

GENERAL LOCATION OF CONTRACT

# THE STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY  
UNITS



**FINAL PLANS**

CONSTRUCTION PLANS FOR:

## HSIP SC, IRON BRANCH ROAD/STATE STREET

CONTRACT NUMBER: **T200800903**  
FEDERAL AID PROJECT NUMBER: **ESTP-S285(03)**

COUNTY: **SUSSEX** M.R. #: **326/331**

DESIGN DESIGNATION: IRON BRANCH ROAD		
FUNCTIONAL CLASS: MAJOR COLLECTOR	D.H.V. PROJECTED: 520	YEAR: 2040
TYPE OF CONSTRUCTION: SAFETY IMPROVEMENTS	DESIGN SPEED: 35 M.P.H.	
A.A.D.T. CURRENT: 4462	YEAR: 2008	TRUCKS: 13 %
A.A.D.T. PROJECTED: 7200	YEAR: 2040	DIRECTION OF DISTRIBUTION: 60 %
DESIGN DESIGNATION: STATE STREET		
FUNCTIONAL CLASS: MAJOR COLLECTOR	D.H.V. PROJECTED: 400	YEAR: 2040
TYPE OF CONSTRUCTION: SAFETY IMPROVEMENTS	DESIGN SPEED: 35 M.P.H.	
A.A.D.T. CURRENT: 3441	YEAR: 2008	TRUCKS: 13 %
A.A.D.T. PROJECTED: 5500	YEAR: 2040	DIRECTION OF DISTRIBUTION: 60 %

**INDEX OF SHEETS**

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TOTAL SHEETS: 48

**APPROVED DESIGN EXCEPTIONS**

DESIGN PARAMETER	REQUIRED	PROVIDED	DATE

**ADDENDA & REVISIONS**

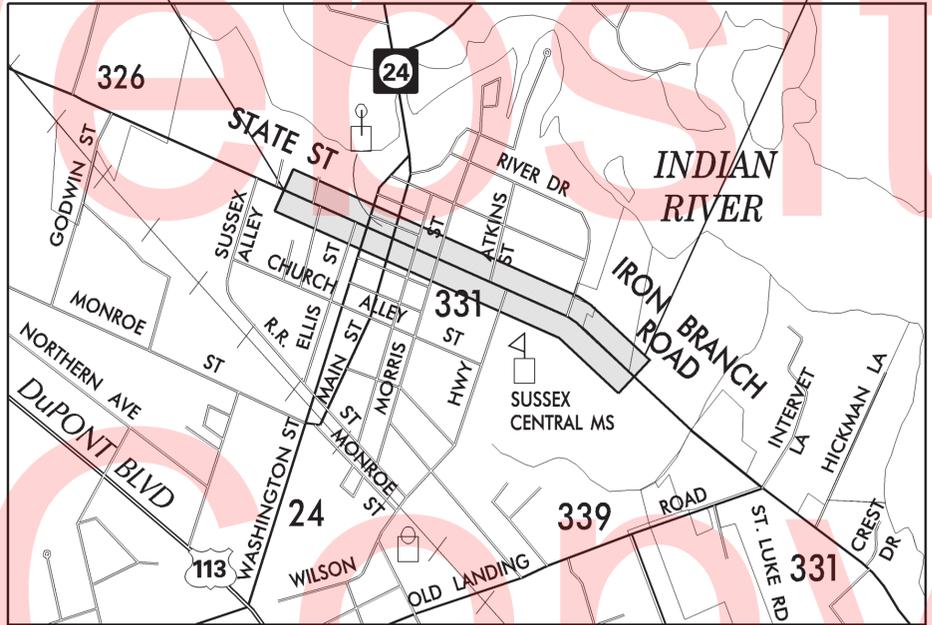
DESCRIPTION	NAME & DATE

**ASSOCIATED CONTRACTS**

CONTRACT NO.	CONTRACT NAME
202	MILLSBORO SCHOOL STREET
717	DAGSBORO TO MILLSBORO
1623	MAIN AND WASHINGTON STREETS, MILLSBORO
66-07-004	MILLSBORO SCHOOL SIDEWALK
67-10-025	ROAD 331 - DAGSBORO TO MILLSBORO

**BEGIN CONTRACT  
STATION 11 + 75.00**

**END CONTRACT  
STATION 34 + 50.00**



LOCATION MAP  
NOT TO SCALE

PREPARED BY  
THE CONSULTING FIRM OF

RECOMMENDED \_\_\_\_\_ DATE \_\_\_\_\_

SEAL

**RECOMMENDED**

*[Signature]* 06/30/2016  
SQUAD MANAGER, CONSTRUCTION DATE

*[Signature]* 06/30/2016  
GROUP ENGINEER, CONSTRUCTION DATE

*[Signature]* 07/01/2016  
ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS (CONSTRUCTION) DATE

**RECOMMENDED**

*[Signature]*  
STORMWATER ENGINEER

DATE 06/30/2016



**RECOMMENDED**

*[Signature]*

SQUAD MANAGER, TRANSPORTATION SOLUTIONS (PROJECT DEVELOPMENT OR BRIDGE DESIGN)

DATE 06/29/2016



**RECOMMENDED**

BRIDGE DESIGN ENGINEER

DATE \_\_\_\_\_

**RECOMMENDED**

DATE \_\_\_\_\_

**RECOMMENDED**

*[Signature]*

GROUP ENGINEER, PROJECT DEVELOPMENT

DATE 07/01/2016



**RECOMMENDED**

*[Signature]*

ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS

DATE 07/01/2016

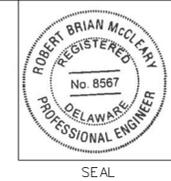


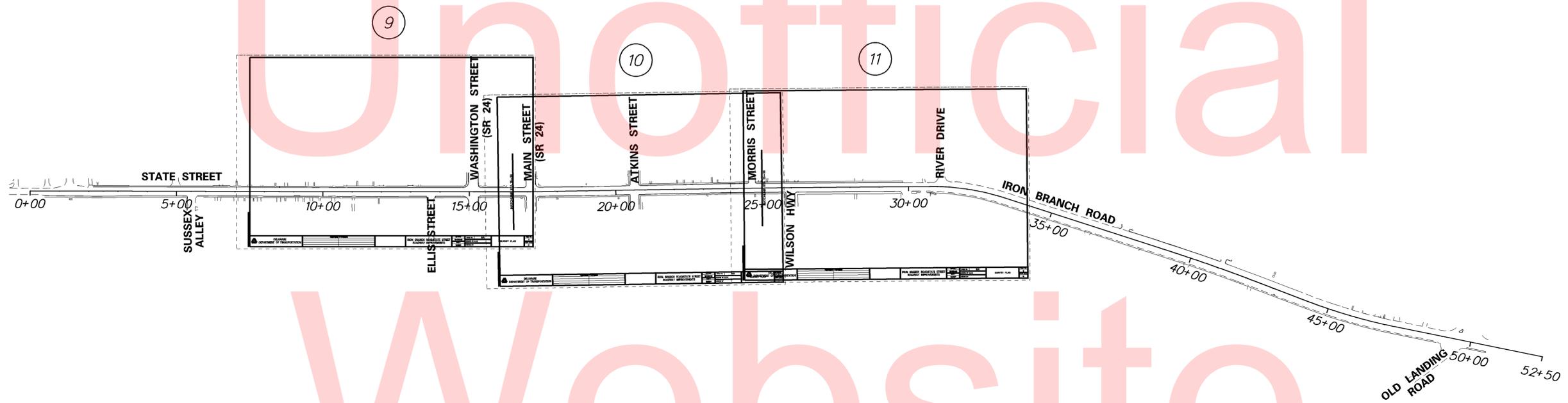
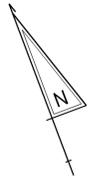
**APPROVED**

*[Signature]*

CHIEF ENGINEER

DATE 07/01/2016

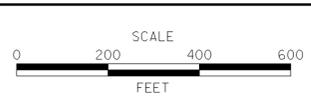




PLAN SHEET INDEX CROSS REFERENCE			
CONSTRUCTION PLAN	9	10	11
PROFILES	12	12	13
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UTILITY RELOCATION PLAN	34	35	36
SIGNING, STRIPING AND CONDUIT PLAN	37	38	39

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ADDENDUMS / REVISIONS	



CONTRACT T200800903	BRIDGE NO. <b>N/A</b>
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

## EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE INLET
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

MANMADE ROADSIDE FEATURES	
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

RIGHT-OF-WAY SYMBOLS	
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE
	HISTORIC RIGHT-OF-WAY BASELINE
	EXISTING RIGHT-OF-WAY
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING DENIAL OF ACCESS
	EXISTING R/W & DENIAL OF ACCESS

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC METER
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER VENT OR CLEANOUT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE GUY WIRE ANCHOR
	UTILITY POLE
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD
	MANHOLE - UNDETERMINED OWNER

UTILITY COMPANY FACILITIES	
	CHESAPEAKE UTILITIES - GAS
	DELMARVA POWER - ELECTRIC
	TOWN OF MILLSBORO - SEWER
	TOWN OF MILLSBORO - WATER
	VERIZON - CABLE
	VERIZON - FIBER OPTIC

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	BRICK PATTERNED SURFACE
	BUTT JOINT
	CONSTRUCTION BASELINE
	CONSTRUCTION SAFETY FENCE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CLEAR ZONE
	DRAINAGE INLET
	CURB OPENING
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPE 1
	GUARDRAIL, TYPE 2
	GUARDRAIL, TYPE 3
	GUARDRAIL END ANCHORAGE
	GUARDRAIL END TREATMENT, TYPE 1
	GUARDRAIL END TREATMENT, TYPE 2
	GUARDRAIL END TREATMENT, TYPE 3
	HORIZONTAL CLEARANCE
	IMPACT ATTENUATOR
	JUNCTION BOX - DRAINAGE
	LIMIT OF CONSTRUCTION
	MAILBOX
	MANHOLE
	PAVEMENT PATCH
	PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK - 4"
	P.C.C. SIDEWALK - 6" (USE 8" DEPTH FOR CHANNELIZATION ISLANDS.)
	UNDERDRAIN
	UNDERDRAIN OUTLET
	UTILITY TRANSFORMER

LANDSCAPING	
	LANDSCAPE PLANTINGS
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

## PROPOSED SYMBOLS

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	CONSTRUCTION SAFETY FENCE
	DRAINAGE INLET
	DO NOT DISTURB
	ENERGY DISSIPATOR
	FENCE
	FLARED END SECTION
	FILL WITH FLOWABLE FILL
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY OTHERS
	UNDERDRAIN / LENGTH
	UNDERDRAIN OUTLET PIPE
	SIGNING

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE

PAVEMENT SECTION(S)	
	OVERLAY PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	FULL DEPTH PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	DRIVEWAY AND ENTRANCE PAVEMENT - 2" BITUMINOUS CONCRETE PAVEMENT, TYPE C 8" GABC, TYPE B

UTILITY COMPANY FACILITIES	
	DELMARVA POWER - ELECTRIC
	TOWN OF MILLSBORO - WATER
	VERIZON - FIBER OPTIC
	MEDIACOM - CABLE

EROSION & SEDIMENT CONTROL	
	DEWATERING BAG
	DEWATERING BASIN
	EARTH DIKE
	INLET SEDIMENT CONTROL
	PERIMETER DIKE/SWALE
	PORTABLE SEDIMENT TANK
	SANDBAG DIKE
	SANDBAG DIVERSION
	STONE CHECK DAM
	STABILIZED CONSTRUCTION ENTRANCE
	SILT FENCE / LENGTH
	SILT FENCE
	SILT FENCE - REINFORCED
	SUMP PIT
	SEDIMENT TRAP / NUMBER
	SEDIMENT TRAP
	SEDIMENT TRAP WITH INLET AS OUTLET
	SEDIMENT TRAP PIPE OUTLET
	STILLING WELL
	TEMPORARY SWALE
	TEMPORARY SLOPE DRAIN
	TURBIDITY CURTAIN / LENGTH
	TURBIDITY CURTAIN

LAST REVISED: 04/18/16  
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# GENERAL NOTES

1. THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.

EROSION POTENTIAL FOR THIS PROJECT	CONTRACTOR ESC SUPERVISOR REQUIREMENT
( ) INSIGNIFICANT	NONE
( ) MINOR	CONTRACTOR TRAINING PROGRAM, AS DEFINED IN SECTION 6.2 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
(X) MEDIUM	CONTRACTOR TRAINING PROGRAM, AS DEFINED IN SECTION 6.2 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
( ) MAJOR	CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 6.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.

3. ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR, INCLUDE:

( )	NONE
(X)	ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
(X)	ALL PLAN SHEETS, IN PDF FORMAT.
( )	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
( )	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
( )	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

NOTE: THE DOCUMENT ENTITLED "RELEASE FOR DELIVERY OF DOCUMENTS IN ELECTRONIC FORM TO A CONTRACTOR" MUST BE SIGNED BY ALL PARTIES PRIOR TO THE DELIVERY OF ANY ELECTRONIC PROJECT FILES.

4. PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR, INCLUDE:

(X)	CROSS SECTIONS
(X)	RIGHT-OF-WAY PLANS (WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR)

5. AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

( )	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
(X)	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR IS INCIDENTAL TO ITEM 743000.
( )	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR SHALL BE PAID FOR UNDER ITEM 743031.

6. THE DISTURBED AREA FOR THIS PROJECT IS 2.64 ACRES.

7. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS ARE VALID FOR A THREE YEAR PERIOD, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR WILL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS. THE STORMWATER ENGINEER WILL REVIEW THE CURRENT SEDIMENT AND STORMWATER MANAGEMENT PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.

## PROJECT NOTES

### SECTION 100

1. ANY DAMAGE TO ITEMS NOTED TO BE RELOCATED OR RESET BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

### SECTION 200

2. THE CONTRACTOR SHALL REMOVE AND RESET ALL MAILBOXES TO MAINTAIN MAIL SERVICE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL RELOCATE MAILBOXES AS REQUIRED BY THE PROPOSED GEOMETRICS AND AS DIRECTED BY THE ENGINEER COMPLYING WITH THE AASHTO ROADSIDE DESIGN GUIDE AND THE AMERICANS WITH DISABILITIES ACT. WHEN RELOCATING MAILBOXES IN CURBED SECTIONS, THE FACE OF THE MAILBOX SHALL BE FLUSH WITH THE BACK EDGE OF CURB. WHEN RELOCATING MAILBOXES IN OPEN SECTIONS, THE FACE OF THE MAILBOX SHALL BE SET BACK 8 INCHES FROM THE EDGE OF THE PAVED SHOULDER. THE BOTTOM OF THE MAILBOX SHALL BE SET 42 TO 48 INCHES ABOVE THE ROADWAY SURFACE. MAILBOXES LOCATED AT DRIVEWAY ENTRANCES SHALL BE PLACED ON THE FAR SIDE OF THE DRIVEWAY IN THE DIRECTION OF TRAVEL. POSTS BEING RESET IN CONCRETE SIDEWALK SHALL BE PLACED IN AN APPROPRIATE SIZE PVC SLEEVE. ACCEPTABLE POST SHALL BE 4 INCH X 4 INCH WOOD POST OR 4 INCH DIAMETER WOOD POST. FOR RELOCATING MULTIPLE MAILBOXES TOGETHER ALL POSTS SHALL BE SEPARATED BY A DISTANCE OF 0.75 OF THEIR FULL HEIGHT ABOVE THE GROUND. NO MAILBOXES WITHIN THE CLEAR ZONE SHALL BE PLACED ON A BOARD WITH MULTIPLE MAILBOXES. EACH MAILBOX SHALL BE PLACED ON AN INDIVIDUAL POST MEETING THE CRITERIA ABOVE. ALL MAILBOXES SHALL BE SET NOT TO IMPEDE A MINIMUM OF 32" PAR (PEDESTRIAN ACCESS ROUTE). IF MAILBOXES ARE NOT SET IN ACCORDANCE WITH THE ABOVE DIRECTIONS, RESETTING OF THE MAILBOXES WILL BE AT THE COST OF THE CONTRACTOR. COST FOR ALL WORK AND MATERIALS SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.

