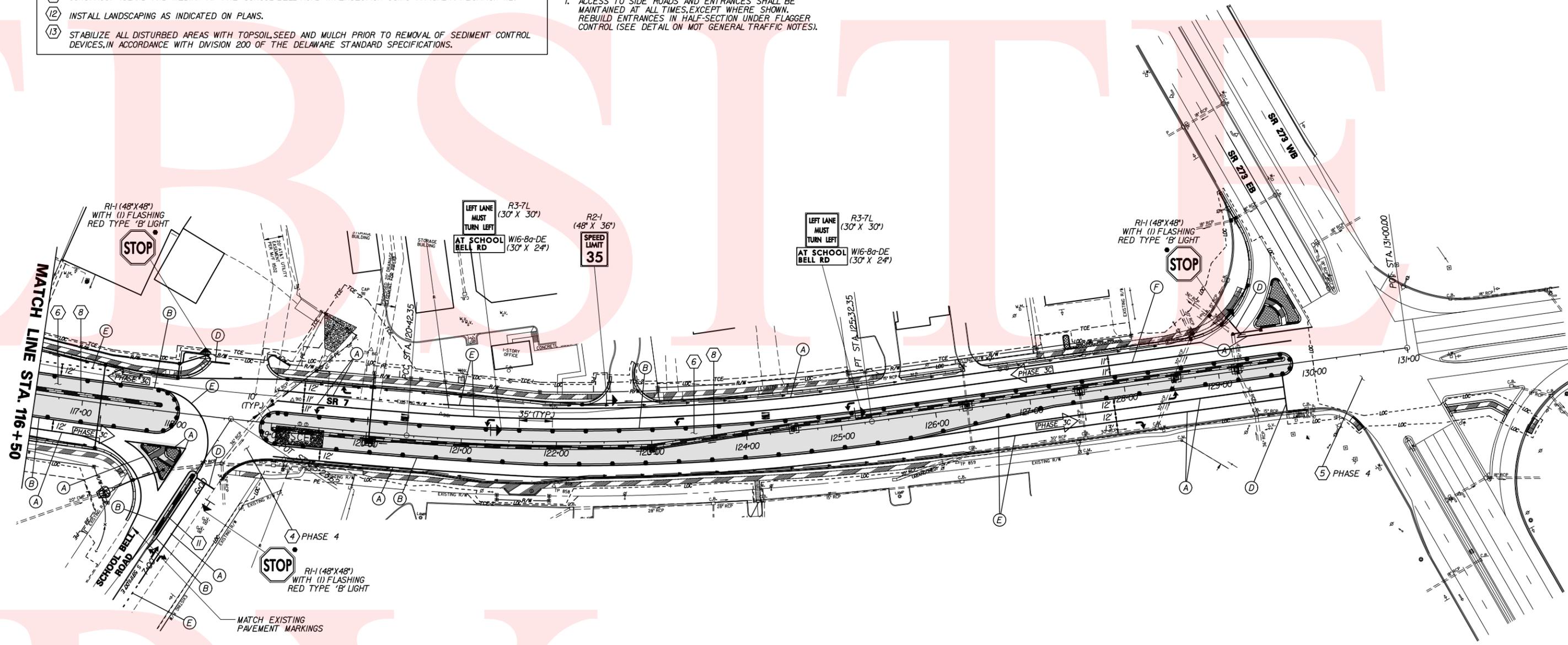
**CONSTRUCTION PHASING,  
MOT AND EROSION CONTROL  
PHASE 3C**



- SEQUENCE OF CONSTRUCTION - PHASE 3C**
- 1 REMOVE TEMPORARY TRAFFIC CONTROL DEVICES FROM PHASE 3B.
  - 2 PLACE ALL TEMPORARY WARNING SIGNS AS SHOWN ON THIS PLAN AND THE MAINTENANCE OF TRAFFIC WARNING SIGN LOCATIONS PLAN, IN ACCORDANCE WITH THE TRAFFIC CONTROL MANUAL.
  - 3 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, REMOVE EXISTING STRIPING AS NECESSARY AND PLACE TEMPORARY STRIPING AS SHOWN FOR PHASE 3.
  - 4 USING TYPICAL APPLICATION I1B OR I7 OF THE TRAFFIC CONTROL MANUAL, PLACE PLASTIC DRUMS ALONG THE MEDIAN OF SR 7.
  - 5 INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN, OR AS DIRECTED BY THE ENGINEER. MAINTAIN DRAINAGE INLET SEDIMENT CONTROL FROM PHASE 3A AND PHASE 3B.
  - 6 SAW CUT AND REMOVE EXISTING AND TEMPORARY PAVEMENT ALONG THE MEDIAN AREA OF SR 7.
  - 7 COMPLETE DRAINAGE SYSTEM.
  - 8 CONSTRUCT PROPOSED MEDIAN CURB AND PATTERNED CONCRETE WHERE SHOWN.
  - 9 GRADE AND EXCAVATE FOR PROPOSED PAVEMENT SECTION.
  - 10 PLACE PROPOSED HOT-MIX PAVING SECTION UP TO TOP OF TYPE B HOT-MIX LAYER WITHIN LIMITS SHOWN. MEDIAN OPENING AREAS THAT PROVIDE CROSS ACCESS TO BE CONSTRUCTED UNDER FLAGGER CONTROL.
  - 11 CONSTRUCT ISLAND AND MEDIAN AT THE SCHOOL BELL ROAD INTERSECTION USING TYPICAL APPLICATION I1B.
  - 12 INSTALL LANDSCAPING AS INDICATED ON PLANS.
  - 13 STABILIZE ALL DISTURBED AREAS WITH TOPSOIL, SEED AND MULCH PRIOR TO REMOVAL OF SEDIMENT CONTROL DEVICES, IN ACCORDANCE WITH DIVISION 200 OF THE DELAWARE STANDARD SPECIFICATIONS.

- MOT LEGEND**
- TEMPORARY PAVEMENT
  - PROPOSED CONSTRUCTION THIS PHASE
  - SIGN
  - TYPE III BARRICADE
  - PLASTIC DRUM
  - EXISTING DIRECTION OF TRAVEL
  - PROPOSED DIRECTION OF TRAVEL
  - TEMPORARY PAVEMENT MARKING
  - 4" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" YELLOW TEMPORARY PAVEMENT MARKINGS
  - 4" DOUBLE YELLOW TEMPORARY PAVEMENT MARKINGS
  - 16" WHITE TEMPORARY PAVEMENT MARKINGS
  - 4" SKIP WHITE REMOVABLE PAVEMENT MARKINGS
  - TYPE C ARROW PANEL

**NOTE:**  
1. ACCESS TO SIDE ROADS AND ENTRANCES SHALL BE MAINTAINED AT ALL TIMES, EXCEPT WHERE SHOWN. REBUILD ENTRANCES IN HALF-SECTION UNDER FLAGGER CONTROL (SEE DETAIL ON MOT GENERAL TRAFFIC NOTES).



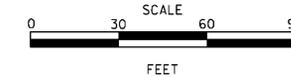
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 S.S.B.  
 DESIGN  
 K.G.M.-A  
 CHKD.  
 J.R.R.

ENVIRONMENTAL COMPLIANCE - 01

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T2003I260I	NEW CASTLE	SEE TITLE SHEET	80	115

SR 7,  
NEWTOWN ROAD TO SR 273

REVISIONS



LEGEND

- TEMPORARY IMPACT AREA
- PERMANENT IMPACT AREA
- WUS --- WATERS OF THE U.S.
- OHW --- ORDINARY HIGH WATER
- WL - CORPS WETLAND BOUNDARY
- XX ## IMPACT AREA TYPE IDENTIFIER (SEE BELOW)
- ## IMPACT AREA LOCATION NUMBER
- W = WETLAND IMPACT T = TEMPORARY IMPACT
- O = OPEN WATER IMPACT

NOTES:

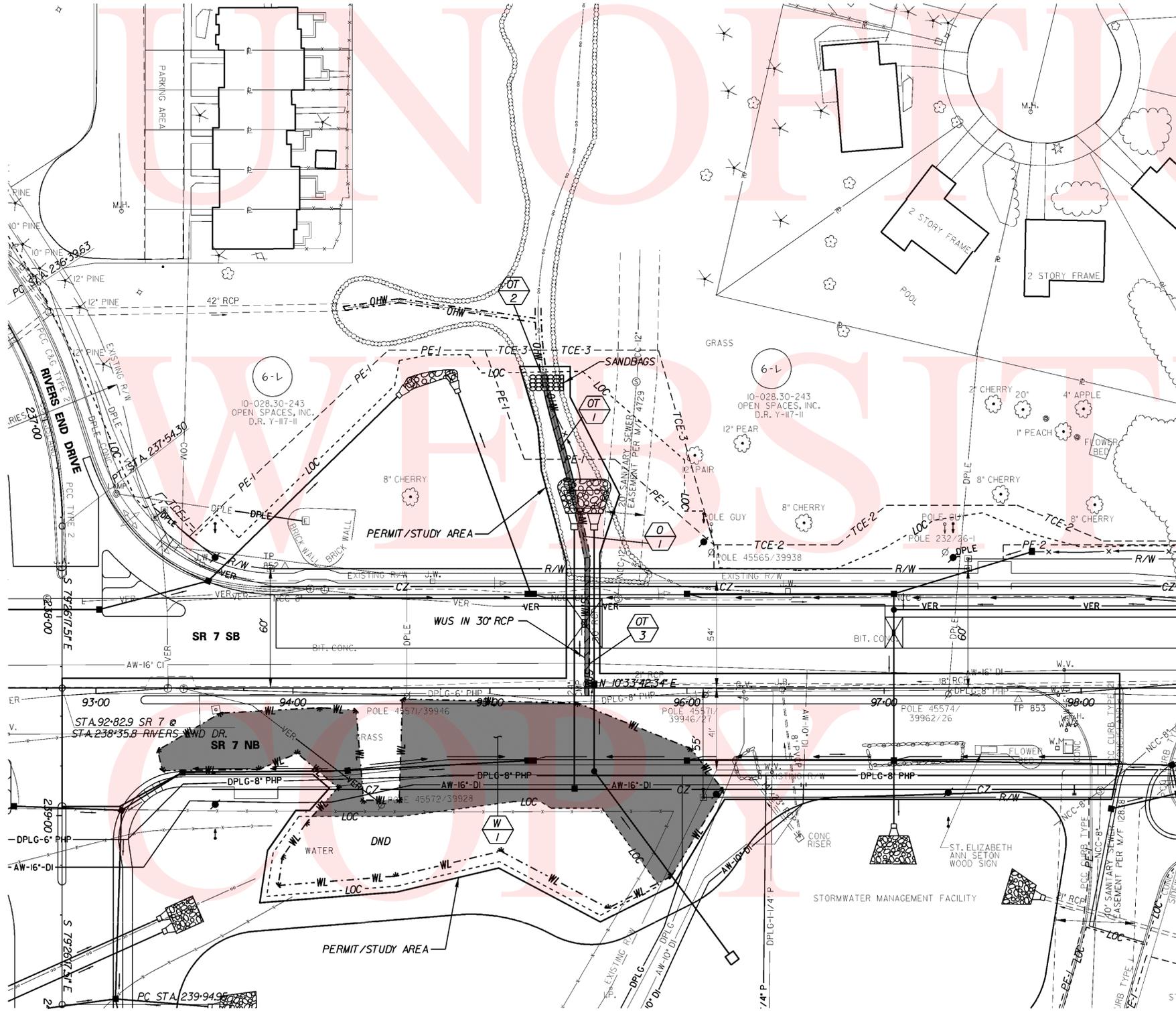
1. IN ACCORDANCE WITH THE 1987 CORPS OF ENGINEERS MANUAL, WETLANDS SHOWN ON SHEETS 1 AND 3 WERE DELINEATED BY WHITMAN, REQUARDT & ASSOCIATES IN SEPTEMBER 2003 AND JANUARY 2008. WETLANDS ON THE REYBOLD VENTURE GROUP VII, LLC, AND THE CHASE MANHATTAN BANK USA SITES WERE DELINEATED BY OTHERS.
2. SHEETS PREPARED BY: WHITMAN, REQUARDT AND ASSOCIATES, LLP  
DATE: OCTOBER 29, 2008.

TABLE OF IMPACT AREAS

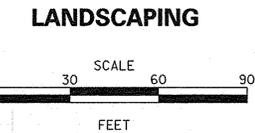
IDENTIFIER	AREA (sf)	AREA (ac)	VOLUME (cy)
<b>PERMANENT IMPACTS</b>			
FILL "W"	11810	0.2711	N/A
FILL/RIPRAP "O"	121	0.0028	7
<b>TOTAL</b>	<b>11931</b>	<b>0.2739</b>	<b>7</b>
<b>TEMPORARY IMPACTS</b>			
STREAM RELOCATION "OT"	138	0.0032	8
E&S SANDBAGS "OT 2"	29	0.0006	2
STREAM DIVERSION "OT 3"	156	0.0036	13
<b>TOTAL</b>	<b>323</b>	<b>0.0074</b>	<b>23</b>

ENVIRONMENTAL COMPLIANCE NOTES:

1. GENERAL NOTES:
    - A. THE PURPOSE OF THESE ENVIRONMENTAL COMPLIANCE SHEETS IS TO IDENTIFY THOSE ITEMS ASSOCIATED WITH ENVIRONMENTAL COMPLIANCE. IMPACT CALCULATIONS ARE FOR AGENCY PERMIT REPORTING PURPOSES ONLY AND ARE NOT TO BE USED FOR BIDDING PURPOSES.
    - B. IF A DEPARTURE FROM THE APPROVED PLANS (WHICH WOULD AFFECT ANY OF THE NATURAL AND/OR CULTURAL RESOURCE ISSUES) IS NECESSARY, THE ENVIRONMENTAL STUDIES SECTION SHOULD BE CONTACTED AT (302) 760-2264 TO ALLOW THE DEPARTMENT TO COORDINATE WITH THE APPROPRIATE AGENCIES FOR APPROVAL.
    - C. USE OF THESE SHEETS DOES NOT ALLEVIATE THE CONTRACTOR'S RESPONSIBILITY TO COMPLY WITH ALL CONDITIONS SET FORTH IN THE ENVIRONMENTAL STATEMENT AND PERMITS.
  2. NATURAL RESOURCE ISSUES:
    - A. PERMIT REQUIREMENTS / APPROVALS\*:
      - CORPS OF ENGINEERS - NATIONWIDE PERMIT #14\*\*
      - NCC DEPT OF LAND USE - NONE (MAINTENANCE-TYPE ACTIVITY)
      - DNREC - PROJECT CONSISTENT WITH DELAWARE, CODE CH. 72, SECTION 7217, EXEMPTION (A), AS AMENDED BY SB 186
    - \* - THE PERMITS/APPROVALS LISTED ARE THOSE REQUIRED FOR THIS PROJECT. THE ENVIRONMENTAL STUDIES SECTION IS RESPONSIBLE FOR COORDINATING/OBTAINING THESE PERMITS.
    - \*\* - THE CONTRACTOR MUST ENSURE THAT THESE PERMITS/APPROVALS (COE, DNREC) ARE IN THEIR POSSESSION PRIOR TO THE BEGINNING OF CONSTRUCTION IN THE PERMITTED AREA(S). ALL PERMITS SHALL BE DISPLAYED ON SITE DURING THE ENTIRE CONSTRUCTION PERIOD.
  - NOTE: THE ABSENCE OF ASTERISKS AFTER A PERMIT/APPROVAL (I.E. DNREC) INDICATES THAT COORDINATION HAS BEEN DONE WITH THAT AGENCY, BUT NO WRITTEN AUTHORIZATION WAS REQUIRED, AS SUCH, NO PAPERWORK FROM THAT AGENCY SHOULD BE ANTICIPATED.
  - B. CONSTRUCTION RESTRICTIONS:
    - FISHERIES - NONE
    - ENDANGERED SPECIES - NONE
    - MIGRATORY BIRDS - NONE
  - C. CHANNEL RESTORATION:
    - THE TOP ELEVATION OF RIPRAP TO BE PLACED IN THE CHANNEL SHALL MATCH THE EXISTING UPSTREAM/DOWNSTREAM CHANNEL BOTTOM ELEVATIONS. TYPE "B" BORROW SHALL BE USED TO BACKFILL THE VOIDS BETWEEN THE STONES OF THE RIPRAP PLACED IN THE CHANNEL. RIPRAP TO BE PLACED ON THE CHANNEL SIDESLOPES SHALL BE BACKFILLED WITH TOPSOIL, SEEDED, AND MULCHED. PAYMENT FOR BACKFILLING THE RIPRAP SHALL BE INCIDENTAL TO ITEM NO 712006-RIPRAP, R-5.
    - CHANNEL RESTORATION PROVISIONS APPLY AT THE FOLLOWING LOCATIONS:
      - STA. 95+50, LT.
      - STA. 119+50, LT.
  - D. PROTECTION OF RESOURCES: REINFORCED SILT FENCE SHALL BE PLACED AT THE LIMITS OF CONSTRUCTION (LOC) WHERE EXISTING WETLANDS ARE IMPACTED. CONTRACTOR ACCESS BEYOND LOC IS STRICTLY PROHIBITED IN ORDER TO PREVENT THE DISTURBANCE OF RESOURCES TO BE SAVED.
  - E. WETLANDS CREATION: DUE TO THE QUANTITY OF PERMANENT WETLANDS/WATERS IMPACTS (GREATER THAN 1/10 ACRE), COMPENSATORY MITIGATION IS REQUIRED TO SATISFY THE CORPS OF ENGINEERS PERMIT. A TOTAL OF 0.2739 ACRES WILL BE DEDUCTED FROM THE BALANCE AVAILABLE AT THE GLENVILLE WETLAND MITIGATION BANK.
3. CULTURAL RESOURCES ISSUES:
  - SEE PROJECT NOTE 20 ON SHEET 4 CONCERNING CONSTRUCTION ACTIVITY RESTRICTIONS ON PARCEL 5-R - 10-028.00-029, OLD FORT U.A.M.E. CHURCH.



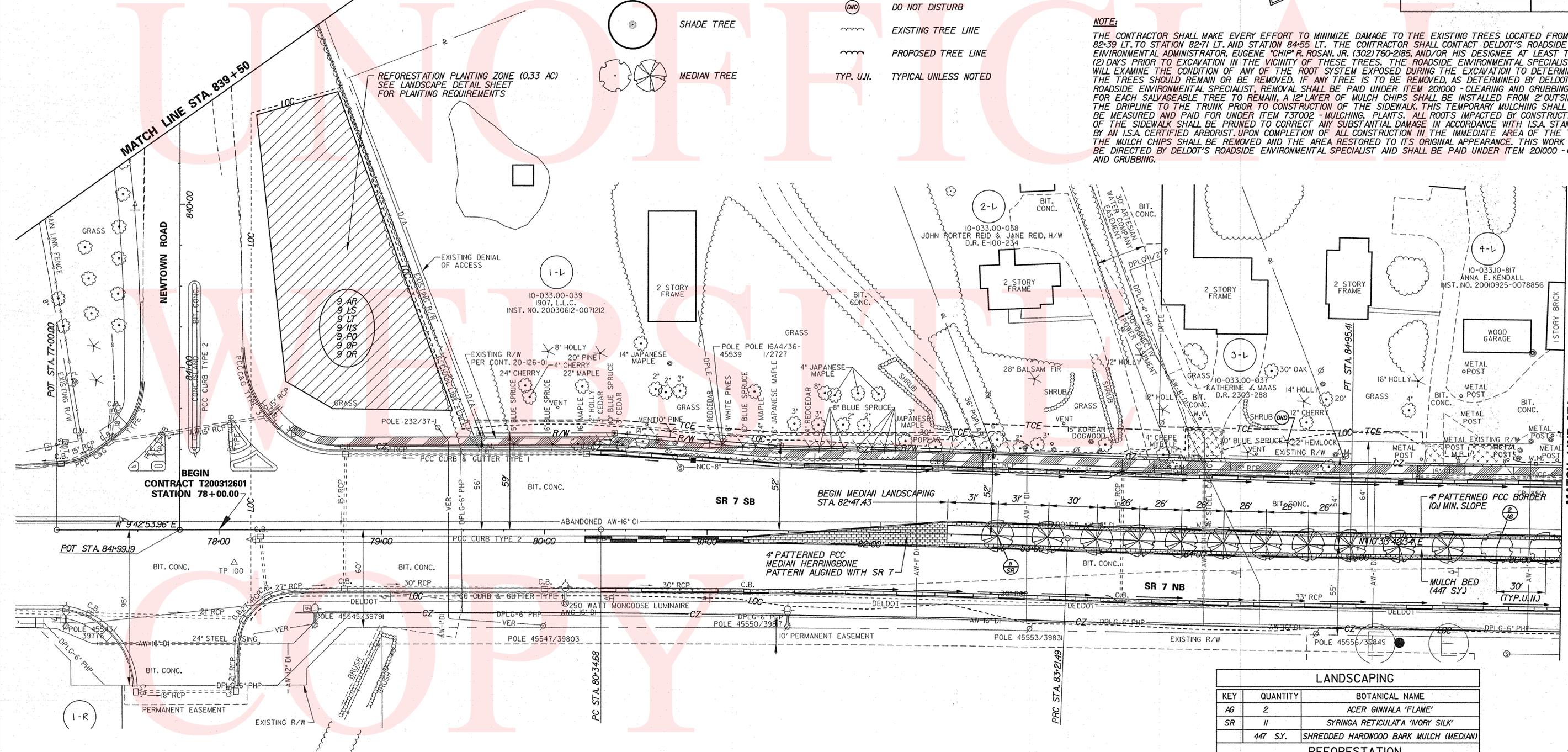
PREL. TRACING SUB DESIGN KOM-A CHKD. JRR



**LANDSCAPING LEGEND**

- |  |                              |           |                      |
|--|------------------------------|-----------|----------------------|
|  | LANDSCAPING QUANTITY/SPECIES |           | EVERGREEN TREE       |
|  | EXISTING DECIDUOUS TREE      |           | DECIDUOUS SHRUBS     |
|  | EXISTING CONIFEROUS TREE     |           | PATTERNED CONCRETE   |
|  | SHADE TREE                   |           | DO NOT DISTURB       |
|  | MEDIAN TREE                  |           | EXISTING TREE LINE   |
|  |                              |           | PROPOSED TREE LINE   |
|  |                              | TYP. U.N. | TYPICAL UNLESS NOTED |

**NOTE:**  
 THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE DAMAGE TO THE EXISTING TREES LOCATED FROM STATION 82+39 LT. TO STATION 82+71 LT. AND STATION 84+55 LT. THE CONTRACTOR SHALL CONTACT DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE "CHIP" R. ROSAN, JR. (302)760-2185, AND/OR HIS DESIGNEE AT LEAST TWO (2) DAYS PRIOR TO EXCAVATION IN THE VICINITY OF THESE TREES. THE ROADSIDE ENVIRONMENTAL SPECIALIST WILL EXAMINE THE CONDITION OF ANY OF THE ROOT SYSTEM EXPOSED DURING THE EXCAVATION TO DETERMINE IF THE TREES SHOULD REMAIN OR BE REMOVED. IF ANY TREE IS TO BE REMOVED, AS DETERMINED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST, REMOVAL SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING. FOR EACH SALVAGEABLE TREE TO REMAIN, A 12" LAYER OF MULCH CHIPS SHALL BE INSTALLED FROM 2' OUTSIDE THE DRIPLINE TO THE TRUNK PRIOR TO CONSTRUCTION OF THE SIDEWALK. THIS TEMPORARY MULCHING SHALL BE MEASURED AND PAID FOR UNDER ITEM 737002 - MULCHING, PLANTS. ALL ROOTS IMPACTED BY CONSTRUCTION OF THE SIDEWALK SHALL BE PRUNED TO CORRECT ANY SUBSTANTIAL DAMAGE IN ACCORDANCE WITH I.S.A. STANDARDS BY AN I.S.A. CERTIFIED ARBORIST. UPON COMPLETION OF ALL CONSTRUCTION IN THE IMMEDIATE AREA OF THE TREE, THE MULCH CHIPS SHALL BE REMOVED AND THE AREA RESTORED TO ITS ORIGINAL APPEARANCE. THIS WORK SHALL BE DIRECTED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST AND SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
AG	2	ACER GINNALA 'FLAME'
SR	11	SYRINGA RETICULATA 'MORY SILK'
	447 SY.	SHREDDED HARDWOOD BARK MULCH (MEDIAN)

REFORESTATION	
QUANTITY	BOTANICAL NAME
9	ACER RUBRUM
9	LIQUIDAMBAR STYRACIFLUA
9	LIRIODENDRON TULIPIFERA
9	NYSSA SYLVATICA
9	PLATANUS OCCIDENTALIS
9	QUERCUS PALUSTRIS
9	QUERCUS RUBRA
68 SY.	SHREDDED HARDWOOD BARK MULCH (REFORESTATION)

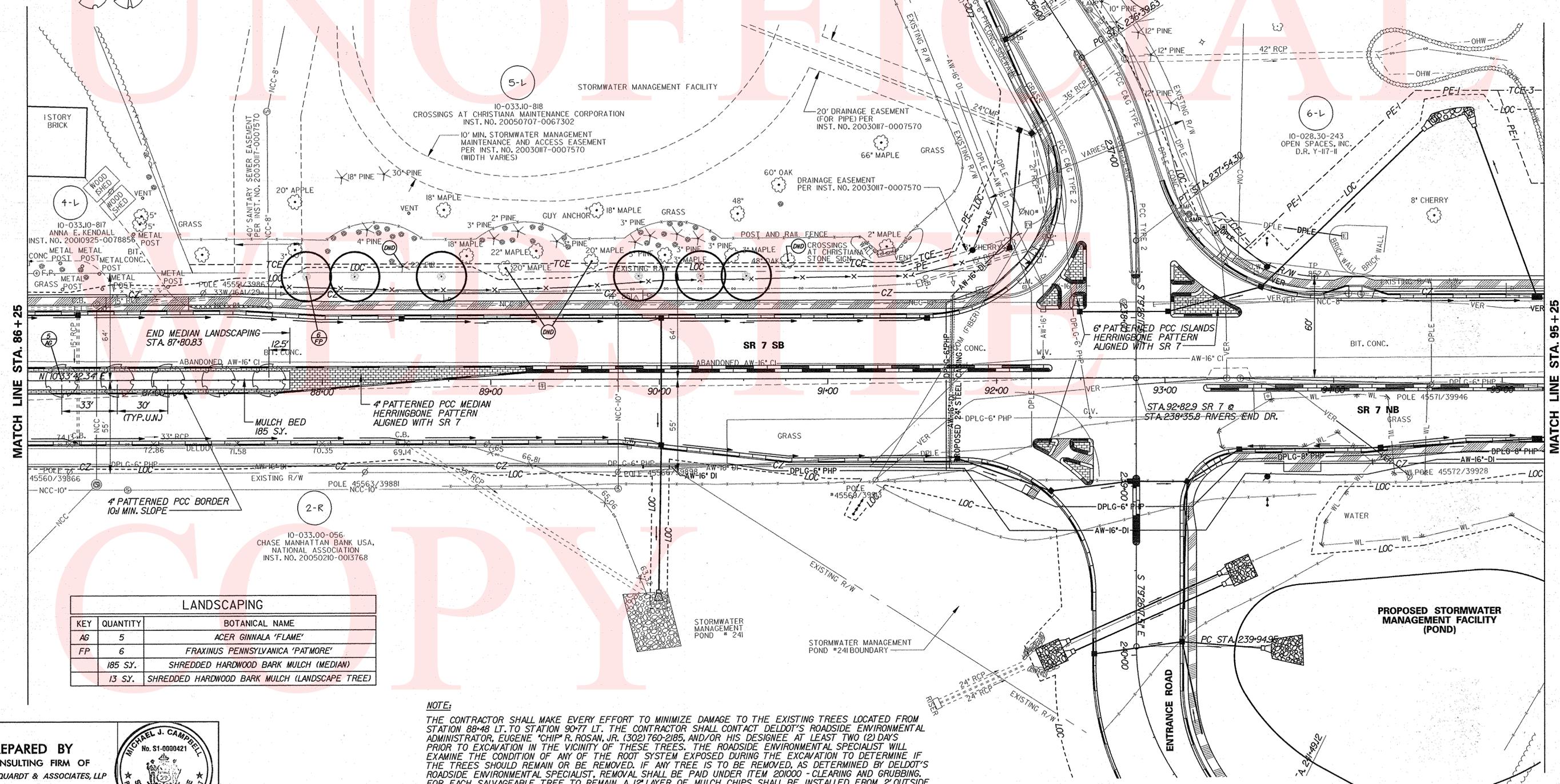
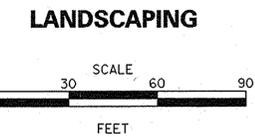
PREPARED BY  
 THE CONSULTING FIRM OF  
 WHITMAN, REQUARDT & ASSOCIATES, LLP

MICHAEL J. CAMPBELL  
 No. S1-0000421  
 REGISTERED LANDSCAPE ARCHITECT  
 STATE OF DELAWARE

MICHAEL J. CAMPBELL, RLA  
 RECOMMENDED  
 JAN 7 20 11

**LANDSCAPING LEGEND**

- |  |                              |  |                                |
|--|------------------------------|--|--------------------------------|
|  | LANDSCAPING QUANTITY/SPECIES |  | EVERGREEN TREE                 |
|  | EXISTING DECIDUOUS TREE      |  | DECIDUOUS SHRUBS               |
|  | EXISTING CONIFEROUS TREE     |  | PATTERNED CONCRETE             |
|  | SHADE TREE                   |  | DO NOT DISTURB                 |
|  | MEDIAN TREE                  |  | EXISTING TREE LINE             |
|  |                              |  | PROPOSED TREE LINE             |
|  |                              |  | TYP. U.N. TYPICAL UNLESS NOTED |



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
AG	5	ACER GINNALA 'FLAME'
FP	6	FRAXINUS PENNSYLVANICA 'PATMORE'
	185 SY.	SHREDDED HARDWOOD BARK MULCH (MEDIAN)
	13 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

**NOTE:**  
 THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE DAMAGE TO THE EXISTING TREES LOCATED FROM STATION 88+48 LT. TO STATION 90+77 LT. THE CONTRACTOR SHALL CONTACT DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE "CHIP" R. ROSAN, JR. (302) 760-2185 AND/OR HIS DESIGNEE AT LEAST TWO (2) DAYS PRIOR TO EXCAVATION IN THE VICINITY OF THESE TREES. THE ROADSIDE ENVIRONMENTAL SPECIALIST WILL EXAMINE THE CONDITION OF ANY OF THE ROOT SYSTEM EXPOSED DURING THE EXCAVATION TO DETERMINE IF THE TREES SHOULD REMAIN OR BE REMOVED. IF ANY TREE IS TO BE REMOVED, AS DETERMINED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST, REMOVAL SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING. FOR EACH SALVAGEABLE TREE TO REMAIN, A 12" LAYER OF MULCH CHIPS SHALL BE INSTALLED FROM 2' OUTSIDE THE DRIPLINE TO THE TRUNK PRIOR TO CONSTRUCTION OF THE SIDEWALK. THIS TEMPORARY MULCHING SHALL BE MEASURED AND PAID FOR UNDER ITEM 737002 - MULCHING, PLANTS. ALL ROOTS IMPACTED BY CONSTRUCTION OF THE SIDEWALK SHALL BE PRUNED TO CORRECT ANY SUBSTANTIAL DAMAGE IN ACCORDANCE WITH I.S.A. STANDARDS BY AN I.S.A. CERTIFIED ARBORIST UPON COMPLETION OF ALL CONSTRUCTION IN THE IMMEDIATE AREA OF THE TREE. THE MULCH CHIPS SHALL BE REMOVED AND THE AREA RESTORED TO ITS ORIGINAL APPEARANCE. THIS WORK SHALL BE DIRECTED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST AND SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.