3. IN AREAS WHERE TREES OR SHRUBS WILL BE OVERHANGING THE PROPOSED SIDEWALK, PRUNING MAY BE NECESSARY TO ACHIEVE A VERTICAL CLEAR SPACE OF 10 FEET ABOVE THE PROPOSED SIDEWALK ELEVATION. THE CONTRACTOR SHALL PRUNE EXISTING TREE AND SHRUB BRANCHES, WHICH OVERHANG THE SIDEWALK, IN ACCORDANCE WITH I.S.A. STANDARDS. THE CONTRACTOR SHALL NOTIFY DELDOT'S ROADSIDE ENVIRONMENTAL SUPERVISOR, DARIN CALLAWAY AT (302) 760-2186 AND/OR HIS DESIGNEE, AT LEAST TWO (2) DAYS PRIOR TO THE PRUNING OPERATION. ALL COSTS ASSOCIATED WITH THE ABOVE WORK TO BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.

4. THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXCAVATE TEST PITS ALONG PROPOSED DRAINAGE RUNS, AT POINTS OF POSSIBLE UTILITY CONFLICTS, TO DETERMINE IF A CONFLICT EXISTS. ANY CONFLICTS SHALL BE COORDINATED BY THE CONTRACTOR, WITH THE ENGINEER AND THE UTILITY COMPANY INVOLVED. THE ENGINEER SHALL ULTIMATELY DETERMINE THE SOLUTION TO THE UTILITY CONFLICT. TEST HOLES SHALL BE MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 208000 - EXCAVATION AND BACKFILL FOR PIPE TRENCHES, BUT ONLY TO THE ACTUAL DEPTH EXCAVATED.

5. ALL EXISTING PIPES TO BE ABANDONED UNDER THIS CONTRACT WHICH ARE LOCATED BELOW THRU LANES, TURN LANES AND SHOULDERS SHALL BE FILLED WITH ITEM 208500-FLOWABLE FILL OR AS DIRECTED BY THE ENGINEER. ALL ABANDONED PIPES LOCATED WITHIN OR BELOW THE PROPOSED PAVEMENT BOX SHALL BE ENTIRELY REMOVED. PAYMENT IS INCLUDED UNDER ITEM 202000-EXCAVATION AND EMBANKMENT.

6. ITEMS TO BE REMOVED UNDER ITEM 211000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:  
 - EXISTING POLE BASES  
 - EXISTING FENCE, POSTS, & CONC. ASSOCIATED WITH FENCE ON PARCEL 22-R  
 - EXISTING FENCE, POSTS, & CONC. ASSOCIATED WITH FENCE ON PARCEL 13-R  
 - EXISTING DRAINAGE STRUCTURES:  
 STA. 14+74, 14'R STA. 15+43, 13'R  
 STA. 18+83, 15'R STA. 19+07, 15'R  
 STA. 20+00, 14'R STA. 20+89, 12'R  
 STA. 30+68, 14'L STA. 32+36, 17'R  
 STA. 32+37, 24'L

### SECTION 300

7. A. THE CONTRACTOR MAY ELECT TO USE ANY OF THE FOLLOWING MATERIALS TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B':  
 a. CRUSHED STONE (PER STANDARD SPECIFICATION 821)  
 b. CRUSHED CONCRETE (PER STANDARD SPECIFICATION 821)  
 c. HOT-MIX MILLINGS (PER SPECIAL PROVISION 302514 MILLED HOT-MIX BASE COURSE)

THE CONTRACTOR WILL NOT BE ALLOWED TO MIX DIFFERENT MATERIALS (OR SIMILAR MATERIALS FROM DIFFERENT SOURCES) TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

ALL OF THE ABOVE LISTED MATERIALS ARE PERMITTED FOR USE ON THE JOB, PROVIDED THEY ARE SEPARATED INTO APPROVED AREAS. EACH AREA OF BASE COURSE MUST BE CONSTRUCTED USING MATERIALS FROM A SINGULAR SOURCE, FULL DEPTH, IN ORDER THAT PROPER TESTING MAY BE ACCOMPLISHED. THE CONTRACTOR AND DELDOT'S PROJECT ENGINEER SHALL AGREE ON THE LIMITS OF EACH SOURCE OF MATERIAL PRIOR TO PLACEMENT.

B. THE QUANTITY USED FOR BASE OF EACH OF THE ABOVE LISTED MATERIALS WILL BE THE CONTRACTOR'S CHOICE, WITH THE TOTAL MEETING THE ADVERTISED QUANTITY OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

C. THE CONTRACTOR MAY ALSO ELECT TO RECYCLE MILLINGS FOR USE IN HOT-MIX AS PERMITTED BY THE STANDARD SPECIFICATIONS. THE CHOICE OF THE QUANTITY OF MILLINGS USED FOR THIS PURPOSE, OR FOR BASE COURSE, LIES WITH THE CONTRACTOR. ALL MILLING MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR.

D. HOT-MIX MILLINGS MAY BE GENERATED FROM THE FOLLOWING SOURCES:  
 a. MATERIAL MADE AVAILABLE WHEN MILLED ON THIS CONTRACT UNDER ITEM 760502.  
 b. MATERIAL MILLED ON THIS CONTRACT AT THE CONTRACTOR'S CHOICE UNDER ITEM 202000.  
 c. MILLED MATERIAL FURNISHED ON THE JOB FROM THE CONTRACTOR'S YARD OR OTHER OUTSIDE SOURCE.  
 ALL MILLED MATERIALS SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 302514 - MILLED HOT-MIX BASE COURSE.

E. PAYMENT CLARIFICATION:  
 a. SHOULD THE CONTRACTOR ELECT TO MILL PORTIONS OF HOT-MIX SHOWN ON THE PLANS TO BE REMOVED UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT THE COST OF MILLING THIS HOT-MIX WILL BE PAID AS ITEM 202000 - EXCAVATION AND EMBANKMENT. THE MILLINGS GENERATED MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE, OR DISPOSED OF TO AN APPROVED SITE. HAULING COSTS FOR DISPOSAL AND/OR RECYCLING ARE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.

b. MILLINGS GENERATED UNDER ITEM 760502 - PAVEMENT MILLINGS, TAPERCUT MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE OR DISPOSED OF BY THE CONTRACTOR TO AN APPROVED SITE. NO SEPARATE PAYMENT WILL BE MADE FOR TRANSPORTING MILLINGS ON SITE OR TO AN APPROVED DISPOSAL SITE.

c. SHOULD THE CONTRACTOR ELECT TO TEMPORARILY STOCKPILE MILLINGS ON THE JOB SITE FOR LATER USE, ALL COSTS FOR STOCKPILING AND SUBSEQUENT REHANDLING SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.

d. MILLINGS USED FOR BASE COURSE SHALL BE PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE. NO SEPARATE PAYMENT WILL BE MADE TO FURNISH MILLINGS FROM AN OUTSIDE SOURCE OR TRANSPORT MILLINGS WITHIN THE PROJECT LIMITS. MILLINGS USED FOR BASE COURSE WILL BE PAID IN PLACE AT THE UNIT BID PRICE FOR ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

e. ALL COSTS TO UTILIZE MILLINGS IN RECYCLED HOT-MIX WILL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE HOT-MIX ITEM USING THE RECYCLED MATERIAL.

f. SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE IS PROVIDED TO SPECIFY THE MEANS OF LAY DOWN AND COMPACTION AS WELL AS THE MATERIAL REQUIREMENTS FOR MILLINGS USED AS BASE COURSE. ALL COSTS TO BRING THE MILLINGS INTO COMPLIANCE WITH THE REQUIREMENTS OF ITEM - 302514 MILLED HOT-MIX BASE COURSE ARE INCIDENTAL TO ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'. NO PAYMENT WILL BE MADE FOR ITEM 302514 - MILLED HOT-MIX BASE COURSE. THE QUANTITY OF MILLINGS USED FOR BASE COURSE WILL BE PAID FOR UNDER ITEM 302007 - GRADED AGGREGATE BASE COURSE.

### SECTION 400

8. THE PAVEMENT SECTION FOR FLEXIBLE PAVEMENT RESIDENTIAL DRIVEWAYS SHALL BE 2" BITUMINOUS CONCRETE PAVEMENT, TYPE 'C' OVER 8" GRADED AGGREGATE BASE COURSE, TYPE 'B', UNLESS OTHERWISE NOTED ON THE PLANS.

## SECTION 600

9. THE DEPARTMENT AND THE CONTRACTOR SHALL INSPECT ALL EXISTING PIPES AND DRAINAGE STRUCTURES TO BE USED IN THE FINAL DRAINAGE SYSTEM AND AGREE ON THE CONDITION PRIOR TO THE START OF CONSTRUCTION. EXISTING PIPES AND DRAINAGE STRUCTURES DAMAGED DUE TO CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IN-KIND AT THE CONTRACTOR'S EXPENSE. THE DEPARTMENT WILL VIDEO INSPECT NEW PIPE RUNS TO CONFIRM CONDITION PRIOR TO ACCEPTANCE. PIPE CLEANING PRIOR TO VIDEO INSPECTION AND MAINTENANCE OF TRAFFIC DURING VIDEO INSPECTION ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO THE PIPE ITEM THAT IS BEING VIDEO INSPECTED.

10. ALL EXISTING PIPES WITHIN THE LOC TO REMAIN SHALL BE FLUSHED BY THE CONTRACTOR. FLUSHING OF PIPES IN JURISDICTIONAL WATERS SHALL INCLUDE MEASURES FOR SEDIMENT CONTAINMENT AND REMOVAL. REFER TO JURISDICTIONAL AREAS IDENTIFIED ON ENVIRONMENTAL COMPLIANCE SHEETS.

11. THE PIPE BID ITEM SHALL BE FOR THE SPECIFIC TYPE OF PIPE MATERIAL. NO ALTERNATE PIPE MATERIALS WILL BE ACCEPTED.

## SECTION 700

12. IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSITIONED IN 10 LINEAR FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK, INCLUDING SAW CUTTING EXISTING CURB SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.

13. WHERE PROPOSED CONCRETE SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, THE EXISTING SIDEWALK SHALL BE SAWCUT AT THE TIE-IN POINT OR MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER AND SHALL BE PAID FOR UNDER ITEM 762002 - SAWCUTTING, CONCRETE, FULL DEPTH.

14. EXISTING CURB, GUTTER AND SIDEWALK WITHIN THE LOC SHALL BE REMOVED AS DIRECTED BY THE ENGINEER, AND PAID FOR UNDER ITEM 758000-REMOVAL OF EXISTING PCC PAVEMENT, CURB AND SIDEWALK.

15. THE CONTRACTOR SHALL CONSTRUCT SIDEWALKS USING CROSS SLOPES DEFINED ON THE TYPICAL SECTIONS. THE MAXIMUM CROSS SLOPE SHALL BE TWO PERCENT (2%).

16. WHERE THE SIDEWALK CROSSES A DRIVEWAY ENTRANCE THE CONTRACTOR SHALL PROVIDE A MINIMUM FOUR FEET (4') OF SIDEWALK WITH A THICKNESS OF SIX INCHES (6") AND A MAXIMUM CROSS SLOPE OF TWO PERCENT (2%). THE CONTRACTOR SHALL REFERENCE THE DELAWARE STANDARD CONSTRUCTION DETAIL FOR ENTRANCES WITH SIDEWALK.

17. STATION, OFFSET, AND ELEVATION DATA GIVEN FOR DRAINAGE STRUCTURES ARE TO BE APPLIED TO THE CENTER OF THE GRATE FOR INLETS, AND TO THE CENTER OF THE STRUCTURE FOR JUNCTION BOXES AND MANHOLES.

18. DRAINAGE INLET GRATES ADJACENT TO THE ROAD, WITHIN THE PROJECT LIMITS, WHICH ARE NOT TYPE 1 OR TYPE 4 SHALL BE REPLACED. THE ACTUAL LOCATIONS, THE NEED FOR ANY GRATE MODIFICATIONS OR FOR NEW FRAMES SHALL BE DETERMINED BY THE ENGINEER. ALL REPLACED GRATES/FRAMES SHALL BE DELIVERED TO THE NEAREST DISTRICT MAINTENANCE YARD WITH THE COST OF DELIVERY INCIDENTAL TO ITEM 708060 - REPLACING DRAINAGE INLET GRATE(S). FINAL PAYMENT FOR REPLACED GRATES/FRAMES SHALL NOT BE MADE UNTIL RECEIPT OF DELIVERED MATERIALS IS PRODUCED, SIGNED BY A DELDOT MAINTENANCE YARD SUPERVISOR.

19. ALL PAVED AREAS TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT.

20. ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM \* 763501 - CONSTRUCTION ENGINEERING.

## SECTION 900

21. THIS PROJECT IS COVERED UNDER AN NPDES GENERAL PERMIT FOR CONSTRUCTION. UNDER THE GENERAL PERMIT, COMPLIANCE WITH DELDOT'S APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS WILL CONSTITUTE COMPLIANCE WITH THE NPDES INDUSTRIAL PERMITTING REQUIREMENTS FOR THIS CONSTRUCTION PROJECT. A COPY OF THE NPDES GENERAL PERMIT AND NOTICE OF INTENT IS KEPT ON FILE IN EACH OF THE CONSTRUCTION OFFICES AND THE DEPARTMENT'S TEAM SUPPORT SECTION. A COPY OF THE GENERAL PERMIT OR THE NOI CAN BE OBTAINED UPON REQUEST FROM EITHER THE DEPARTMENT'S STORMWATER ENGINEER OR THE APPROPRIATE CONSTRUCTION ENGINEER.

## MISCELLANEOUS

22. THE CONTRACTOR SHALL CONTACT WILLIAM LOTHARP, THE CHIEF OF SCHEDULING FOR DART FIRST STATE, 14 DAYS PRIOR TO THE START OF CONSTRUCTION AT 302-576-6006.

23. REFER TO THE CONSTRUCTION PLAN SHEETS FOR THE LOCATION OF THE CLEAR ZONE AREA LIMITS.

24. ANY STAGING AND/OR STOCKPILING AREA(S) OUTSIDE THE PROJECT'S LIMIT OF CONSTRUCTION (LOC) THAT INDIVIDUALLY OR CUMULATIVELY ARE LARGER THAN 10,000 SQ. FT., MUST BE APPROVED BY DELDOT'S ARCHAEOLOGIST. CONTACT THE CONSTRUCTION AREA ENGINEER WHO WILL COORDINATE WITH DELDOT'S ARCHAEOLOGIST.