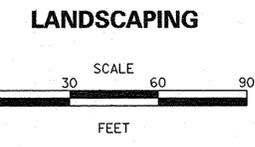
**PREPARED BY**  
 THE CONSULTING FIRM OF  
 WHITMAN, REQUARDT & ASSOCIATES, LLP

*M. J. Campbell*  
 MICHAEL J. CAMPBELL, R.L.A.  
 RECOMMENDED **JAN 7** 20 **11**

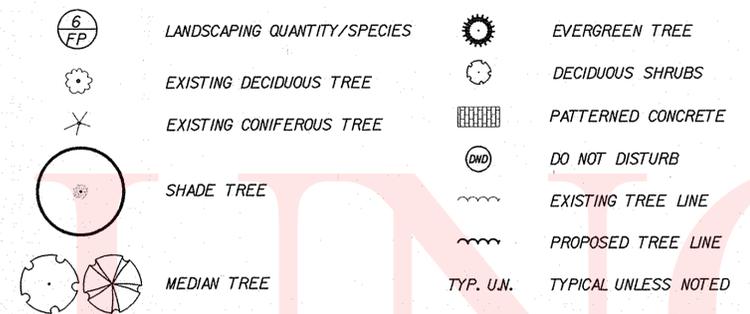
PREL. TRACING SUB DESIGN MJC CHKD. JRR

**SR 7,  
NEWTOWN ROAD TO SR 273**

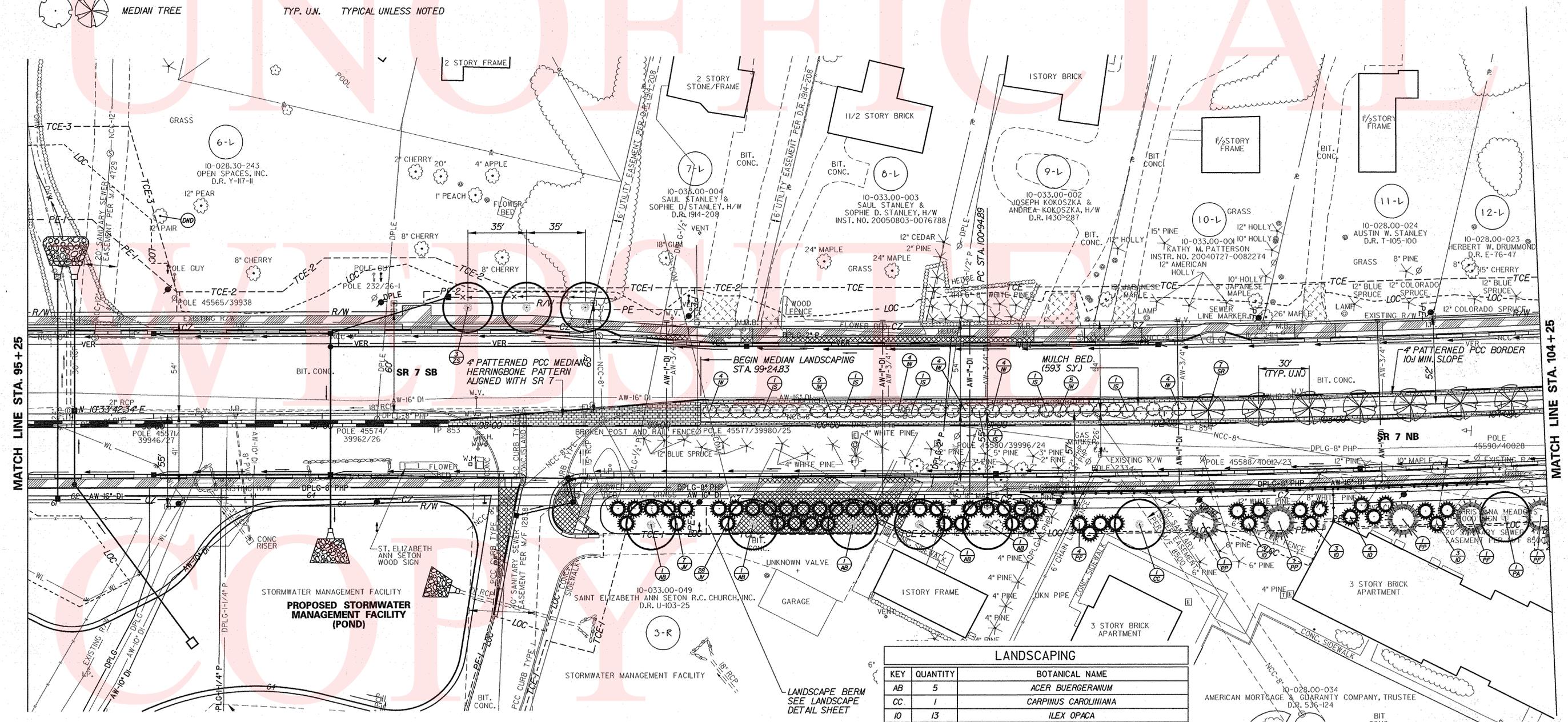
REVISIONS	



**LANDSCAPING LEGEND**



**NOTE:**  
ACCESS FOR INSTALLATION OF LANDSCAPING ON PARCEL 4-R OUTSIDE OF PE, TCE, AND FEE SIMPLE ACQUISITIONS SHOWN ON THESE PLANS IS VIA SEPARATE RIGHT OF ENTRY WITH PROPERTY OWNER.



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
AB	5	ACER BUERGERANUM
CC	1	CARPINUS CAROLINIANA
IO	13	ILEX OPACA
JV	35	JUNERERUS VIRGINIANA
PA	1	PLATANUS ACERIFOLIA
PF	6	PINUS FLEXILIS
PP	3	PICEA PUNGENS
SR	7	SYRINGA RETICULATA 'IVORY SILK'
ZS	3	ZELKOVA SERRATA 'GREEN VASE'
IS	6	ILEX VERTICILLATA 'SOUTHERN GENTLEMAN'
IW	30	ILEX VERTICILLATA 'WINTER RED'
	593 SY.	SHREDDED HARDWOOD BARK MULCH (MEDIAN)
	143 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

PREPARED BY  
THE CONSULTING FIRM OF  
WHITMAN, REQUARDT & ASSOCIATES, LLP

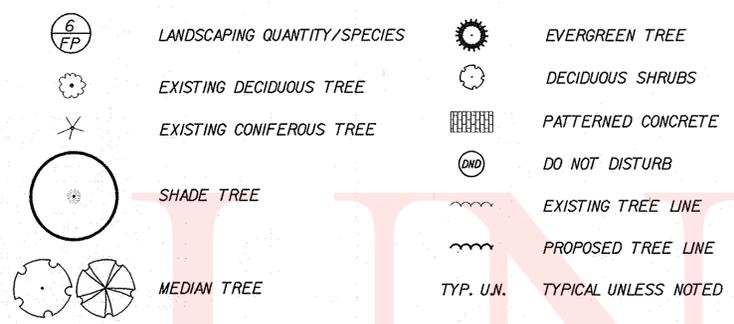
*Michael J. Campbell*  
MICHAEL J. CAMPBELL, RLA

RECOMMENDED JAN 7 20 11

**SR 7,  
NEWTOWN ROAD TO SR 273**

REVISIONS	

**LANDSCAPING LEGEND**

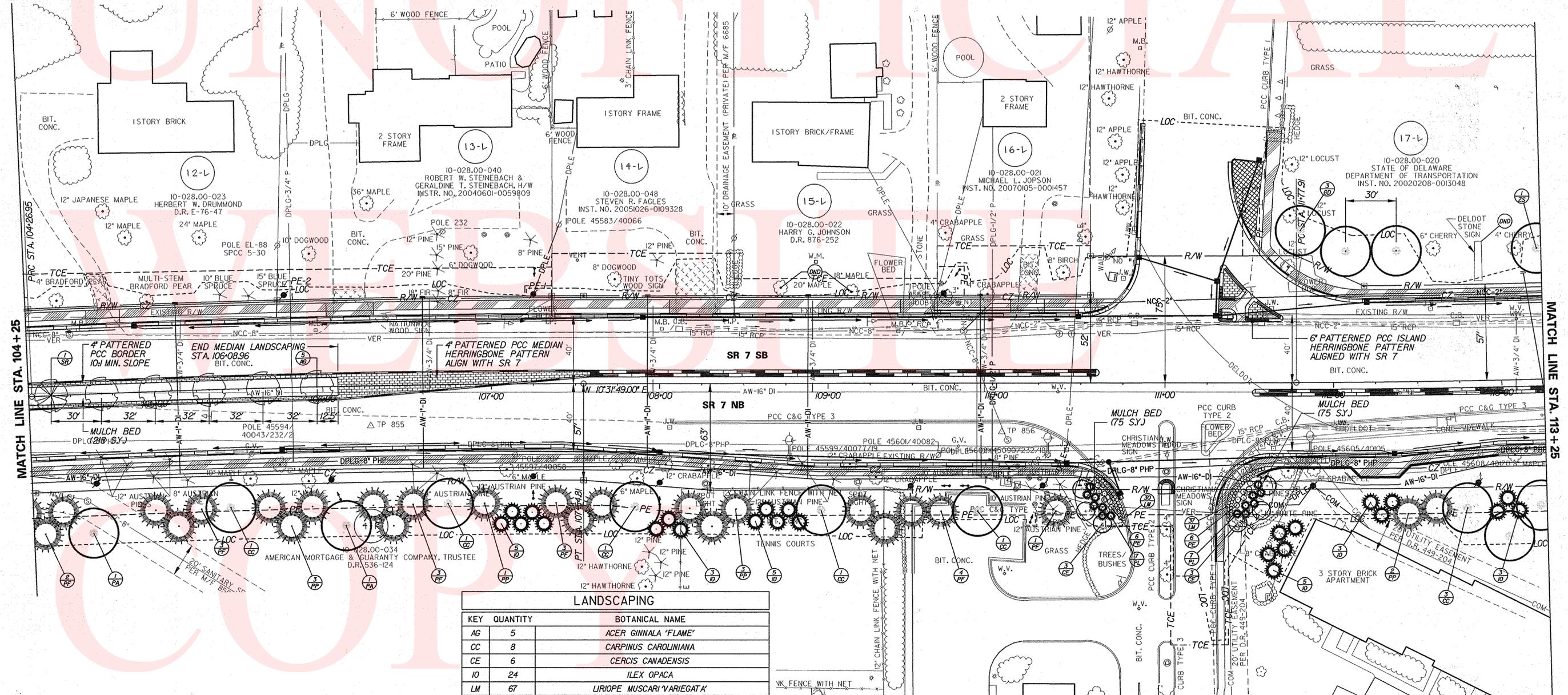
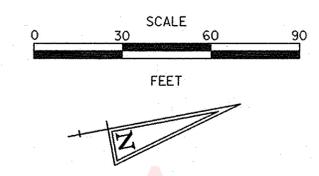


**NOTE:**

THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE DAMAGE TO THE EXISTING TREES LOCATED FROM STATION 108+72 LT. TO STATION 109+4 LT. THE CONTRACTOR SHALL CONTACT DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE "CHIP" R. ROSAN, JR. (302) 760-2185, AND/OR HIS DESIGNEE, AT LEAST TWO (2) DAYS PRIOR TO EXCAVATION IN THE VICINITY OF THESE TREES. THE ROADSIDE ENVIRONMENTAL SPECIALIST WILL EXAMINE THE CONDITION OF ANY OF THE ROOT SYSTEM EXPOSED DURING THE EXCAVATION TO DETERMINE IF THE TREES SHOULD REMAIN OR BE REMOVED. IF ANY TREE IS TO BE REMOVED, AS DETERMINED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST, REMOVAL SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING. FOR EACH SALVAGEABLE TREE TO REMAIN, A 12" LAYER OF MULCH CHIPS SHALL BE INSTALLED FROM 2' OUTSIDE THE DRIPLINE TO THE TRUNK PRIOR TO CONSTRUCTION OF THE SIDEWALK. THIS TEMPORARY MULCHING SHALL BE MEASURED AND PAID FOR UNDER ITEM 737002 - MULCHING, PLANTS. ALL ROOTS IMPACTED BY CONSTRUCTION OF THE SIDEWALK SHALL BE PRUNED TO CORRECT ANY SUBSTANTIAL DAMAGE IN ACCORDANCE WITH ISA STANDARDS BY AN ISA CERTIFIED ARBORIST. UPON COMPLETION OF ALL CONSTRUCTION IN THE IMMEDIATE AREA OF THE TREE, THE MULCH CHIPS SHALL BE REMOVED AND THE AREA RESTORED TO ITS ORIGINAL APPEARANCE. THIS WORK SHALL BE DIRECTED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST AND SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.

ACCESS FOR INSTALLATION OF LANDSCAPING ON PARCEL 4-R OUTSIDE OF PE, TCE, AND FEE SIMPLE ACQUISITIONS SHOWN ON THESE PLANS IS VIA SEPARATE RIGHT OF ENTRY WITH PROPERTY OWNER.

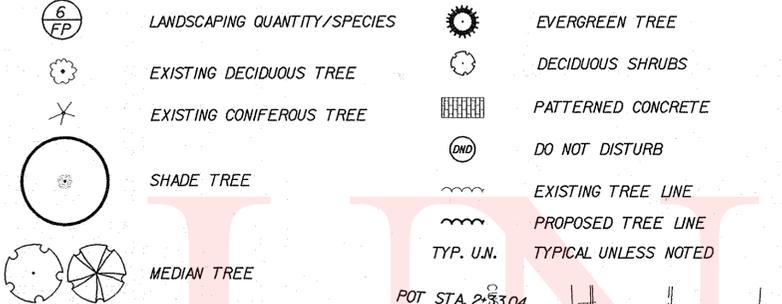
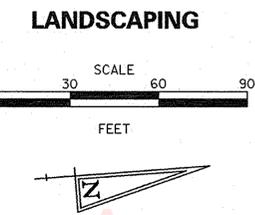
**LANDSCAPING**



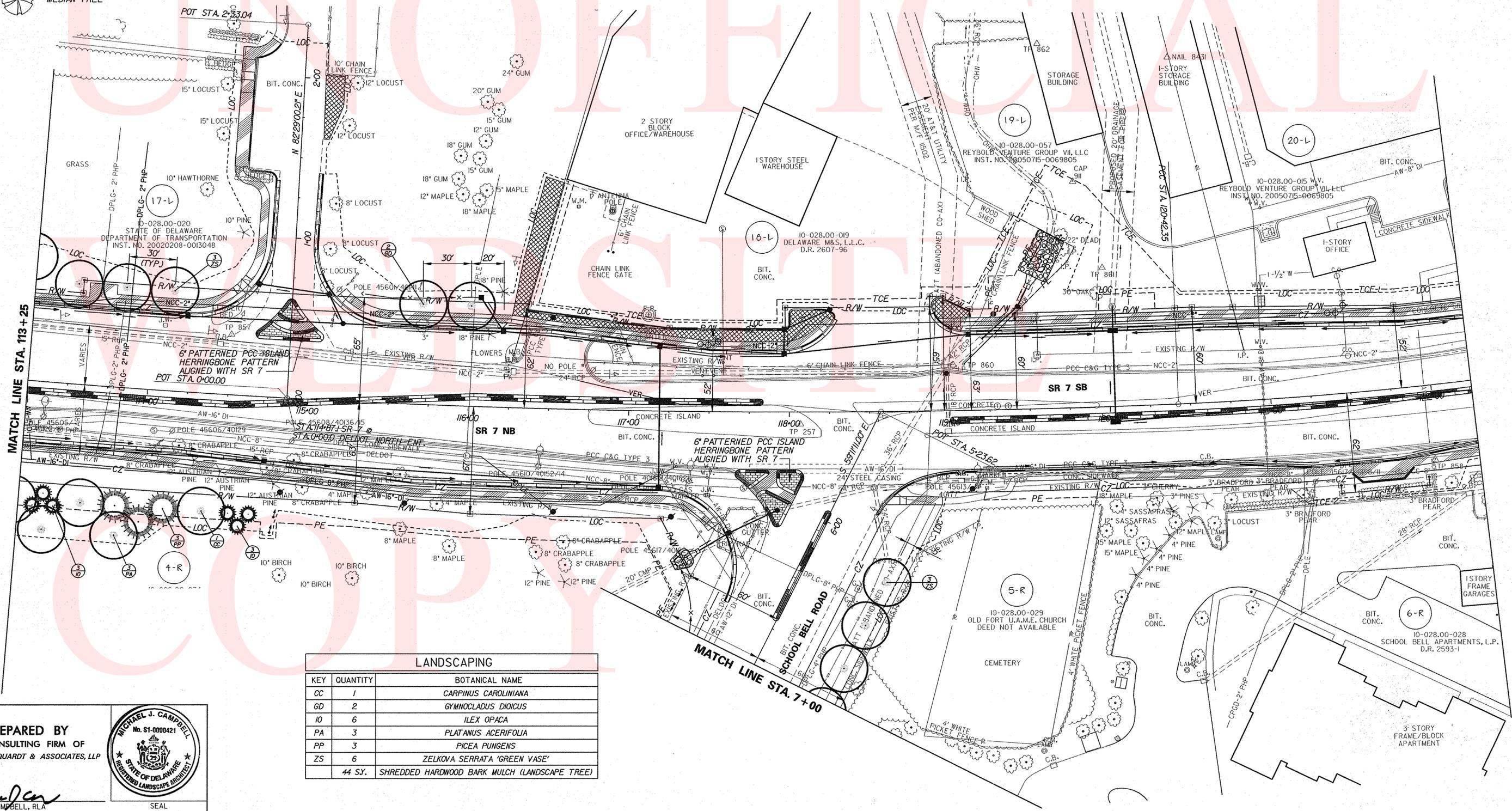
LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
AG	5	ACER GINNALA 'FLAME'
CC	8	CARPINUS CAROLINIANA
CE	6	CERCIS CANADENSIS
IO	24	ILEX OPACA
LM	67	LIRIOPE MUSCARI 'VARIEGATA'
PA	2	PLATANUS ACERIFOLIA
PE	12	PENNISETUM ALOPECUROIDES
PF	16	PINUS FLEXILIS
PL	14	PRUNUS LAUROCERASUS 'OTTO LUYKENS'
PP	9	PICEA PUNGENS
SR	1	SYRINGA RETICULATA 'NORY SILK'
GD	3	GYMNOCLADUS DIOICUS
ZS	1	ZELKOVA SERRATA 'GREEN VASE'
	218 SY.	SHREDDED HARDWOOD BARK MULCH (MEDIAN)
	275 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

PREPARED BY  
THE CONSULTING FIRM OF  
WHITMAN, REQUARDT & ASSOCIATES, LLP

MICHAEL J. CAMPBELL, RLA  
RECOMMENDED **JAN 7** 20 **11**



**NOTE:**  
ACCESS FOR INSTALLATION OF LANDSCAPING ON PARCEL 4-R OUTSIDE OF P.E.T.C.E. AND FEE SIMPLE ACQUISITIONS SHOWN ON THESE PLANS IS VIA SEPARATE RIGHT OF ENTRY WITH PROPERTY OWNER.



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
CC	1	CARPINUS CAROLINIANA
GD	2	GYMNOCLADUS DIOICUS
IO	6	ILEX OPACA
PA	3	PLATANUS ACERIFOLIA
PP	3	PICEA PUNGENS
ZS	6	ZELKOVA SERRATA 'GREEN VASE'
	44 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

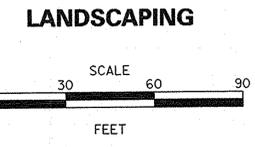
PREL. TRACING  
 SUBJ  
 DESIGN  
 MJC  
 CHVD  
 JRR

**PREPARED BY**  
THE CONSULTING FIRM OF  
WHITMAN, REQUARDT & ASSOCIATES, LLP

RECOMMENDED **JAN 7** 20 **11**

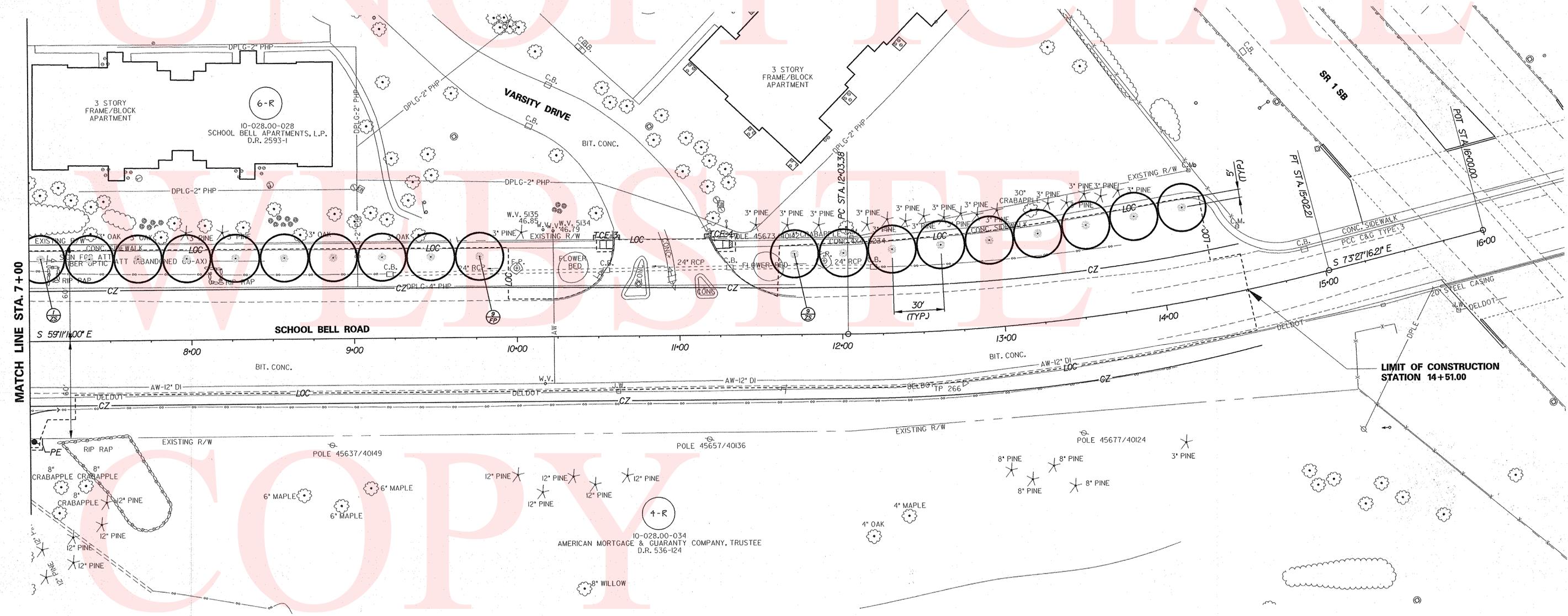
**SR 7,  
NEWTOWN ROAD TO SR 273**

REVISIONS	



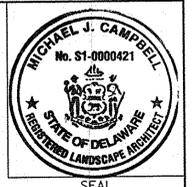
**LANDSCAPING LEGEND**

- |  |                              |           |                      |
|--|------------------------------|-----------|----------------------|
|  | LANDSCAPING QUANTITY/SPECIES |           | EVERGREEN TREE       |
|  | EXISTING DECIDUOUS TREE      |           | DECIDUOUS SHRUBS     |
|  | EXISTING CONIFEROUS TREE     |           | PATTERNED CONCRETE   |
|  | SHADE TREE                   |           | DO NOT DISTURB       |
|  | MEDIAN TREE                  |           | EXISTING TREE LINE   |
|  |                              |           | PROPOSED TREE LINE   |
|  |                              | TYP. U.N. | TYPICAL UNLESS NOTED |



PREL. TRACING  
 SUB  
 DESIGN  
 MJC  
 CHKD.  
 JRR

**PREPARED BY**  
 THE CONSULTING FIRM OF  
 WHITMAN, REQUARDT & ASSOCIATES, LLP  
  
 MICHAEL J. CAMPBELL, RLA  
 RECOMMENDED JAN 7 2011



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
FP	9	FRAXINUS PENNSYLVANICA 'PAT MORE'
ZS	10	ZELKOVA SERRATA 'GREEN VASE'
	41 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

**LANDSCAPING LEGEND**

- |  |                              |  |                               |
|--|------------------------------|--|-------------------------------|
|  | LANDSCAPING QUANTITY/SPECIES |  | EVERGREEN TREE                |
|  | EXISTING DECIDUOUS TREE      |  | DECIDUOUS SHRUBS              |
|  | EXISTING CONIFEROUS TREE     |  | PATTERNED CONCRETE            |
|  | SHADE TREE                   |  | DO NOT DISTURB                |
|  | MEDIAN TREE                  |  | EXISTING TREE LINE            |
|  |                              |  | PROPOSED TREE LINE            |
|  |                              |  | TYP. UN. TYPICAL UNLESS NOTED |

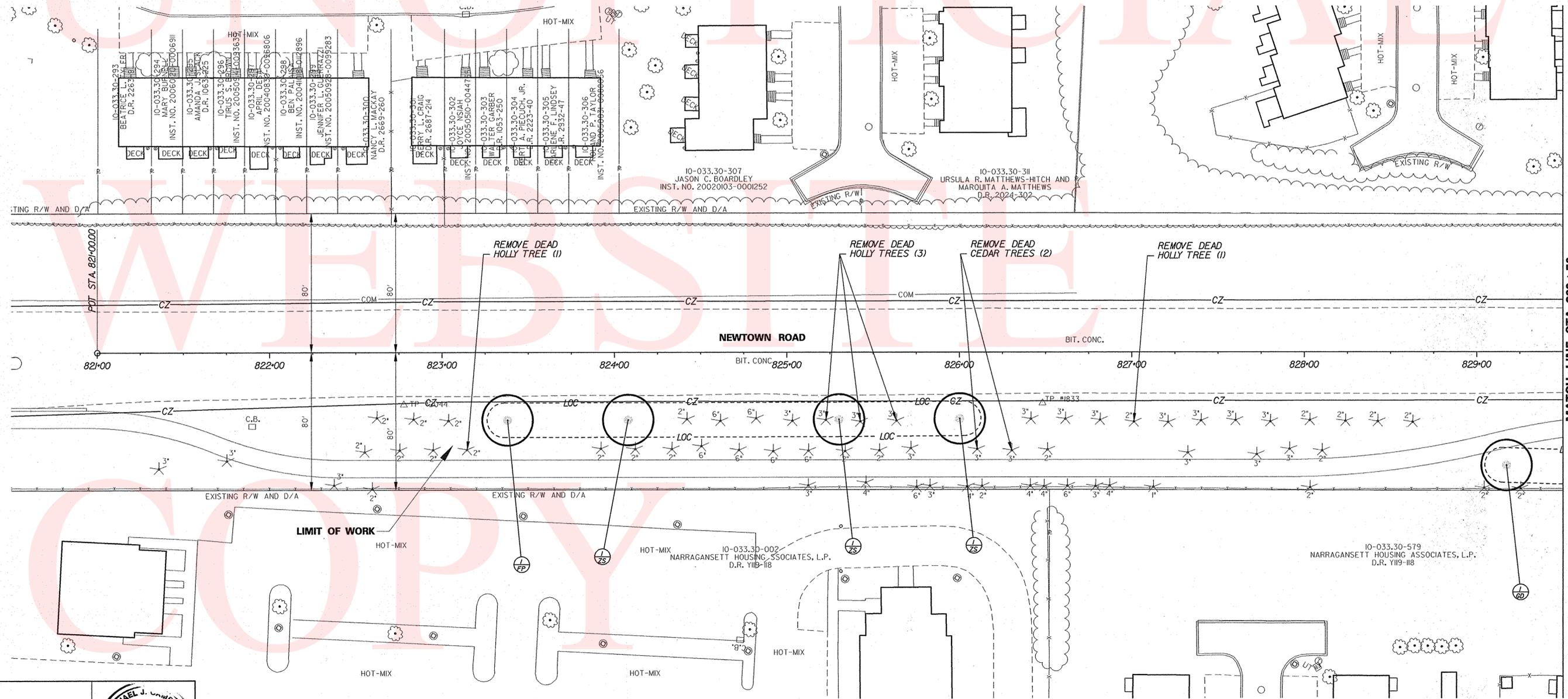
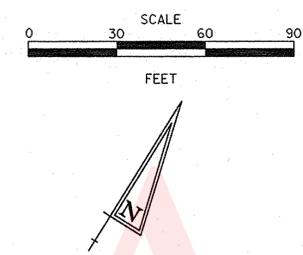
**NOTE:**

WORK WITHIN THE LOC ALONG NEWTOWN ROAD IS LIMITED TO TREE PLANTING AND TREE REPLACEMENT ONLY. DISTURBANCE SHALL BE LIMITED TO EXCAVATION REQUIRED FOR THE REMOVAL OF TREES IDENTIFIED TO BE REMOVED AND THE PROPOSED PLANTING PITS ONLY.

THE CONTRACTOR SHALL REMOVE ONLY TREES LOCATED WITHIN THE LOC WHICH HAVE BEEN IDENTIFIED TO BE REMOVED ON THE PLANS. ANY PLANT MATERIAL NOT IDENTIFIED FOR REMOVAL THAT IS DAMAGED DURING PLANTING INSTALLATION SHALL BE REPLACED BY THE CONTRACTOR WITHIN 48 HOURS.

ANY DISTURBANCE TO THE EXISTING LAWN INCURRED DURING PLANTING OPERATIONS AND VEHICULAR ACCESS REQUIRED FOR PLANTING SHALL BE RESTORED TO PRE-EXISTING CONDITIONS AND IS INCIDENTAL TO ITEM 731523 - PLANTING.

**LANDSCAPING**



LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
GD	1	GYMNOCLADUS DIOICUS
FP	1	FRAXINUS PENNSYLVANICA 'PATMORE'
ZS	3	ZELKOVA SERRATA 'GREEN VASE'
	11 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

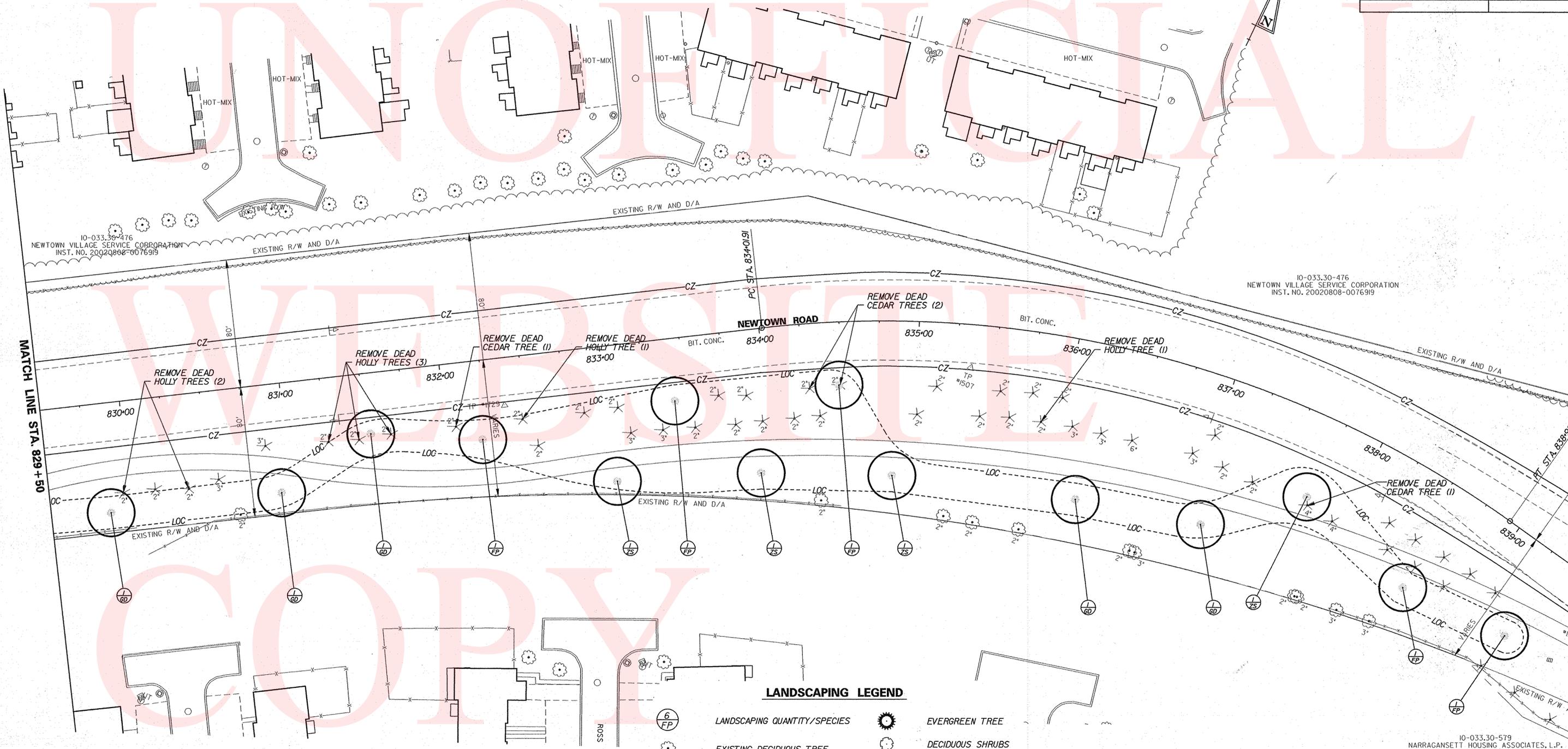
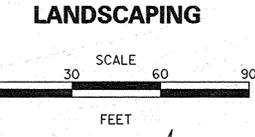
PREPARED BY  
THE CONSULTING FIRM OF  
WHITMAN, REQUARDT & ASSOCIATES, LLP

MICHAEL J. CAMPBELL, RLA  
RECOMMENDED **JAN 7** 20 **11**

PREL. TRACING    SUB    DESIGN    MJC    CHKD.    JRR

MATCH LINE STA. 829+50

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	88	115
<b>SR-7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				



PREL. TRACING  
 SUB  
 DESIGN  
 MJC  
 CHKD.  
 JRR

**PREPARED BY**  
 THE CONSULTING FIRM OF  
**WHITMAN, REQUARDT & ASSOCIATES, LLP**

*Michael J. Campbell*  
 MICHAEL J. CAMPBELL, RLA  
 RECOMMENDED **JAN 7** 20 **11**

LANDSCAPING		
KEY	QUANTITY	BOTANICAL NAME
GD	5	GYMNOCALADUS DIOICUS
FP	5	FRAXINUS PENNSYLVANICA 'PATMORE'
ZS	4	ZELKOVA SERRATA 'GREEN VASE'
	31 SY.	SHREDDED HARDWOOD BARK MULCH (LANDSCAPE TREE)

**LANDSCAPING LEGEND**

	LANDSCAPING QUANTITY/SPECIES		EVERGREEN TREE
	EXISTING DECIDUOUS TREE		DECIDUOUS SHRUBS
	EXISTING CONIFEROUS TREE		PATTERNED CONCRETE
	SHADE TREE		DO NOT DISTURB
	MEDIAN TREE		EXISTING TREE LINE
			PROPOSED TREE LINE
		TYP. U.N.	TYPICAL UNLESS NOTED

**NOTE:**  
 WORK WITHIN THE LOC ALONG NEWTOWN ROAD IS LIMITED TO TREE PLANTING AND TREE REPLACEMENT ONLY. DISTURBANCE SHALL BE LIMITED TO EXCAVATION REQUIRED FOR THE REMOVAL OF TREES IDENTIFIED TO BE REMOVED AND THE PROPOSED PLANTING PITS ONLY.

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ANY DISTURBANCE TO THE EXISTING LAWN INCURRED DURING PLANTING OPERATIONS AND VEHICULAR ACCESS REQUIRED FOR PLANTING SHALL BE RESTORED TO PRE-EXISTING CONDITIONS AND IS INCIDENTAL TO ITEM 737523 - PLANTING.