WITHIN 30 DAYS, DELDOT WILL:

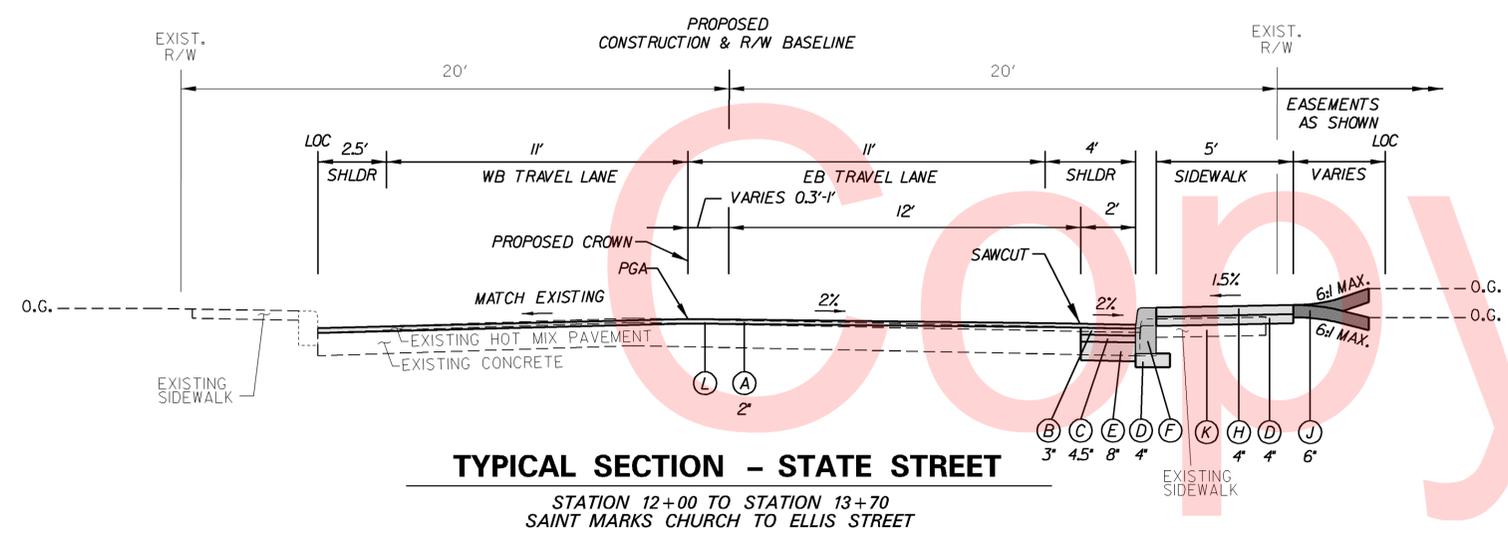
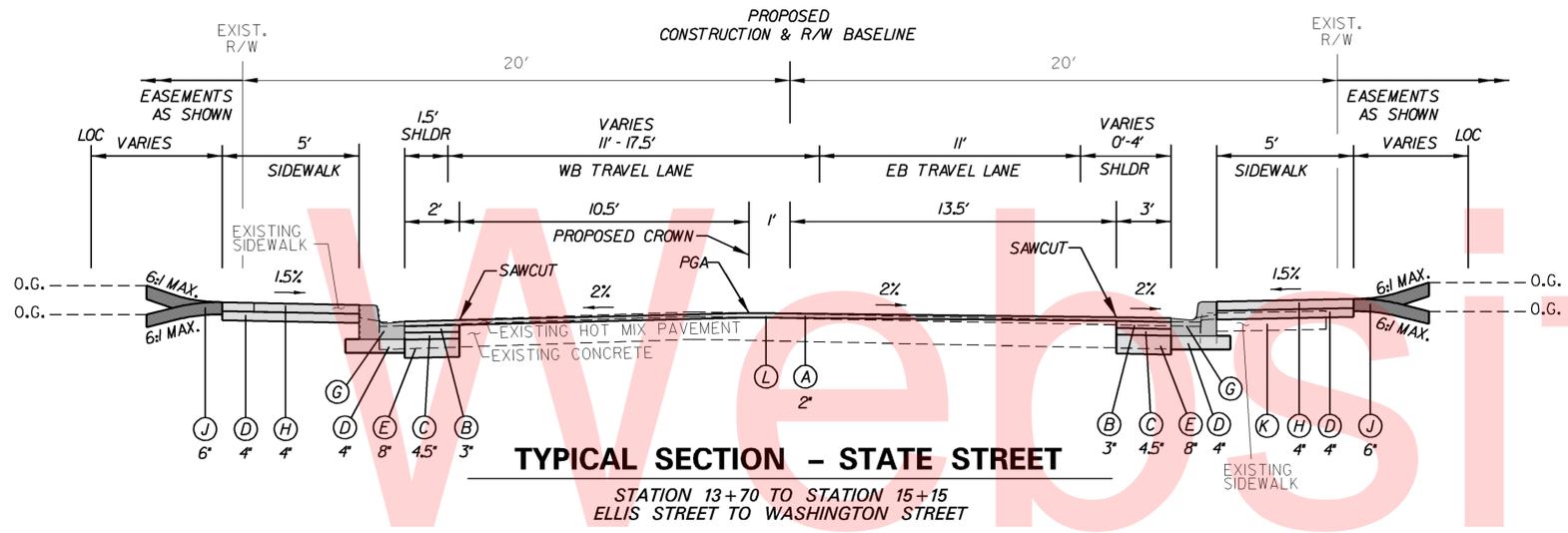
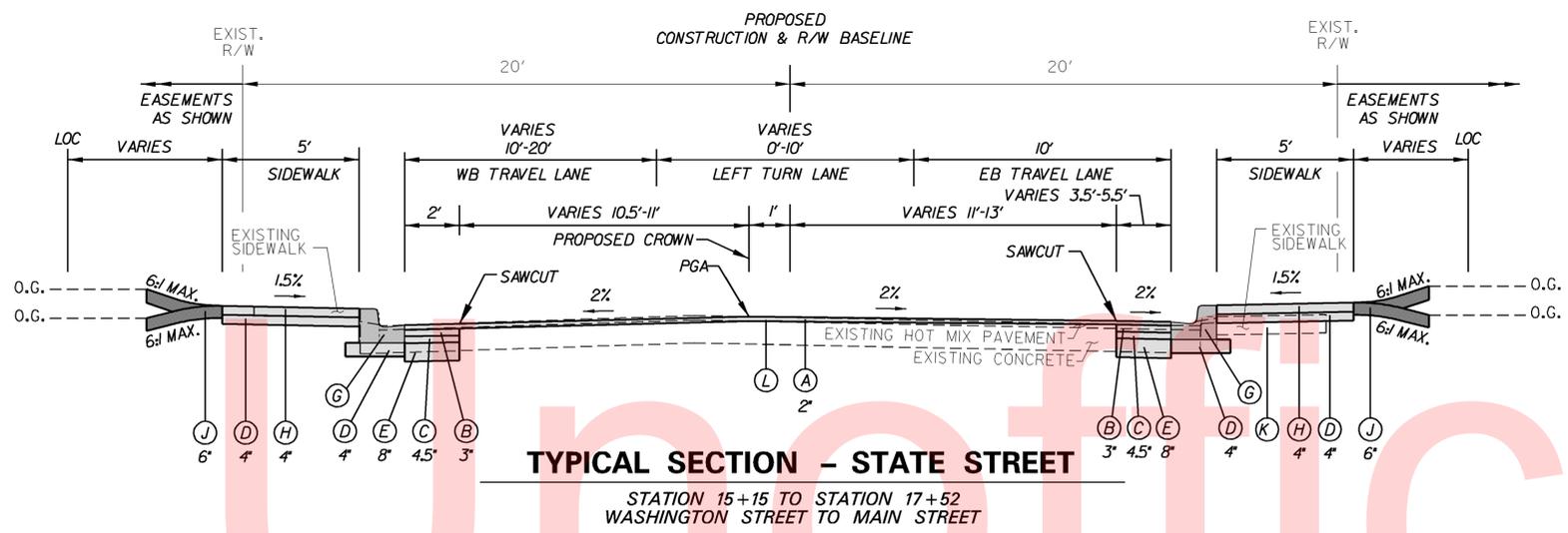
- 1) APPROVE THE USE OF THE PROPOSED STAGING AND STOCKPILE AREA(S);
- 2) REJECT THE REQUEST; OR
- 3) PERFORM AN ARCHAEOLOGICAL SURVEY TO DETERMINE WHETHER TO APPROVE OR REJECT THE REQUEST, WHICH MAY TAKE UP TO 3 MONTHS. IF AN ARCHAEOLOGICAL SURVEY IS NECESSARY, DELDOT OR A CONSULTANT ON ITS BEHALF WILL UNDERTAKE THE SURVEY.

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 <b>DELAWARE DEPARTMENT OF TRANSPORTATION</b>	ADDENDUMS / REVISIONS		<p style="text-align: center;"><b>NOT TO SCALE</b></p>	<p style="text-align: center;"><b>HSIP SC, IRON BRANCH ROAD/STATE STREET</b></p>	CONTRACT	BRIDGE NO.	<p style="text-align: center;"><b>N/A</b></p>	<p style="text-align: center;"><b>NOTES</b></p>	SHEET NO.
	T200800903	DESIGNED BY: CM			4				
	COUNTY	CHECKED BY: BB			TOTAL SHTS.				
	SUSSEX				48				

**LEGEND**

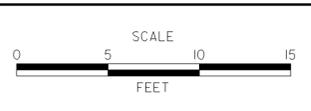
- (A) ITEM 401807 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22 (CARBONATE STONE)
- (B) ITEM 401822 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING
- (C) ITEM 401823 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING
- (D) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
- (E) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING
- (F) ITEM 701010 - PORTLAND CEMENT CONCRETE CURB, TYPE 1-8
- (G) ITEM 701022 - INTEGRAL P.C.C. CURB AND GUTTER, TYPE 3-8
- (H) ITEM 705001 - P.C.C. SIDEWALK, 4"
- (J) ITEM 908004 - TOPSOIL, 6" DEPTH  
ITEM 908014 - PERMANENT GRASS SEEDING, DRY GROUND
- (K) ITEM 209006 - BORROW, TYPE F
- (L) ITEM 760507 - PROFILE MILLING, HOT-MIX
- (M) ITEM 401821 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, PATCHING
- (N) ITEM 401572 - SAFETY EDGE FOR ROADWAY PAVEMENT
- (O) ITEM 401810 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22
- (P) ITEM 401819 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22
- (Q) ITEM 209001 - BORROW, TYPE A



NOTE: HORIZONTAL CLEARANCE IS MINIMUM 1.5' BEHIND FACE OF CURB AND MINIMUM 3' BEHIND FACE OF CURB AT INTERSECTIONS.

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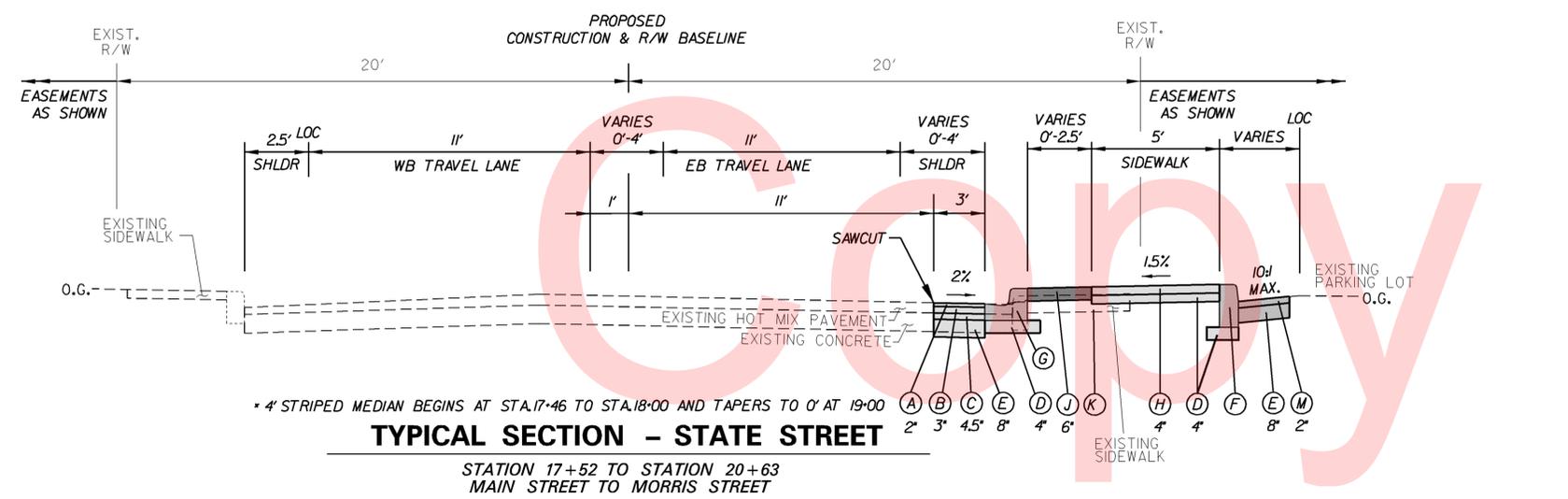
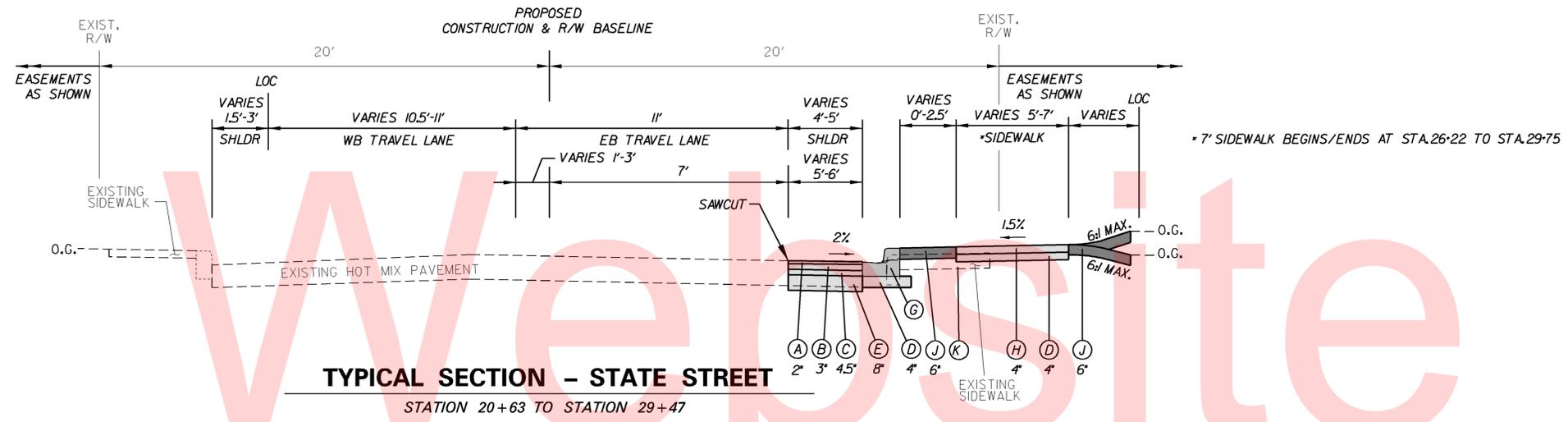
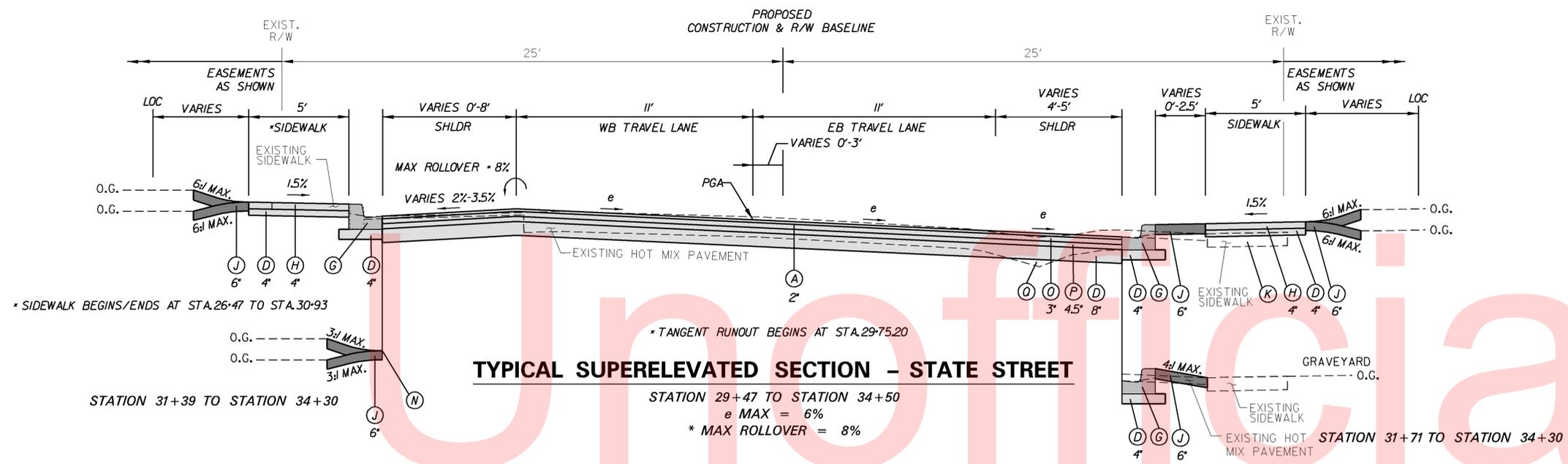
ADDENDUMS / REVISIONS



CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

**LEGEND**

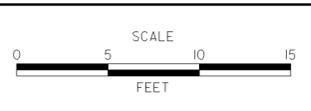
- (A) ITEM 401807 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 76-22 (CARBONATE STONE)
- (B) ITEM 401822 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22, PATCHING
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- (H) ITEM 705001 - P.C.C. SIDEWALK, 4"
- (J) ITEM 732002 - TOPSOIL, 6" DEPTH  
ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND
- (K) ITEM 209006 - BORROW, TYPE F
- (L) ITEM 760507 - PROFILE MILLING, HOT-MIX
- (M) ITEM 401821 - WMA, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22, PATCHING
- (N) ITEM 401572 - SAFETY EDGE FOR ROADWAY PAVEMENT
- (O) ITEM 401810 - WMA, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 64-22
- (P) ITEM 401819 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22
- (Q) ITEM 209001 - BORROW, TYPE A



NOTE: HORIZONTAL CLEARANCE IS MINIMUM 1.5' BEHIND FACE OF CURB AND MINIMUM 3' BEHIND FACE OF CURB AT INTERSECTIONS.

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ADDENDUMS / REVISIONS



CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

<b>TYPICAL SECTIONS</b>	SHEET NO. 6
	TOTAL SHTS. 48

**DATUM REFERENCE:**

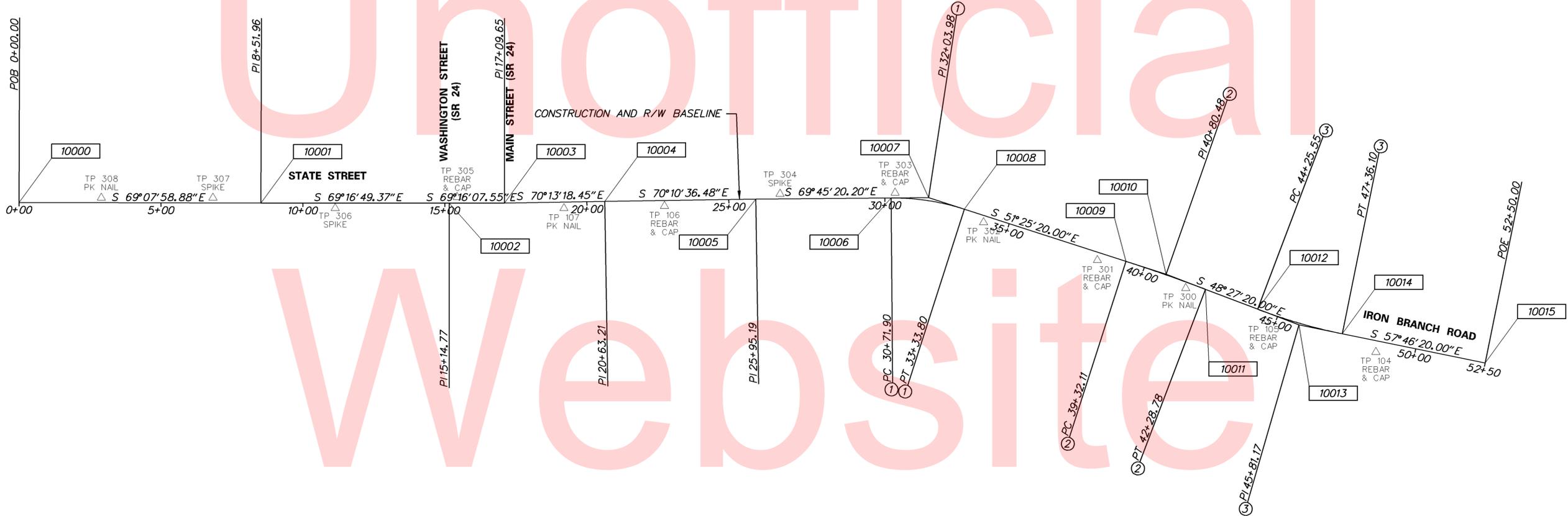
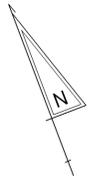
HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM NAD 83(NSRS 2007).  
 VERTICAL - THIS PROJECT IS REFERENCED TO NAVD 88, BASED ON NGS

**BENCHMARKS:**

HU2675 - ELEV. 46.71  
 HU2670 - ELEV. 25.63

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
10000	0+00.00	0.00	216128.1097	690401.1311
10001	8+51.96	0.00	215824.6437	691197.2065
10002	15+14.77	0.00	215590.1423	691817.1542
10003	17+09.65	0.00	215521.1573	691999.4173
10004	20+63.21	0.00	215401.5211	692332.1167
10005	25+95.19	0.00	215221.1157	692832.5759
10015	52+50.00	0.00	213773.3406	695031.8894

**NOTE:**  
 OFFSETS IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE LEFT OF CONSTRUCTION BASELINE.



**CIRCULAR CURVE NO. ①**

Element	STATION	NORTHING	EASTING
Element: Circular			
PC (10006)	30+71.90	215056.1640	693279.8320
PI (10007)	32+03.98	215010.4605	693403.7542
CC (10050)		214288.2172	692996.6070
PT (10008)	33+33.80	214928.0976	693507.0105
Radius:	818.51		
Delta:	18° 20' 00.20" Right		
Degree of Curvature(Arc):	7° 00' 00.04"		
Length:	261.90		
Tangent:	132.08		
Chord:	260.79		
Middle Ordinate:	10.45		
External:	10.59		
Tangent Direction:	S 69° 45' 20.20" E		
Radial Direction:	S 20° 14' 39.77" W		
Chord Direction:	S 60° 35' 20.10" E		
Radial Direction:	S 38° 34' 40.00" W		
Tangent Direction:	S 51° 25' 20.00" E		

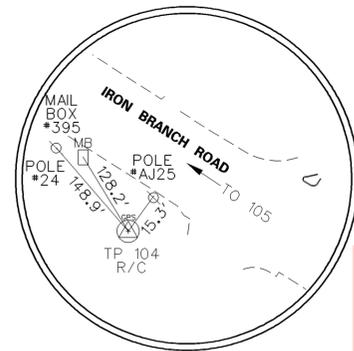
**CIRCULAR CURVE NO. ②**

Element	STATION	NORTHING	EASTING
Element: Circular			
PC (10009)	39+32.11	214555.0056	693974.7468
PI (10010)	40+80.48	214462.4877	694090.7341
CC (10051)		210075.8370	690401.9169
PT (10011)	42+28.78	214364.0909	694201.7777
Radius:	5729.58		
Delta:	2° 58' 00.00" Right		
Degree of Curvature(Arc):	1° 00' 00.00"		
Length:	296.67		
Tangent:	148.37		
Chord:	296.63		
Middle Ordinate:	1.92		
External:	1.92		
Tangent Direction:	S 51° 25' 20.00" E		
Radial Direction:	S 38° 34' 40.00" W		
Chord Direction:	S 49° 56' 20.00" E		
Radial Direction:	S 41° 32' 40.00" W		
Tangent Direction:	N 48° 27' 20.00" E		

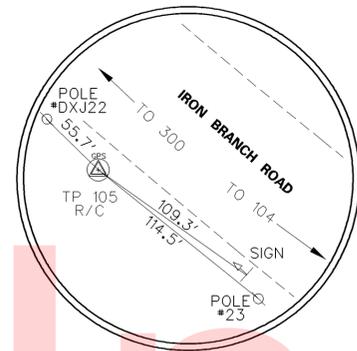
**CIRCULAR CURVE NO. ③**

Element	STATION	NORTHING	EASTING
Element: Circular			
PC (10012)	44+25.55	214233.5929	694349.0486
PI (10013)	45+81.17	214130.3850	694465.5217
CC (10052)		215663.0114	695615.6693
PT (10014)	47+36.10	214047.3945	694597.1668
Radius:	1909.86		
Delta:	9° 19' 00.00" Left		
Degree of Curvature(Arc):	3° 00' 00.00"		
Length:	310.56		
Tangent:	155.62		
Chord:	310.21		
Middle Ordinate:	6.31		
External:	6.33		
Tangent Direction:	S 48° 27' 20.00" E		
Radial Direction:	S 41° 32' 40.00" W		
Chord Direction:	S 53° 06' 50.00" E		
Radial Direction:	S 32° 13' 40.00" W		
Tangent Direction:	S 57° 46' 20.00" E		

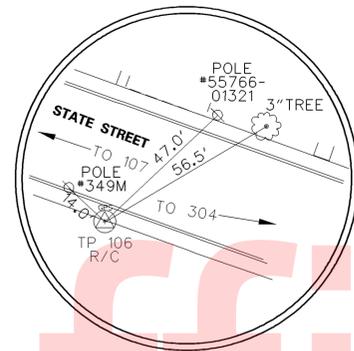
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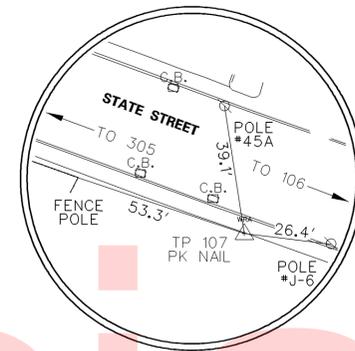
TRAVERSE POINT #104  
REBAR & CAP



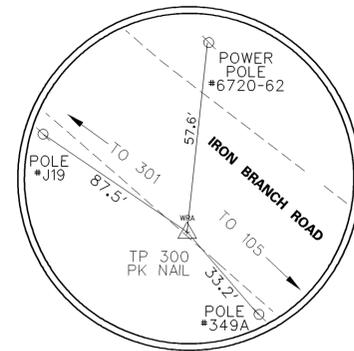
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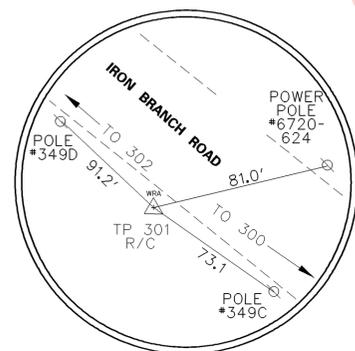
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REBAR & CAP



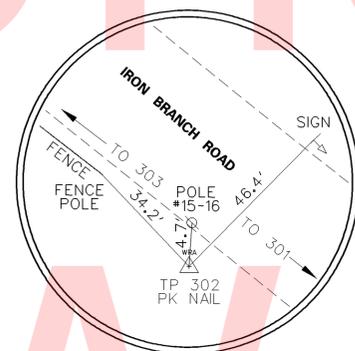
TRAVERSE POINT #107  
PK NAIL



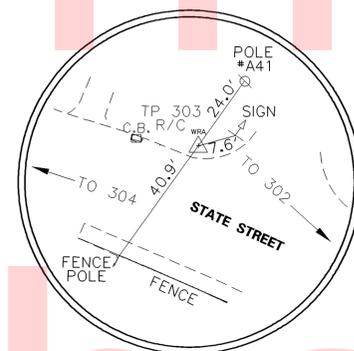
TRAVERSE POINT #300  
PK NAIL



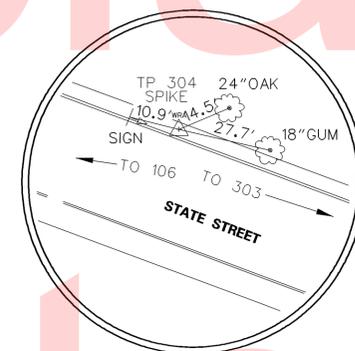
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REBAR & CAP



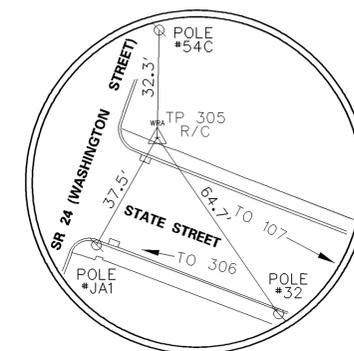
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PK NAIL



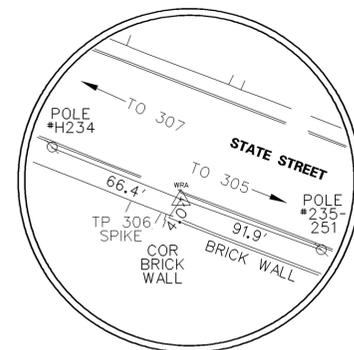
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REBAR & CAP



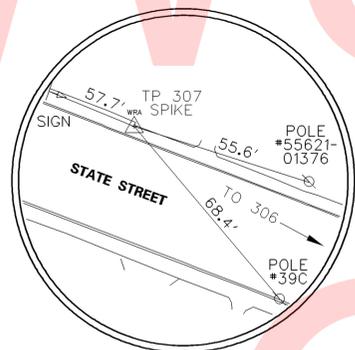
TRAVERSE POINT #304  
SPIKE



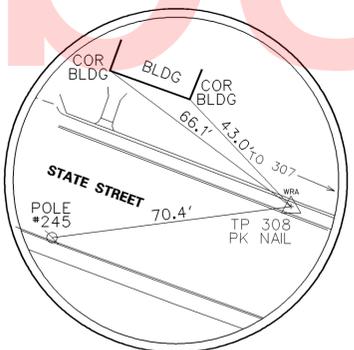
TRAVERSE POINT #305  
REBAR & CAP



TRAVERSE POINT #306  
SPIKE



TRAVERSE POINT #307  
SPIKE



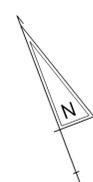
TRAVERSE POINT #308  
PK NAIL

HORIZONTAL / VERTICAL CONTROL DATA					
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
TP 104	48+65.54	39.21	213945.2015	694685.7499	23.45
TP 105	44+65.55	29.04	214185.2498	694360.4662	17.50
TP 106	22+73.47	19.27	215312.0860	692523.3780	23.05
TP 107	19+19.25	21.33	215430.1664	692189.4331	23.13
TP 300	41+62.62	20.27	214392.3524	694138.7371	15.38
TP 301	38+35.20	26.66	214594.5974	693882.3590	20.63
TP 302	34+12.80	22.93	214860.9041	693554.4704	21.49
TP 303	30+85.92	-18.55	215068.4883	693299.6671	21.73
TP 304	26+81.08	-17.79	215208.0866	692919.3180	22.29
TP 305	15+43.35	-21.21	215599.8593	691851.3915	23.02
TP 306	11+13.71	17.98	215715.2195	691435.6681	22.41
TP 307	6+83.04	-18.44	215902.0386	691045.9420	24.12
TP 308	2+89.99	-17.32	216041.0033	690678.2675	26.34

NOTE:  
OFFSETS IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE CONSTRUCTION BASELINE.