**TOTAL LANDSCAPE PLANTING SCHEDULE**

KEY	QUANTITY	BOTANICAL NAME	COMMON NAME	PLANTING SIZE	METHOD	SPACING	MATURE SIZE / COMMENTS
<b>REFORESTATION TREES</b>							
AR	9	ACER RUBRUM	RED MAPLE	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
LS	9	LIQUIDAMBAR STYRACIFLUA	AMERICAN SWEETGUM	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
LT	9	LIRIODENDRON TULIPIFERA	TULIPTREE	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
NS	9	NYSSA SYLVATICA	BLACKGUM	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
PO	9	PLATANUS OCCIDENTALIS	AMERICAN SYCAMORE	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
QP	9	QUERCUS PALUSTRIS	PIN OAK	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
QR	9	QUERCUS RUBRA	RED OAK	#7 CONT. (6' - 8' HEIGHT)	CONT.	VARIES 10' - 15' O.C.	RANDOMLY PLANT IN GROUPS OF 3
<b>LANDSCAPE TREES</b>							
AB	5	ACER BUERGERANUM	TRIDENT MAPLE	2' - 2 1/2' CAL.	B&B	PLANT 30' MIN. APART	30' - 40' HEIGHT 25' SPREAD
AG	12	ACER GINNALA 'FLAME'	FLAME AMUR MAPLE	2' - 2 1/2' CAL.	B&B	PLANT 30' MIN. APART	15' - 20' HEIGHT 15' - 20' SPREAD
CC	10	CARPINUS CAROLINIANA	AMERICAN HORNBEE	3' - 3 1/2' CAL. (MINIMUM 16' HEIGHT)	B&B	PLANT 30' MIN. APART	45' HEIGHT 35' SPREAD
CE	6	CCERCIS CANADENSIS	EASTERN REDBUD	2 1/2' CAL.	B&B	PLANT 15' MIN. APART	45' HEIGHT 35' SPREAD
FP	21	FRAXINUS PENNSYLVANICA 'PATMORE'	PATMORE GREEN ASH	3' - 3 1/2' CAL. (MINIMUM 16' HEIGHT)	B&B	PLANT 30' MIN. APART	45' HEIGHT 35' SPREAD
GD	11	GYMNOCLADUS DIOICUS	KENTUCKY COFFEETREE	3' - 3 1/2' CAL. (MINIMUM 16' HEIGHT)	B&B	PLANT 30' MIN. APART	55' HEIGHT 45' SPREAD
IO	43	ILEX OPACA	AMERICAN HOLLY	8' - 10' HT.	B&B	PLANT 30' MIN. APART	55' HEIGHT 45' SPREAD
PF	22	PINUS FLEXIUS	EASTERN RED CEDAR	8' - 10' HT	B&B	PLANT 10' MIN. APART	30 - 35' HEIGHT 15' - 25' SPREAD
PP	15	PICEA PUNGENS	EASTERN RED CEDAR	8' - 10' HT.	B&B	PLANT 10' MIN. APART	30 - 35' HEIGHT 15' - 25' SPREAD
JV	35	JUNIPERUS VIRGINIANA	EASTERN RED CEDAR	2' - 2 1/2' CAL.	B&B	PLANT 10' MIN. APART	30 - 35' HEIGHT 15' - 25' SPREAD
PA	6	PLATANUS ACERIFOLIA	LONDON PLANE	3' - 3 1/2' CAL.	B&B	PLANT 30' MIN. APART	30 - 35' HEIGHT 15' - 25' SPREAD
SR	19	SYRINGA RETICULATA 'IVORY SILK'	IVORY SILK LILAC	2' - 2 1/2' CAL.	B&B	PLANT 26' MIN. APART	20' - 25' HEIGHT 15' - 20' SPREAD
ZS	27	ZELKOVA SERRATA 'GREEN VASE'	GREEN VASE JAPANESE ZELKOVA	3' - 3 1/2' CAL. (MINIMUM 16' HEIGHT)	B&B	PLANT 30' MIN. APART	50' HEIGHT 40' SPREAD
<b>SHRUBS/PERENNIALS</b>							
IS	6	ILEX VERTICILLATA 'SOUTHERN GENTLEMAN'	SOUTHERN GENTLEMAN WINTERBERRY	3' - 4' HEIGHT	B&B	PLANT 8' APART	6' - 8' HEIGHT 8' SPREAD
IW	30	ILEX VERTICILLATA 'WINTER RED'	WINTER RED WINTERBERRY	3' - 4' HEIGHT	B&B	PLANT 8' APART	6' - 8' HEIGHT 8' SPREAD
LM	67	LIRIOPE MUSCARI 'VARIEGATA'	VARIATED LILYTURF	#2	CONTAINER	PLANT 5' APART	6' - 8' HEIGHT 8' SPREAD
PL	14	PRUNUS LAUROCERASUS 'OTTO LUYKENS'	CHERRY LAUREL	2' - 3' HEIGHT	B&B	PLANT 5' APART	6' - 8' HEIGHT 8' SPREAD

**PLANTING DETAIL NOTES:**

- ALL PRUNING SHALL BE DONE BY OR UNDER THE DIRECTION OF AN I.S.A. CERTIFIED ARBORIST OR CERTIFIED NURSERY PROFESSIONAL. DO NOT HEAVILY PRUNE TREES OR SHRUBS AT PLANTING.
- ALL DEAD, BROKEN AND CROSSING BRANCHES SHALL BE PRUNED OFF FOLLOWING INSTALLATION.
- THE BASE OF THE PLANTING PIT SHALL BE A MINIMUM WIDTH OF TWICE THE ROOT BALL SIZE AND A MAXIMUM OF THREE TIMES THE ROOT BALL SIZE.
- WHEN PLANTING TREES ALONG STREETS, THERE MUST BE A MINIMUM OF SIX FEET (6') BETWEEN THE BACK OF CURB AND THE EDGE OF SIDEWALK AND SHALL BE CENTERED BETWEEN THE BACK OF CURB AND THE EDGE OF SIDEWALK.
- WHEN PLANTING TREES ALONG SIDEWALKS, THE TREES SHALL BE LIMBED TO SEVEN FEET (7') FOR PEDESTRIAN CLEARANCE.
- SHRUBS SHALL BE INSTALLED IN MASSES OF NO LESS THAN THREE (3) PLANTS. A MINIMUM OF SIX FEET (6') WIDTH IS REQUIRED FROM THE BACK OF CURB TO THE EDGE OF SIDEWALK FOR INSTALLATION OF SHRUBS.
- ALL SHRUB MASSES SHALL BE MULCHED AS ONE CONTINUOUS BED.
- AUGERED HOLES SHALL BE HAND DUG TO FINAL WIDTH AND TO ELIMINATE GLAZING.

**MULCH REQUIREMENTS**

4 INCH THICK MULCH REQUIRED FOR MEDIAN AREAS:	1,443 S.Y.
4 INCH THICK MULCH REQUIRED FOR REFORESTATION TREES:	68 S.Y.
4 INCH THICK MULCH REQUIRED FOR LANDSCAPE TREES:	216 S.Y.
<b>TOTAL MULCH REQUIRED:</b>	<b>1,727 S.Y.</b>

**MEDIAN PLANTING SOIL REQUIREMENTS**

MEDIAN PLANTING SOIL TO 30" DEPTH 1180 CY

**REFORESTATION CALCULATIONS**

TOTAL NUMBER OF SHADE TREES TO BE REMOVED WITHIN THE LIMITS OF CONSTRUCTION	40 EA.
TOTAL NUMBER OF SHADE TREES TO BE PLANTED WITHIN THE PROJECT LIMITS	40 EA.
NON-SHADE AND FOREST TREES TO BE REMOVED WITHIN THE LIMITS OF CONSTRUCTION	113 EA.
TOTAL NUMBER OF NON-SHADE (MEDIAN/LANDSCAPE TREES) TO BE PLANTED WITHIN THE PROJECT LIMITS	50 EA.
NUMBER OF TREES TO BE MITIGATED ON A SQUARE FOOT BASIS (113/50)	63 EA.
SQUARE FOOTAGE OF SHADE, NON-SHADE AND FOREST TREES BASED ON A 17' CANOPY OR 225 S.F. PER TREE (82 x 225) TO BE PLANTED WITHIN THE PROJECT LIMITS	14,175 S.F. (0.33 AC)
TOTAL AREA OF REFORESTATION PROVIDED	14,175 S.F. (0.33 AC)

**LANDSCAPE DETAILS**

SCALE: NONE

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	89	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

**LANDSCAPE NOTES:**

- ALL LANDSCAPING SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SECTION 737 - PLANTING.
- THE CONTRACTOR SHALL VERIFY PLANT MATERIAL QUANTITIES SHOWN ON THE PLANS WITH THE TOTALS IN THE PLANT SCHEDULE. WHERE DISCREPANCIES OCCUR BETWEEN THE PLANS AND THE PLANT SCHEDULE, THE PLANS SHALL GOVERN.
- THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING MISS UTILITY TO LOCATE ALL UNDERGROUND UTILITY LINES PRIOR TO INSTALLATION OF PLANT MATERIAL. THE CONTRACTOR SHALL ALSO BE COGNIZANT OF PROPOSED UTILITY LOCATIONS AS SHOWN ON THE PLANS.
- ALL PLANTING OPERATIONS SHALL BE UNDER THE SUPERVISION OF A CERTIFIED LANDSCAPE PROFESSIONAL AS CERTIFIED BY A NATIONAL, STATE OR OTHER PROFESSIONALLY RECOGNIZED ORGANIZATION.
- EACH TREE AND SHRUB PLANTING AREA SHALL BE SPREAD WITH 3 INCH DEEP SHREDDED HARDWOOD BARK MULCH.
- ALL 16 FOOT HEIGHT SHADE TREES SHALL HAVE A 5 FOOT DIAMETER MULCH SAUCER. ALL REFORESTATION TREES SHALL HAVE A 3.5 FOOT DIAMETER MULCH SAUCER.
- NO PLANT MATERIAL SHALL BE PLANTED DIRECTLY IN FRONT OF ROADWAY SIGN SIGHT LINES.
- ALL TREES MUST BE PLANTED BEHIND THE CLEAR ZONE (C2) LABELED ON THE DRAWINGS.
- ALL SHADE TREES SHALL HAVE STRAIGHT TRUNKS, A SINGLE LEADER AND SHALL BE FREE OF BRANCHING A MINIMUM 7 FOOT HEIGHT ABOVE GRADE.
- ALL ZELKOVA SPECIES MUST BE PLANTED BETWEEN MARCH 1ST AND MAY 1ST.
- ALL EXISTING TREES TO REMAIN THAT ARE DISTURBED DURING CONSTRUCTION SHALL HAVE THE DAMAGE CORRECTED BY AN ISA CERTIFIED ARBORIST AT THE EXPENSE OF THE CONTRACTOR WHO WAS RESPONSIBLE FOR THE DAMAGE.
- THE CONTRACTOR SHALL BE REQUIRED TO WATER ALL MAJOR AND MINOR TREES, SHRUBS AND ALL HERBACEOUS BEDS BI-WEEKLY DURING THE PERIOD FROM JUNE 15 THROUGH OCTOBER 1. WATERING, ONCE INITIATED, SHALL CONTINUE WITHOUT INTERRUPTION UNTIL ALL PLANTS ON THE PROJECT HAVE BEEN WATERED. PAYMENT SHALL BE PER 1000 GALLONS OF WATER APPLIED AND SHALL BE BASED ON THE FOLLOWING SCHEDULE: MAJOR TREES- 15 GALLONS PER TREE, MINOR TREES- 10 GALLONS PER TREE, SHRUBS- 5 GALLONS PER SHRUB, PERENNIALS- 10 GALLONS PER 100 SQUARE FEET OF PLANTING BED. WATER USED FOR THIS ITEM SHALL MEET THE REQUIREMENTS OF SECTION 803 OF THE STANDARD SPECIFICATIONS. TREE WATERING BAGS, IF UTILIZED, SHALL BE FILLED AS PART OF THE WATERING OPERATION; PAYMENT SHALL BE DETAILED HEREIN. TREE WATERING BAGS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED PRIOR TO FINAL INSPECTION.

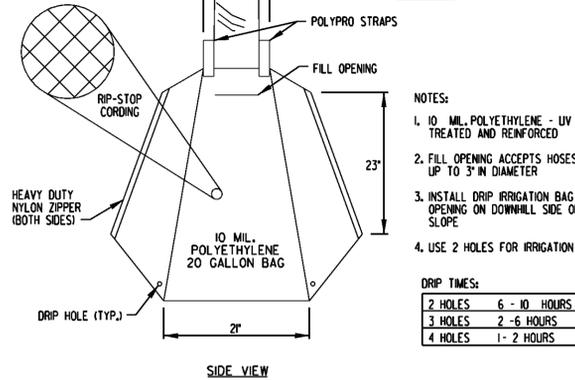
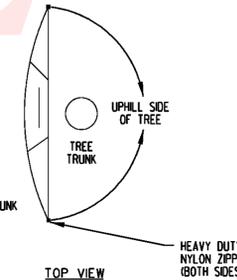
**REFORESTATION REQUIREMENTS**

ON JANUARY 10, 2005, WHITMAN, REQUARDT AND ASSOCIATES, LLP CONDUCTED A SITE INVESTIGATION TO INVENTORY THE EXISTING SHADE TREES, INDIVIDUAL TREES, AND THE EXISTING WOODLAND AREAS WITHIN THE PROJECT LIMITS.

ON MARCH 21, 2005, DELDOT OFFICIALS AND WR&A PERSONNEL ALSO CONDUCTED A SITE INVESTIGATION TO INVENTORY THE EXISTING SHADE TREES, INDIVIDUAL TREES AND THE EXISTING WOODLAND AREAS WITHIN THE PROJECT LIMITS.

BASED ON THESE TWO SITE INVESTIGATIONS AND IN ACCORDANCE WITH THE DELDOT LANDSCAPING AND REFORESTATION ACT, THE FOLLOWING REFORESTATION REQUIREMENTS WILL HAVE TO BE IMPLEMENTED AS PART OF THIS PROJECT DUE TO THE REMOVAL OF TREES WITHIN THE LIMITS OF CONSTRUCTION AS SHOWN ON THE PLANS:

RANDOMLY PLANT REFORESTATION TREES IN GROUPS OF 3 OF THE SAME SPECIES. PLANT SPACING SHALL BE RANDOM AND VARY BETWEEN 10-15' TO MIMIC A NATURAL FOREST HABITAT. REFORESTATION PLANTINGS SHALL BE PLANTED ACCORDING TO DELDOT STANDARD DETAILS AND SPECIFICATIONS.

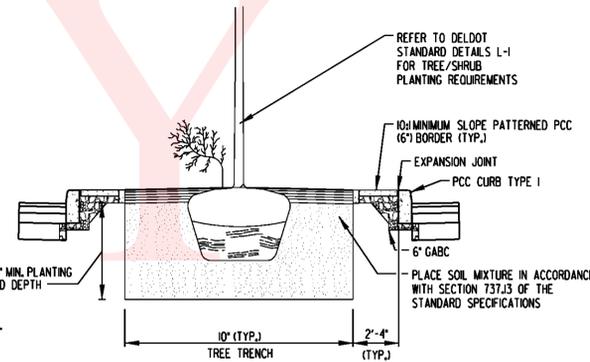


- NOTES:**
- 10 MIL. POLYETHYLENE - UV TREATED AND REINFORCED
  - FILL OPENING ACCEPTS HOSES UP TO 3" IN DIAMETER
  - INSTALL DRIP IRRIGATION BAG OPENING ON DOWNHILL SIDE OF SLOPE
  - USE 2 HOLES FOR IRRIGATION
- DRIP TIMES:**
- |         |              |
|---------|--------------|
| 2 HOLES | 6 - 10 HOURS |
| 3 HOLES | 2 - 6 HOURS  |
| 4 HOLES | 1 - 2 HOURS  |

NOTE: DRIP IRRIGATION DEVICE IS INCIDENTAL TO ITEM 737523-PLANTING.

**PORTABLE DRIP IRRIGATION BAG DETAIL**

NO SCALE

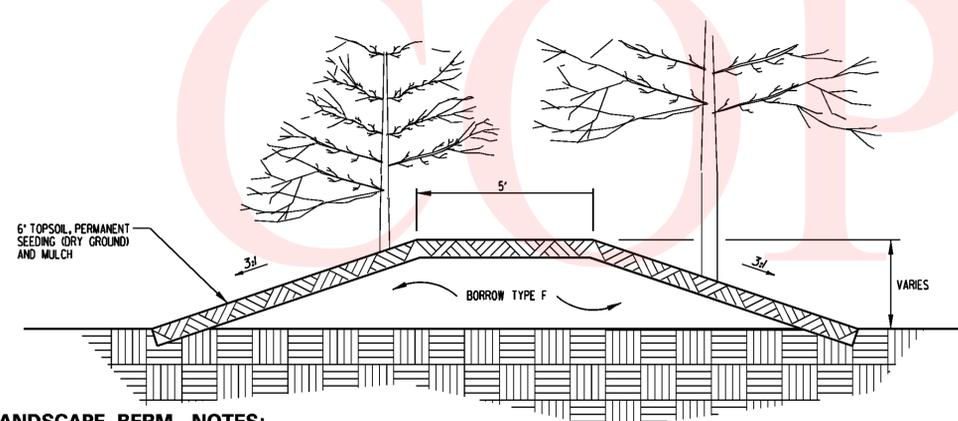


NOTES: SOIL MIXTURE IS INCIDENTAL TO ITEM 737523 - PLANTING.

SOIL MIXTURE IS ONLY TO BE PLACED IN THE ENTIRE MEDIAN AREAS BELOW THE MULCH. SOIL MIXTURE IS NOT TO BE PLACED UNDER THE PCC BORDER.

**MEDIAN SOIL MIXTURE DETAIL**

NO SCALE



**LANDSCAPE BERM NOTES:**

- THE LANDSCAPE BERM IS TO BE CONSTRUCTED ACCORDING TO DELDOT STANDARD SPECIFICATIONS SECTION 202 EXCAVATION AND EMBANKMENT. ALL FILL MATERIAL IS TO BE FREE OF CONSTRUCTION MATERIALS AND DEBRIS. THE BERM SHALL BE LIGHTLY COMPACTED AND SLOPES ARE TO BE NO STEEPER THAN 3:1 UNLESS OTHERWISE NOTED ON THE LANDSCAPE PLAN.
- EXISTING GROUND SHALL BE CLEARED OF ALL PAVEMENT MATERIAL, DEBRIS, STUMPS, AND TURF. SCARIFY THE SURFACE TO A DEPTH OF 6" PRIOR TO PLACEMENT OF FILL MATERIAL.

**LANDSCAPE BERM DETAIL**

NO SCALE  
STA. 98+65.33 TO STA. 101+24.58

PREL. TRACING SUB DESIGN MUC CHKD. JRR



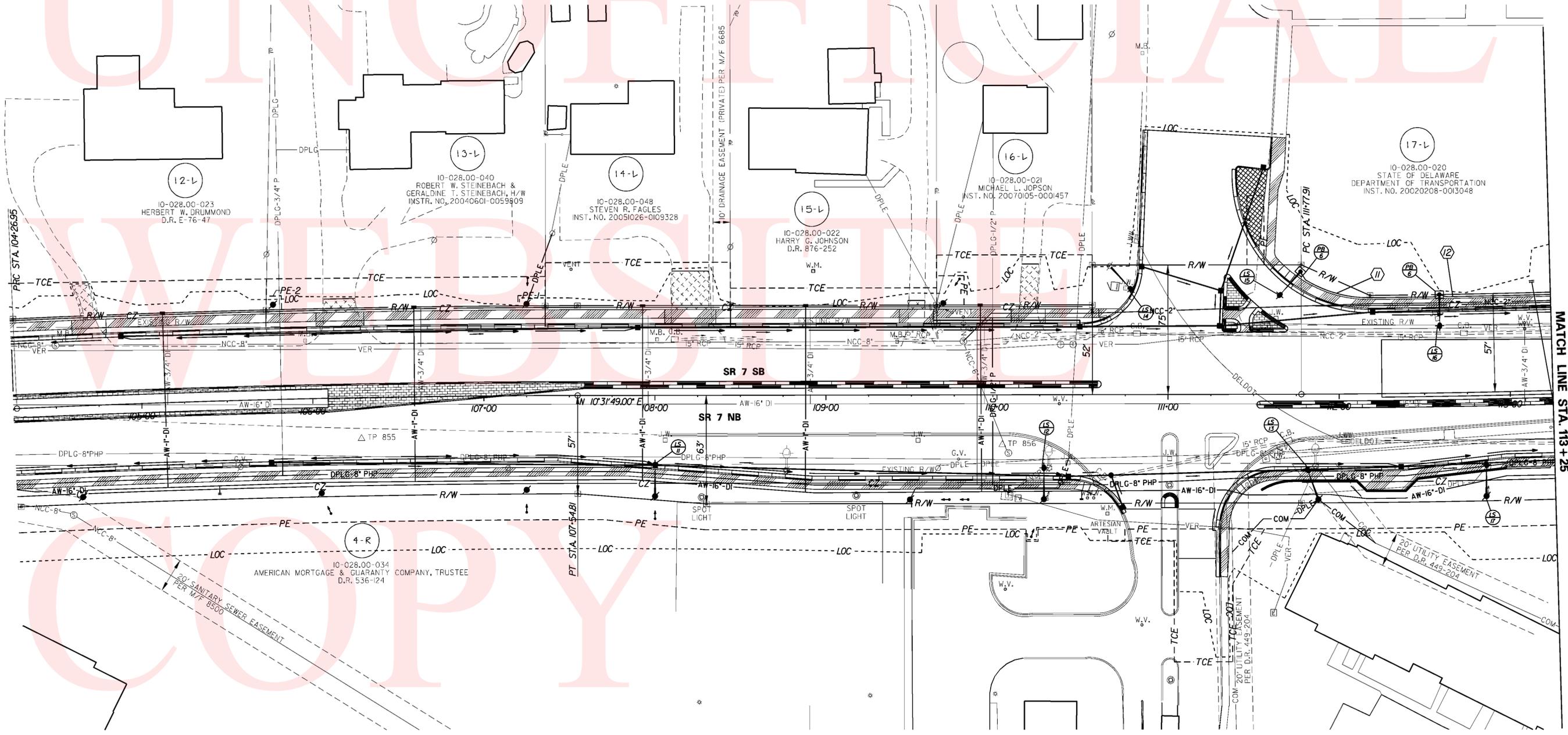
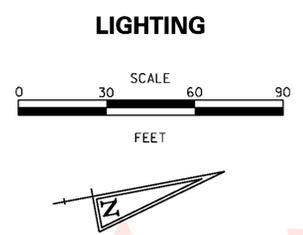
CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	91	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

NO.	CKT. NO.	STATION	OFFSET	ARM LENGTH	LIGHT STANDARD	TRANSFORMER BASE
11	-	108+00	58.4' RT.	12'	SEE NOTE 6	-
12	-	110+27	61.0' RT.	12'	SEE NOTE 6	-
13	-	111+88	61.0' RT.	12'	SEE NOTE 6	-
14	-	110+66	74.0' LT.	20'	SEE NOTE 6	-
15	-	111+78	71.0' LT.	20'	SEE NOTE 5	BREAKAWAY
16	-	112+59	57.0' LT.	20'	SEE NOTE 5	BREAKAWAY
17	-	112+85	61.2' RT.	12'	SEE NOTE 6	-

SERVICE RUN	# OF CONDUITS	SIZE	LENGTH L.F.	AMOUNT AND TYPE OF CABLE/WIRE	INSTALLATION
11	1	2.5"	86	(2) 1/2" * 6 AWG. (1) 1/2" * 6 GROUND	TRENCH
12	1	2.5"	122	(4) 1/2" * 6 AWG. (1) 1/2" * 6 GROUND	TRENCH

NOTE: SERVICE RUN DISTANCE REFLECTS CONDUIT LENGTHS ONLY.

- NOTES:**
- ALL LIGHTING STANDARD POLE BASES SHALL BE TYPE 6.
  - ALL LIGHTING JUNCTION WELLS SHALL BE TYPE I.
  - LIGHTING CIRCUITS ARE AS NOTED IN LIGHTING SERVICE SCHEDULE.
  - LOCATION OF CONDUIT MAY BE ADJUSTED IN FIELD TO AVOID EXISTING OR OTHER PROPOSED CONSTRUCTION FEATURES, SUBJECT TO APPROVAL OF THE ENGINEER. ALL CONDUITS AND POLES SHALL BE LOCATED WITHIN EXISTING PROPOSED RIGHT-OF-WAY OR PERMANENT EASEMENT.
  - SEE SHEET 90, ROADWAY LUMINAIRE AND POLE DETAIL.
  - 250 W HPS ROADWAY COBRA HEAD LUMINAIRE WITH CUT-OFF TYPE III MOUNTED ON UTILITY POLE WITH 12' ARM LENGTH. ALL LUMINAIRES ATTACHED TO UTILITY POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.



PREL. TRACING    SUB    DESIGN    KGM-A    CHKD.    JRR

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	92	115

<b>SR 7, NEWTOWN ROAD TO SR 273</b>	
REVISIONS	

LIGHTING STANDARD SCHEDULE						
NO.	CKT. NO.	STATION	OFFSET	ARM LENGTH	LIGHT STANDARD	TRANSFORMER BASE
18	-	114+86	59.0' RT.	12'	SEE NOTE 6	-
19	-	116+54	64.0' RT.	12'	SEE NOTE 6	-
20	-	117+27	66.0' RT.	12'	SEE NOTE 6	-
21	-	119+13	49.0' RT.	12'	SEE NOTE 6	-
22	-	121+24	53.0' RT.	12'	SEE NOTE 6	-
23	-	113+81	61.0' LT.	20'	SEE NOTE 5	BREAKAWAY
24	-	115+16	69.0' LT.	12'	SEE NOTE 6	-
25	-	118+12	59.0' LT.	20'	SEE NOTE 5	BREAKAWAY
26	-	119+28	60.0' LT.	12'	SEE NOTE 6	-

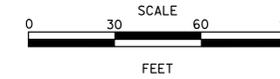
LIGHTING SERVICE SCHEDULE					
SERVICE RUN	# OF CONDUITS	SIZE	LENGTH L.F.	AMOUNT AND TYPE OF CABLE/WIRE	INSTALLATION
13	1	2.5"	183	(41/2# AWG, III/1# 6 GROUND)	BORE/TRENCH
14	1	2.5"	257	(21/2# AWG, III/1# 6 GROUND)	TRENCH
15	2	2.5"	10	(41/2# AWG, III/1# 6 GROUND)	TRENCH
16	1	2.0"	9	(11/2# 8 UF.W./GROUND)	TRENCH
17	1	2.0"	28	(11/2# 8 UF.W./GROUND)	TRENCH

NOTE: SERVICE RUN DISTANCE REFLECTS CONDUIT LENGTHS ONLY.

**NOTES:**

- ALL LIGHTING STANDARD POLE BASES SHALL BE TYPE 6.
- ALL LIGHTING JUNCTION WELLS SHALL BE TYPE 1.
- LIGHTING CIRCUITS ARE AS NOTED IN LIGHTING SERVICE SCHEDULE.
- LOCATION OF CONDUIT MAY BE ADJUSTED IN FIELD TO AVOID EXISTING OR OTHER PROPOSED CONSTRUCTION FEATURES, SUBJECT TO APPROVAL OF THE ENGINEER. ALL CONDUITS AND POLES SHALL BE LOCATED WITHIN EXISTING PROPOSED RIGHT-OF-WAY OR PERMANENT EASEMENT.
- SEE SHEET 90, ROADWAY LUMINAIRE AND POLE DETAIL AND SERVICE REQUIREMENTS.
- 250 W HPS ROADWAY COBRA HEAD LUMINAIRE WITH CUT-OFF TYPE III MOUNTED ON UTILITY POLE WITH 12' ARM LENGTH. ALL LUMINAIRES ATTACHED TO UTILITY POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.

**LIGHTING**



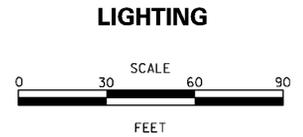
PREL. TRACING  
 S.B.  
 DESIGN  
 KGM-A  
 CHKD.  
 JRR

LIGHTING STANDARD SCHEDULE						
NO.	CKT. NO.	STATION	OFFSET	ARM LENGTH	LIGHT STANDARD	TRANSFORMER BASE
27	-	122-70	55' RT.	12'	SEE NOTE 6	-
28	-	124-51	55' RT.	12'	SEE NOTE 6	-
29	-	129-36	30' LT.	20'	SEE NOTE 5	BREAKAWAY

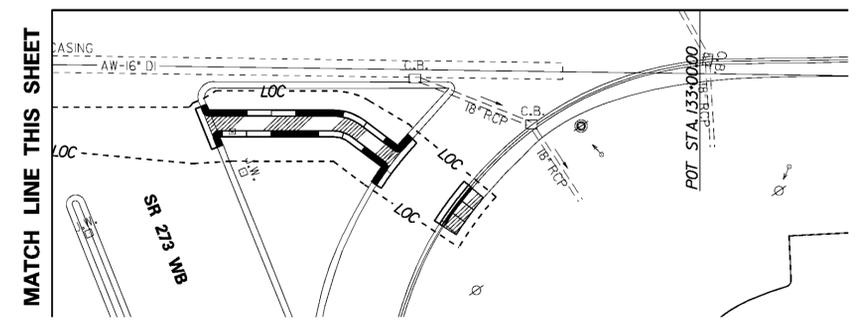
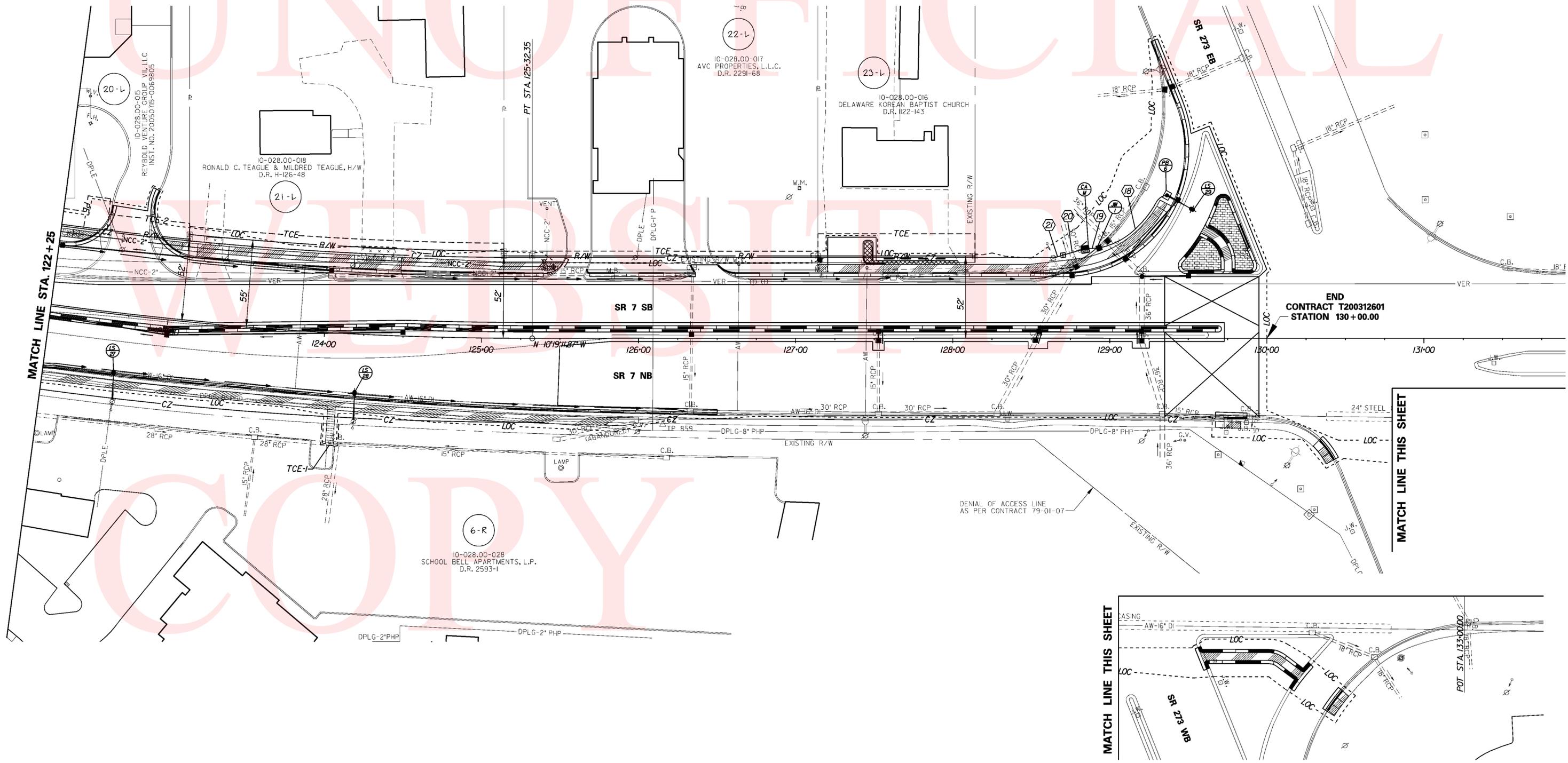
LIGHTING SERVICE SCHEDULE					
SERVICE RUN	# OF CONDUITS	SIZE	LENGTH L.F.	AMOUNT AND TYPE OF CABLE/WIRE	INSTALLATION
18	1	2.5"	57'	(2)1/2" #6 AWG, (1)1/2" #6 GROUND	TRENCH
19	1	2.5"	7'	(2)1/2" #6 AWG, (1)1/2" #6 GROUND	TRENCH
20	1	2.0"	12'	(1)1/2" #8 U.F.W./GROUND	TRENCH
21	1	2.0"	7'	(1)1/2" #8 U.F.W./GROUND	TRENCH

NOTE: SERVICE RUN DISTANCE REFLECTS CONDUIT LENGTHS ONLY.