DATUM REFERENCE:  
HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM NAD 83(NSRS 2007).  
VERTICAL - THIS PROJECT IS REFERENCED TO NAVD 88, BASED ON NGS

BENCHMARKS:  
HU2675 - ELEV. 46.71  
HU2670 - ELEV. 25.63



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DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
1	14+76.31	+	34" x 24"	1	++	19.47
2*	15+42.55	+	34" x 24"	1	++	19.78

+ MATCH PROPOSED CURBLINE  
 ++ MATCH FLOWLINE OF PROPOSED CURB AND GUTTER  
 \* TEMPORARY REINFORCEMENT BAR REQUIRED

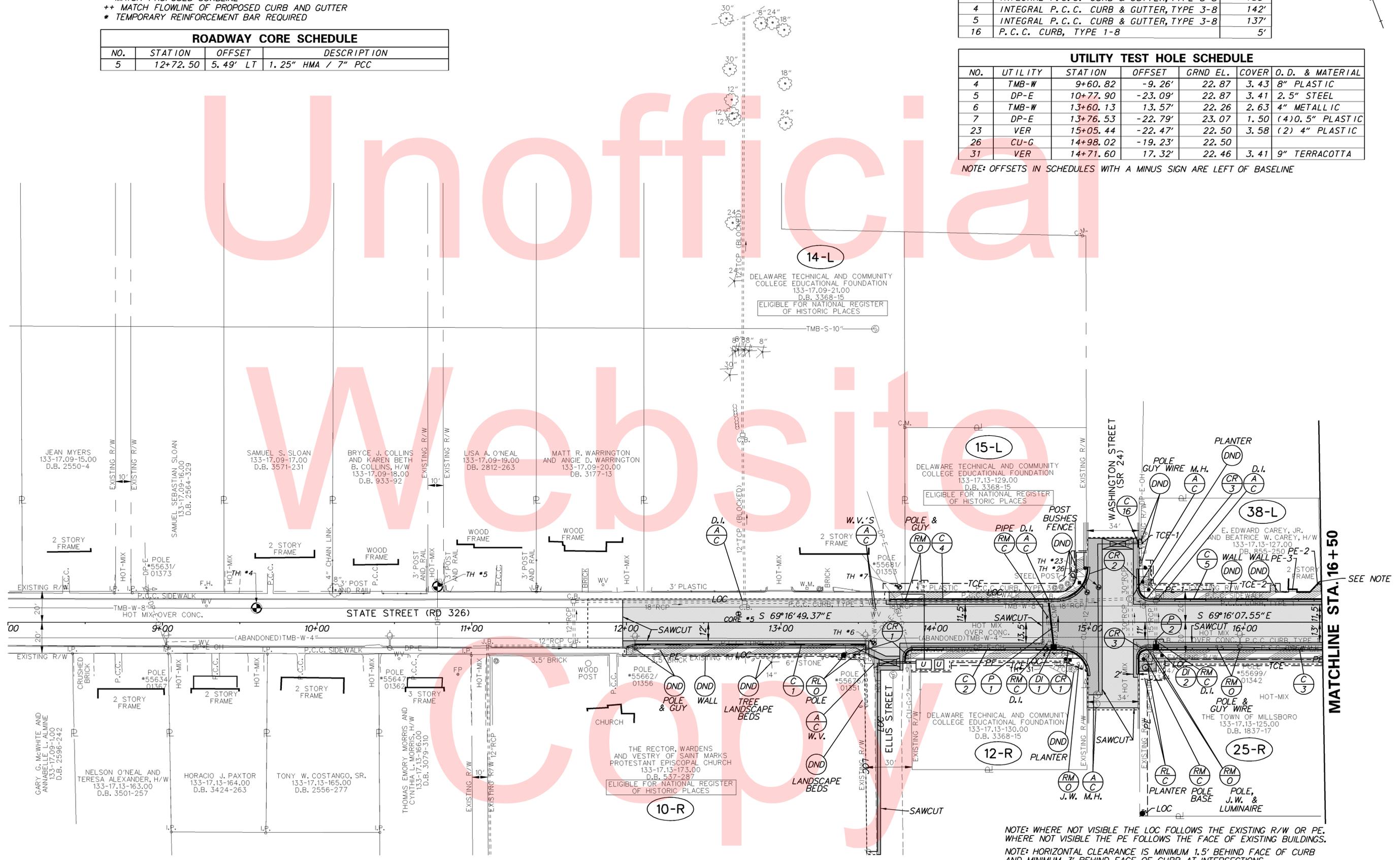
ROADWAY CORE SCHEDULE			
NO.	STATION	OFFSET	DESCRIPTION
5	12+72.50	5.49' LT	1.25" HMA / 7" PCC

DRAINAGE PIPE SCHEDULE							
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.	
1	15" RCP	IV	29.00'	.005	19.47	19.32	
2	15" RCP	V	4.00'	.008	19.78	19.75	

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P.C.C. CURB, TYPE 1-8	155'
2	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	137'
3	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	135'
4	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	142'
5	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	137'
16	P.C.C. CURB, TYPE 1-8	5'

UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O.D. & MATERIAL
4	TMB-W	9+60.82	-9.26'	22.87	3.43	8" PLASTIC
5	DP-E	10+77.90	-23.09'	22.87	3.41	2.5" STEEL
6	TMB-W	13+60.13	13.57'	22.26	2.63	4" METALLIC
7	DP-E	13+76.53	-22.79'	23.07	1.50	(4) 0.5" PLASTIC
23	VER	15+05.44	-22.47'	22.50	3.58	(2) 4" PLASTIC
26	CU-G	14+98.02	-19.23'	22.50		
31	VER	14+71.60	17.32'	22.46	3.41	9" TERRACOTTA

NOTE: OFFSETS IN SCHEDULES WITH A MINUS SIGN ARE LEFT OF BASELINE



NOTE: WHERE NOT VISIBLE THE LOC FOLLOWS THE EXISTING R/W OR PE. WHERE NOT VISIBLE THE PE FOLLOWS THE FACE OF EXISTING BUILDINGS.  
 NOTE: HORIZONTAL CLEARANCE IS MINIMUM 1.5' BEHIND FACE OF CURB AND MINIMUM 3' BEHIND FACE OF CURB AT INTERSECTIONS.

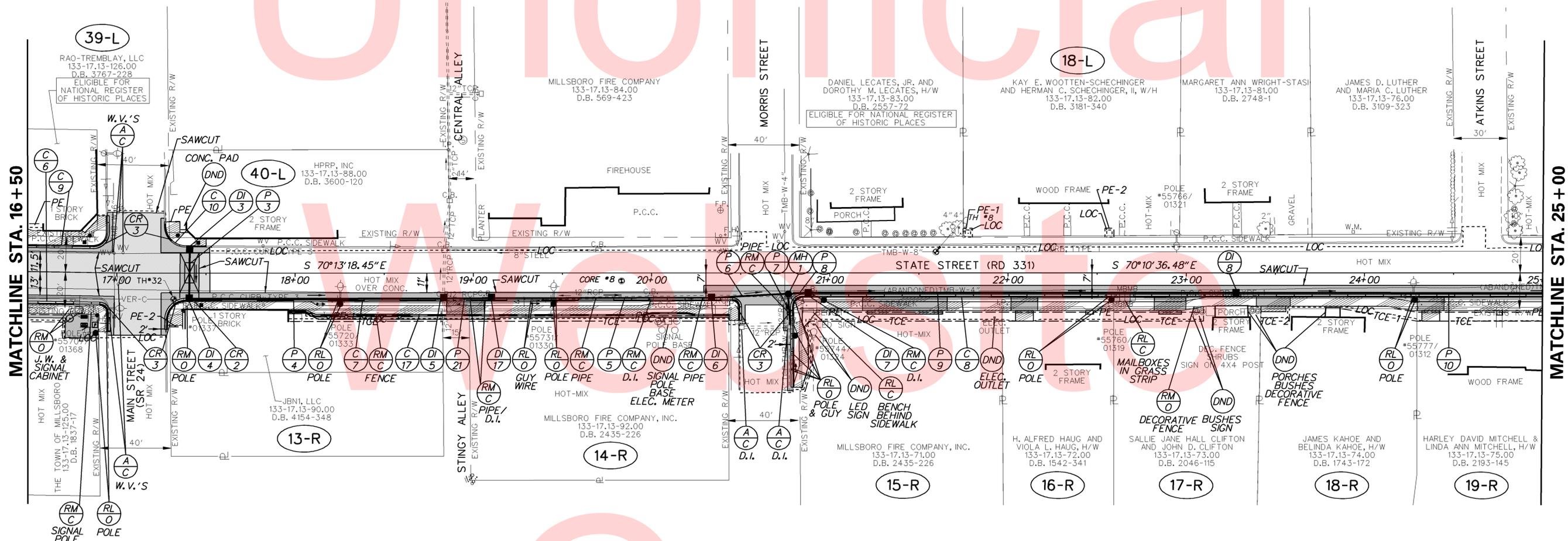
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ROADWAY CORE SCHEDULE			
NO.	STATION	OFFSET	DESCRIPTION
8	19+83.16	4.45' RT	3" HMA / 6.75" PCC

DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
3	17+40.91	+	34" x 18"	1		20.54
4	17+40.58	+	34" x 24"	1		20.26
5	18+83.37	+	34" x 24"	1		19.64
6	20+33.57	+	48" x 30"	1		18.89
7	20+87.95	+	48" x 48"	1		18.36
8	23+27.95	+	48" x 48"	1		17.46
17	19+10.21	+	34" x 24"	1		19.34

DRAINAGE PIPE SCHEDULE						
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.
3	15" RCP	IV	27.00'	.003	20.54	20.46
4	15" RCP	IV	140.00'	.003	20.26	19.84
5	15" RCP	IV	152.00'	.003	19.34	18.98
6	18" RCP	IV	36.00'	.003	18.78	18.66
7	15" RCP	IV	15.00'	.01	19.85	19.70
8	21" RCP	IV	8.00'	.006	18.46	18.41
9	21" RCP	IV	236.00'	.003	18.21	17.51
10	21" RCP	IV	221.00'	.003	17.31	16.74
21	15" RCP	IV	24.00'	.003	19.64	19.54

MANHOLE BOX SCHEDULE				
NO.	STATION	OFFSET	BOX SIZE	INV. EL.
1	20+76.77	11.81'	48" x 30"	18.46



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
6	INTEGRAL CURB & GUTTER, TYPE 3-8	60'
7	INTEGRAL CURB & GUTTER, TYPE 3-8	346'
8	INTEGRAL CURB & GUTTER, TYPE 3-8	471'
9	INTEGRAL CURB & GUTTER, TYPE 3-8	60'
10	P.C.C. CURB, TYPE 1-8	31'
17	P.C.C. CURB, TYPE 1-8	248'

UTILITY TEST HOLE SCHEDULE					
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER O.D. & MATERIAL
8	TMB-W	21+59.61	-11.74'	22.62	4.03 8" PLASTIC
32	VER-C	17+31.92	14.54'	23.58	2.94 (2)4" PLASTIC

NOTE: OFFSETS IN SCHEDULE WITH A MINUS SIGN ARE LEFT OF BASELINE

FENCE SCHEDULE			
NO.	QTY.	DESCRIPTION	NOTES
1	70 LF	CHAIN LINK FENCE, 6 FT. HIGH (ITEM 727004)	STA. 17+96, 22' RT. TO STA. 18+66, 24' RT. (+/-)

NOTE: HORIZONTAL CLEARANCE IS 1.5' BEHIND FACE OF CURB AND 3' BEHIND FACE OF CURB AT INTERSECTIONS.  
NOTE: WHERE NOT VISIBLE THE LOC FOLLOWS THE EXISTING R/W OR PE. WHERE NOT VISIBLE THE PE FOLLOWS THE FACE OF EXISTING BUILDINGS.

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	ADDENDUMS / REVISIONS			<b>HSIP SC,</b> <b>IRON BRANCH ROAD/STATE STREET</b>		CONTRACT T200800903	BRIDGE NO. N/A	<b>CONSTRUCTION PLAN</b>	SHEET NO. 10
				COUNTY SUSSEX	DESIGNED BY: CM	CHECKED BY: BB	TOTAL SHTS. 48		

DRAINAGE INLET SCHEDULE						
NO.	STATION	OFFSET	BOX SIZE	GRATE	T.G. EL.	INV. EL.
9	25+53.03	+	48" x 48"	1		16.44
10	26+37.13	+	48" x 48"	1		16.00
11	28+01.13	+	48" x 48"	1		15.32
12	29+49.13	+	48" x 48"	1		14.69
13	30+26.12	+	72" x 48"	1		14.27
14	30+76.63	+	48" x 48"	1		13.93
15	30+76.64	+	48" x 48"	1		13.66
16	29+51.56	+	72" x 24"	1		17.00

+ MATCH PROPOSED CURBLINE  
 ++ MATCH FLOWLINE OF PROPOSED CURB AND GUTTER

DRAINAGE PIPE SCHEDULE							
NO.	SIZE / TYPE	CLASS	LENGTH	SLOPE	INT. EL.	DIS. EL.	
11	21" RCP	IV	80.00'	.003	16.44	16.20	
12	21" RCP	IV	160.00'	.003	16.00	15.52	
13	21" RCP	IV	144.00'	.003	15.32	14.89	
14	24" RCP	IV	72.00'	.003	14.69	14.47	
15	24" RCP	IV	45.00'	.003	14.27	14.13	
16	24" RCP	IV	22.00'	.003	13.93	13.86	
17	24" RCP	IV	160.00'	.003	13.66	13.18	
18	15" RCP	IV	40.00'	.016	18.42	17.80	
19	24" RCP	IV	72.00'	.010	12.50	11.75	
20	15" RCP	IV	23.00'	.005	17.00	16.88	

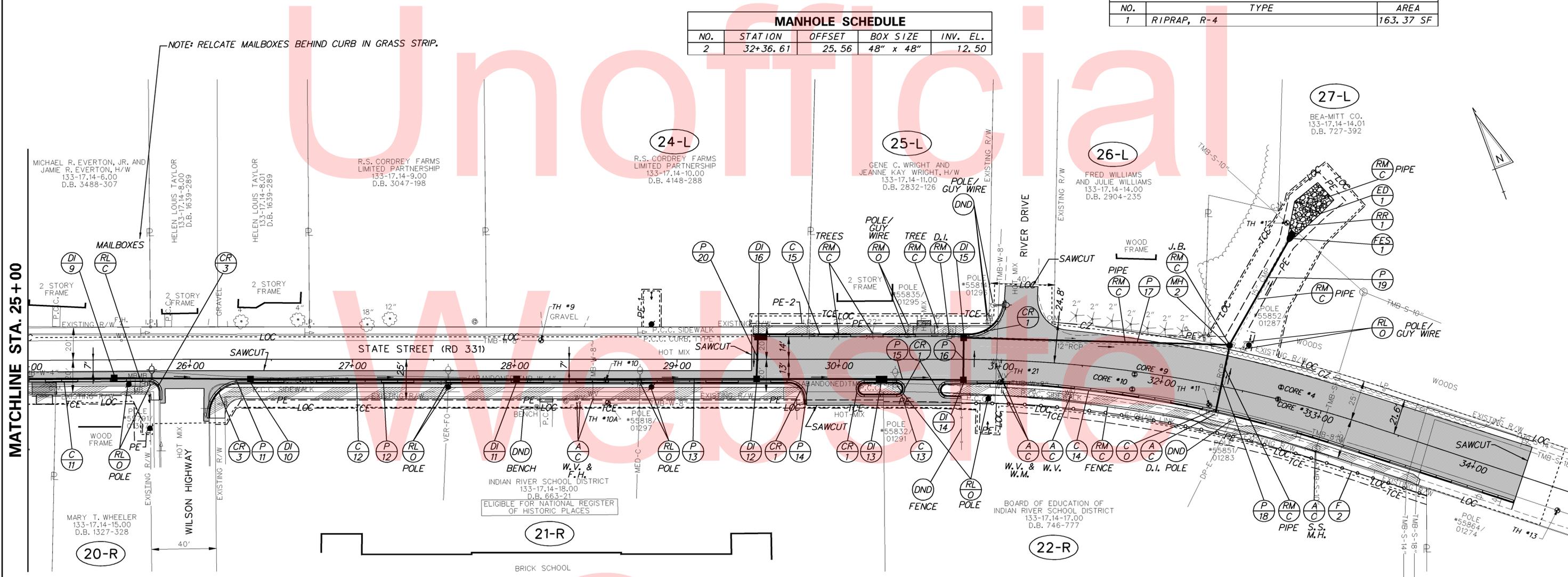
ENERGY DISSIPATOR SCHEDULE									
NO.	STATION	OFFSET	RIPRAP SIZE	D (FT)	L1 (FT)	L (FT)	W1 (FT)	W2 (FT)	
*1	32+58.00	100.50	R-5	1.50	1.50	9.30	1.30	7.80	

\* SEE ENERGY DISSIPATOR DETAIL

FLARED END SECTION SCHEDULE			
NO.	SIZE / TYPE	SLOPE	SAFETY GRATE
1	24" RCP	.01	NO

RIPRAP SCHEDULE		
NO.	TYPE	AREA
1	RIPRAP, R-4	163.37 SF

MANHOLE SCHEDULE				
NO.	STATION	OFFSET	BOX SIZE	INV. EL.
2	32+36.61	25.56	48" x 48"	12.50



ROADWAY CORE SCHEDULE				
NO.	STATION	OFFSET	DESCRIPTION	
3	32+72.55	0.07' RT	5.25" HMA /1.25" SURF TREATMENT	
4	32+72.55	7.64' LT	6.5" HMA	
9	31+82.49	1.73' RT	6" HMA /1.5" SURF TREATMENT	
10	31+82.49	11.04' RT	7.75" HMA /0.5" SURF TREATMENT	

FENCE SCHEDULE			
NO.	QTY.	DESCRIPTION	NOTES
2	294 LF	CHAIN LINK FENCE, 6 FT. HIGH (ITEM 727004)	STA. 30+76, 27' RT. TO STA. 33+78, 30' RT. (+/-)

UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O.D. & MATERIAL
9	TMB-W	28+16.25	-11.66	21.30	3.59	8" PLASTIC
10	VER-C	28+56.40	12.54	21.25	2.78	2.5" STEEL
10A	TMB-W	28+57.44	26.74	21.92	3.10	8" AC
11	TMB-W	32+31.63	12.90	21.08	3.83	8" AC
12	TMB-S	32+54.00	-106.6	13.10	4.60	10" AC
21	TMB-W	30+99.66	13.60	21.10	3.94	8" DIP

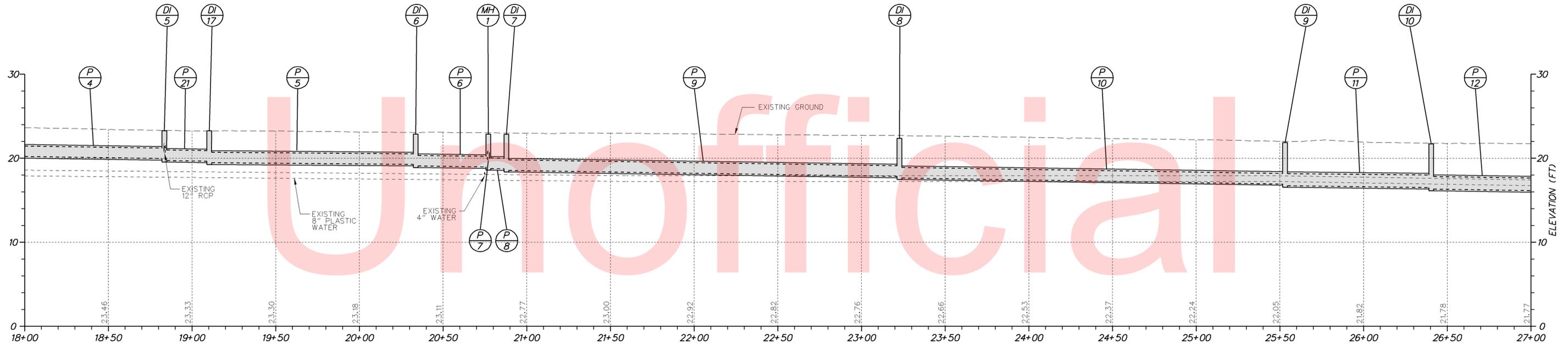
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
11	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	92'
12	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	394'
13	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	71'
14	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	384'
15	INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-8	168'

NOTE: HORIZONTAL CLEARANCE IS 1.5' BEHIND FACE OF CURB AND MINIMUM 3' BEHIND FACE OF CURB AT INTERSECTIONS.

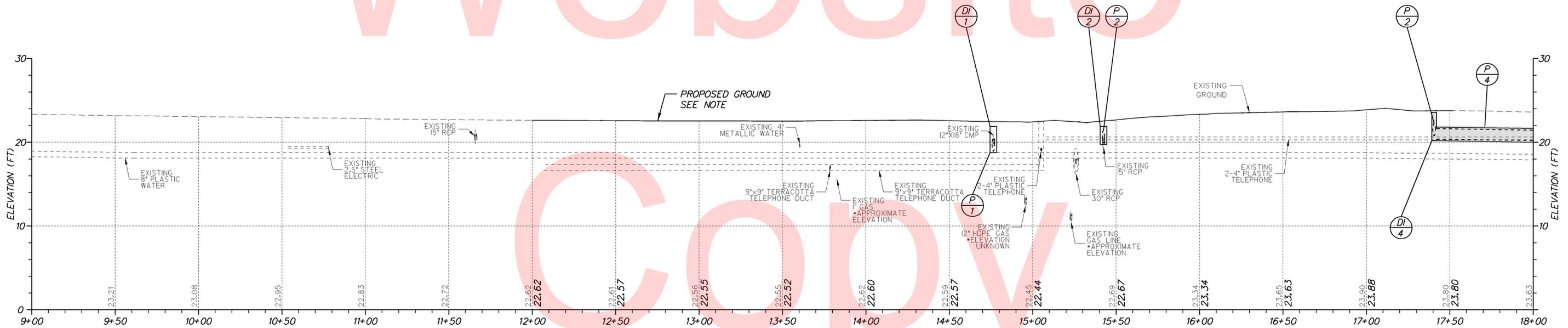
NOTE: WHERE NOT VISIBLE THE LOC FOLLOWS THE EXISTING R/W OR PE. WHERE NOT VISIBLE THE PE FOLLOWS THE FACE OF EXISTING BUILDINGS.

ASBESTOS HAZMAT NOTES:

- STA 30+97: 8" AC WATER MAIN TO BE SPOT ADJUSTED BY OTHERS. SEE UTILITY RELOCATION PLANS FOR DETAILS.
- STA. 32+36.5: EXCAVATION FOR P17 IS TO BE MONITORED BY HAZMAT SUB-CONTRACTOR. P18 IS IN CLOSE PROXIMITY TO 8" AC WATER MAIN. EXERCISE CAUTION WHEN EXCAVATING.
- STA. 32+58: ENCASE 10 FT LENGTH OF 10" AC GRAVITY SEWER MAIN IN CONCRETE. EXCAVATION TO BE MONITORED BY HAZ-MAT SUB-CONTRACTOR.



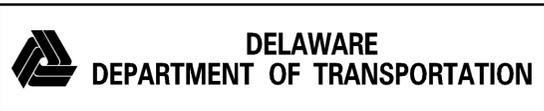
**EAST STATE STREET**



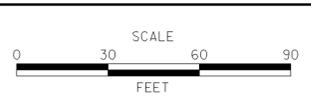
**WEST STATE STREET**

NOTE: SEE GG SHEETS FOR ELEVATION AND LOCATION OF PROPOSED CROWN LINE

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ADDENDUMS / REVISIONS	



**HSIP SC,  
IRON BRANCH ROAD/STATE STREET**

CONTRACT T200800903	BRIDGE NO. <b>NA</b>
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

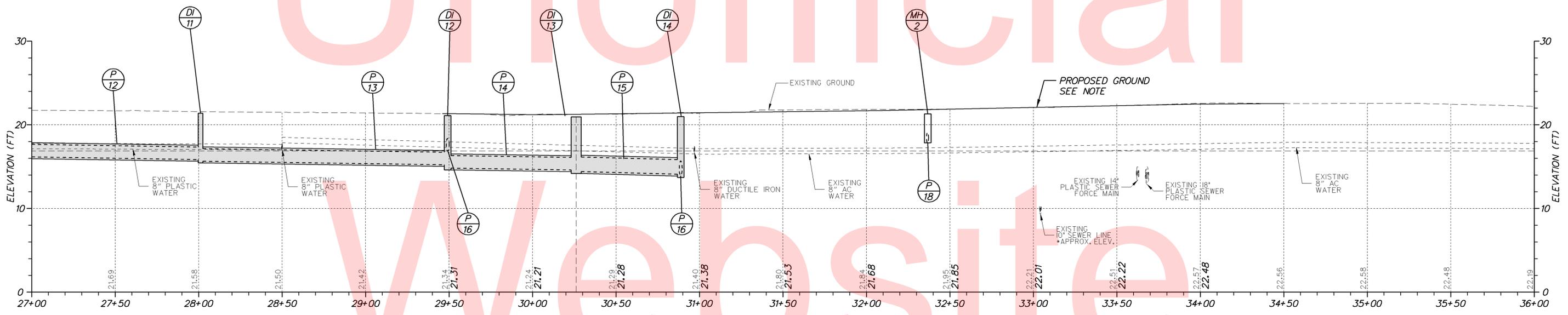
**PROFILES**

SHEET NO. 12
TOTAL SHTS. 48

# Unofficial

# Website

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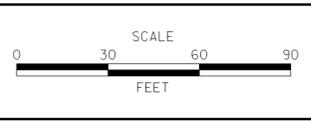
**EAST STATE STREET/IRON BRANCH ROAD**

NOTE: SEE GG SHEETS FOR ELEVATION AND LOCATION OF PROPOSED CROWN LINE

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**DELAWARE**  
DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS	



**HSIP SC,**  
**IRON BRANCH ROAD/STATE STREET**

CONTRACT T200800903	BRIDGE NO. <b>NA</b>
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

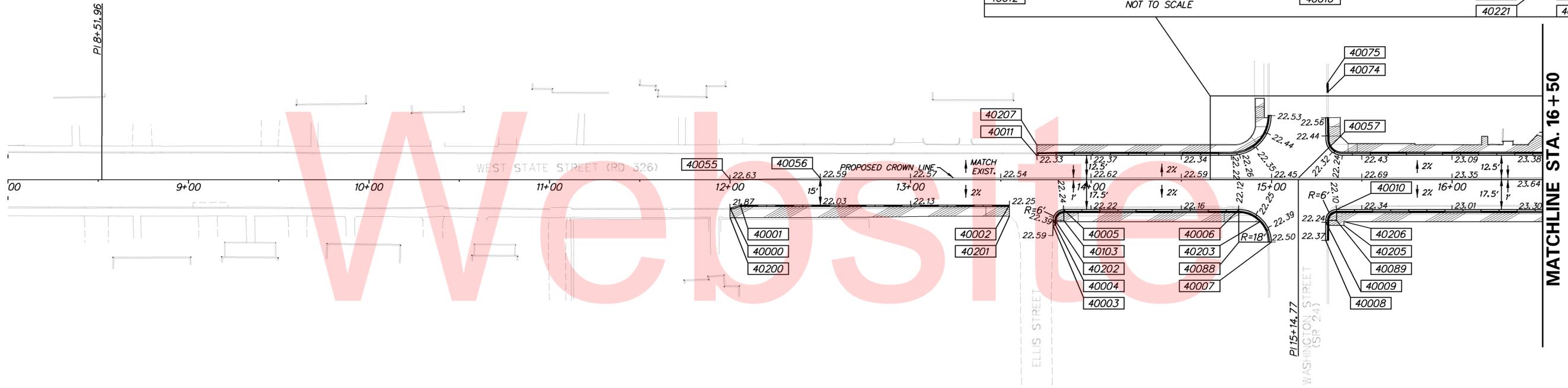
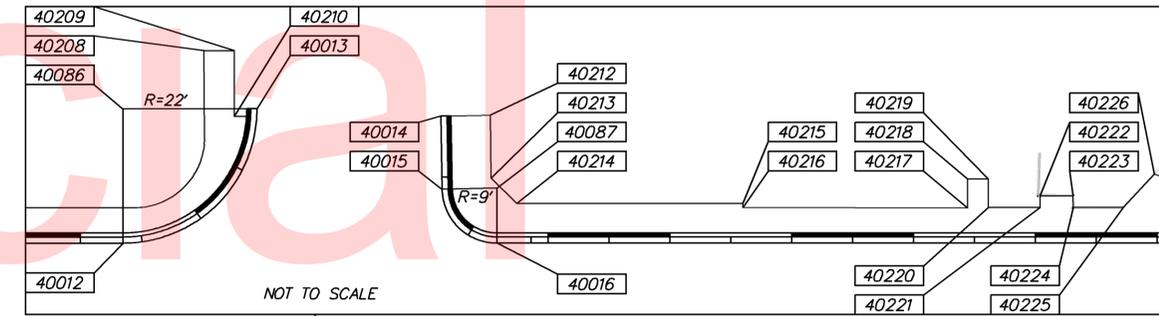
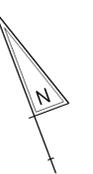
**PROFILES**

SHEET NO. 13
TOTAL SHTS. 48

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40000	12+00.00	15.65	215686.8651	691517.2022
40001	12+05.70	14.00	215686.3963	691523.1180
40002	13+54.78	14.00	215633.6516	691662.5586
40003	13+78.88	25.13	215614.7122	691681.1584
40004	13+78.93	22.39	215617.2630	691682.1785
40005	13+84.93	16.50	215620.6470	691689.8723
40006	14+81.80	16.50	215586.3737	691780.4802
40007	14+99.80	34.50	215563.1696	691790.9477
40008	15+29.92	33.49	215553.4610	691819.4723
40009	15+30.01	22.46	215563.7481	691823.4539
40010	15+36.01	16.50	215567.1939	691831.1733
40011	13+70.03	-13.31	215653.7976	691686.4861
40012	14+77.70	-13.50	215615.8849	691787.2568
40013	14+99.70	-35.53	215628.7020	691815.6262
40014	15+29.90	-34.31	215616.8789	691843.4439
40015	15+30.11	-22.34	215605.6089	691839.4042
40016	15+39.10	-13.50	215594.1547	691844.6905
40055	12+00.00	-0.29	215701.7829	691522.8451
40056	12+50.00	-1.00	215684.7529	691569.8608
40057	15+14.77	-1.00	215591.0777	691817.5080
40074	15+30.69	-48.09	215629.4892	691849.0633
40075	15+30.64	-53.09	215634.1814	691850.7906
40086	14+77.70	-35.50	215636.4620	691795.0403
40087	15+39.10	-22.50	215602.5720	691847.8763
40088	14+81.80	34.50	215569.5379	691774.1118
40089	15+36.01	22.50	215561.5824	691829.0494

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40200	12+00.00	20.98	215681.8868	691515.3191
40201	13+54.78	20.09	215627.9580	691660.4050
40202	13+80.58	23.17	215615.9494	691683.4481
40203	14+93.56	23.17	215575.9771	691789.1222
40205	15+40.12	25.17	215557.6337	691831.9489
40206	15+40.12	23.17	215559.5042	691832.6569
40207	13+70.00	-19.34	215659.4489	691688.5872
40208	14+90.99	-45.00	215640.6453	691810.8324
40209	14+96.03	-45.00	215638.8629	691815.5446
40210	14+96.03	-34.27	215628.8291	691811.7469
40212	15+38.06	-34.46	215614.1243	691851.1324
40213	15+38.13	-24.17	215604.4836	691847.5533
40214	15+42.37	-20.00	215599.0786	691850.0489
40215	15+79.52	-20.01	215585.9397	691884.7988
40216	15+79.52	-19.38	215585.3486	691884.5757
40217	16+16.39	-19.26	215572.1867	691919.0121
40218	16+16.39	-24.00	215576.6161	691920.6909
40219	16+19.82	-24.00	215575.4033	691923.8952
40220	16+19.82	-19.37	215571.0757	691922.2551
40221	16+28.29	-19.37	215568.0723	691930.1797
40222	16+28.29	-21.29	215569.8671	691930.8590
40223	16+33.71	-21.28	215567.9453	691935.9298
40224	16+33.77	-19.37	215566.1349	691935.3033
40225	16+41.93	-19.37	215563.2429	691942.9340
40226	16+47.07	-24.82	215566.5255	691949.6683

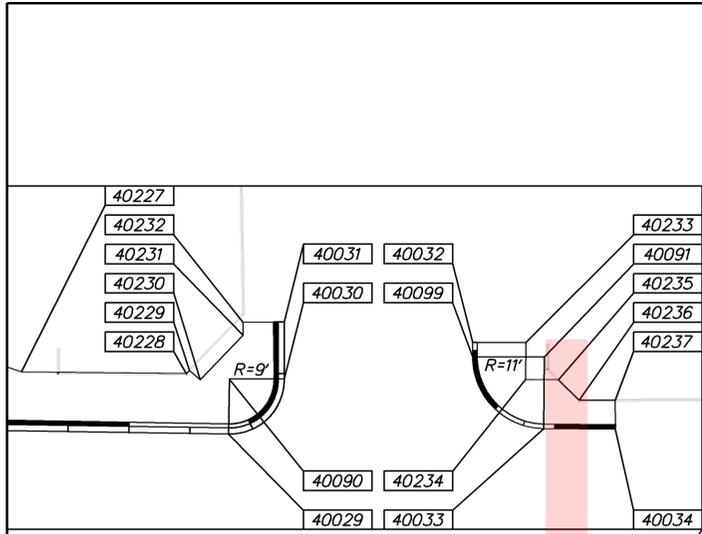
- NOTES:
- OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE CONSTRUCTION BASELINE.
  - UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND GUTTER IS GIVEN TO EDGE OF PAVEMENT.
  - RADIARE GIVEN TO EDGE OF PAVEMENT.
  - UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT INTESECTIONS ARE GIVEN AT 10 FOOT INTERVALS.
  - ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501-CONSTRUCTION ENGINEERING.
  - PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR EXISTING CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.