- NOTES:**
- ALL LIGHTING STANDARD POLE BASES SHALL BE TYPE 6.
  - ALL LIGHTING JUNCTION WELLS SHALL BE TYPE 1.
  - LIGHTING CIRCUITS ARE AS NOTED IN LIGHTING SERVICE SCHEDULE.
  - LOCATION OF CONDUIT MAY BE ADJUSTED IN FIELD TO AVOID EXISTING OR OTHER PROPOSED CONSTRUCTION FEATURES, SUBJECT TO APPROVAL OF THE ENGINEER. ALL CONDUITS AND POLES SHALL BE LOCATED WITHIN EXISTING PROPOSED RIGHT-OF-WAY OR PERMANENT EASEMENT.
  - SEE SHEET 90, ROADWAY LUMINAIRE AND POLE DETAIL AND SERVICE REQUIREMENTS.
  - 250 W HPS ROADWAY COBRA HEAD LUMINAIRE WITH CUT-OFF TYPE III MOUNTED ON UTILITY POLE WITH 12' ARM LENGTH. ALL LUMINAIRES ATTACHED TO UTILITY POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.

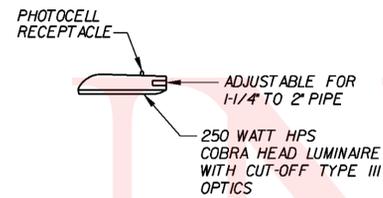


CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	93	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				



PREL. TRACING SUB DESIGN KGM-A CHKD. JRR

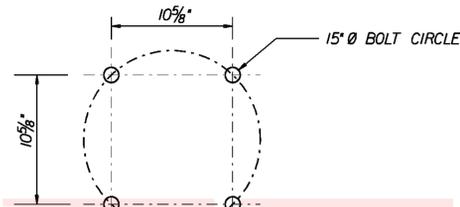
MATCH LINE THIS SHEET



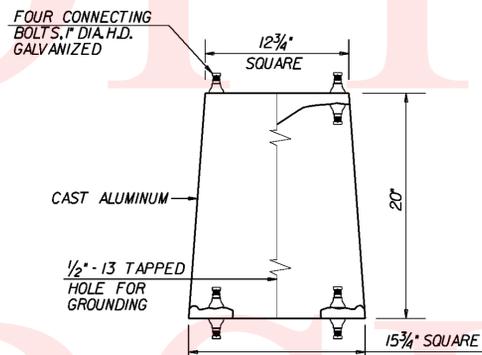
**LUMINAIRE FIXTURE**  
SCALE: NONE

**NOTES:**

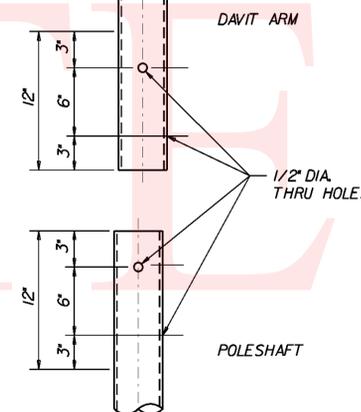
1. ALL LUMINAIRES SHALL BE 250 WATT H.P.S.IES TYPE III HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0 DEGREE TILT ANGLE.



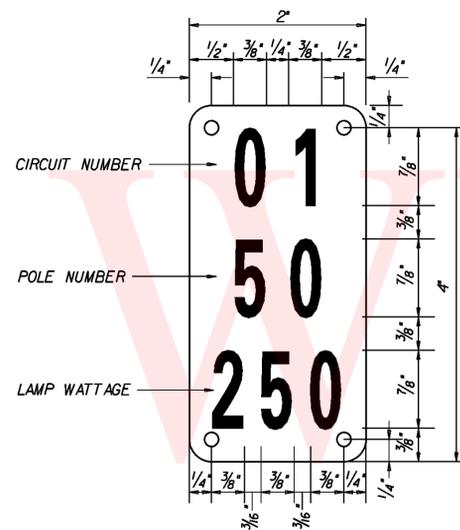
**FOUNDATION ANCHOR BOLT DETAIL**  
SCALE: NONE



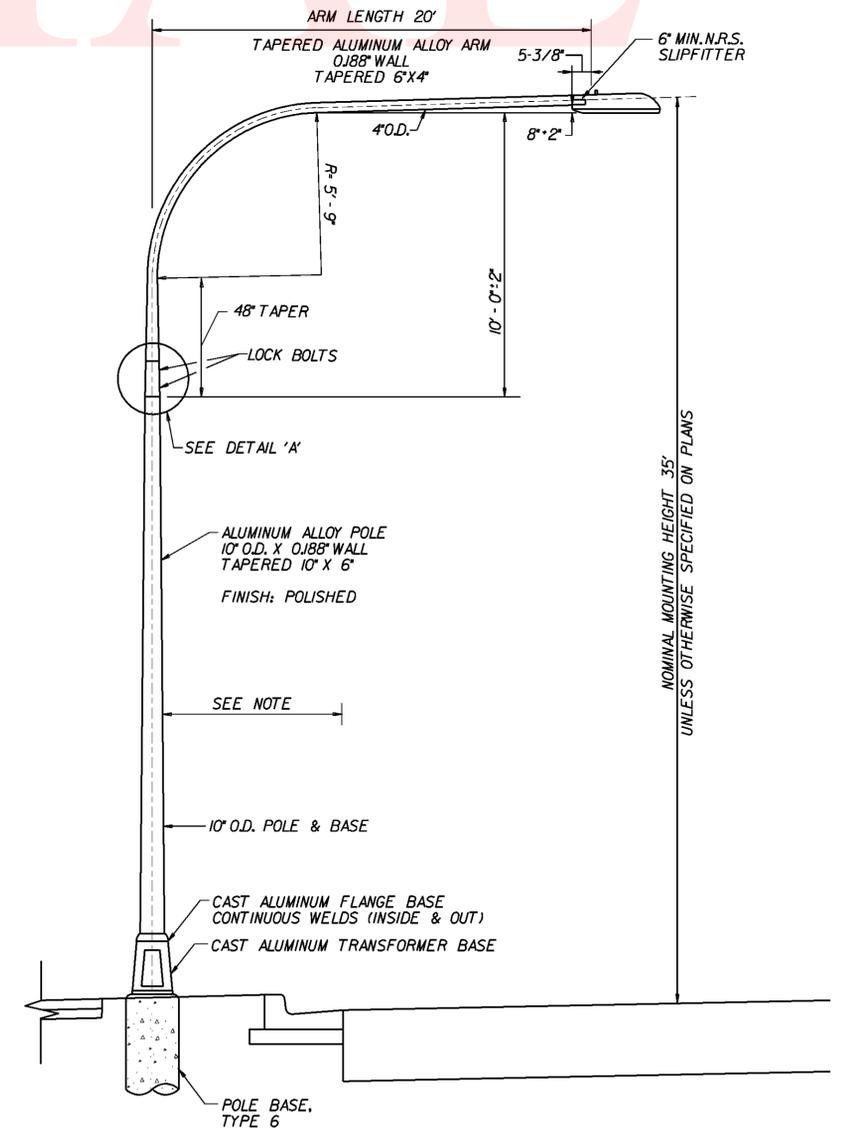
**TRANSFORMER BASE DETAIL**  
SCALE: NONE



**DETAIL 'A'**  
SCALE: NONE

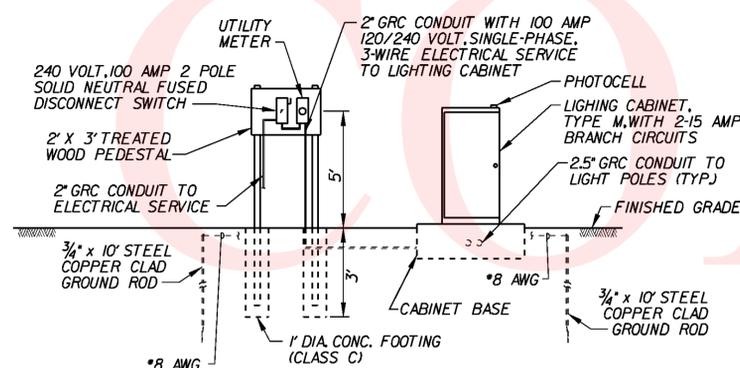


**TYPICAL POLE IDENTIFICATION TAG**  
SCALE: NONE

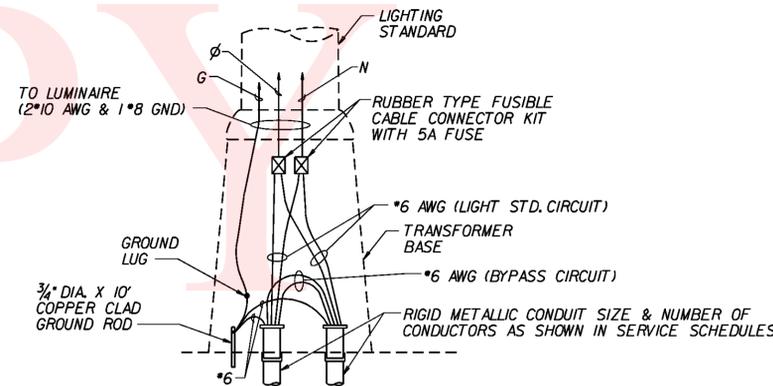


**DAVIT ARM LIGHT STANDARD**  
SCALE: NONE

NOTE: SEE PLANS FOR LOCATION OF LIGHTING STANDARD WITH RESPECT TO ROADWAY.



**METERED SERVICE PEDESTAL AND LIGHTING CABINET DETAIL**  
SCALE: NONE



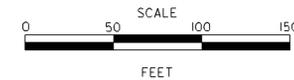
**WIRING IN TRANSFORMER BASE**  
SCALE: NONE

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T200312601	NEW CASTLE	SEE TITLE SHEET	95	115

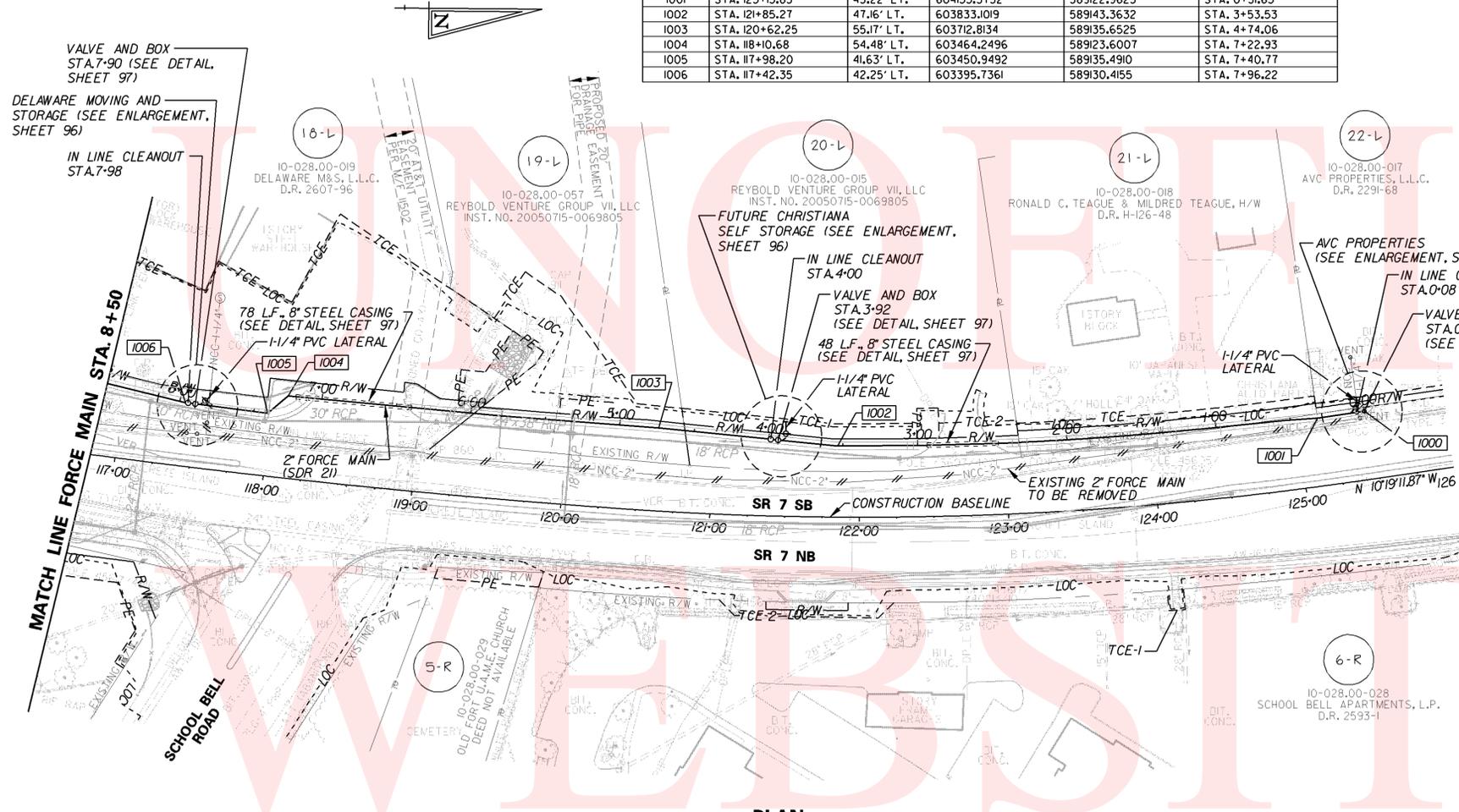
  

<b>SR 7, NEWTOWN ROAD TO SR 273</b>	
REVISIONS	

**UTILITY RELOCATION  
PLAN**



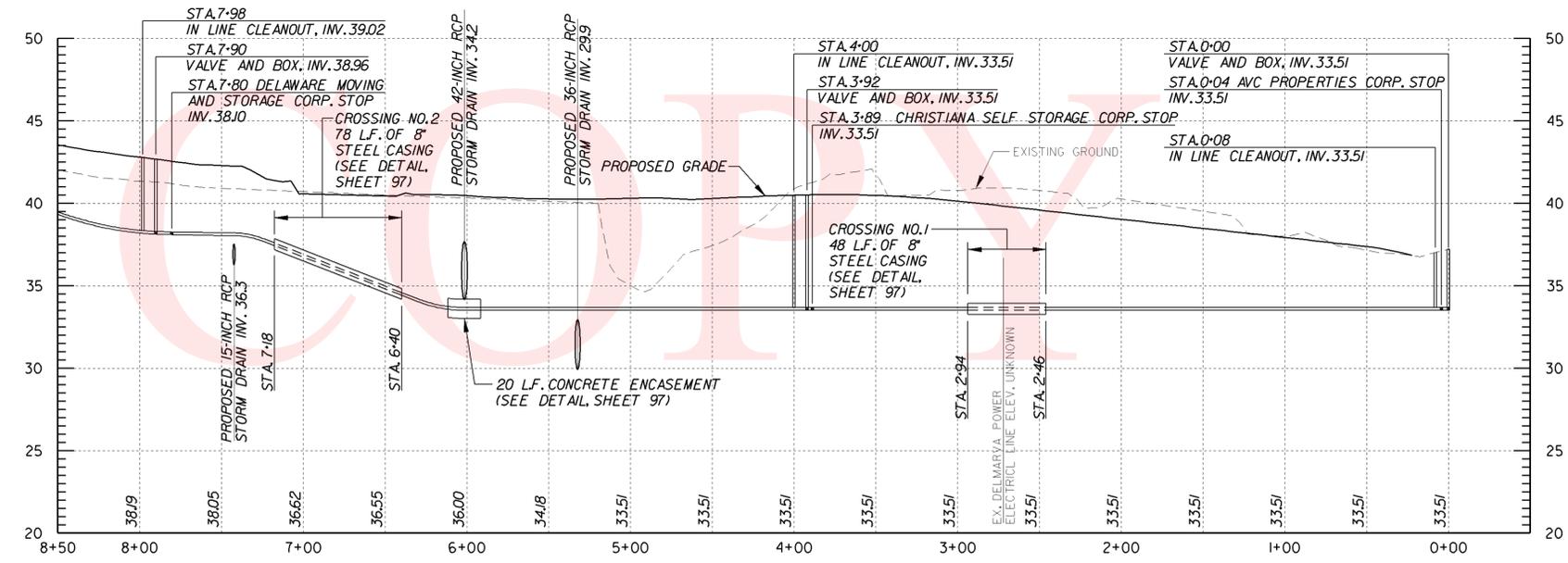
PROPOSED 2" FORCE MAIN COORDINATE SCHEDULE					
POINT NO.	BASELINE STATION	OFFSET	NORTH	EAST	= FORCE MAIN STATION
1000	STA. 125+45.85	45.34' LT.	604184.4932	589107.2492	STA. 0+00.00
1001	STA. 125+13.85	45.22' LT.	604153.3752	589122.9625	STA. 0+31.63
1002	STA. 121+85.27	47.16' LT.	603833.1019	589143.3632	STA. 3+53.53
1003	STA. 120+62.25	55.17' LT.	603712.8134	589135.6525	STA. 4+74.06
1004	STA. 118+10.68	54.48' LT.	603464.2496	589123.6007	STA. 7+22.93
1005	STA. 117+98.20	41.63' LT.	603450.9492	589135.4910	STA. 7+40.77
1006	STA. 117+42.35	42.25' LT.	603395.7361	589130.4155	STA. 7+96.22



**PLAN**  
SCALE: 1" = 50'

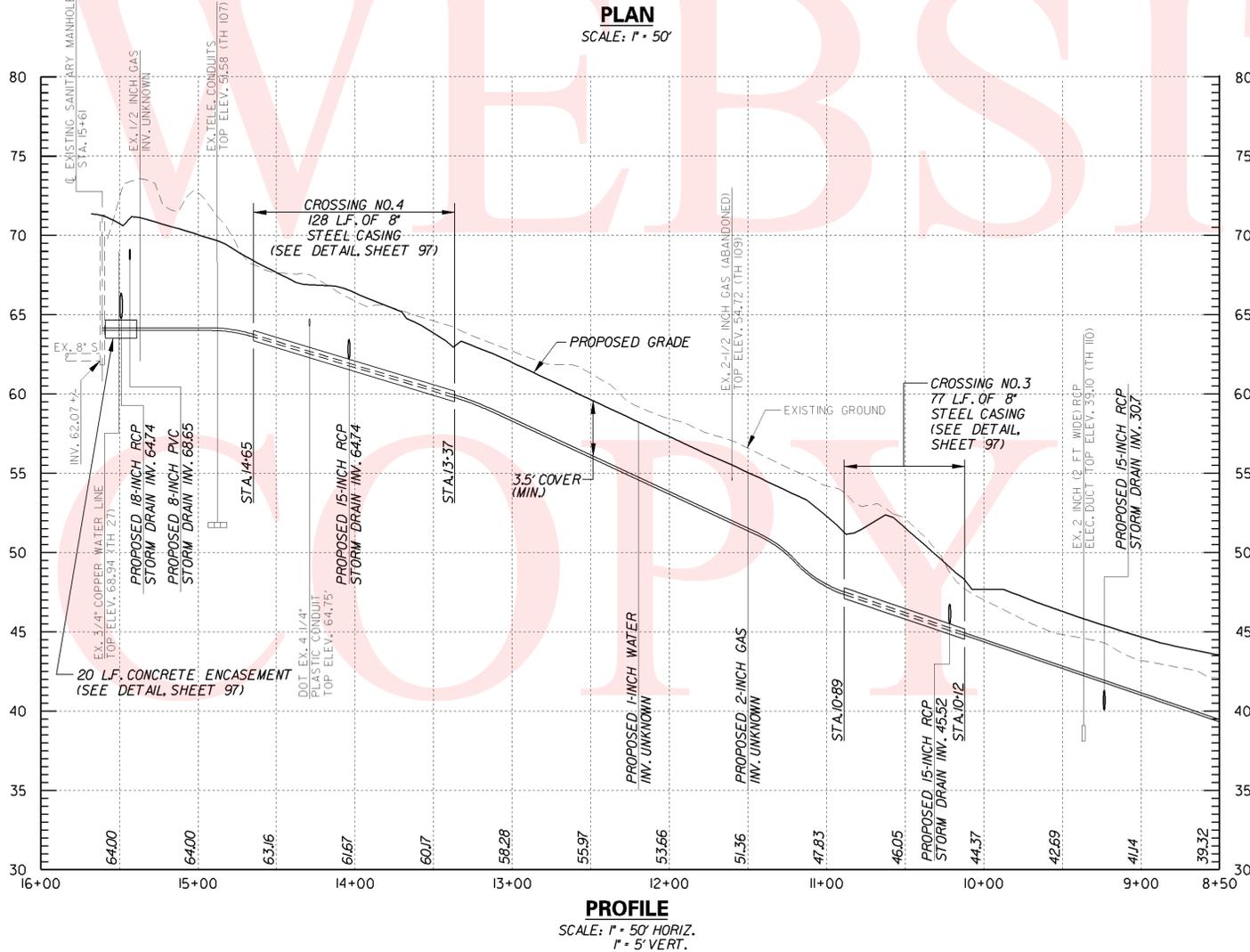
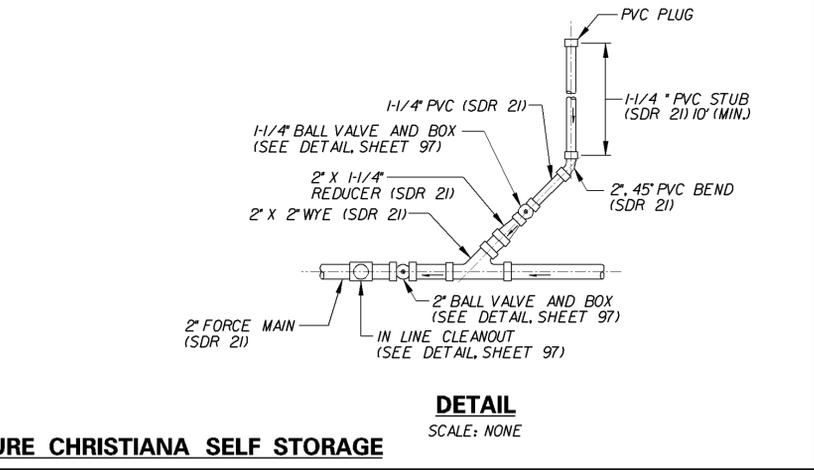
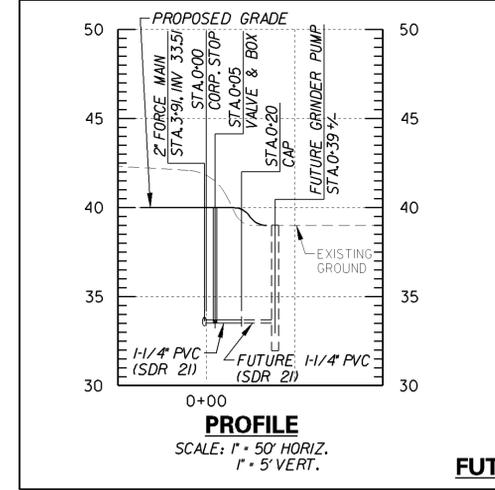
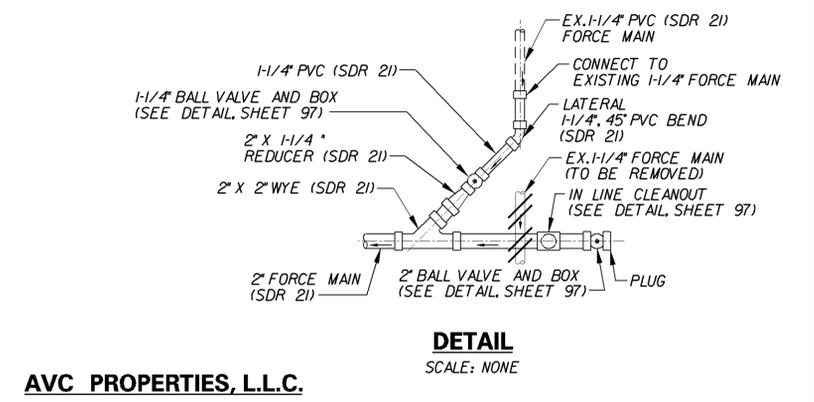
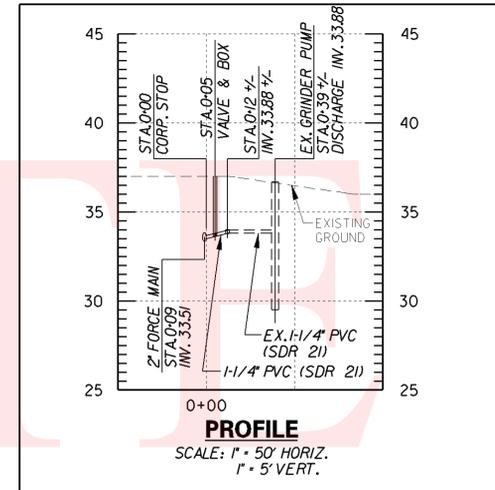
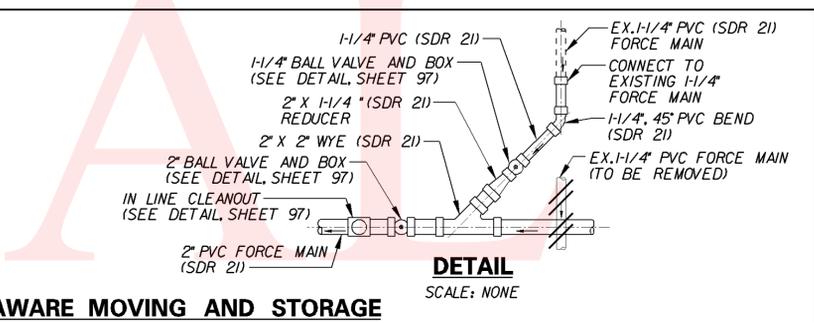
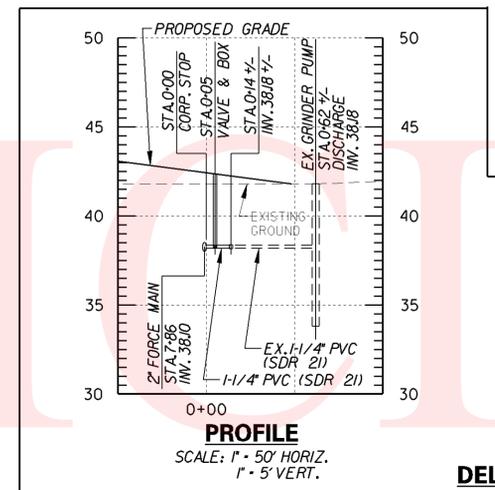
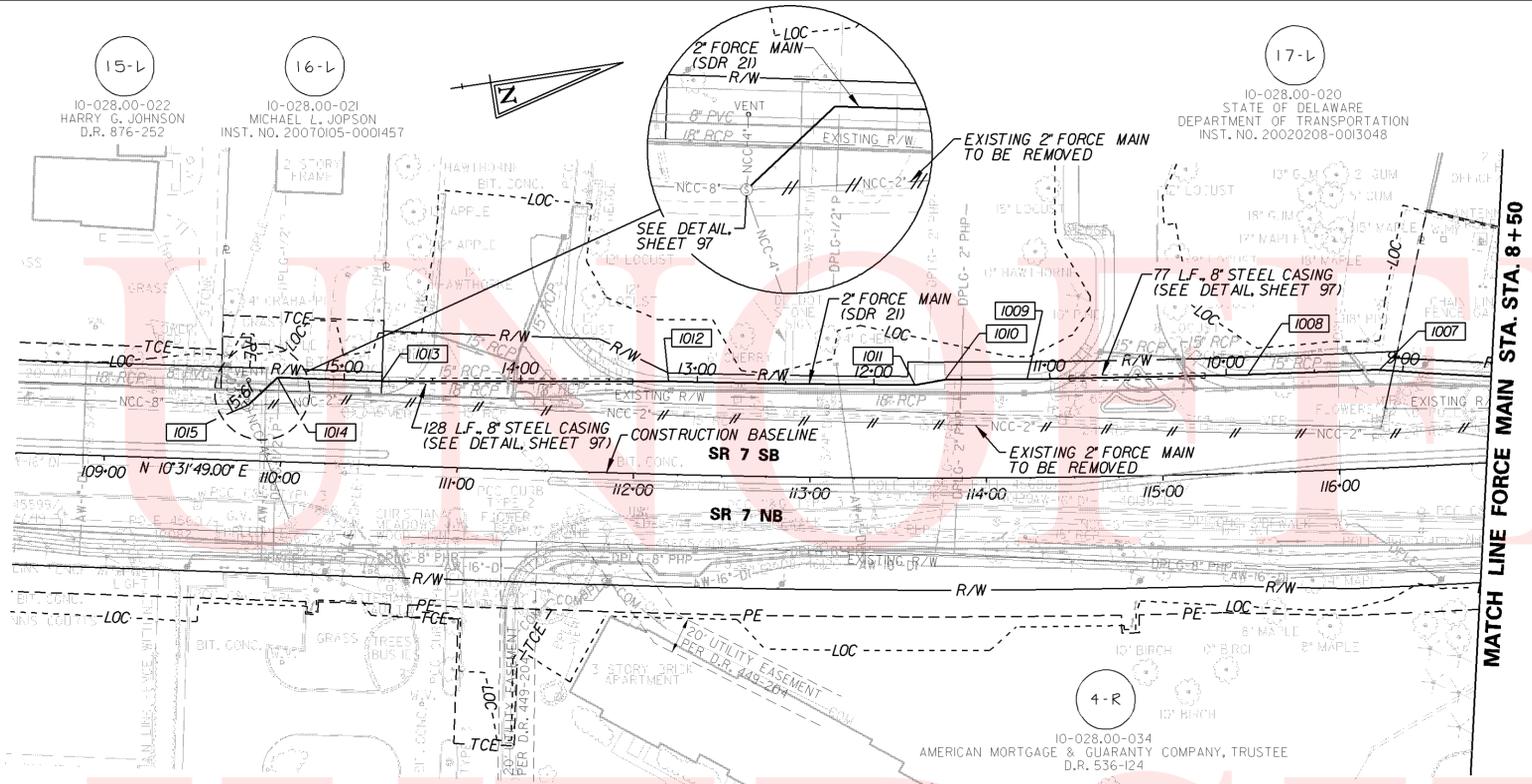
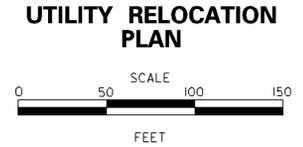
**FORCE MAIN GENERAL NOTES AND SPECIFICATIONS:**

- ALL FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS, NEW CASTLE COUNTY DEPARTMENT OF SPECIAL SERVICES STANDARD SPECIFICATIONS AND DETAILS, OSHA REQUIREMENTS AND THE DELAWARE DEPARTMENT OF TRANSPORTATION (DELDOT) STANDARD SPECIFICATIONS. ALL SPECIFICATIONS, STANDARDS AND DETAILS SHALL BE THE LATEST REVISION. IN THE EVENT OF ANY DEVIATION AMONG THE SPECIFICATIONS, THE ENGINEER'S DECISION SHALL PREVAIL.
- ALL MAINTENANCE OF TRAFFIC AND TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE CONSTRUCTION PHASING PLANS. INSTALLATION OF THE PROPOSED FORCE MAIN SHALL BE COORDINATED WITH PHASE 2 CONSTRUCTION. SEE PLAN SHEETS 59-61, 74, AND 75 FOR MORE DETAILS.
- EXISTING UTILITIES ARE LOCATED IN ACCORDANCE WITH PLANS SUPPLIED BY VARIOUS UTILITIES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY UTILITY LOCATIONS AND ALLOW FOR THEIR LOCATIONS BEFORE ANY EXCAVATION IS PERFORMED. THE CONTRACTOR SHALL CALL "MISS UTILITY" (1-800-282-8555) TO HAVE UNDERGROUND UTILITIES LOCATED AND MARKED. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT THE EXISTING UTILITIES AND MAINTAIN UNINTERRUPTED SERVICE. ANY DAMAGE DONE TO UTILITIES SHALL BE IMMEDIATELY AND COMPLETELY REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ALL CONSTRUCTION STAKEOUT SHALL BE PROVIDED BY THE CONTRACTOR. IF DISCREPANCIES EXIST BETWEEN THE CONTRACT DOCUMENTS AND THE FIELD CONDITIONS, THE CONTRACTOR SHALL ADVISE THE ENGINEER AND REQUEST A RESOLUTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY MATERIAL FOR THE PURPOSE OF EQUIPMENT ACCESS TO OR ALONG THE WORK AREA.
- IF AT ANYTIME THE WATER SUPPLY IS INTERRUPTED, THE CONTRACTOR WILL SUPPLY A TEMPORARY HOOKUP TO A POTABLE WATER SUPPLY.
- ALL FORCE MAIN ELEVATIONS SHOWN ON THESE DRAWINGS REFER TO INVERT ELEVATIONS UNLESS OTHERWISE NOTED.
- FIELD CONDITIONS MAY WARRANT THE INSTALLATION OF EROSION & SEDIMENT CONTROL MEASURES IN ADDITION TO THOSE SHOWN ON THESE PLANS. CONTRACTOR SHALL IMPLEMENT ADEQUATE EROSION & SEDIMENT CONTROL MEASURES FOR ANY ADDITIONAL REQUIREMENTS.
- BARE COPPER WIRE AND POLYETHYLENE DETECTION TAPE TO RUN ALONG ENTIRE LENGTH OF FORCE MAIN INSTALLED BY CUT AND COVER TECHNIQUE.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN APPROVAL AND ACCEPTANCE OF THE FORCE MAIN AND LATERALS BY NEW CASTLE COUNTY UPON COMPLETION OF CONSTRUCTION. AS BUILT PLANS MUST BE SUBMITTED TO NEW CASTLE COUNTY BEFORE ACCEPTANCE.
- 2-INCH AND 1 1/4-INCH FORCE MAIN SHALL BE PVC, SDR 21, IN ACCORDANCE WITH ASTM D 2241. JOINING METHOD SHALL BE SOLVENT CEMENT FOR ALL PIPE JOINTS IN ACCORDANCE WITH ASTM D 2627.
- CONTRACTOR SHALL INSTALL AND TEST 2" FORCE MAIN, LATERALS AND APPURTENANCES PRIOR TO MAKING FINAL CONNECTIONS TO DELAWARE MOVING AND STORAGE AND AVC PROPERTIES. CONTRACTOR SHALL NOTIFY BOTH PROPERTIES AT LEAST TWO WEEKS PRIOR TO MAKING CONNECTION. INTERRUPTION OF SEWER SERVICE SHALL NOT BE MORE THAN TWO HOURS AND SHALL BE DONE DURING NON-BUSINESS HOURS.
- THE CONTRACTOR SHALL REFER TO PLAN SHEETS 3 AND 4 FOR ADDITIONAL GENERAL PROJECT NOTES RELATED TO THIS CONTRACT.
- FOR CLARITY, NOT ALL FITTINGS, CLOSURES AND SPECIALS HAVE TO BE SHOWN ON THESE PLANS. LOCATION AND TYPE SHALL BE DETERMINED BY CONTRACTOR WHERE APPROPRIATE TO MAINTAIN PIPE ALIGNMENT AS SHOWN.