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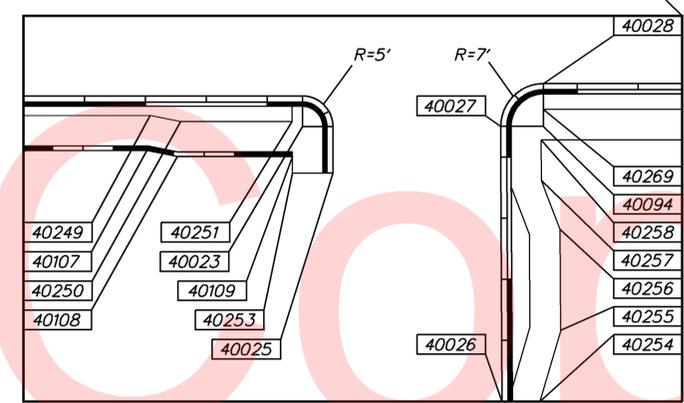
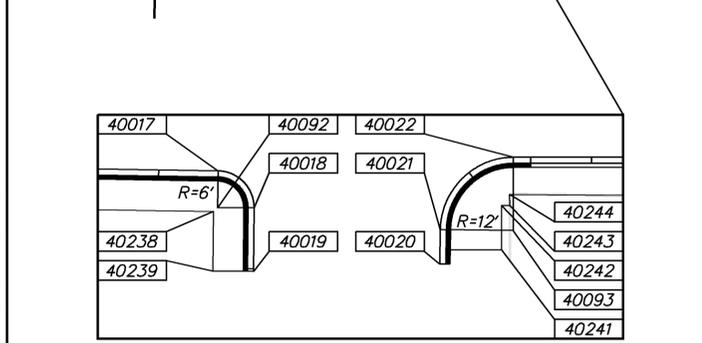
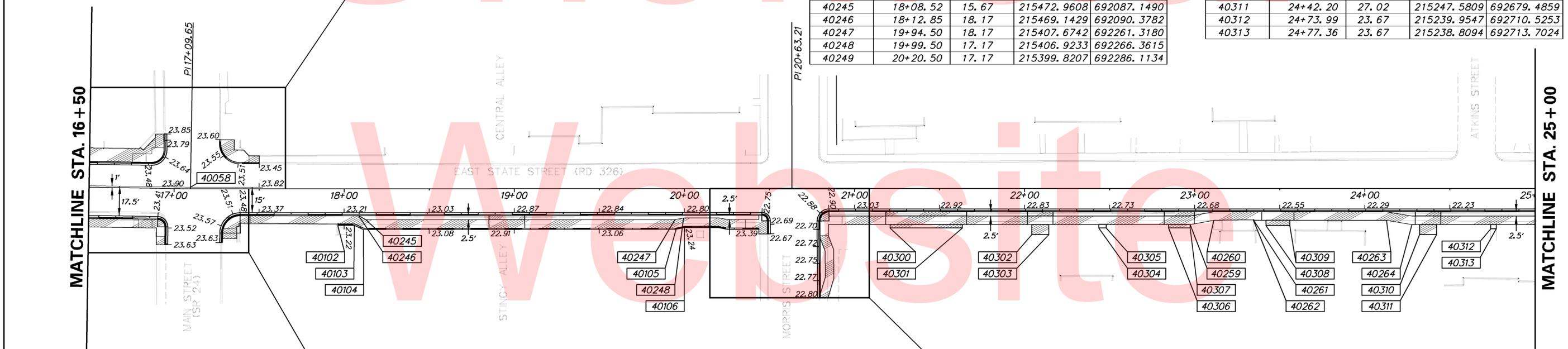
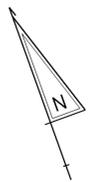
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COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40017	16+90.64	16.50	215512.4569	691975.7920
40018	16+96.64	22.44	215504.7741	691979.2993
40019	16+96.73	32.85	215495.0102	691975.7078
40020	17+26.76	31.47	215485.7596	692004.8681
40021	17+26.85	25.83	215491.0303	692006.8618
40022	17+38.85	14.00	215498.1030	692022.1560
40023	20+45.47	19.00	215389.6431	692308.9986
40025	20+50.47	26.60	215380.7980	692311.1251
40026	20+78.27	64.03	215336.1785	692324.5673
40027	20+78.06	19.00	215378.6124	692339.6396
40028	20+85.06	12.00	215382.8238	692348.5986
40029	16+86.42	-13.50	215542.0080	691982.4655
40030	16+95.42	-22.70	215547.4285	691994.1378
40031	16+95.25	-31.99	215556.1784	691997.2735
40032	17+26.75	-28.85	215542.5204	692025.2654
40033	17+38.40	-14.77	215525.3325	692031.4667
40034	17+50.16	-14.66	215521.2421	692042.4950
40058	17+09.67	-1.00	215522.0927	691999.7713
40059	20+45.00	-1.00	215408.6237	692315.3200
40060	20+85.00	-3.00	215396.9536	692353.6336
40090	16+86.42	-22.50	215550.4253	691985.6514
40091	17+38.50	-26.52	215536.3533	692035.5415
40092	16+90.64	22.50	215506.8454	691973.6681
40093	17+38.85	26.00	215486.8108	692018.0955
40094	20+85.06	19.00	215376.2386	692346.2248

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40102	17+96.17	21.41	215471.7295	692073.5877
40103	18+06.01	21.41	215468.4026	692082.8407
40104	18+11.34	23.92	215464.2420	692087.0146
40105	19+95.07	23.92	215402.0707	692259.9081
40106	20+00.07	22.92	215401.3199	692264.9515
40107	20+19.93	22.92	215394.6025	692283.6320
40108	20+24.93	23.92	215391.9696	692287.9986
40109	20+43.80	23.92	215385.5823	692305.7613
40227	16+52.24	-23.16	215563.1417	691953.9206
40228	16+79.30	-23.30	215553.6950	691979.2766
40229	16+79.86	-23.80	215553.9602	691979.9732
40230	16+81.60	-22.36	215551.9955	691981.0939
40231	16+88.58	-29.73	215556.4216	691990.2307
40232	16+88.54	-31.84	215558.4125	691990.9466
40233	17+35.45	-28.85	215539.5732	692033.4493
40234	17+35.41	-22.81	215533.9073	692031.3723
40235	17+40.79	-22.77	215532.0454	692036.4239
40236	17+44.17	-19.39	215527.7224	692038.4606
40237	17+50.15	-19.41	215525.7184	692044.0972
40238	16+89.94	23.17	215506.4672	691972.7838
40239	16+90.04	32.91	215497.3230	691969.4202
40240	17+28.47	29.14	215487.3707	692007.2594
40241	17+36.94	29.13	215484.5094	692015.2359
40242	17+36.94	21.87	215491.3429	692017.6955
40243	17+38.27	21.88	215490.8881	692018.9388
40244	17+38.24	20.08	215492.5920	692019.5279
40245	18+08.52	15.67	215472.9608	692087.1490
40246	18+12.85	18.17	215469.1429	692090.3782
40247	19+94.50	18.17	215407.6742	692261.3180
40248	19+99.50	17.17	215406.9233	692266.3615
40249	20+20.50	17.17	215399.8207	692286.1134

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40250	20+25.50	18.17	215397.1878	692290.4801
40251	20+43.80	18.17	215390.9929	692307.7075
40252	20+43.80	15.67	215393.3453	692308.5537
40253	20+43.80	26.60	215383.0538	692304.8517
40254	20+84.44	64.05	215334.0679	692330.3677
40255	20+87.87	52.38	215343.8864	692337.5545
40256	20+87.78	35.81	215359.4971	692343.0900
40257	20+84.74	27.84	215368.0320	692342.9297
40258	20+84.70	21.17	215374.3196	692345.1582
40259	22+97.27	21.17	215302.2322	692545.1344
40260	23+11.24	18.67	215299.8467	692559.1241
40261	23+34.54	18.34	215292.2572	692581.1492
40262	23+34.55	18.67	215291.9435	692581.0483
40263	24+12.73	18.67	215265.4308	692654.5967
40264	24+27.73	21.17	215257.9921	692667.8601
40300	21+20.90	23.17	215360.1642	692378.5288
40301	21+63.40	23.17	215345.7503	692418.5140
40302	22+04.20	27.00	215328.3089	692455.5951
40303	22+14.20	27.00	215324.9182	692465.0011
40304	22+43.60	23.17	215318.5513	692493.9663
40305	22+48.01	23.17	215317.0587	692498.1068
40306	22+84.62	23.17	215304.6402	692532.5463
40307	22+97.28	23.17	215300.3505	692544.4567
40308	23+41.96	21.76	215286.5179	692586.9691
40309	23+55.96	21.76	215281.7702	692600.1395
40310	24+32.20	27.01	215250.9807	692670.0815
40311	24+42.20	27.02	215247.5809	692679.4859
40312	24+73.99	23.67	215239.9547	692710.5253
40313	24+77.36	23.67	215238.8094	692713.7024

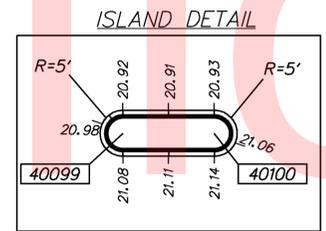
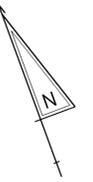


- NOTES:
1. OFFSETS SHOWN IN THE GEOMETRY TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE CONSTRUCTION BASELINE.
  2. UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND GUTTER IS GIVEN TO EDGE OF PAVEMENT.
  3. RADIARE GIVEN TO EDGE OF PAVEMENT.
  4. UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT INTESECTIONS ARE GIVEN AT 10 FOOT INTERVALS.
  5. ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501-CONSTRUCTION ENGINEERING.
  6. PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR EXISTING CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.

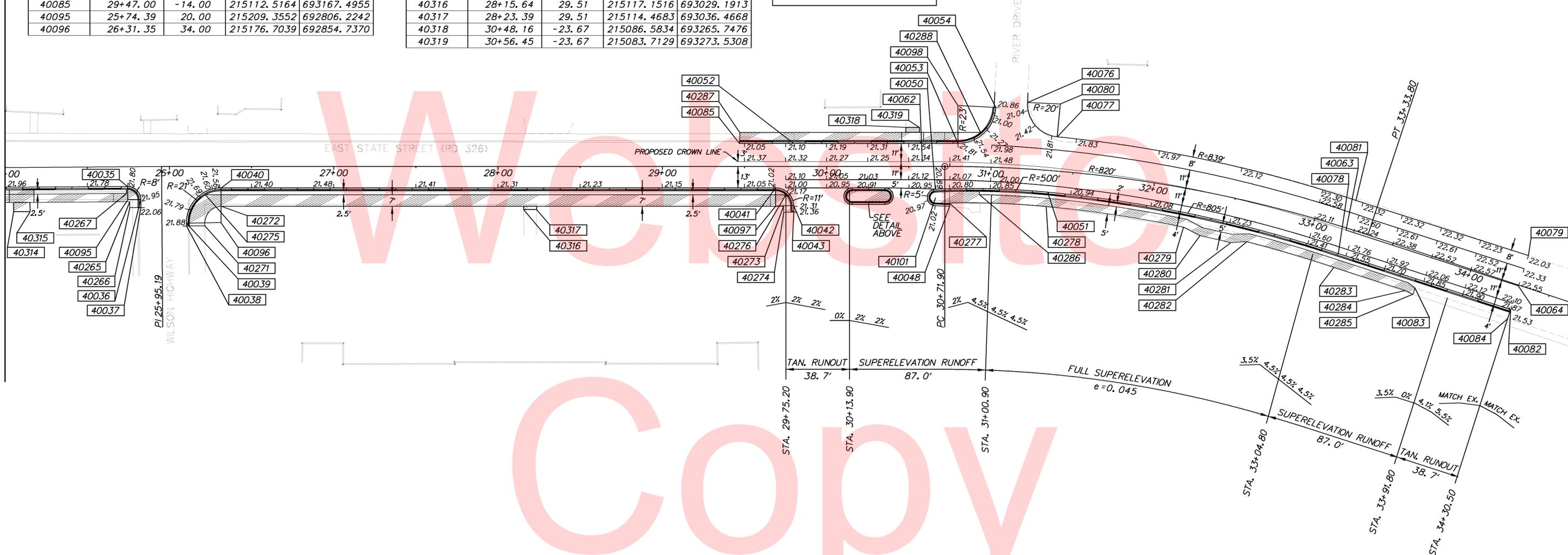
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40035	25+74.39	12.00	215216.8811	692808.9371
40036	25+82.39	20.00	215206.6422	692813.7501
40037	25+82.39	24.84	215202.0912	692812.1096
40038	26+10.39	36.12	215181.9651	692834.3406
40039	26+10.36	34.48	215183.5180	692834.8733
40040	26+31.35	13.00	215196.4066	692862.0036
40041	29+68.88	13.00	215079.6128	693178.6826
40042	29+79.88	23.76	215065.7164	693185.2789
40043	29+79.97	27.67	215062.0156	693184.0067
40048	30+74.84	24.19	215032.4740	693274.1431
40050	30+66.04	13.81	215045.2308	693269.5546
40051	31+35.51	13.19	215109.8701	693333.0929
40052	29+80.00	-14.00	215101.0976	693198.4570
40053	30+79.76	-14.04	215066.5315	693292.1807
40054	31+01.55	-36.31	215078.9934	693321.2550
40061	25+95.19	-3.00	215223.9379	692833.5933
40062	30+58.15	-3.00	215063.7367	693267.9689
40063	33+20.25	1.29	214935.4372	693495.5609
40064	34+30.50	0.55	215079.0122	693226.5503
40076	31+29.72	-22.93	215055.1614	693342.7641
40077	31+36.18	-20.80	215050.4675	693347.9188
40078	33+20.56	-17.71	214950.2907	693507.4089
40079	34+30.50	-18.92	214882.5914	693594.4053
40080	31+36.32	-32.80	215061.3045	693353.0724
40081	33+20.01	16.29	214923.7022	693486.2180
40082	34+30.50	11.64	214858.6976	693575.3462
40083	33+71.10	21.48	214888.0449	693523.7708
40084	34+30.50	21.42	214851.0544	693569.2496
40085	29+47.00	-14.00	215112.5164	693167.4955
40095	25+74.39	20.00	215209.3552	692806.2242
40096	26+31.35	34.00	215176.7039	692854.7370

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
40097	29+68.88	24.00	215069.2923	693174.8763
40098	30+79.55	-37.04	215088.1107	693300.1392
40099	30+15.45	18.00	215058.8078	693220.6442
40100	30+35.45	18.00	215051.8873	693239.4087
40101	30+66.25	18.81	215040.4712	693268.0231
40110	34+00.02	21.45	214870.0326	693545.4034
40265	25+75.72	21.17	215207.8053	692807.0825
40266	25+75.72	24.84	215204.3521	692805.8377
40267	25+73.72	16.17	215213.1872	692806.8966
40271	26+21.54	36.08	215178.1446	692844.8100
40272	26+31.50	24.17	215185.8787	692858.2769
40273	29+73.78	24.17	215067.4391	693179.4186
40274	29+73.87	27.80	215063.9992	693178.2361
40276	29+75.24	17.17	215073.5033	693183.2056
40277	30+74.93	22.19	215034.3199	693274.9190
40278	31+35.36	21.86	215012.0596	693329.3370
40279	32+22.30	23.32	214971.4436	693403.4512
40280	32+33.44	26.51	214963.1277	693411.0424
40281	32+53.29	26.85	214952.6001	693427.1051
40282	32+63.03	24.01	214949.7952	693436.5483
40283	33+19.89	24.95	214916.9083	693480.8369
40284	33+66.09	24.75	214888.6118	693516.8178
40285	33+71.09	25.51	214884.9053	693520.2550
40286	30+93.44	16.80	215032.8429	693293.7201
40287	29+47.04	-20.67	215118.7567	693169.8412
40288	30+93.08	-20.95	215068.0377	693307.3630
40314	25+04.96	26.82	215226.4848	692738.5996
40315	25+14.96	26.81	215223.1026	692748.0091
40316	28+15.64	29.51	215117.1516	693029.1913
40317	28+23.39	29.51	215114.4683	693036.4668
40318	30+48.16	-23.67	215086.5834	693265.7476
40319	30+56.45	-23.67	215083.7129	693273.5308

- NOTES:
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  - UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND GUTTER IS GIVEN TO EDGE OF PAVEMENT.
  - RADIARE GIVEN TO EDGE OF PAVEMENT.
  - UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT INTESECTIONS ARE GIVEN AT 10 FOOT INTERVALS.
  - ALL WORK REQUIRED FOR CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501-CONSTRUCTION ENGINEERING.
  - PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR EXISTING CURBS SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.
  - DIMENSIONS THROUGH THE SUPER ELEVATION ARE GIVEN TO THE CROWN LINE AND GRADE BREAK POINTS.