**PROFILE**  
SCALE: 1" = 50' HORIZ.  
1" = 5' VERT.

PREL. TRACING    SUB    DESIGN    KMH    CHRD.    JMM



**PROPOSED 2" FORCE MAIN COORDINATE SCHEDULE**

POINT NO.	BASELINE STATION	OFFSET	NORTH	EAST	= FORCE MAIN STATION
1007	STA. 116+24.97	55.17' LT.	6032811180	589106.4611	STA. 9+13.31
1008	STA. 115+49.63	55.17' LT.	603206.9744	589098.0964	STA. 9+87.93
1009	STA. 114+23.47	55.17' LT.	603083.0871	589081.9093	STA. 11+2.87
1010	STA. 113+76.57	55.17' LT.	603037.1240	589075.1963	STA. 11+59.31
1011	STA. 113+59.38	52.17' LT.	603019.8372	589075.6071	STA. 11+76.61
1012	STA. 112+18.51	52.17' LT.	602882.1385	589052.7618	STA. 13+16.20
1013	STA. 110+55.65	47.17' LT.	602721.4488	589028.1225	STA. 14+78.76
1014	STA. 109+96.58	47.17' LT.	602663.3673	589017.3259	STA. 15+37.84
1015	STA. 109+80.41	30.99' LT.	602644.5140	589030.2688	STA. 15+60.71

PREL. TRACING    SUB    DESIGN    KMH    CHKD.    JMM



**SR 7,  
NEWTOWN ROAD TO SR 273**

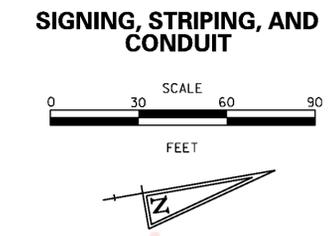
REVISIONS

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	26 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,070 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	0 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,191 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE, 6" GAP) (ITEM 748506)	111 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE, 30" GAP) (ITEM 748506)	295 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	2 EA
(H)	12" SOLID YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748027)	0 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF
(J)	2"x6" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF

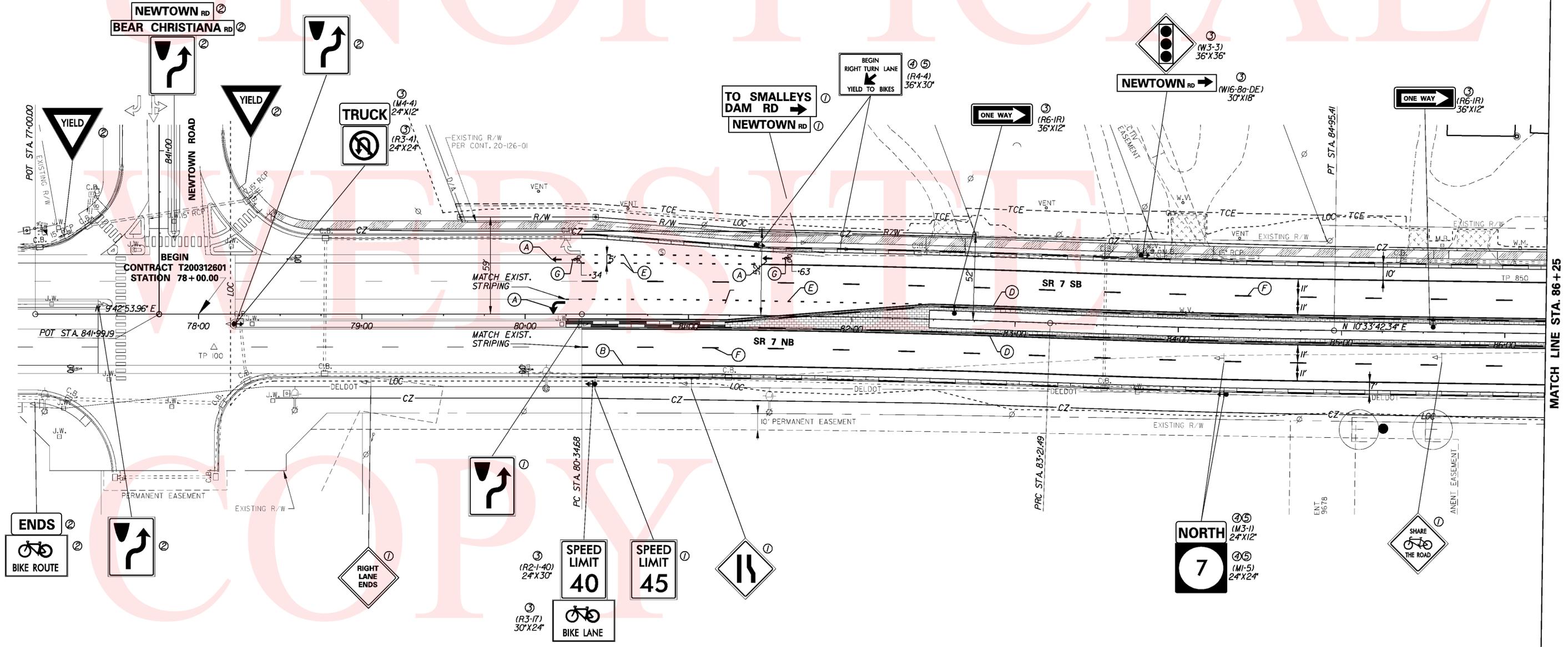
SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

ITMS LEGEND	
-ITMS-CON-	MULTIDUCT ITMS CONDUIT
-DOT-UG-	EXISTING ITMS CONDUIT
J.W.	EXISTING CONDUIT JUNCTION WELL
(JW)	PROPOSED CONDUIT JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(CD)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
(JW)	EXISTING CONDUIT JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(CD)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)

**ITMS NOTES:**  
 1. STATION AND OFFSET DATA IS GIVEN AT ANGLE BREAKS IN CONDUIT AND AT JUNCTION WELL LOCATIONS.



**NOTES:**  
 1. PROPOSED STRIPING DIMENSIONS ARE SHOWN TO THE FACE OF CURB UNLESS NOTED OTHERWISE.  
 2. THE CONTRACTOR SHALL INSTALL AND REMOVE THE REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS WITHIN THE CONTRACT LIMITS, AS SHOWN. DELDOT SHALL FABRICATE AND SUPPLY THE REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS TO THE CONTRACTOR. FOR COORDINATION AND PICK UP OF ALL SIGNS AND POSTS, THE CONTRACTOR SHALL CONTACT DELDOT'S SIGN SHOP AT (302)760-2581. ALLOW 4 WEEKS FOR DELIVERY.



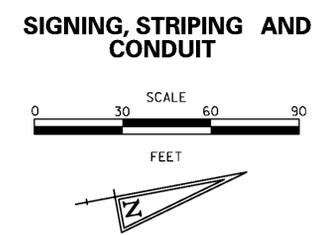
PREL. TRACING SUB DESIGN KGM-A CHKD. JRR



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	55 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,550 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	0 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,804 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE 6" GAP) (ITEM 748506)	258 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE 30" GAP) (ITEM 748506)	609 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	3 EA
(H)	1/2" SOLID YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748027)	0 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF
(J)	2"x6" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	59 SF

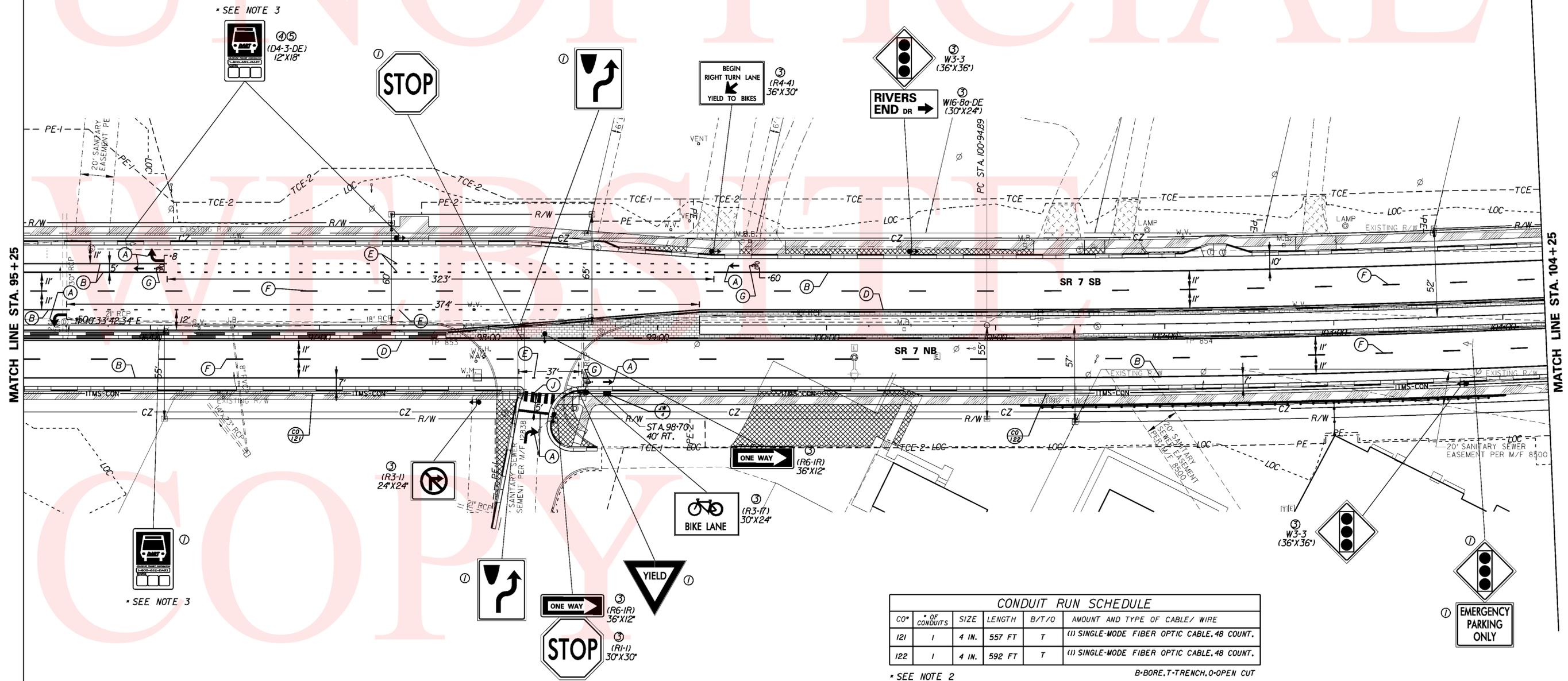
SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

ITMS LEGEND	
-ITMS-CON-	MULTIDUCT ITMS CONDUIT
-DOT-UG-	EXISTING ITMS CONDUIT
■	ITMS CONDUIT JUNCTION WELL
J.W. □	EXISTING CONDUIT JUNCTION WELL
⊙	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊙	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
⊙	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊙	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)



**NOTES:**

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- THE CONTRACTOR SHALL INSTALL AND REMOVE THE REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS WITHIN THE CONTRACT LIMITS, AS SHOWN. DELDOT SHALL FABRICATE AND SUPPLY THE REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS TO THE CONTRACTOR. FOR COORDINATION AND PICK UP OF ALL SIGNS AND POSTS, THE CONTRACTOR SHALL CONTACT DELDOT'S SIGN SHOP AT (302)760-2581. ALLOW 4 WEEKS FOR DELIVERY.



CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/WIRE
121	1	4 IN.	557 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT.
122	1	4 IN.	592 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT.

\* SEE NOTE 2 B-BORE, T-TRENCH, O-OPEN CUT

**ITMS NOTES:**

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- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPLICING OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.

PREL. TRACING SUB DESIGN CHKD. JRR

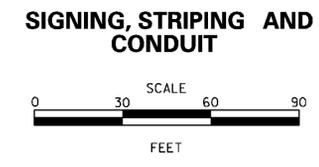
**SR 7,  
NEWTOWN ROAD TO SR 273**

REVISIONS	

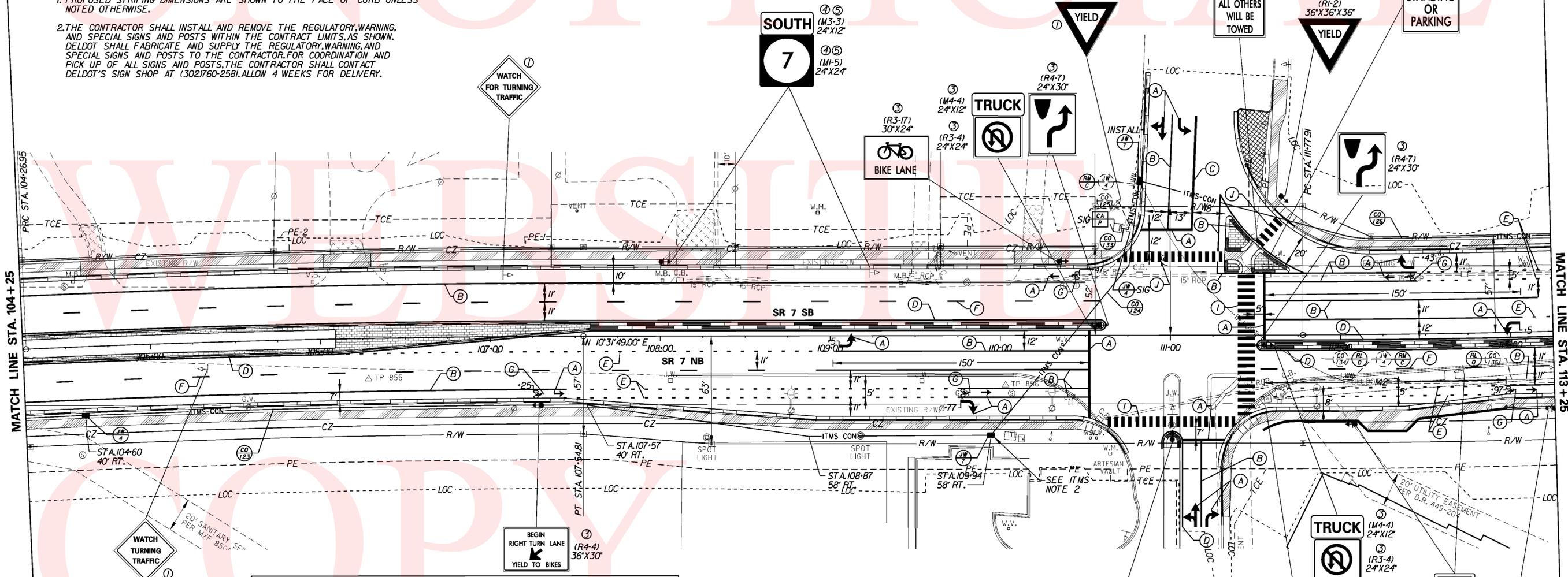
PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	347 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	2,113 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	135 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,698 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE 6' GAP) (ITEM 748506)	309 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE 30' GAP) (ITEM 748506)	325 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	5 EA
(H)	1/2" SOLID YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748027)	0 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	470 SF
(J)	2"x6" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	415 SF

SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

ITMS LEGEND	
-ITMS-CON-	MULTIDUCT ITMS CONDUIT
-DOT-UG-	EXISTING ITMS CONDUIT
J.W.	ITMS CONDUIT JUNCTION WELL
⊖	EXISTING CONDUIT JUNCTION WELL
⊖	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊖	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
⊖	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊖	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)



**NOTES:**  
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CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/WIRE
123	1	4 IN.	533 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT.
124	1	4 IN.	177 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT.
125	1	4 IN.	29 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 12 COUNT.
126	1	4 IN.	500 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
*133	1	4 IN.	3 FT	-	(1) SINGLE-MODE FIBER OPTIC CABLE, 12 COUNT.
*134	1	4 IN.	169 FT	-	<RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, <RELOCATE> (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, <RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
*135	1	4 IN.	521 FT	-	<RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, <RELOCATE> (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, <RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.

\* DENOTES EXISTING CONDUIT  
 \*\* SEE NOTE 2  
 B-BORE, T-TRENCH, O-OPEN CUT

**ITMS NOTES:**  
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PREL. TRACING SUB DESIGN CHKD. JRR

## SR 7, NEWTOWN ROAD TO SR 273

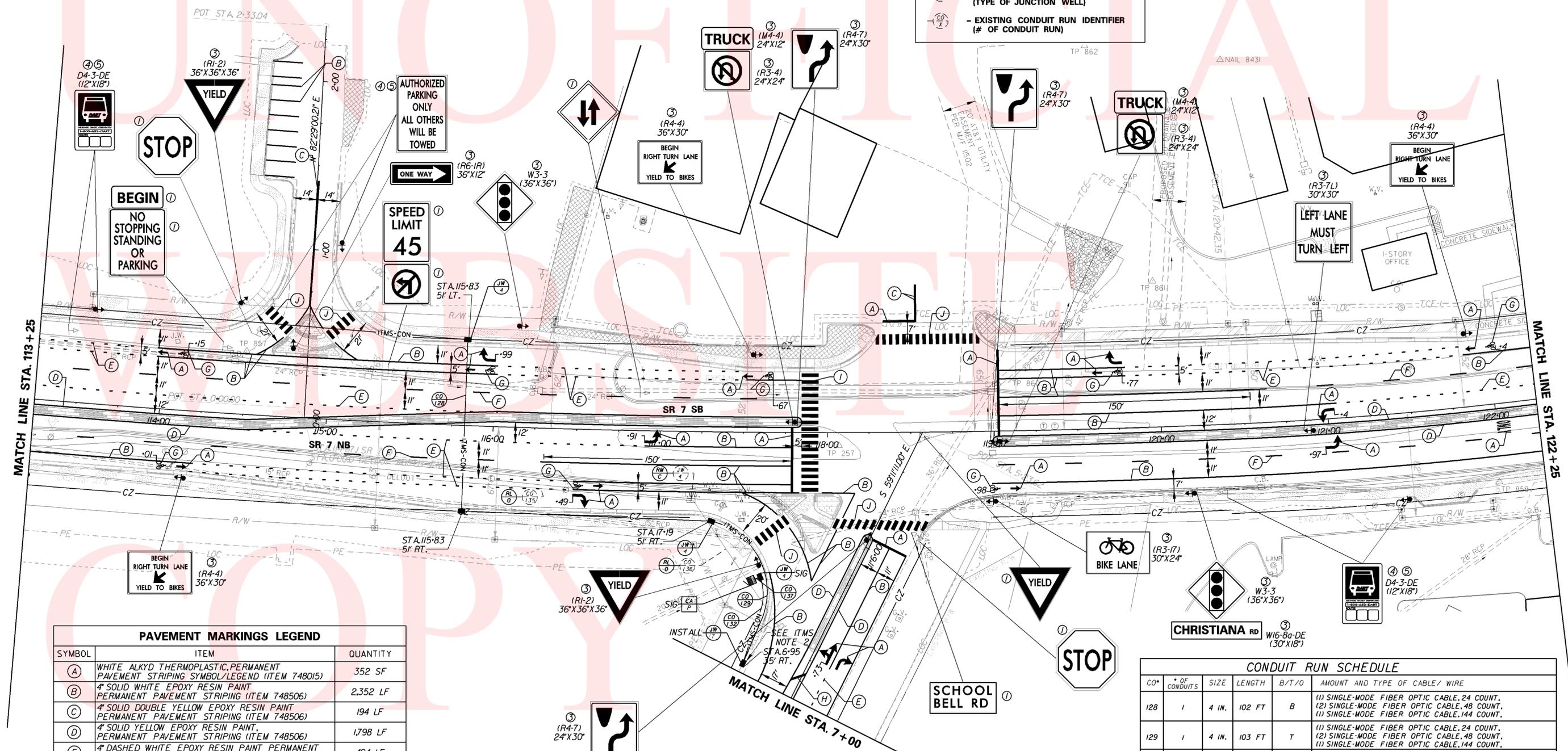
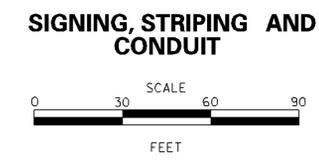
### REVISIONS

SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

ITS LEGEND	
MD(SOD)	4" MULTIDUCT UNDER NEW PAVEMENT OR IN SODDED TRENCH (ITEM 745513)
MD(PAVE)	4" MULTIDUCT UNDER EXISTING PAVEMENT (ITEM 745514)
■	CONDUIT JUNCTION WELL, TYPE 4 (ITEM 744523) - UNLESS OTHERWISE NOTED

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  - STRIPING MODIFICATIONS IN DELAWARE MOVING AND STORAGE PARKING LOT DONE BY OTHERS, SHOWN FOR REFERENCE ONLY.

ITMS LEGEND	
-ITMS-CON-	MULTIDUCT ITMS CONDUIT
-DOT-UG-	EXISTING ITMS CONDUIT
■	ITMS CONDUIT JUNCTION WELL
□	EXISTING CONDUIT JUNCTION WELL
○	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
○	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
○	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
○	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	352 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	2,352 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	194 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,798 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE 6" GAP) (ITEM 748506)	494 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE 30" GAP) (ITEM 748506)	315 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	7 EA
(H)	12" SOLID YELLOW ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING (ITEM 748027)	10 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	360 SF
(J)	2"x6" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	535 SF

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/WIRE
128	1	4 IN.	102 FT	B	(1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
129	1	4 IN.	103 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
132	1	4 IN.	54 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 12 COUNT.
*135	1	4 IN.	521 FT	-	<RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, <RELOCATE> (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, <RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
*136	1	4 IN.	483 FT	-	<RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 24 COUNT, <RELOCATE> (2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT, <RELOCATE> (1) SINGLE-MODE FIBER OPTIC CABLE, 144 COUNT.
137	1	4 IN.	5 FT	T	(1) SINGLE-MODE FIBER OPTIC CABLE, 12 COUNT.

\* DENOTES EXISTING CONDUIT

B-BORE, T-TRENCH, O-OPEN CUT

- ITMS NOTES:**
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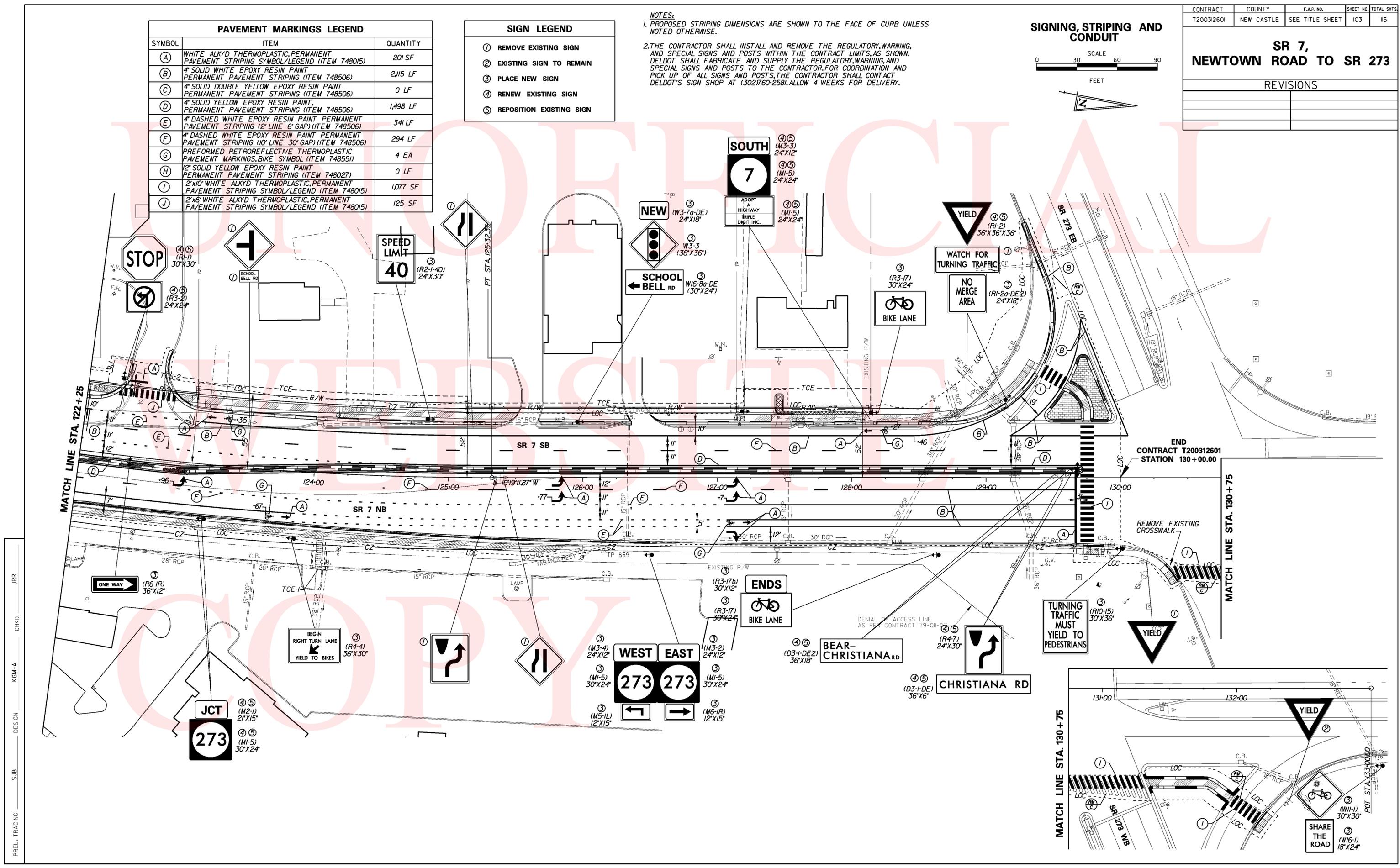
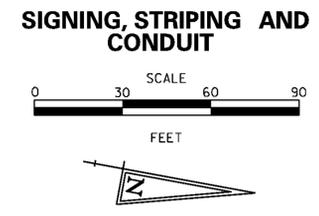
PREL. TRACING S.S.B. DESIGN KGM-A CHKD. JRR

\*\* SEE NOTE 2

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	201 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	2,115 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	0 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,498 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE 6" GAP) (ITEM 748506)	341 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE 30" GAP) (ITEM 748506)	294 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	4 EA
(H)	12" SOLID YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748027)	0 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	1,077 SF
(J)	2"x8" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	125 SF

SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

**NOTES:**  
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PREL. TRACING SUB DESIGN CHKD. JRR  
 KGM-A

**SR 7,  
NEWTOWN ROAD TO SR 273**

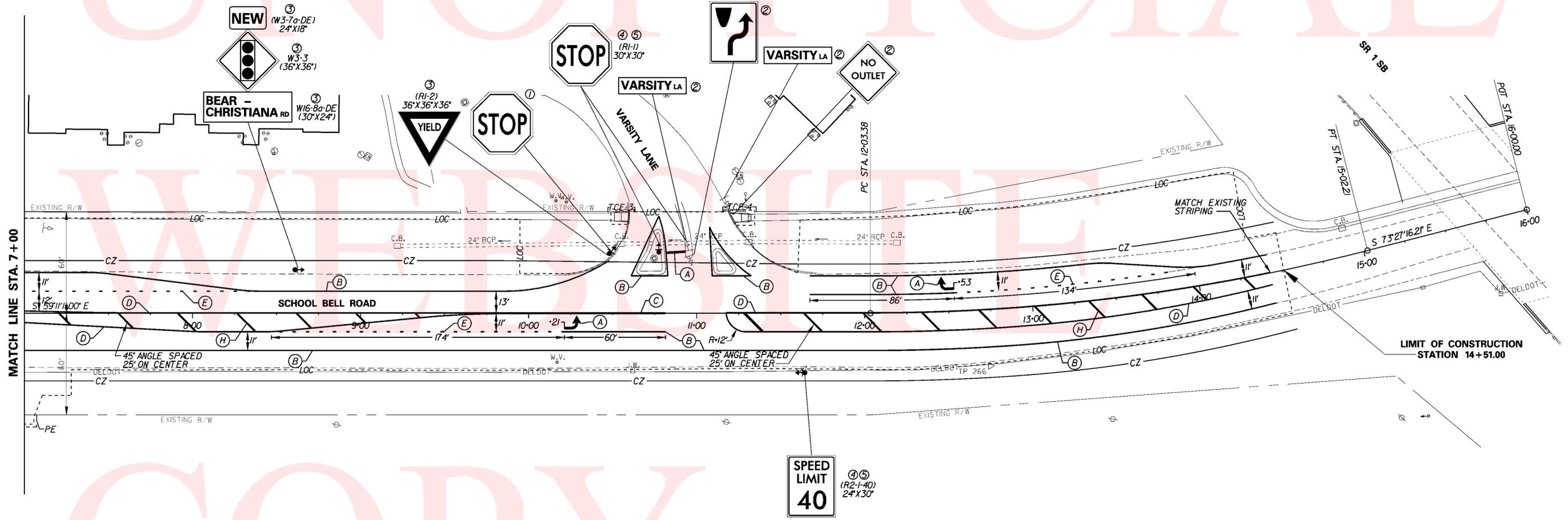
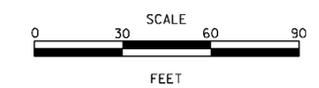
REVISIONS	

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	47 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	1,707 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	222 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	526 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2' LINE 6' GAP) (ITEM 748506)	111 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10' LINE 30' GAP) (ITEM 748506)	0 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, SYMBOL/LEGEND (ITEM 748546)	0 SF
(H)	1/2" SOLID YELLOW ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING (ITEM 748027)	222 LF
(I)	2"x10' WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF
(J)	2"x6' WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF

SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

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**SIGNING, STRIPING AND CONDUIT**



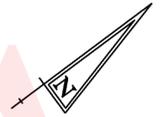
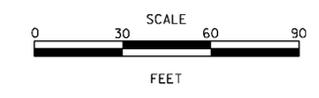
PREL. TRACING SUB DESIGN CHKD. JRR

COPY

EXISTING STORMWATER MANAGEMENT FACILITY

CONTRACT	COUNTY	F.A.P. NO.	SHEET NO.	TOTAL SHTS.
T2003I260I	NEW CASTLE	SEE TITLE SHEET	105	115
<b>SR 7, NEWTOWN ROAD TO SR 273</b>				
REVISIONS				

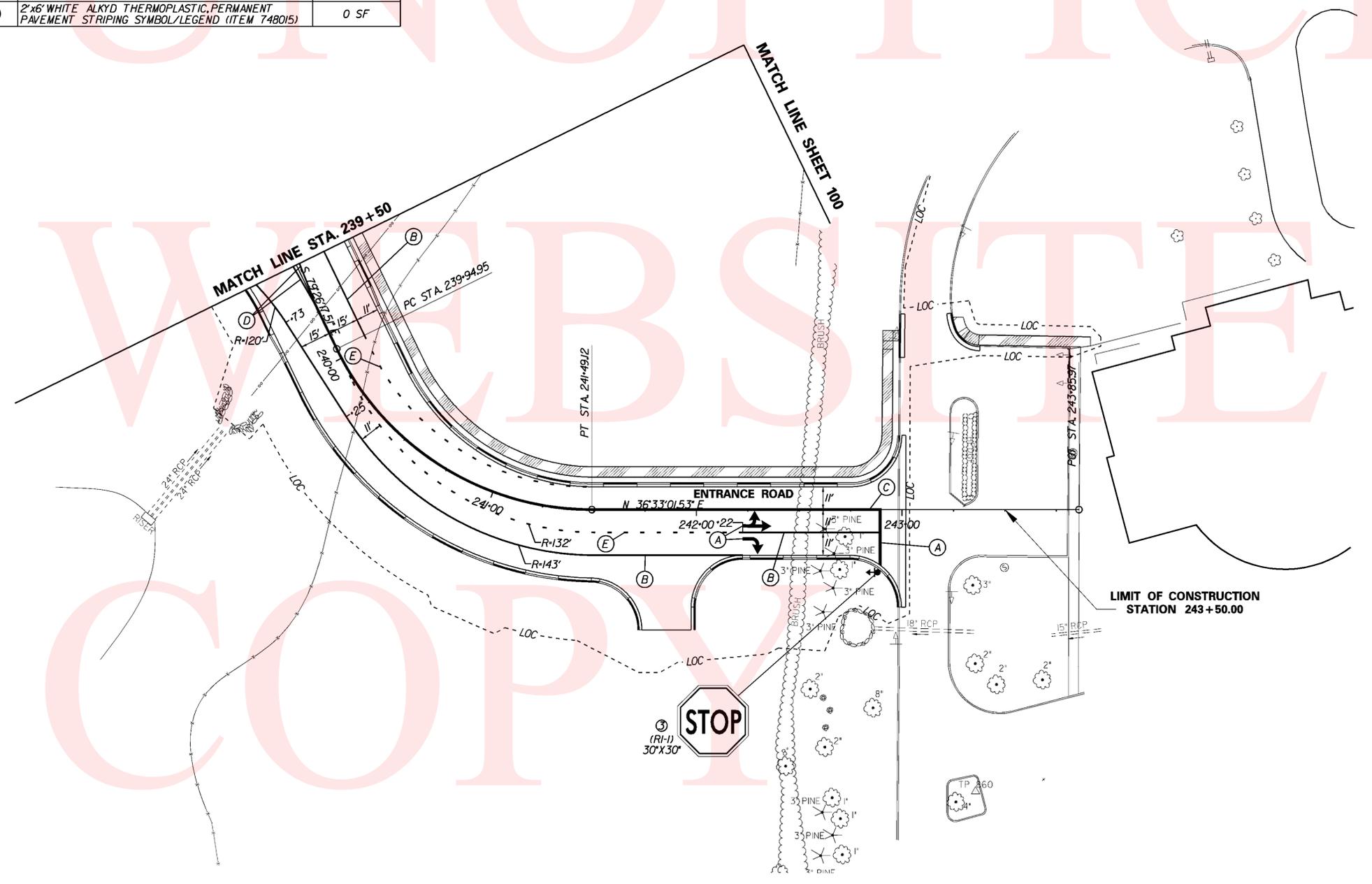
**SIGNING, STRIPING AND CONDUIT**



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	72 SF
(B)	4" SOLID WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	400 LF
(C)	4" SOLID DOUBLE YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748506)	642 LF
(D)	4" SOLID YELLOW EPOXY RESIN PAINT, PERMANENT PAVEMENT STRIPING (ITEM 748506)	39 LF
(E)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (2" LINE, 6" GAP) (ITEM 748506)	369 LF
(F)	4" DASHED WHITE EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (10" LINE, 30" GAP) (ITEM 748506)	0 LF
(G)	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL (ITEM 748551)	0 EA
(H)	1/2" SOLID YELLOW EPOXY RESIN PAINT PERMANENT PAVEMENT STRIPING (ITEM 748027)	0 LF
(I)	2"x10" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF
(J)	2"x6" WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	0 SF

SIGN LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

**NOTES:**  
 1. PROPOSED STRIPING DIMENSIONS ARE SHOWN TO THE FACE OF CURB UNLESS NOTED OTHERWISE.  
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PREL. TRACING SUB DESIGN CHKD. JRR KGM-A

CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1	1	2.0 IN.	82 FT	T	NEW (1) 2" x 8 U.F.W./GROUND
2	1	2.0 IN.	8 FT	T	NEW (1) 2" x 8 U.F.W./GROUND
3	3	2.5 IN.	4 FT	T	NEW (4) 4" x 18, NEW (2) 9" x 14
10	1	4.0 IN.	4 FT	T	NEW (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
11	1	2.5 IN.	15 FT	T	NEW (1) 9" x 14
12	1	2.5 IN.	26 FT	T	NEW (2) 9" x 14
13	1	2.5 IN.	35 FT	B	NEW (2) 9" x 14, NEW (4) 4" x 18
14	1	2.5 IN.	13 FT	T	NEW (2) 16" x 14, NEW (4) 4" x 18
16	1	2.5 IN.	7 FT	T	NEW (1) 9" x 14
17	1	2.5 IN.	98 FT	B	NEW (1) 9" x 14, NEW (2) 4" x 18
*25	1	2.5 IN.	56 FT	-	NEW (1) 4" x 18
26	1	2.5 IN.	109 FT	B	NEW (1) 4" x 18
30	1	4.0 IN.	22 FT	T	NEW (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
31	1	2.5 IN.	8 FT	T	EMPTY
32	1	2.5 IN.	11 FT	T	EMPTY
33	1	2.5 IN.	13 FT	T	EMPTY

\* DENOTES EXISTING CONDUIT B - BORE, T - TRENCH, O - OPEN CUT

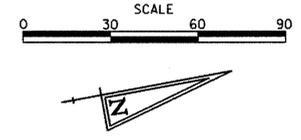
**SIGNAL NOTES**

- PROPOSED LOOP DETECTORS: TYPE "2 - 6"x25" - TO BE INSTALLED ON SR7 LEFT TURN MOVEMENTS. TYPE "2 - 6"x25" - TO BE INSTALLED ON SIDE STREET THROUGH AND LEFT TURN MOVEMENTS.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL SIGNAL POLES SHALL BE 30 FEET SPAN WIRES SHALL HAVE 5-PERCENT DISPLACEMENT (SAG) AT MIDPOINT, AS SHOWN IN THE SPAN WIRE SCHEDULE (FIELD ADJUSTMENTS AS REQUIRED).
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- INSTALL LOUVERS ON ALL SIDE STREET SIGNAL INDICATIONS. TEMPORARILY INSTALL A SECOND SPAN WIRE TO ATTACH TO THE BOTTOM OF THE SIGNAL HEADS WITH LOUVERS TO STABILIZE THE HEADS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 1**



**DELAWARE DEPARTMENT OF TRANSPORTATION TRAFFIC SECTION**

SMYRNA		DELAWARE	
PERMIT NO. N-472	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 106 TOTAL 115

**SR 7 AT RIVERS END DRIVE/ BANK ONE ENTRANCE**

PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
REVISION			

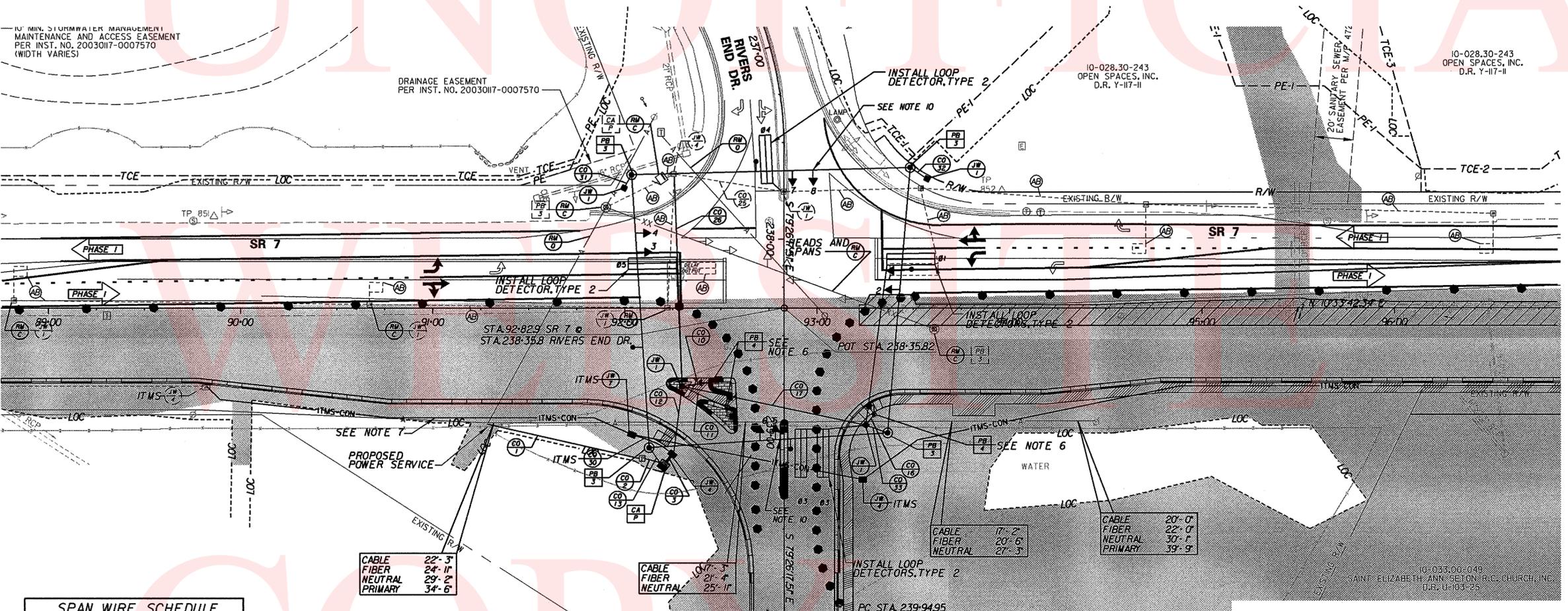
REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 1)

**LEGEND**

PROPOSED SIGNAL CABINET	(RM C)	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	(RM D)	REMOVE BY OTHERS
PROPOSED POLE BASE	(AB)	ABANDON
EXISTING POLE BASE	(PB 3)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	(PB 1)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTAL BASE	(PB 3)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED WOOD POLE	(JW 1)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	(JW 1)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
UTILITY POLE	(JW 1)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED JUNCTION WELL	(CO #)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
EXISTING JUNCTION WELL	(CO #)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED SIGNAL HEAD	(OH #)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	(OH #)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PEDESTRIAN SIGNAL HEAD	(OH #)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING PEDESTRIAN SIGNAL HEAD	(OH #)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PUSHBUTTON AND SIGN	(MA XX)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PUSHBUTTON AND SIGN	(MA XX)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED VIDEO DETECTION	(CA P)	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING VIDEO DETECTION	(CA P)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED MICROWAVE DETECTION	(CA P)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING MICROWAVE DETECTION	(CA P)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
OVERHEAD SIGNING	(S)	SERVICE PEDESTAL
PROPOSED OPTICOM RECEIVER	(S)	
EXISTING OPTICOM RECEIVER	(S)	
BACK GUY	(S)	
PROPOSED MAST ARM	(S)	
EXISTING MAST ARM	(S)	
PROPOSED LUMINAIRE	(S)	
EXISTING LUMINAIRE	(S)	
PROPOSED SPAN WIRE	(S)	
EXISTING SPAN WIRE	(S)	
RIGHT-OF-WAY OR PROPERTY LINE	(S)	
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	(S)	
EXISTING LOOP DETECTOR	(S)	

10' MIN. STORMWATER MANAGEMENT MAINTENANCE AND ACCESS EASEMENT PER INST. NO. 20030117-0007570 (WIDTH VARIES)

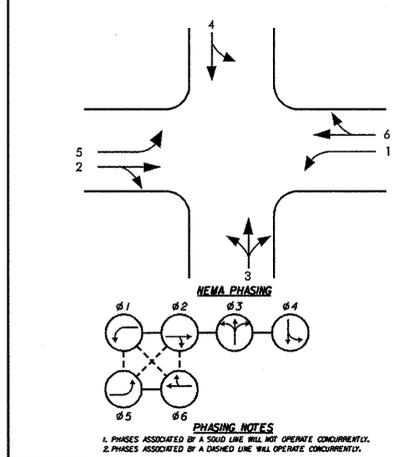
DRAINAGE EASEMENT PER INST. NO. 20030117-0007570



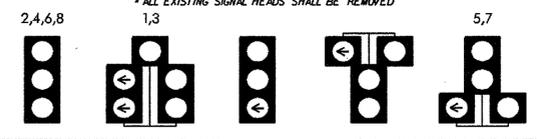
SPAN WIRE SCHEDULE				
SPAN	LENGTH	SPAN MOUNT HEIGHT	5% SAG	SPAN MIDPOINT
NORTH	138 FT	28 FT	6.9 FT	21.1 FT
SOUTH	141 FT	28 FT	7.0 FT	21.0 FT
EAST	125 FT	28 FT	6.3 FT	21.7 FT
WEST	145 FT	28 FT	7.2 FT	20.8 FT

MOT CONSTRUCTION SEQUENCE LEGEND	
	TEMPORARY CONSTRUCTION
	WORK AREA THIS PHASE
	EXISTING TRAFFIC FLOW AREA
	TEMPORARY WHITE PAVEMENT MARKING ARROW

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7 2010	Mark Allen
RECOMMENDED	20	
RECOMMENDED	20	

APPROVED TRAFFIC ENGINEER *Mark Allen* 10/7/10 DATE  
 APPROVED for INSTL. CHIEF TRAFFIC ENGINEER *Mark Allen* 10/7/10 DATE

PREL. TRACING DESIGN CHKD.

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
*1	1	2.0 IN.	82 FT	-	EX (1) 2/*8 U.F.W/GROUND
*2	1	2.0 IN.	8 FT	-	EX (1) 2/*8 U.F.W/GROUND
*3	3	2.5 IN.	4 FT	-	<REMOVE>(2) 4/*18, EX(2) 9/*14, EX(1) 4/*18, NEW (2) 9/*14, NEW (3) 4/*18.
	1	4.0 IN.	4 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
4	1	2.5 IN.	6 FT	T	NEW (1) 9/*14
5	1	2.5 IN.	60 FT	B	NEW (1) 9/*14
6	1	2.5 IN.	11 FT	T	NEW (1) 9/*14
7	1	2.5 IN.	11 FT	T	NEW (2) 9/*14
*10	1	2.5 IN.	15 FT	-	EX(1) 9/*14
*11	1	2.5 IN.	26 FT	-	EX(2) 9/*14
*12	1	2.5 IN.	35 FT	-	<REMOVE> (2) 4/*18, EX(2) 9/*14, EX(1) 4/*18, NEW (3) 4/*18, NEW (2) 9/*14
*13	1	2.5 IN.	13 FT	-	EX(2) 16/*14, EX(4) 4/*18
*16	1	2.5 IN.	7 FT	-	EX(1) 9/*14
*17	1	2.5 IN.	98 FT	-	<REMOVE>(1) 4/*18, EX(1) 9/*14, EX(1) 4/*18, NEW (1) 4/*18
27	1	2.5 IN.	89 FT	B	NEW (2) 9/*14, NEW (1) 4/*18
*30	1	4.0 IN.	22 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*31	1	2.5 IN.	8 FT	-	EMPTY
*32	1	2.5 IN.	11 FT	-	EMPTY
*33	1	2.5 IN.	13 FT	-	EMPTY

\* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

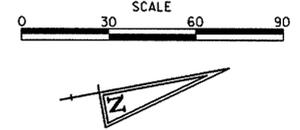
**SIGNAL NOTES**

- EXISTING LOOP DETECTORS:  
TYPE \*2 - 6'x25' - BANKONE ENTRANCE THROUGH AND LEFT TURN MOVEMENTS.  
PROPOSED LOOP DETECTORS:  
TYPE \*2 - 6'x25' - TO BE INSTALLED ON SR7 LEFT TURN MOVEMENTS.  
TYPE \*2 - 6'x25' - TO BE INSTALLED ON RIVERS END DRIVE THROUGH AND LEFT TURN MOVEMENTS.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB, RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 2**



DELAWARE DEPARTMENT OF TRANSPORTATION				
TRAFFIC SECTION				
SMYRNA		DELAWARE		
PERMIT NO. N-472	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 107	TOTAL 115

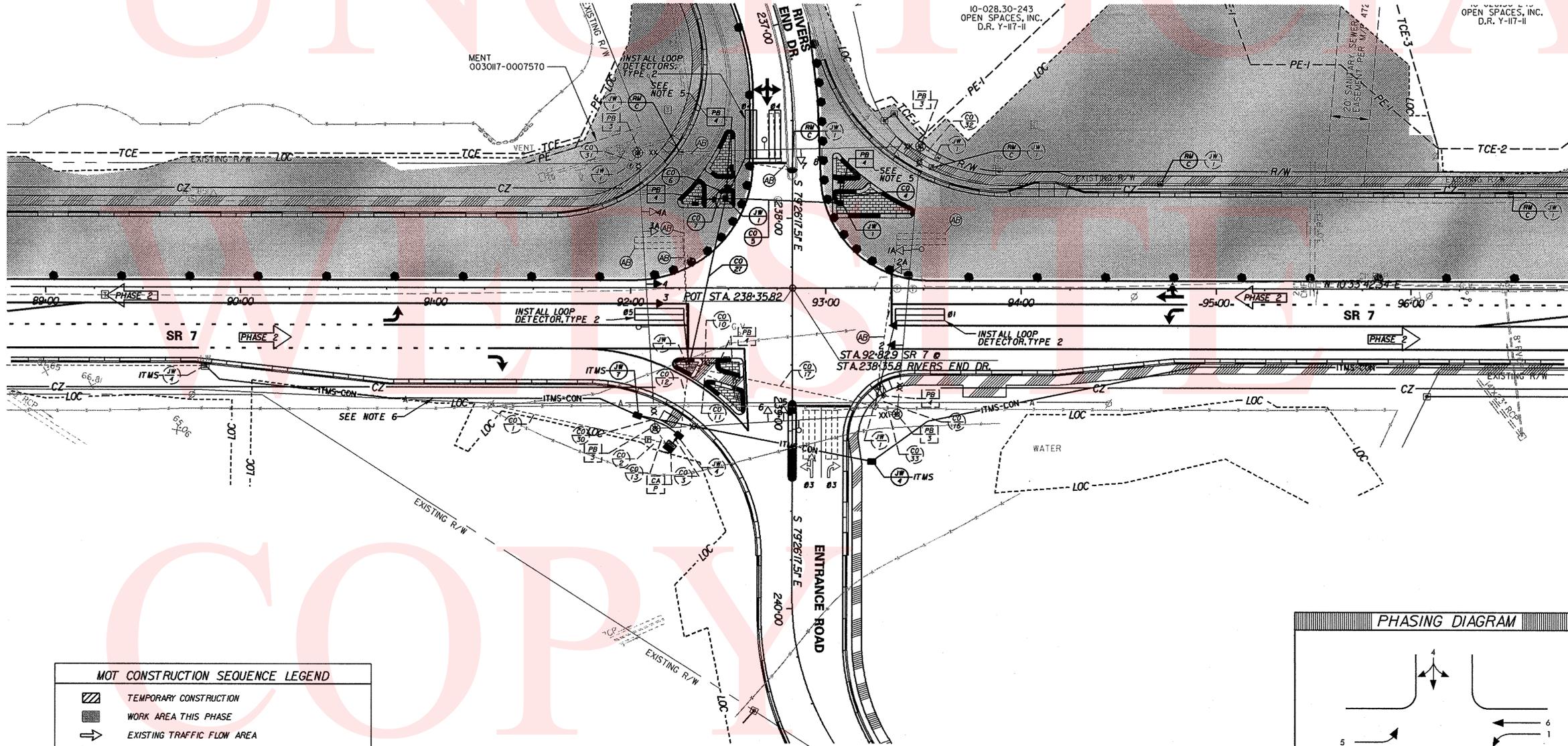
**SR 7 AT RIVERS END DRIVE/  
BANK ONE ENTRANCE**

PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
REVISION			

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 2)

**LEGEND**

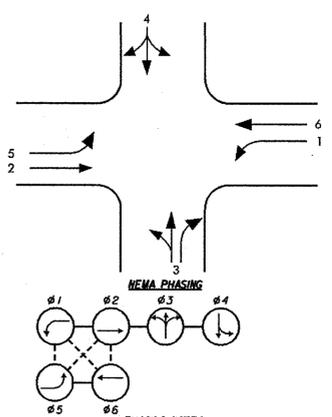
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY OTHERS
	PROPOSED POLE BASE		ABANDON
	EXISTING POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTAL BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTAL BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING WOOD POLE		PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	UTILITY POLE		EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING JUNCTION WELL		EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
	EXISTING PEDESTRIAN SIGNAL HEAD		EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
	PROPOSED PUSHBUTTON AND SIGN		SERVICE PEDESTAL
	EXISTING PUSHBUTTON AND SIGN		
	PROPOSED VIDEO DETECTION		
	EXISTING VIDEO DETECTION		
	PROPOSED MICROWAVE DETECTION		
	EXISTING MICROWAVE DETECTION		
	OVERHEAD SIGNING		
	PROPOSED OPTICOM RECEIVER		
	EXISTING OPTICOM RECEIVER		
	BACK CUT		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED SPAN WIRE		
	EXISTING SPAN WIRE		
	RIGHT-OF-WAY OR PROPERTY LINE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR		



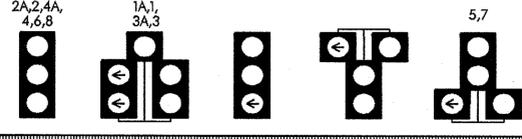
**MOT CONSTRUCTION SEQUENCE LEGEND**

	TEMPORARY CONSTRUCTION
	WORK AREA THIS PHASE
	EXISTING TRAFFIC FLOW AREA
	TEMPORARY WHITE PAVEMENT MARKING ARROW

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7	2010	<i>[Signature]</i>
RECOMMENDED		20	
RECOMMENDED		20	

*[Signature]* 10/7/10  
APPROVED TRAFFIC ENGINEER DATE

*[Signature]* 10/7/10  
APPROVED for INSTL. DATE  
CHIEF TRAFFIC ENGINEER

PHASING NOTES  
1. PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.  
2. PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.

CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
*1	1	2.0 IN.	82 FT	-	EX (1) 2"/8 U.F.W/GROUND
*2	1	2.0 IN.	8 FT	-	EX (1) 2"/8 U.F.W/GROUND
*3	3	2.5 IN.	4 FT	-	NEW (2) 4"/18, EX (4) 9"/14, EX (2) 4"/18, <REMOVE> (1) 4"/18
	1	4.0 IN.	4 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*4	1	2.5 IN.	6 FT	-	EX (1) 9"/14
*5	1	2.5 IN.	60 FT	-	EX (1) 9"/14
*6	1	2.5 IN.	11 FT	-	EX (1) 9"/14
*7	1	2.5 IN.	11 FT	-	EX (2) 9"/14
9	1	2.5 IN.	47 FT	B	NEW (1) 4"/18
*10	1	2.5 IN.	15 FT	-	EX (1) 9"/14
*11	1	2.5 IN.	26 FT	-	EX (2) 9"/14
*12	1	2.5 IN.	35 FT	-	NEW (2) 4"/18, EX (4) 9"/14, EX (2) 4"/18, <REMOVE> (1) 4"/18
*13	1	2.5 IN.	13 FT	-	EX (2) 16"/14, EX (4) 4"/18
14	1	2.5 IN.	234 FT	T	EMPTY
15	1	2.5 IN.	55 FT	B	NEW (1) 4"/18
*16	1	2.5 IN.	7 FT	-	EX (1) 9"/14
*17	1	2.5 IN.	98 FT	-	NEW (1) 4"/18, EX (1) 9"/14, EX (1) 4"/18, <REMOVE> (1) 4"/18
18	1	2.5 IN.	234 FT	T	EMPTY
*27	1	2.5 IN.	89 FT	-	EX (2) 9"/14, EX (1) 4"/18
*30	1	4.0 IN.	22 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*31	1	2.5 IN.	8 FT	-	EMPTY
*32	1	2.5 IN.	11 FT	-	EMPTY
*33	1	2.5 IN.	13 FT	-	EMPTY

**SIGNAL NOTES**

- EXISTING LOOP DETECTORS: TYPE "2" - 6"x25" - SIDE STREET MOVEMENTS. PROPOSED LOOP DETECTORS: TYPE "2" - 6"x25" - TO BE INSTALLED ON SR7 LEFT TURN MOVEMENTS.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPLICING OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 3**



**DELAWARE DEPARTMENT OF TRANSPORTATION TRAFFIC SECTION**

SMYRNA		DELAWARE	
PERMIT NO. N-472	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 108 TOTAL 115

**SR 7 AT RIVERS END DRIVE/ BANK ONE ENTRANCE**

PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
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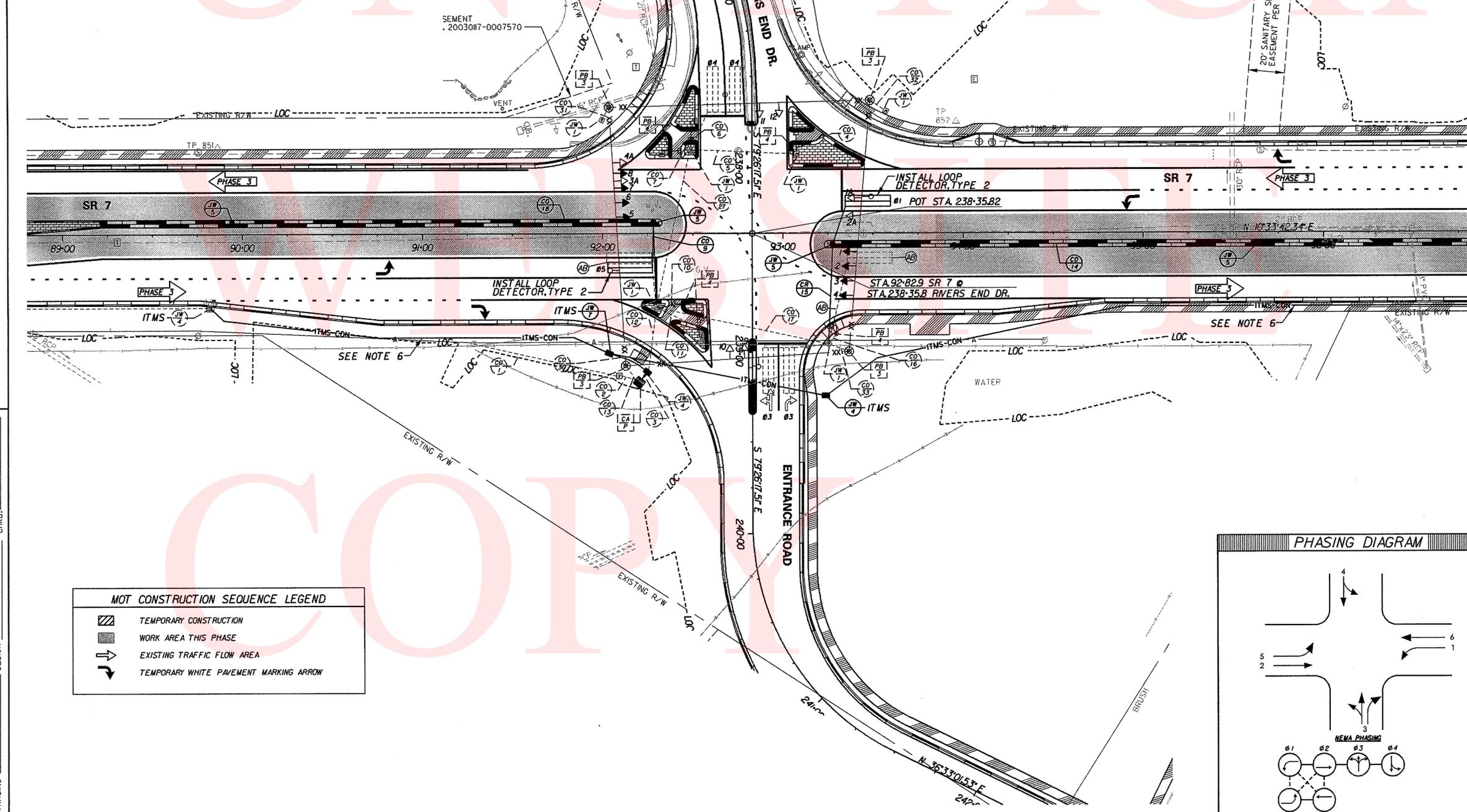
**REVISION**

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 3)

**LEGEND**

PROPOSED SIGNAL CABINET	PROPOSED POLE BASE	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	EXISTING POLE BASE	REMOVE BY OTHERS
PROPOSED PEDESTAL BASE	EXISTING PEDESTAL BASE	ABANDON
EXISTING PEDESTAL BASE	PROPOSED WOOD POLE	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED WOOD POLE	EXISTING WOOD POLE	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
UTILITY POLE	PROPOSED JUNCTION WELL	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED JUNCTION WELL	EXISTING JUNCTION WELL	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING JUNCTION WELL	PROPOSED SIGNAL HEAD	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED SIGNAL HEAD	EXISTING SIGNAL HEAD	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED PEDESTRIAN SIGNAL HEAD	EXISTING PEDESTRIAN SIGNAL HEAD	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING PEDESTRIAN SIGNAL HEAD	PROPOSED PUSHBUTTON AND SIGN	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED VIDEO DETECTION	EXISTING VIDEO DETECTION	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING VIDEO DETECTION	PROPOSED MICROWAVE DETECTION	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED MICROWAVE DETECTION	EXISTING MICROWAVE DETECTION	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
OVERHEAD SIGNING	EXISTING OVERHEAD SIGNING	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED OPTIKOM RECEIVER	EXISTING OPTIKOM RECEIVER	SERVICE PEDESTAL
EXISTING OPTIKOM RECEIVER	BACK GUY	
PROPOSED MAST ARM	EXISTING MAST ARM	
PROPOSED LUMINAIRE	EXISTING LUMINAIRE	
PROPOSED SPAN WIRE	EXISTING SPAN WIRE	
RIGHT-OF-WAY OR PROPERTY LINE	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	
EXISTING LOOP DETECTOR	EXISTING LOOP DETECTOR	

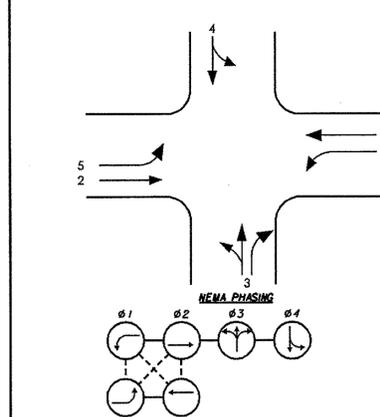
\* DENOTES EXISTING CONDUIT B - BORE, T - TRENCH, O - OPEN CUT



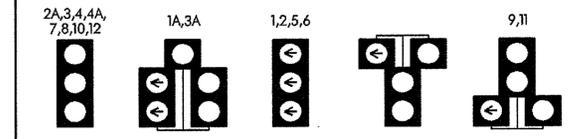
**MOT CONSTRUCTION SEQUENCE LEGEND**

	TEMPORARY CONSTRUCTION
	WORK AREA THIS PHASE
	EXISTING TRAFFIC FLOW AREA
	TEMPORARY WHITE PAVEMENT MARKING ARROW

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7 2010	<i>[Signature]</i>
RECOMMENDED	20	
RECOMMENDED	20	

APPROVED TRAFFIC ENGINEER *[Signature]* 10/7/10 DATE  
 APPROVED for INSTL. CHIEF TRAFFIC ENGINEER *[Signature]* 10/7/10 DATE

PREL. TRACING DESIGN CHKD.

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
*1	1	2.0 IN.	82 FT	-	EX (1) 2/*8 U.F.W/GROUND
*2	1	2.0 IN.	8 FT	-	EX (1) 2/*8 U.F.W/GROUND
*3	3	2.5 IN.	4 FT	-	NEW (6) 4/*18, EX (6) 9/*14, EX (2) 4/*18, <REMOVE> (2) 4/*18
	1	4.0 IN.	4 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*4	1	2.5 IN.	6 FT	-	EX (1) 9/*14
*5	1	2.5 IN.	60 FT	-	EX (1) 9/*14
*6	1	2.5 IN.	11 FT	-	EX (1) 9/*14
*7	1	2.5 IN.	11 FT	-	EX (1) 9/*14
*9	1	2.5 IN.	47 FT	-	EX (3) 9/*14, EX (1) 4/*18, NEW (3) 4/*18, <REMOVE> (1) 4/*18
*10	1	2.5 IN.	15 FT	-	EX (1) 9/*14
*11	1	2.5 IN.	26 FT	-	EX (1) 9/*14
*12	1	2.5 IN.	35 FT	-	NEW (6) 4/*18, EX (6) 9/*14, EX (2) 4/*18, <REMOVE> (2) 4/*18
*13	1	2.5 IN.	13 FT	-	EX (2) 16/*14, EX (4) 4/*18
*14	1	2.5 IN.	234 FT	-	NEW (2) 4/*18
*15	1	2.5 IN.	55 FT	-	NEW (3) 4/*18, <REMOVE> (1) 4/*18
*16	1	2.5 IN.	7 FT	-	EX (1) 9/*14
*17	1	2.5 IN.	98 FT	-	NEW (3) 4/*18, EX (1) 9/*14, EX (1) 4/*18, <REMOVE> (1) 4/*18
*18	1	2.5 IN.	234 FT	-	NEW (2) 4/*18
*27	1	2.5 IN.	45 FT	-	EX (3) 9/*14, EX (1) 4/*18
*30	1	4.0 IN.	28 FT	-	EX (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*31	1	2.5 IN.	8 FT	-	EMPTY
*32	1	2.5 IN.	11 FT	-	EMPTY
*33	1	2.5 IN.	13 FT	-	EMPTY

\* DENOTES EXISTING CONDUIT  
 B - BORE, T - TRENCH, O - OPEN CUT

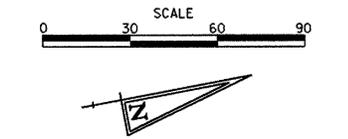
**SIGNAL NOTES**

- DETECTION - 40 MPH - 4 SECONDS PASSAGE TIME AT 235 FEET FROM STOP BAR.
- EXISTING LOOP DETECTORS:  
TYPE "2" - 6'x25' - SIDE STREET MOVEMENTS.
- PROPOSED LOOP DETECTORS:  
TYPE "1" - 6'x6' - TO BE INSTALLED ON SR 7 THROUGH MOVEMENTS.  
TYPE "2" - 6'x25' - TO BE INSTALLED ON SR 7 LEFT TURN MOVEMENTS.  
SYSTEM - 6'x6' - TO BE INSTALLED ALONG SR 7 RECEIVING LANES, AS SHOWN.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED, ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- UNCOVER PEDESTRIAN SIGNAL HEADS.
- REMOVE LOUVERS ON ALL SIDE STREET SIGNAL INDICATIONS. REMOVE SECOND SPAN WIRE ATTACHMENT TO THE BOTTOM OF THE SIGNAL HEADS WITH LOUVERS.
- RELOCATE SIGNAL HEADS TO FINAL LOCATIONS AS SHOWN.

**TRAFFIC SIGNAL PLAN ULTIMATE**



**DELAWARE DEPARTMENT OF TRANSPORTATION TRAFFIC SECTION**

SMYRNA		DELAWARE	
PERMIT NO. N-472	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 109 TOTAL 115

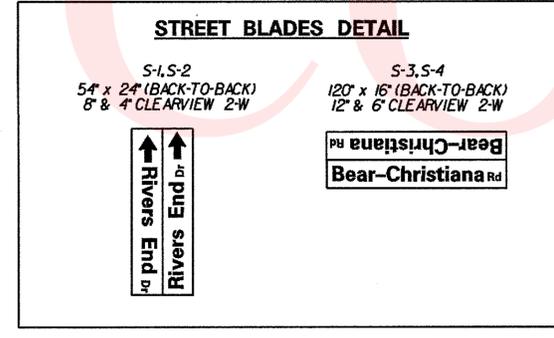
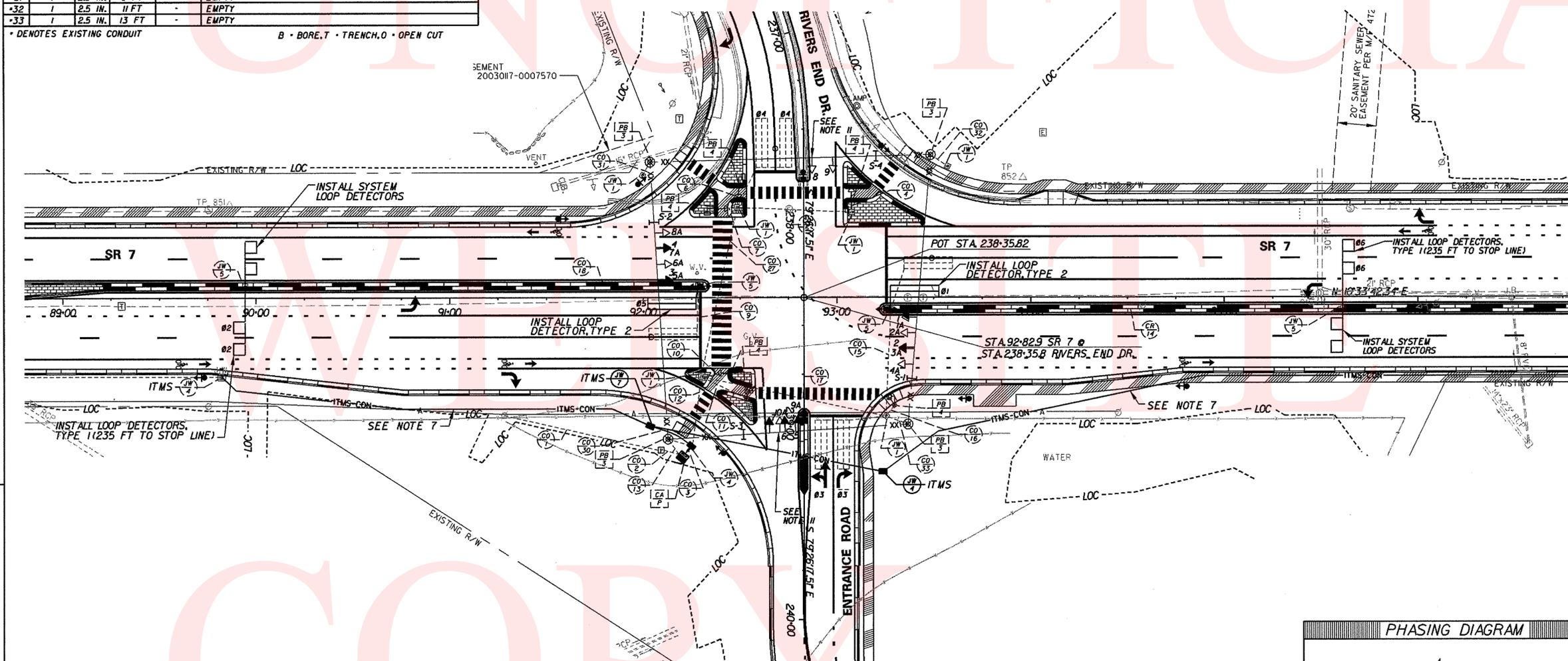
**SR 7 AT RIVERS END DRIVE/ BANK ONE ENTRANCE**

PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
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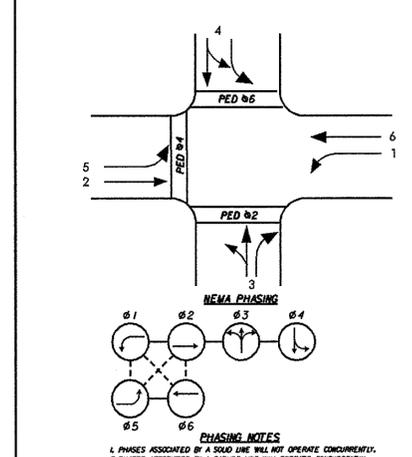
**REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (ULTIMATE)**

**LEGEND**

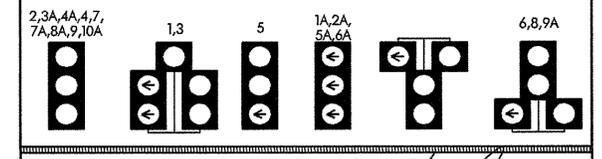
PROPOSED SIGNAL CABINET	RM C	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	RM O	REMOVE BY OTHERS
PROPOSED POLE BASE	AB	ABANDON
EXISTING POLE BASE	PB 3	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	PB 3	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTAL BASE	PB 3	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED WOOD POLE	JW 1	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	JW 1	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
UTILITY POLE	JW 1	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED JUNCTION WELL	CO 8	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
EXISTING JUNCTION WELL	CO 8	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED SIGNAL HEAD	OH 8	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	OH 8	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PEDESTRIAN SIGNAL HEAD	DR 8	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING PEDESTRIAN SIGNAL HEAD	DR 8	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PUSHBUTTON AND SIGN	MA XX	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PUSHBUTTON AND SIGN	MA XX	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED VIDEO DETECTION	CA F	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING VIDEO DETECTION	CA F	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED MICROWAVE DETECTION	CA F	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING MICROWAVE DETECTION	CA F	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
OVERHEAD SIGNING	SE	SERVICE PEDESTAL
PROPOSED OPTIKOM RECEIVER		
EXISTING OPTIKOM RECEIVER		
BACK GUY		
PROPOSED MAST ARM		
EXISTING MAST ARM		
PROPOSED LUMINAIRE		
EXISTING LUMINAIRE		
PROPOSED SPAN WIRE		
EXISTING SPAN WIRE		
RIGHT-OF-WAY OR PROPERTY LINE		
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
EXISTING LOOP DETECTOR		



**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7 2016	10/7/16
RECOMMENDED	20	10/7/16
RECOMMENDED	20	10/7/16

APPROVED TRAFFIC ENGINEER  
 APPROVED for INSTL. CHIEF TRAFFIC ENGINEER

DATE  
 DATE

PREL. TRACING DESIGN CHKD.

CONDUIT RUN SCHEDULE					
CO*	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
-1	1	2.0 IN.	9 FT	-	EX (112)*18 U.F.W./GROUND
-2	2	2.5 IN.	3 FT	-	<REMOVE> (144)*18, EX (114)*18, NEW(14)*18, NEW(216)*14, NEW(219)*14
3	1	2.5 IN.	12 FT	T	NEW(1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
10	1	2.5 IN.	6 FT	T	NEW(216)*14, NEW(144)*18
11	1	2.5 IN.	82 FT	B	NEW(119)*14
12	1	2.5 IN.	6 FT	T	NEW(119)*14, NEW(114)*18
-17	1	2.5 IN.	30 FT	-	NEW(1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
-20	1	2.5 IN.	90 FT	-	<REMOVE> (144)*18, NEW(114)*18
21	1	2.5 IN.	123 FT	B	NEW(219)*14, NEW(114)*18
22	1	2.5 IN.	7 FT	T	EMPTY
23	1	2.5 IN.	18 FT	T	EMPTY
24	1	2.5 IN.	10 FT	T	EMPTY

\* DENOTES EXISTING CONDUIT B - BORE, T - TRENCH, O - OPEN CUT

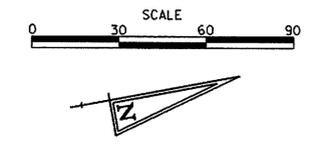
**SIGNAL NOTES**

- EXISTING LOOP DETECTORS:  
TYPE "2" - 6x25' - DELDOT MOVEMENTS.  
PROPOSED LOOP DETECTORS:  
TYPE "2" - 6x25' - TO BE INSTALLED ON SR7 LEFT TURN MOVEMENTS.  
TYPE "2" - 6x25' - TO BE INSTALLED ON CHRISTIANA MEADOWS THROUGH AND LEFT TURN MOVEMENTS.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL SIGNAL POLES SHALL BE 28 FEET. SPAN WIRES SHALL HAVE 5-PERCENT DISPLACEMENT (SAG) AT MIDPOINT, AS SHOWN IN THE SPAN WIRE SCHEDULE (FIELD ADJUSTMENTS AS REQUIRED).
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (SOJ OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- INSTALL LOUVERS ON ALL SIDE STREET SIGNAL INDICATORS. TEMPORARILY INSTALL A SECOND SPAN WIRE TO ATTACH TO THE BOTTOM OF THE SIGNAL HEADS WITH LOUVERS TO STABILIZE THE HEADS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 1**



**SR 7 AT DELDOT/ CHRISTIANA MEADOWS**

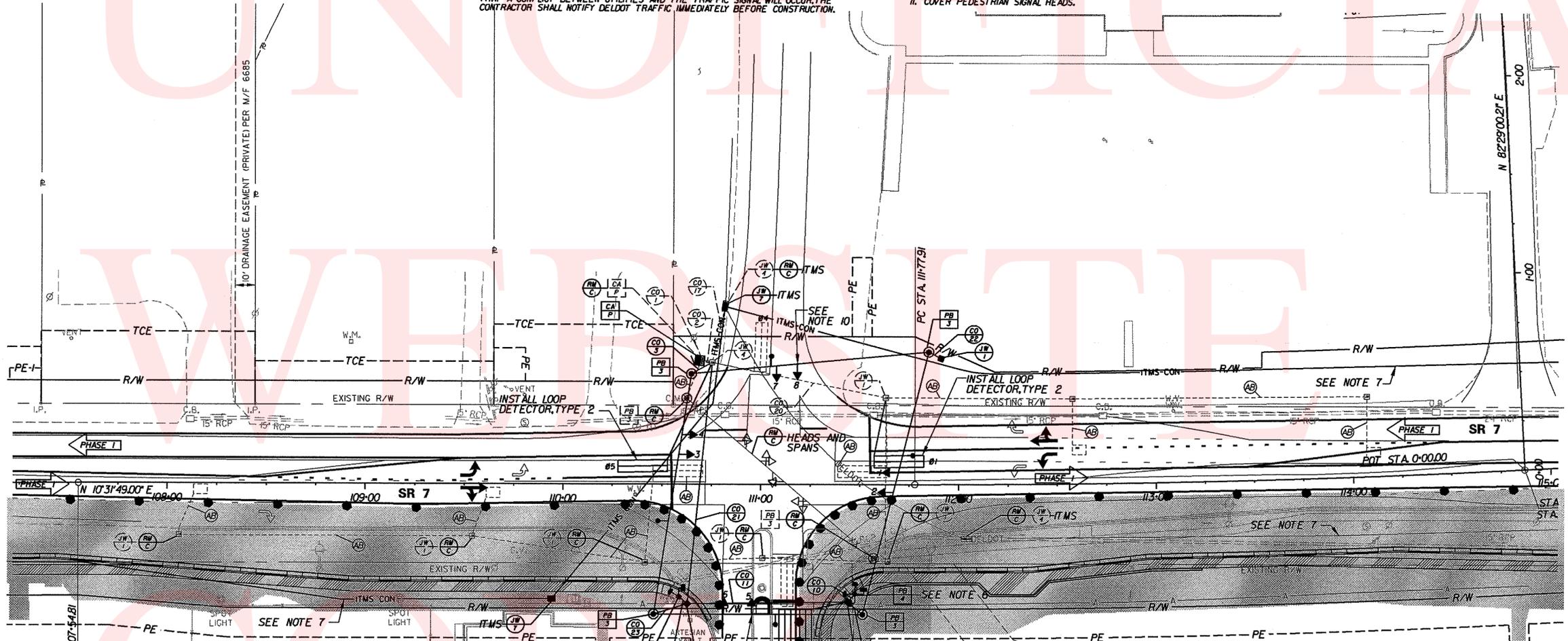
PREL. TRACING K. MANUEL DATE 04-15-06 DESIGN T. FOESTER DATE 04-15-06

**REVISION**

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 1)

**LEGEND**

	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY OTHERS
	PROPOSED POLE BASE		ABANDON
	PROPOSED PEDESTAL BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTAL BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED WOOD POLE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	UTILITY POLE		PROPOSED CONDUIT RUN IDENTIFIER (TYPE OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		EXISTING CONDUIT RUN IDENTIFIER (TYPE OF CONDUIT RUN)
	EXISTING JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
	EXISTING SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
	PROPOSED PUSHBUTTON AND SIGN		EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
	EXISTING PUSHBUTTON AND SIGN		SERVICE PEDESTAL
	PROPOSED VIDEO DETECTION		
	EXISTING VIDEO DETECTION		
	PROPOSED MICROWAVE DETECTION		
	EXISTING MICROWAVE DETECTION		
	OVERHEAD SIGNING		
	PROPOSED OPTICON RECEIVER		
	EXISTING OPTICON RECEIVER		
	BACK GUY		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED SPAN WIRE		
	EXISTING SPAN WIRE		
	RIGHT-OF-WAY OR PROPERTY LINE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR		



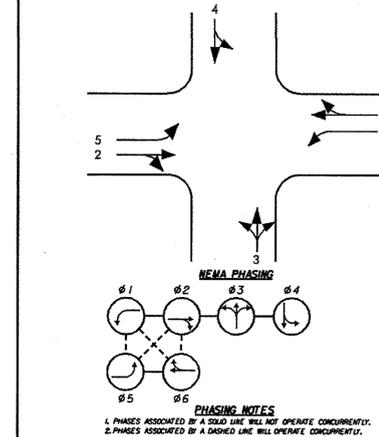
**MOT CONSTRUCTION SEQUENCE LEGEND**

	TEMPORARY CONSTRUCTION
	WORK AREA THIS PHASE
	EXISTING TRAFFIC FLOW AREA
	TEMPORARY WHITE PAVEMENT MARKING ARROW

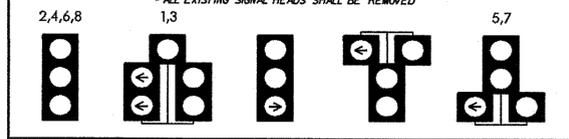
**SPAN WIRE SCHEDULE**

SPAN	LENGTH	SPAN MOUNT HEIGHT	5% SAG	SPAN MIDPOINT
NORTH	137 FT	28 FT	6.9 FT	21.1 FT
SOUTH	123 FT	28 FT	6.2 FT	21.8 FT
EAST	121 FT	28 FT	6.1 FT	21.9 FT
WEST	106 FT	28 FT	5.3 FT	22.7 FT

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED Oct 7 2012  
RECOMMENDED 20  
RECOMMENDED 20

APPROVED TRAFFIC ENGINEER [Signature] 10/7/10 DATE  
APPROVED for INSTL. CHIEF TRAFFIC ENGINEER [Signature] 10/7/10 DATE

CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
*1	1	2.0 IN.	9 FT	-	EX. (1) 2" x 8 U.F. W/GROUND
*2	2	2.5 IN.	3 FT	-	(REMOVE) 2 x 3/4" x 1/8, EX. 1/2" x 1/4, EX. 1/4" x 1/8, EX. 1/2" x 1/4, NEW 1/2" x 1/4, NEW 1/2" x 1/4
*3	1	2.5 IN.	12 FT	-	EX. (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
7	1	2.5 IN.	12 FT	T	NEW 1/2" x 1/4
8	1	2.5 IN.	10 FT	T	NEW 1/2" x 1/4
9	1	2.5 IN.	65 FT	B	NEW 1/2" x 1/4, NEW 1/2" x 1/4
*10	1	2.5 IN.	6 FT	-	EX. 1/2" x 1/4
*11	1	2.5 IN.	82 FT	-	EX. 1/2" x 1/4, EX. 1/2" x 1/4
*12	1	2.5 IN.	6 FT	-	EX. 1/2" x 1/4
15	1	2.5 IN.	19 FT	T	NEW 1/2" x 1/4
*17	1	2.5 IN.	30 FT	-	EX. (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*21	1	2.5 IN.	123 FT	-	EX. 1/2" x 1/4, EX. 1/2" x 1/4
*22	1	2.5 IN.	7 FT	-	EMPTY
*23	1	2.5 IN.	18 FT	-	EMPTY
*24	1	2.5 IN.	10 FT	-	EMPTY

\* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

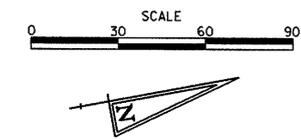
**SIGNAL NOTES**

- EXISTING LOOP DETECTORS:  
TYPE #2 - 6x25" - CHRISTIANA MEADOWS MOVEMENTS.  
PROPOSED LOOP DETECTORS:  
TYPE #2 - 6x25" - TO BE INSTALLED ON SR7 LEFT TURN MOVEMENTS.  
TYPE #2 - 6x25" - TO BE INSTALLED ON DELDOT.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC DIVISION, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET SCREWS, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (SO4 OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 2**



DELAWARE DEPARTMENT OF TRANSPORTATION  
**TRAFFIC SECTION**  
 SMYRNA DELAWARE  
 PERMIT NO. N-482 CONTRACT NO. 23-126-01 FED. AID NO. N/A SHEET NO. 111 TOTAL 115

**SR 7 AT DELDOT/ CHRISTIANA MEADOWS**

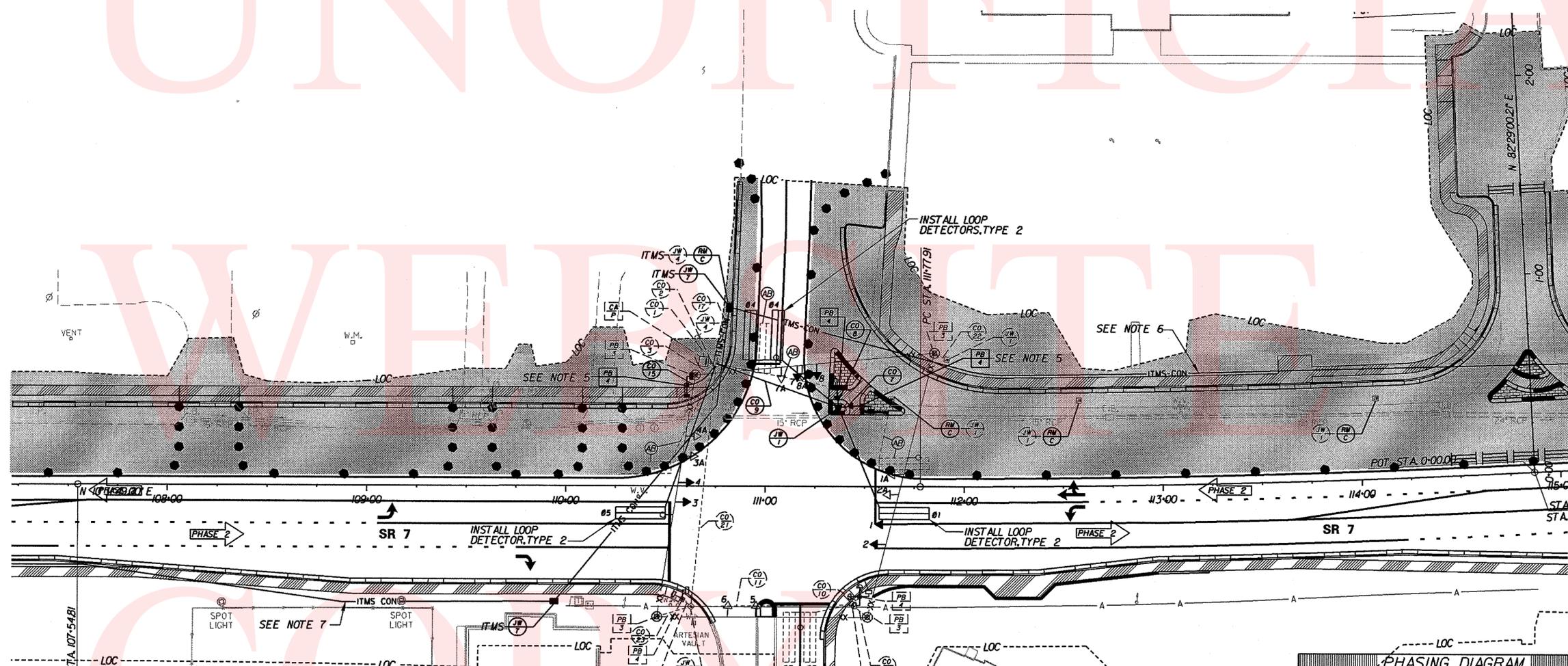
PREL. TRACING: K. MANUEL DATE: 04-15-06 DESIGN: T. FOESTER DATE: 04-15-06

**REVISION**

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 2)

**LEGEND**

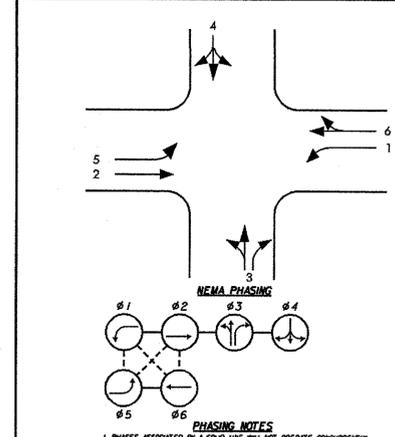
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY OTHERS
	PROPOSED POLE BASE		ABANDON
	EXISTING POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTAL BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTAL BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING WOOD POLE		PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	UTILITY POLE		EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING JUNCTION WELL		EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
	EXISTING PEDESTRIAN SIGNAL HEAD		EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
	PROPOSED PUSHBUTTON AND SIGN		SERVICE PEDESTAL
	EXISTING PUSHBUTTON AND SIGN		
	PROPOSED VIDEO DETECTION		
	EXISTING VIDEO DETECTION		
	PROPOSED MICROWAVE DETECTION		
	EXISTING MICROWAVE DETECTION		
	OVERHEAD SIGNING		
	PROPOSED OPTICON RECEIVER		
	EXISTING OPTICON RECEIVER		
	BACK CUT		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED SPAN WIRE		
	EXISTING SPAN WIRE		
	RIGHT-OF-WAY OR PROPERTY LINE		
	PROPOSED LOOP DETECTOR TYPE 1 OR 2		
	EXISTING LOOP DETECTOR		



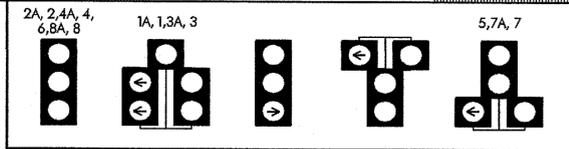
**MOT CONSTRUCTION SEQUENCE LEGEND**

	TEMPORARY CONSTRUCTION
	WORK AREA THIS PHASE
	EXISTING TRAFFIC FLOW AREA
	TEMPORARY WHITE PAVEMENT MARKING ARROW

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED Oct 7 2010 H. J. [Signature]  
 RECOMMENDED \_\_\_\_\_ 20 \_\_\_\_\_  
 RECOMMENDED \_\_\_\_\_ 20 \_\_\_\_\_

W. [Signature] 10/7/10  
 APPROVED TRAFFIC ENGINEER DATE  
[Signature] 10/7/10  
 APPROVED for INSTL. DATE  
 CHIEF TRAFFIC ENGINEER

CONDUIT RUN SCHEDULE					
CONDUIT NO.	NO. OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/WIRE
*1	1	2.0 IN.	9 FT	-	EX. 112/*8 U.F.W./GROUND
*2	2	2.5 IN.	3 FT	-	<REMOVE> 2#14/*18, EX. 1614/*18, EX. 1216/*14, EX. 1419/*14, NEW 1214/*18
*3	1	2.5 IN.	12 FT	-	EX. 11 SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*4	1	2.5 IN.	12 FT	-	EX. 1216/*14, EX. 1414/*18
*5	1	2.5 IN.	234 FT	T	EMPTY
*6	1	2.5 IN.	51 FT	B	NEW 114/*18
*7	1	2.5 IN.	12 FT	-	EX. 119/*14
*8	1	2.5 IN.	10 FT	-	EX. 1219/*14
*9	1	2.5 IN.	66 FT	-	<REMOVE> X114/*18, EX. 119/*14, EX. 114/*18, NEW 114/*18
*10	1	2.5 IN.	5 FT	-	EX. 119/*14
*11	1	2.5 IN.	82 FT	-	EX. 119/*14, EX. 114/*18
*12	1	2.5 IN.	6 FT	-	EX. 119/*14
*13	1	2.5 IN.	62 FT	B	NEW 114/*18
*14	1	2.5 IN.	20 FT	-	EX. 119/*14
*15	1	2.5 IN.	20 FT	-	EX. 119/*14
*16	1	2.5 IN.	234 FT	T	EMPTY
*17	1	2.5 IN.	30 FT	-	EX. 11 SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*18	1	2.5 IN.	68 FT	-	EX. 1219/*14, EX. 114/*18
*19	1	2.5 IN.	7 FT	-	EMPTY
*20	1	2.5 IN.	18 FT	-	EMPTY
*21	1	2.5 IN.	10 FT	-	EMPTY

\* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

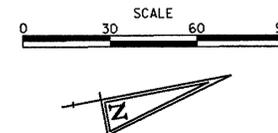
**SIGNAL NOTES**

- EXISTING LOOP DETECTORS: TYPE "2" - 6"x25" - SIDE STREET MOVEMENTS.
- PROPOSED LOOP DETECTORS: TYPE "2" - 6"x25" - TO BE INSTALLED ON SR7 STREET LEFT TURN MOVEMENTS. TYPE "2" - 6"x25" - TO BE INSTALLED ON SIDE STREET THROUGH AND LEFT TURN MOVEMENTS.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - COVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DELDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPLICING OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- COVER PEDESTRIAN SIGNAL HEADS.

**TRAFFIC SIGNAL PLAN PHASE 3**



DELAWARE DEPARTMENT OF TRANSPORTATION

**TRAFFIC SECTION**

SMYRNA DELAWARE

PERMIT NO. N-482	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 112	TOTAL 115
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**SR 7 AT DELDOT/ CHRISTIANA MEADOWS**

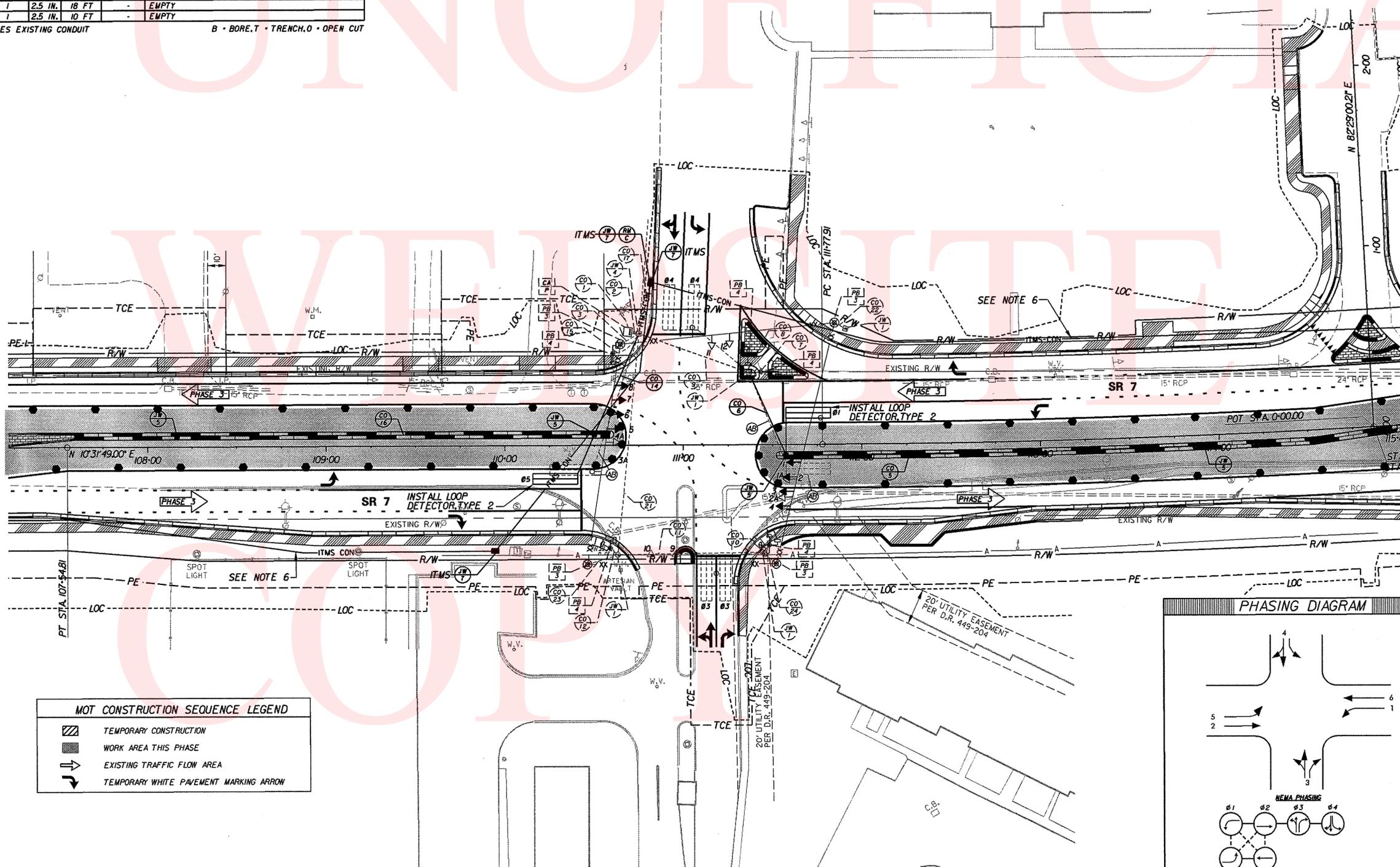
PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
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**REVISION**

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (PHASE 3)

**LEGEND**

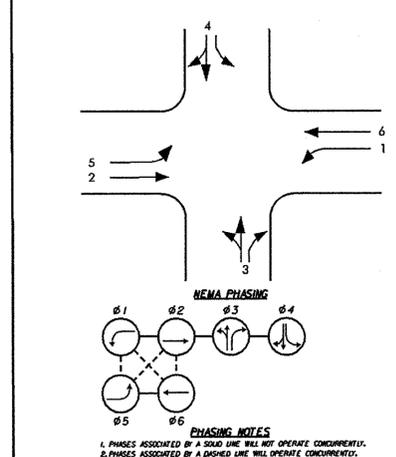
PROPOSED SIGNAL CABINET	(P)	REMOVE
EXISTING SIGNAL CABINET	(A)	ABANDON
PROPOSED POLE BASE	(PB)	REMOVE EXISTING CONCRETE BASE
EXISTING POLE BASE	(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTAL BASE	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED WOOD POLE	(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
UTILITY POLE	(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED JUNCTION WELL	(CR)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
EXISTING JUNCTION WELL	(CR)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED SIGNAL HEAD	(SH)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	(SH)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PEDESTRIAN SIGNAL HEAD	(SH)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING PEDESTRIAN SIGNAL HEAD	(SH)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED PUSHBUTTON AND SIGN	(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PUSHBUTTON AND SIGN	(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED VIDEO DETECTION	(CA)	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING VIDEO DETECTION	(CA)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED MICROWAVE DETECTION	(CA)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING MICROWAVE DETECTION	(CA)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
OVERHEAD SIGNING	(B)	METERED SERVICE PEDESTAL
PROPOSED OPTICOM RECEIVER		
EXISTING OPTICOM RECEIVER		
BACK GUY		
PROPOSED MAST ARM		
EXISTING MAST ARM		
PROPOSED LUMINAIRE		
EXISTING LUMINAIRE		
PROPOSED SPAN WIRE		
EXISTING SPAN WIRE		
RIGHT-OF-WAY OR PROPERTY LINE		
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
EXISTING LOOP DETECTOR		



**MOT CONSTRUCTION SEQUENCE LEGEND**

[Hatched Box]	TEMPORARY CONSTRUCTION
[Solid Grey Box]	WORK AREA THIS PHASE
[Arrow]	EXISTING TRAFFIC FLOW AREA
[Arrow]	TEMPORARY WHITE PAVEMENT MARKING ARROW

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7 2010	[Signature]
RECOMMENDED	20	
RECOMMENDED	20	

APPROVED TRAFFIC ENGINEER [Signature] 10/7/10 DATE

APPROVED for INSTL. CHIEF TRAFFIC ENGINEER [Signature] 10/7/10 DATE

PREL. TRACING DESIGN CHRD.

CONDUIT RUN SCHEDULE					
CONDUIT NO.	NO. OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/WIRE
*1	1	2.0 IN.	9 FT	-	EX1112/*8 U.F.W./GROUND
*2	2	2.5 IN.	3 FT	-	<REMOVE X214/*18, EX1614/*18, EX1216/*14, EX1419/*14, NEW1614/*18
*3	1	2.5 IN.	12 FT	-	EX1216/*14, EX1414/*18
*4	1	2.5 IN.	234 FT	-	NEW1214/*18
*5	1	2.5 IN.	51 FT	-	<REMOVE X114/*18, NEW1314/*18
*6	1	2.5 IN.	12 FT	-	EX1119/*14
*7	1	2.5 IN.	10 FT	-	<REMOVE X114/*18, EX1119/*14, EX1114/*18, NEW1314/*18
*8	1	2.5 IN.	66 FT	-	EX1119/*14
*9	1	2.5 IN.	5 FT	-	EX1119/*14
*10	1	2.5 IN.	82 FT	-	EX1119/*14, EX1114/*18
*11	1	2.5 IN.	6 FT	-	EX1119/*14
*12	1	2.5 IN.	62 FT	-	<REMOVE X114/*18, NEW1314/*18
*13	1	2.5 IN.	20 FT	-	EX1119/*14
*14	1	2.5 IN.	234 FT	-	NEW1214/*18
*15	1	2.5 IN.	30 FT	-	EX111 SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
*16	1	2.5 IN.	7 FT	-	EMPTY
*17	1	2.5 IN.	18 FT	-	EMPTY
*18	1	2.5 IN.	10 FT	-	EMPTY
*19	1	2.5 IN.	68 FT	-	EX1219/*14, EX1114/*18

\* DENOTES EXISTING CONDUIT B • BORE, T • TRENCH, O • OPEN CUT

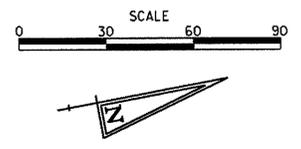
**SIGNAL NOTES**

- DETECTION - 40 MPH - 4 SECONDS PASSAGE TIME AT 235 FEET FROM STOP BAR.
- EXISTING LOOP DETECTORS:  
TYPE \*2 - 6"x25" - SIDE STREET MOVEMENTS.
- PROPOSED LOOP DETECTORS:  
TYPE \*1 - 6"x6" - TO BE INSTALLED ON SR 7 THROUGH MOVEMENTS.  
TYPE \*2 - 6"x25" - TO BE INSTALLED ALONG SR 7 RECEIVING LANES, AS SHOWN.
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**SIGNAL NOTES CONT.**

- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (SOJ) OR FLATTER LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- DEDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPLICING OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 382-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- UNCOVER PEDESTRIAN SIGNAL HEADS.
- REMOVE LOUVERS ON ALL SIDE STREET SIGNAL INDICATORS. REMOVE SECOND SPAN WIRE ATTACH TO THE BOTTOM OF THE SIGNAL HEADS WITH LOUVERS.
- RELOCATE SIGNAL HEADS TO FINAL LOCATIONS AS SHOWN.

**TRAFFIC SIGNAL PLAN ULTIMATE**



DELAWARE DEPARTMENT OF TRANSPORTATION  
**TRAFFIC SECTION**  
 SMYRNA DELAWARE  
 PERMIT NO. N-482 CONTRACT NO. 23-126-01 FED. AID NO. N/A SHEET NO. 113 TOTAL 115

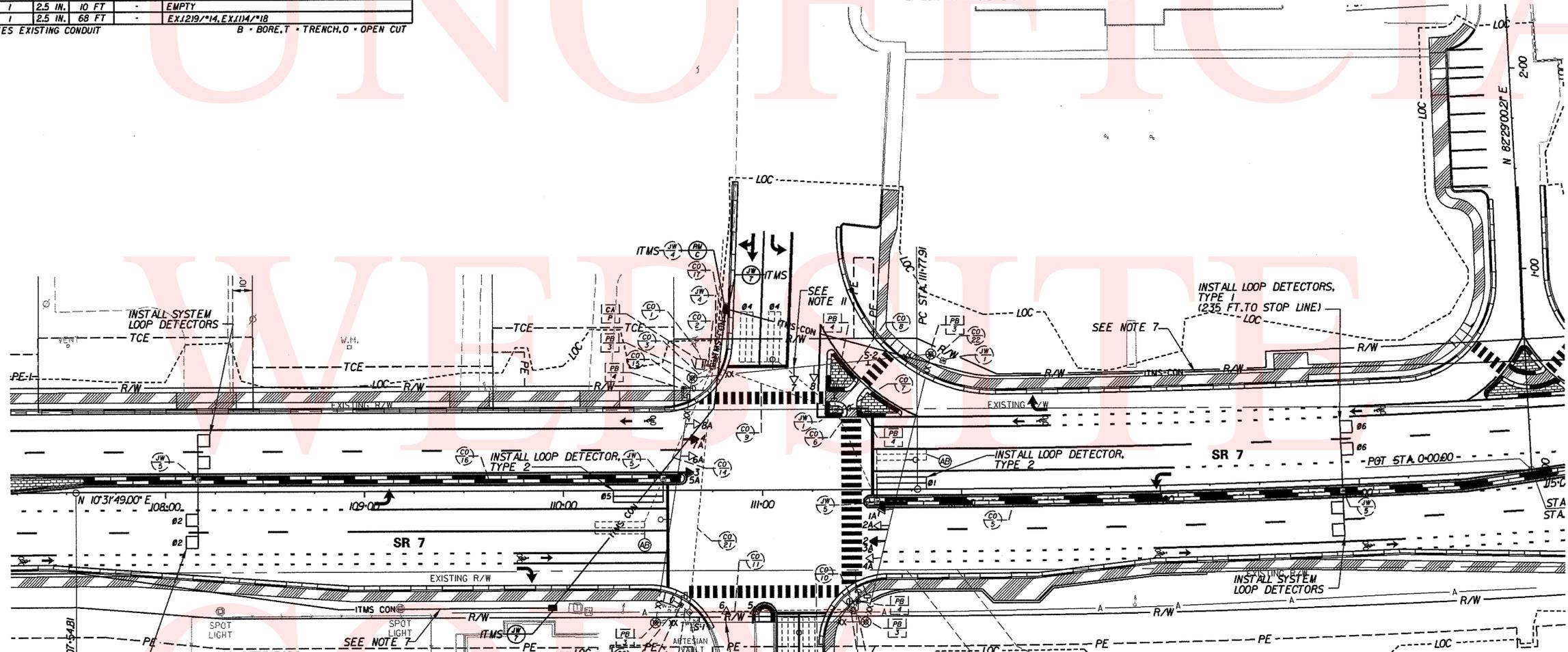
**SR 7 AT DELDOT/CHRISTIANA MEADOWS**

PREL. TRACING K. MANUEL DATE 04-15-06 DESIGN T. FOESTER DATE 04-15-06

REBUILD TRAFFIC SIGNAL DUE TO SR 7 WIDENING (ULTIMATE)

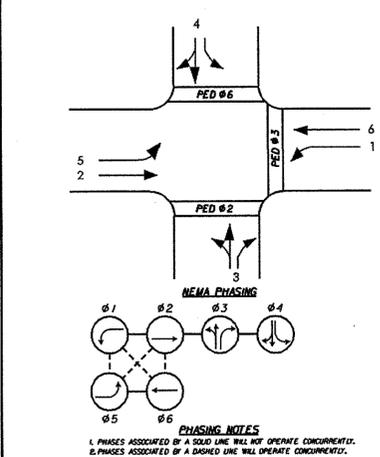
**LEGEND**

PROPOSED SIGNAL CABINET	(RM C)	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	(RM O)	REMOVE BY OTHERS
PROPOSED POLE BASE	(AB)	ABANDON
EXISTING POLE BASE	(PB 3)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	(PB 3)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTAL BASE	(PB 3)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED WOOD POLE	(JW 1)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	(JW 1)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
UTILITY POLE	(JW 1)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED JUNCTION WELL	(CJ 2)	PROPOSED CONDUIT RUN IDENTIFIER (TYPE OF CONDUIT RUN)
EXISTING JUNCTION WELL	(CJ 2)	EXISTING CONDUIT RUN IDENTIFIER (TYPE OF CONDUIT RUN)
PROPOSED SIGNAL HEAD	(OH 2)	PROPOSED OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	(OH 2)	EXISTING OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
PROPOSED PEDESTRIAN SIGNAL HEAD	(OH 2)	EXISTING OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
EXISTING PEDESTRIAN SIGNAL HEAD	(OH 2)	EXISTING OVERHEAD RUN IDENTIFIER (TYPE OF OVERHEAD RUN)
PROPOSED PUSHBUTTON AND SIGN	(MA XX)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PUSHBUTTON AND SIGN	(MA XX)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED VIDEO DETECTION	(CA F)	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING VIDEO DETECTION	(CA F)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED MICROWAVE DETECTION	(CA F)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING MICROWAVE DETECTION	(CA F)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
OVERHEAD SIGNING	(E)	SERVICE PEDESTAL
PROPOSED OPTIDOM RECEIVER		
EXISTING OPTIDOM RECEIVER		
BACK GUN		
PROPOSED MAST ARM		
EXISTING MAST ARM		
PROPOSED LUMINAIRE		
EXISTING LUMINAIRE		
PROPOSED SPAN WIRE		
EXISTING SPAN WIRE		
RIGHT-OF-WAY OR PROPERTY LINE		
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
EXISTING LOOP DETECTOR		

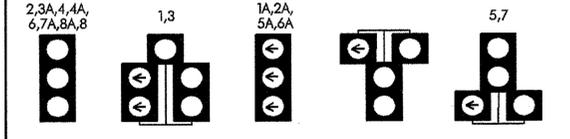


**STREET BLADES DETAIL**  
 S-I-S-2  
 120' x 16' (BACK-TO-BACK)  
 12' & 9' CLEARVIEW 2-W  
 Bear-Christiana Rd

**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED Oct 7 2010  
 RECOMMENDED \_\_\_\_\_  
 RECOMMENDED \_\_\_\_\_

APPROVED TRAFFIC ENGINEER 10/7/10 DATE  
 APPROVED for INSTL. 10/7/10 DATE  
 CHIEF TRAFFIC ENGINEER

PREL. TRACING DESIGN CHKD.

**SR 7 AT SCHOOL BELL ROAD**

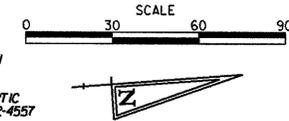
PREL. TRACING DATE 04-15-06 DESIGN T. FOESTER DATE 04-15-06

REVISION

**LEGEND**

PROPOSED SIGNAL CABINET	(Symbol)	REMOVE
EXISTING SIGNAL CABINET	(Symbol)	ABANDON
PROPOSED POLE BASE	(Symbol)	REMOVE EXISTING CONCRETE BASE
EXISTING POLE BASE	(Symbol)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	(Symbol)	EXISTING PEDESTAL BASE IDENTIFIER (TYPE OF PEDESTAL BASE)
EXISTING PEDESTAL BASE	(Symbol)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED WOOD POLE	(Symbol)	EXISTING WOOD POLE IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	(Symbol)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
UTILITY POLE	(Symbol)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED JUNCTION WELL	(Symbol)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING JUNCTION WELL	(Symbol)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED SIGNAL HEAD	(Symbol)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING SIGNAL HEAD	(Symbol)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED PEDESTRIAN SIGNAL HEAD	(Symbol)	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING PEDESTRIAN SIGNAL HEAD	(Symbol)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED PUSHBUTTON AND SIGN	(Symbol)	METERED SERVICE PEDESTAL
EXISTING PUSHBUTTON AND SIGN	(Symbol)	
PROPOSED VIDEO DETECTION	(Symbol)	
EXISTING VIDEO DETECTION	(Symbol)	
PROPOSED MICROWAVE DETECTION	(Symbol)	
EXISTING MICROWAVE DETECTION	(Symbol)	
OVERHEAD SIGNING	(Symbol)	
PROPOSED OPTICOM RECEIVER	(Symbol)	
EXISTING OPTICOM RECEIVER	(Symbol)	
BACK CUT	(Symbol)	
PROPOSED MAST ARM	(Symbol)	
EXISTING MAST ARM	(Symbol)	
PROPOSED LUMINAIRE	(Symbol)	
EXISTING LUMINAIRE	(Symbol)	
PROPOSED SPAN WIRE	(Symbol)	
EXISTING SPAN WIRE	(Symbol)	
RIGHT-OF-WAY OR PROPERTY LINE	(Symbol)	
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	(Symbol)	
EXISTING LOOP DETECTOR	(Symbol)	

**TRAFFIC SIGNAL PLAN**



**SIGNAL NOTES CONT.**

- INSTALLATION OF PROPOSED UTILITY POLE TO BE COMPLETED PRIOR TO SIGNAL CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANIES AND DELDOT TRAFFIC TO DETERMINE THE RELOCATION OF EXISTING UTILITY POLES AND REPLACEMENT OF AERIAL COMMUNICATION CABLE. POLES MUST BE INSTALLED OUTSIDE OF THE CLEAR ZONE.
- THE PROPOSED SIGNAL INSTALLATION SHALL BE COORDINATED WITH THE APPROPRIATE CONSTRUCTION PHASES. SEE CONSTRUCTION PHASING PLANS FOR ADDITIONAL DETAILS.
- DEDOT OIT SHALL COORDINATE THE SUPPLY, INSTALLATION, AND SPlicing OF ALL PROPOSED FIBER OPTIC CABLE AND INNERDUCT. THE CONTRACTOR SHALL CONTACT DELDOT OIT AT (302) 632-6431 OR (302) 392-4557 A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMPLETING THE ITMS JUNCTION WELL AND CONDUIT INSTALLATION TO SCHEDULE THE FIBER OPTIC CABLE AND INNERDUCT INSTALLATION.
- DURING THE PHASE 1, THE SIGNAL CABINET AND CONTROLLER SHALL BE INSTALLED ALONG WITH THE PROPOSED STRAIN POLES AND PEDESTRIAN SIGNALS ALONG THE EAST SIDE OF THE INTERSECTION. CONDUIT RUNS 1 THRU 4, 11, 12, 13, 15 & 17 SHALL BE INSTALLED ALONG WITH THE ASSOCIATED JUNCTION WELLS.
- DURING PHASE 2, THE STRAIN POLES, PEDESTRIAN SIGNALS AND JUNCTION WELLS SHALL BE CONSTRUCTED ALONG THE WEST SIDE OF THE INTERSECTION. CONDUIT RUNS 5 THRU 8.
- DURING PHASE 3, THE SPAN WIRE, SIGNAL HEADS AND OPTICOM RECEIVERS SHALL BE INSTALLED ALONG WITH CONDUIT RUNS 9, 10, 14, 16, 19 AND THE ASSOCIATED JUNCTION WELLS.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.

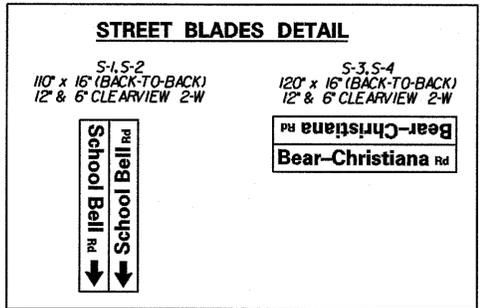
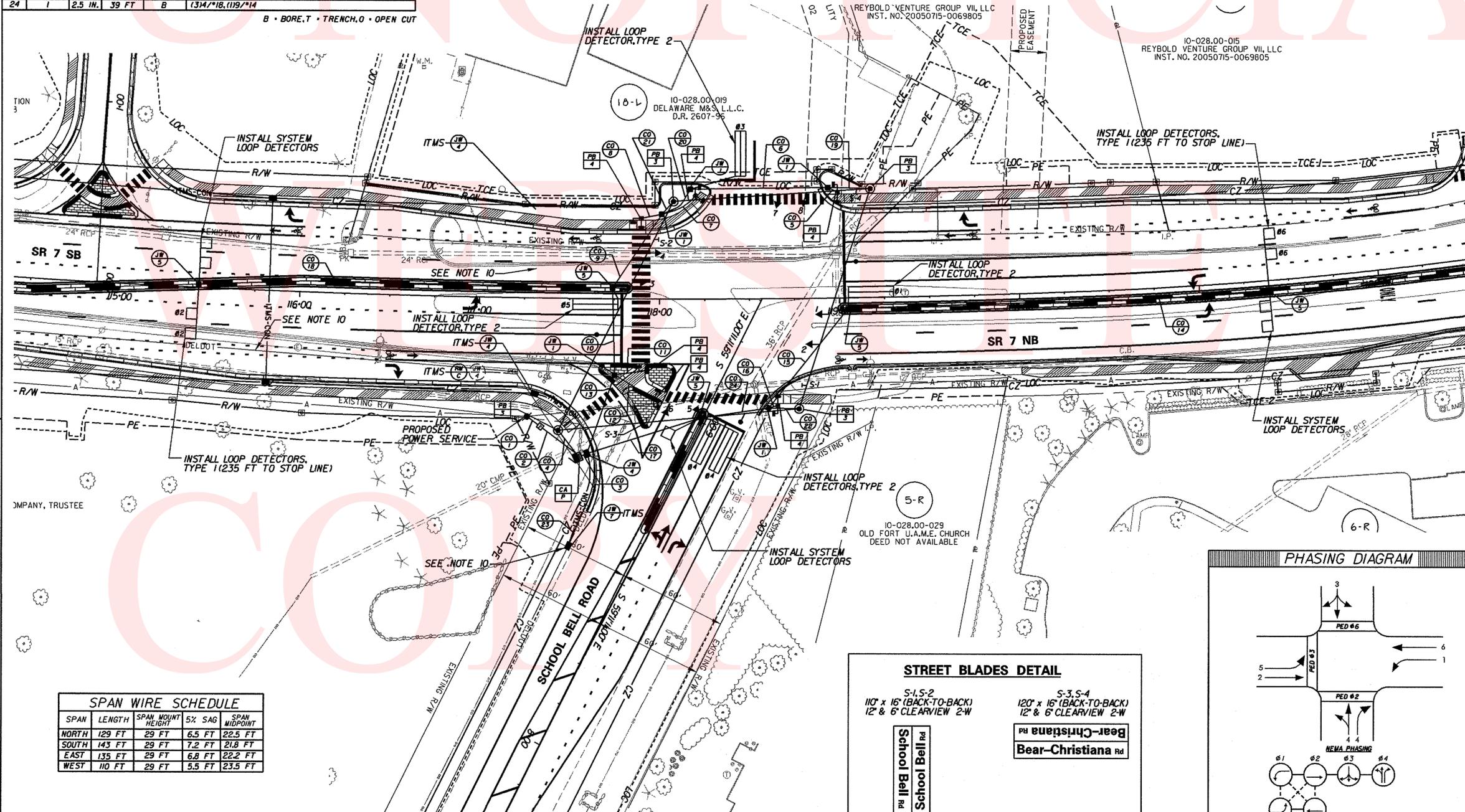
**SIGNAL NOTES**

- DETECTION - 40 MPH - 4 SECONDS PASSAGE TIME AT 235 FEET FROM STOP LINE.
- PROPOSED LOOP DETECTORS: TYPE "1" - 6'x6' TO BE INSTALLED ON SRT THROUGH MOVEMENTS AND TYPE "2" - 6'x25' TO BE INSTALLED ON SRT LEFT TURN MOVEMENTS AND ALL SIDE STREET MOVEMENTS. SYSTEM - 6'x6' TO BE INSTALLED ALONG SRT RECEIVING LANES, AS SHOWN.
- ALL SIGNAL POLES SHALL BE 30 FEET. SPAN WIRES SHALL HAVE 5-PERCENT DISPLACEMENT (SAG) AT MIDPOINT, AS SHOWN IN THE SPAN WIRE SCHEDULE (FIELD ADJUSTMENTS AS REQUIRED).
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50% OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.

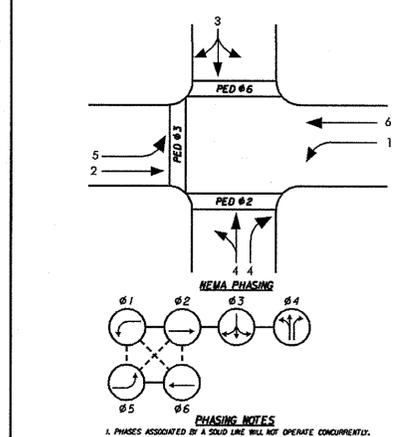
**CONDUIT RUN SCHEDULE**

CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE / WIRE
1	1	2 IN.	8 FT	T	NEW (1) 2" B U.F.W./GROUND
2	1	2 IN.	24 FT	T	NEW (1) 2" B U.F.W./GROUND
3	3	2.5 IN.	5 FT	T	NEW (4) 1/2" B, NEW (1) 1/4" B
4	1	4.0 IN.	5 FT	T	NEW (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
5	1	2.5 IN.	17 FT	T	NEW (2) 1/2" B, NEW (1) 3/4" B
6	1	2.5 IN.	6 FT	T	NEW (1) 1/2" B
7	1	2.5 IN.	77 FT	B	NEW (1) 1/2" B
8	1	2.5 IN.	18 FT	T	NEW (1) 1/2" B
9	1	2.5 IN.	12 FT	T	NEW (2) 1/2" B
10	1	2.5 IN.	49 FT	B	NEW (2) 1/2" B, NEW (1) 1/4" B
11	1	2.5 IN.	49 FT	B	NEW (4) 1/2" B, NEW (2) 1/2" B
12	1	2.5 IN.	18 FT	T	NEW (1) 1/2" B
13	1	2.5 IN.	26 FT	T	NEW (2) 1/2" B
14	1	2.5 IN.	46 FT	B	NEW (3) 1/2" B, NEW (1) 1/4" B
15	1	2.5 IN.	266 FT	T	NEW (2) 1/2" B
16	1	2.5 IN.	71 FT	B	NEW (3) 1/2" B
17	1	2.5 IN.	10 FT	T	NEW (1) 1/2" B
18	1	2.5 IN.	66 FT	B	NEW (5) 1/2" B, NEW (1) 1/2" B
19	1	2.5 IN.	230 FT	T	NEW (2) 1/2" B
20	1	2.5 IN.	24 FT	T	EMPTY
21	1	2.5 IN.	22 FT	T	NEW (1) 1/2" B, NEW (1) 1/4" B
22	1	2.5 IN.	10 FT	T	EMPTY
23	1	2.5 IN.	17 FT	T	EMPTY
24	1	4.0 IN.	54 FT	T	NEW (1) SINGLE-MODE FIBER OPTIC CABLE, 6 COUNT
25	1	2.5 IN.	39 FT	B	(3) 1/2" B, (1) 1/2" B

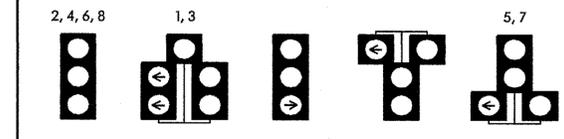
B = BORE, T = TRENCH, O = OPEN CUT



**PHASING DIAGRAM**



**SIGNAL HEAD DIAGRAM**



RECOMMENDED	Oct 7 2016	Handwritten Signature
RECOMMENDED	20	
RECOMMENDED	20	

APPROVED TRAFFIC ENGINEER  
 APPROVED FOR INSTL.  
 CHIEF TRAFFIC ENGINEER

DATE 10/7/16  
 DATE 10/7/16

PREL. TRACING DESIGN CHKD.

PERMIT NO. N-192	CONTRACT NO. 23-126-01	FED. AID NO. N/A	SHEET NO. 115	TOTAL 115
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SR 273 AT SR 7

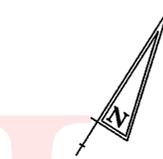
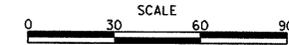
PREL. TRACING K. MANUEL	DATE 04-15-06	DESIGN T. FOESTER	DATE 04-15-06
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REVISION	
MODIFY TRAFFIC SIGNAL DUE TO SR 7 WIDENING	

LEGEND

PROPOSED SIGNAL CABINET	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	REMOVE BY OTHERS
PROPOSED POLE BASE	ABANDON
EXISTING POLE BASE	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	EXISTING PEDESTAL BASE IDENTIFIER (TYPE OF PEDESTAL BASE)
EXISTING PEDESTAL BASE	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED WOOD POLE	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
UTILITY POLE	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED JUNCTION WELL	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING JUNCTION WELL	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED SIGNAL HEAD	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING SIGNAL HEAD	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED PEDESTRIAN SIGNAL HEAD	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PEDESTRIAN SIGNAL HEAD	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED PUSHBUTTON AND SIGN	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING PUSHBUTTON AND SIGN	SERVICE PEDESTAL
PROPOSED VIDEO DETECTION	
EXISTING VIDEO DETECTION	
PROPOSED MICROWAVE DETECTION	
EXISTING MICROWAVE DETECTION	
OVERHEAD SIGNING	
PROPOSED OPTICOM RECEIVER	
EXISTING OPTICOM RECEIVER	
BACK GUY	
PROPOSED MAST ARM	
EXISTING MAST ARM	
PROPOSED LUMINAIRE	
EXISTING LUMINAIRE	
PROPOSED SPAN WIRE	
EXISTING SPAN WIRE	
RIGHT-OF-WAY OR PROPERTY LINE	
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	
EXISTING LOOP DETECTOR	

TRAFFIC SIGNAL PLAN



CONDUIT RUN SCHEDULE

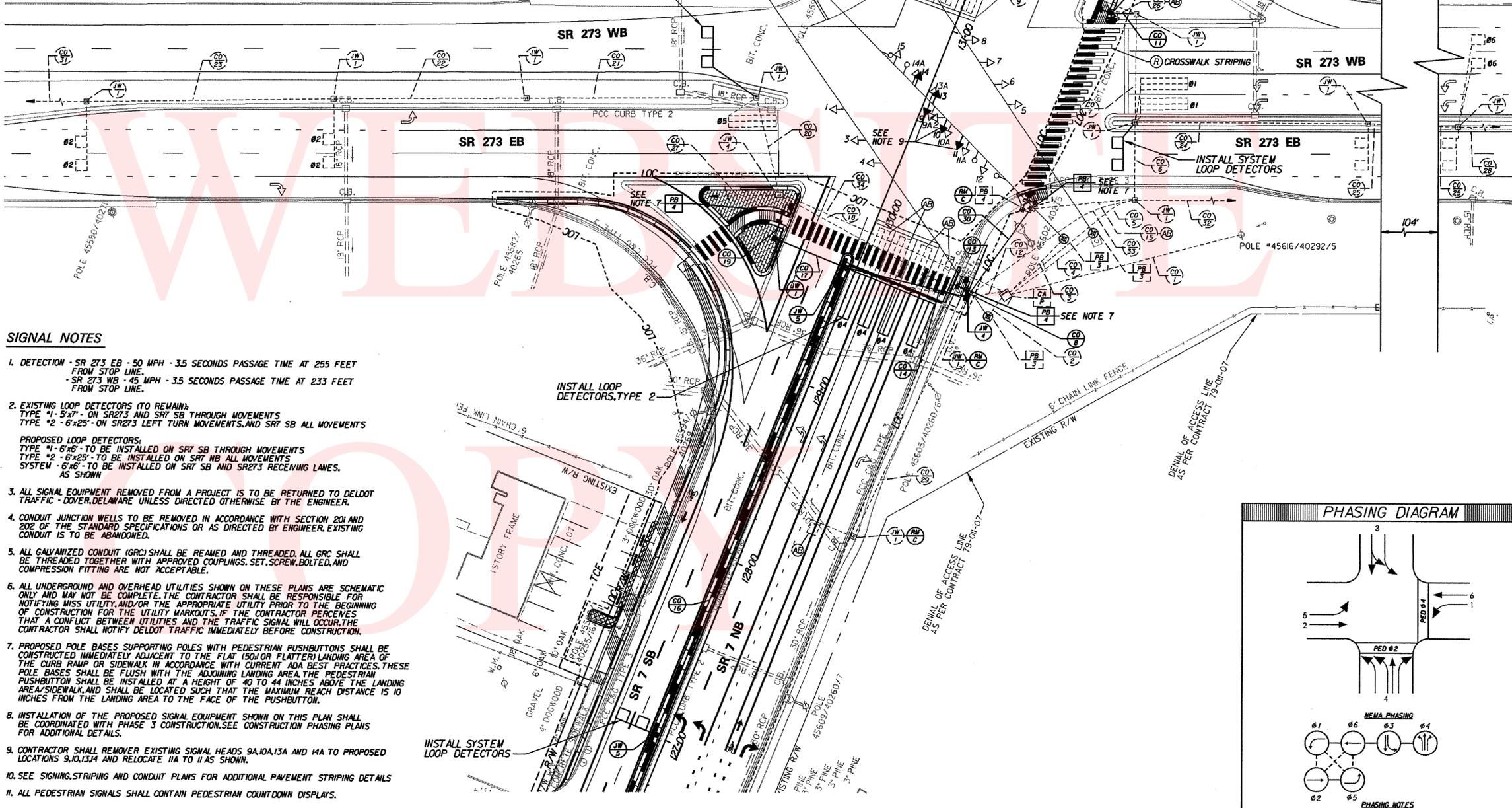
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE / WIRE
*1	1	2.0 IN.	125 FT	-	EX.(1)2/*18 U.F.W./GROUND
*2	1	2.5 IN.	13 FT	-	EX.(1)19/*14, EX.(2) COMM. CABLE
*3	1	2.5 IN.	40 FT	-	EX.(2)16/*14, EX.(1)4/*18
*4	1	2.5 IN.	52 FT	-	EX.(1)16/*14
*5	1	2.5 IN.	82 FT	-	<REMOVE>X219/*14, EX.(1)514/*18, EX. COMM. CABLE NEW(1)19/*14, NEW(1) 4/*18
*6	1	2.5 IN.	41 FT	-	<REMOVE>X119/*14, EX.(1)514/*18, NEW(1)19/*14, NEW(1) 4/*18
*7	1	2.5 IN.	55 FT	-	<REMOVE>X119/*14, EX.(2)14/*18, NEW(1)19/*14
8	1	2.5 IN.	4 FT	T	NEW(1)19/*14
*9	1	2.5 IN.	71 FT	-	EX.(2)14/*18
*10	1	2.5 IN.	171 FT	-	EX.(1)14/*18 CUT
*11	1	2.5 IN.	14 FT	O	NEW(1)19/*14
*12	1	2.5 IN.	20 FT	-	<REMOVE>X214/*18, NEW(1)314/*18, NEW(2)19/*14
*13	1	2.5 IN.	3 FT	T	NEW(1)19/*14
*14	1	2.5 IN.	66 FT	B	NEW(1)314/*18, NEW(1)19/*14
*15	1	1.5 IN.	58 FT	-	<ABANDON>X119/*14
*16	1	2.5 IN.	63 FT	T	NEW(1)14/*18
*17	1	2.5 IN.	40 FT	B	NEW(1)14/*18
*18	1	4.0 IN.	109 FT	-	EX.(2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT
*19	1	2.5 IN.	15 FT	B	NEW(1)19/*14
*20	1	2.5 IN.	52 FT	-	EX.(1)314/*18, EX.(1) COMM. CABLE, NEW(1)14/*18
*21	1	2.5 IN.	121 FT	-	EX.(2)14/*18, EX.(1) COMM. CABLE
*22	1	2.5 IN.	101 FT	-	EX.(2)14/*18, EX.(1) COMM. CABLE
*23	1	2.5 IN.	130 FT	-	EX.(1)14/*18, EX.(1) COMM. CABLE
*24	1	2.5 IN.	123 FT	-	EX.(2)14/*18
*25	1	2.5 IN.	122 FT	-	EX.(1)314/*18
*26	1	1.5 IN.	14 FT	-	<ABANDON>X119/*14
*27	1	4.0 IN.	XX FT	-	EX.(2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT
*28	1	2.5 IN.	140 FT	-	EX.(1)314/*18

CONDUIT RUN SCHEDULE CONT.

CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE / WIRE
*29	1	1.5 IN.	136 FT	-	<REMOVE>X114/*18
*30	1	2.5 IN.	61 FT	T	NEW (1)19/*14
*31	1	2.5 IN.	XX FT	-	EX.(1) COMM. CABLE
*32	1	2.5 IN.	XX FT	-	EX.(1) COMM. CABLE
*33	1	4.0 IN.	XX FT	-	EX.(2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT
*34	1	2.5 IN.	103 FT	-	EX.(2) SINGLE-MODE FIBER OPTIC CABLE, 48 COUNT
*35	1	4.0 IN.	126 FT	-	EX.(1)314/*18, NEW(1)14/*18, EX. COMM. CABLE EX.(1) SINGLE FIBER OPTIC CABLE, 24 COUNT

\* DENOTES EXISTING CONDUIT  
B = BORE, T = TRENCH, O = OPEN CUT

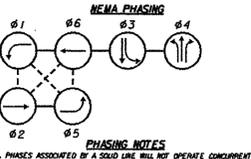
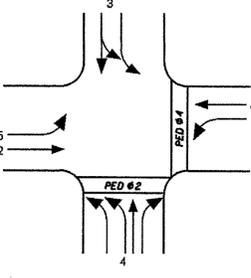
\* DENOTES EXISTING CONDUIT  
BRUSH B = BORE, T = TRENCH, O = OPEN CUT



SIGNAL NOTES

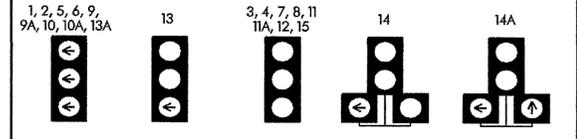
- DETECTION - SR 273 EB - 50 MPH - 3.5 SECONDS PASSAGE TIME AT 255 FEET FROM STOP LINE.  
- SR 273 WB - 45 MPH - 3.5 SECONDS PASSAGE TIME AT 233 FEET FROM STOP LINE.
- EXISTING LOOP DETECTORS (TO REMAIN):  
TYPE \*1 - 5'x7' - ON SR273 AND SR7 SB THROUGH MOVEMENTS  
TYPE \*2 - 6'x25' - ON SR273 LEFT TURN MOVEMENTS, AND SR7 SB ALL MOVEMENTS  
PROPOSED LOOP DETECTORS:  
TYPE \*1 - 6'x6' - TO BE INSTALLED ON SR7 SB THROUGH MOVEMENTS  
TYPE \*2 - 6'x25' - TO BE INSTALLED ON SR7 NB ALL MOVEMENTS  
SYSTEM - 6'x6' - TO BE INSTALLED ON SR7 SB AND SR273 RECEIVING LANES, AS SHOWN
- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET, SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (SOLO OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJACING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED AT A HEIGHT OF 40 TO 44 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON.
- INSTALLATION OF THE PROPOSED SIGNAL EQUIPMENT SHOWN ON THIS PLAN SHALL BE COORDINATED WITH PHASE 3 CONSTRUCTION. SEE CONSTRUCTION PHASING PLANS FOR ADDITIONAL DETAILS.
- CONTRACTOR SHALL REMOVE EXISTING SIGNAL HEADS 9A, 10A, 13A AND 14A TO PROPOSED LOCATIONS 9, 10, 13, 14 AND RELOCATE 11A TO 11 AS SHOWN.
- SEE SIGNING, STRIPING AND CONDUIT PLANS FOR ADDITIONAL PAVEMENT STRIPING DETAILS
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.

PHASING DIAGRAM



PHASING NOTES  
1. PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.  
2. PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.

SIGNAL HEAD DIAGRAM



RECOMMENDED	Oct 7 2010	Hand Signed
RECOMMENDED	20	
RECOMMENDED	20	

APPROVED TRAFFIC ENGINEER  
*[Signature]*  
DATE 10/7/10

APPROVED for INSTL.  
CHIEF TRAFFIC ENGINEER  
*[Signature]*  
DATE 10/7/10

PREL. TRACING DESIGN CHKD.