MATCHLINE STA. 25+00

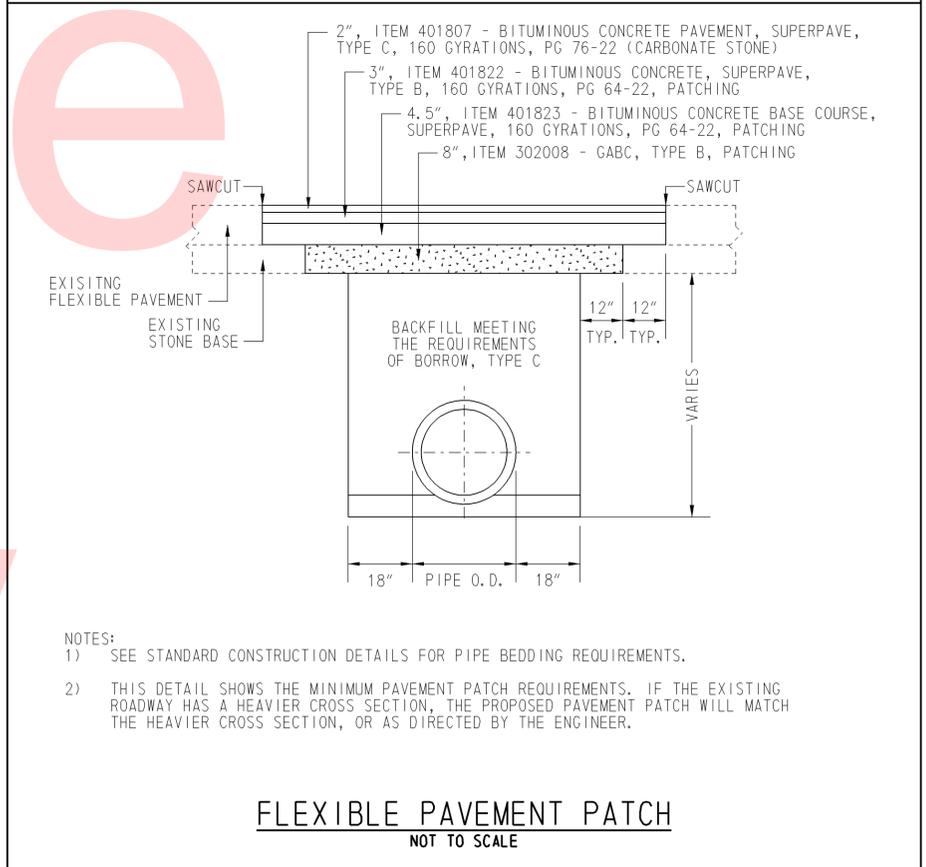
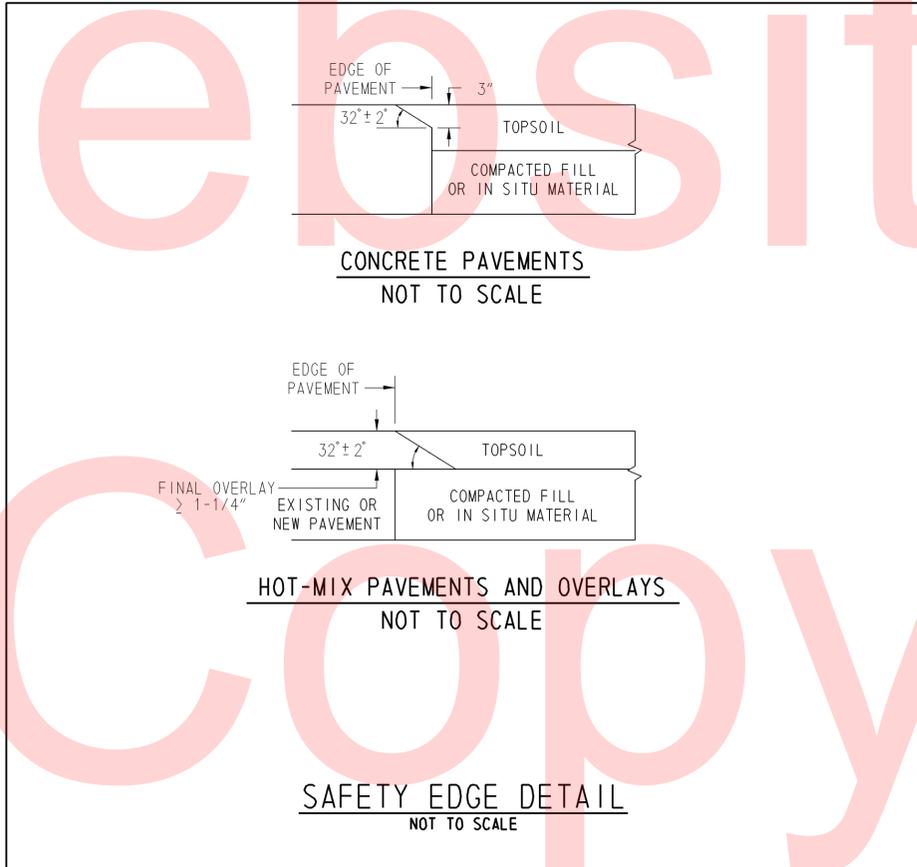
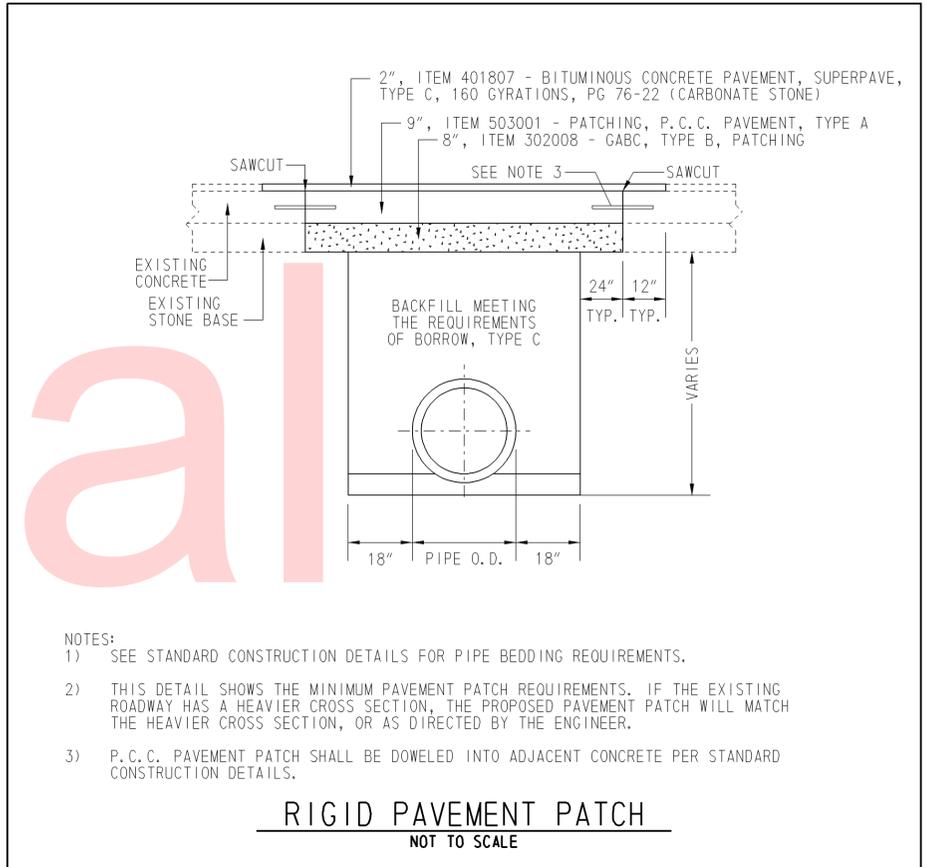


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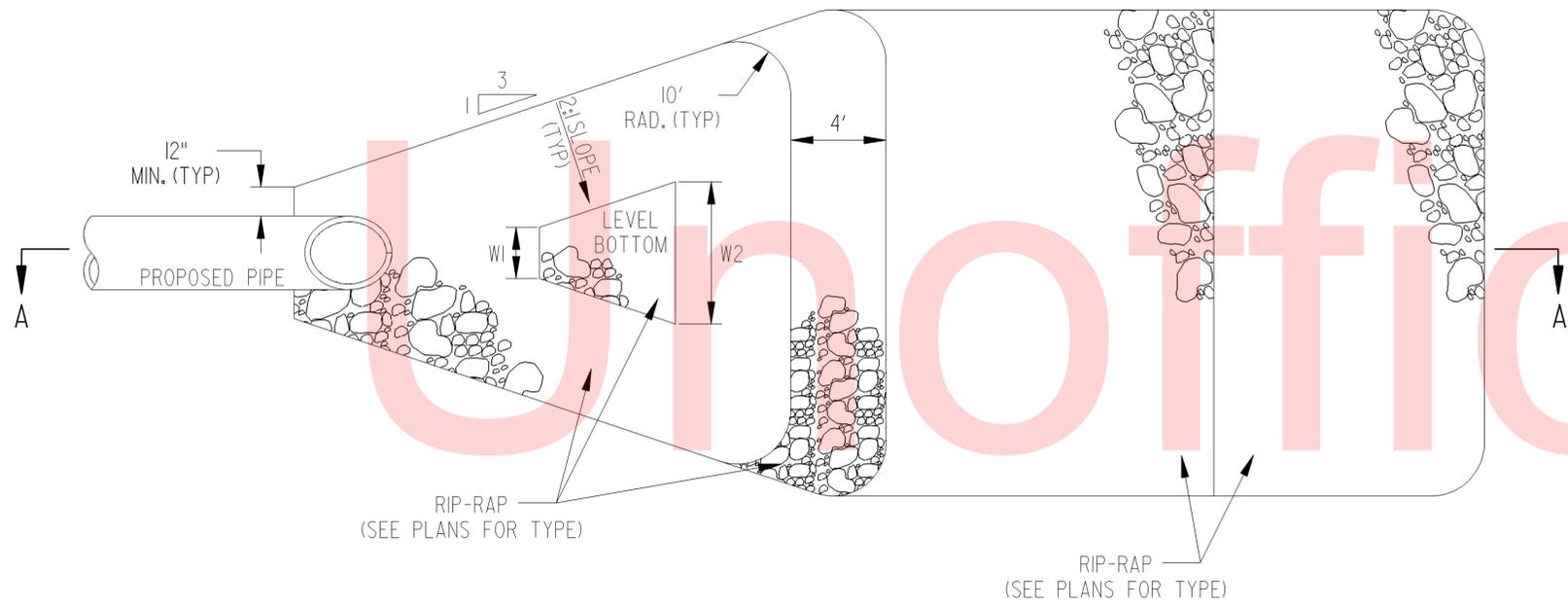
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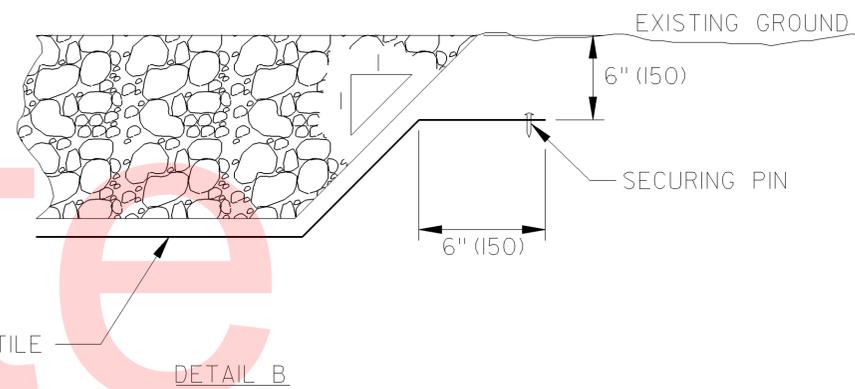


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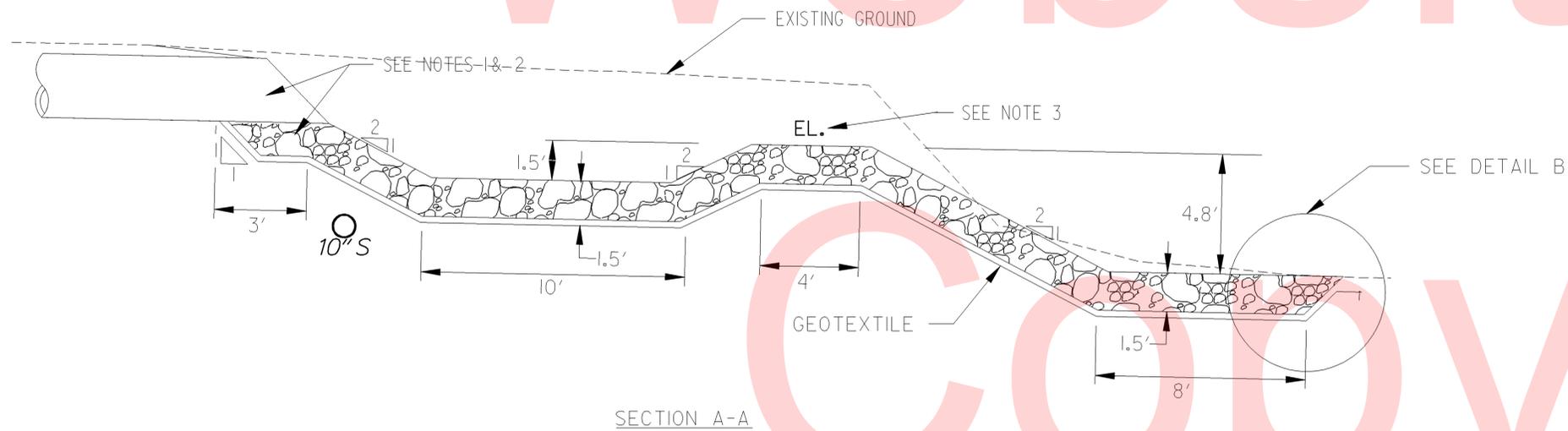
<p><b>DELAWARE</b> DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS		<p><b>NOT TO SCALE</b></p>	<p><b>HSIP SC,</b> <b>IRON BRANCH ROAD/STATE STREET</b></p>	CONTRACT	BRIDGE NO.	<b>N/A</b>	<p><b>CONSTRUCTION DETAILS</b></p>	SHEET NO.
	T200800903	DESIGNED BY: CM			17				
	COUNTY	CHECKED BY: BB			TOTAL SHTS.				
	SUSSEX				48				



PLAN VIEW



DETAIL B



SECTION A-A

RIPRAP ENERGY DISSIPATOR DETAIL  
NOT TO SCALE

NOTES:

1. RIPRAP IS TO BE PLACED PRIOR TO PLACING FLARED END SECTION.
2. PLACE DELAWARE NO. 3 STONE UNDER PIPE.
3. ELEVATION (EL.) SHOULD NOT BE HIGHER THAN PIPE INVERT.

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ENVIRONMENTAL COMPLIANCE NOTES

1. GENERAL NOTES:

- A. THE PURPOSE OF THIS SHEET IS TO IDENTIFY THOSE ITEMS ASSOCIATED WITH ENVIRONMENTAL COMPLIANCE. IMPACT CALCULATIONS ARE FOR THE AGENCY PERMIT REPORTING PURPOSES ONLY AND ARE NOT TO BE USED FOR BIDDING PURPOSES.
- B. IF A DEPARTURE FROM THE APPROVED PLANS (WHICH WOULD AFFECT ANY NATURAL AND/OR CULTURAL RESOURCES) IS NECESSARY, THE ENVIRONMENTAL STUDIES SECTION SHALL BE CONTACTED AT (302)760-2264 TO ALLOW FOR COORDINATION WITH THE APPROPRIATE RESOURCE AGENCIES AND APPROVAL.
- C. USE OF THIS SHEET DOES NOT ALLEVIATE THE CONTRACTOR'S RESPONSIBILITY TO COMPLY WITH ALL CONDITIONS SET FORTH IN THE ENVIRONMENTAL STATEMENT AND PERMITS.

NOTE: NO WETLANDS EXIST WITHIN THE PROJECT LIMITS.

SHEET PREPARED BY: BRYAN BEHRENS, DELDOT PROJECT MANAGER  
SHEET LAST UPDATED: 8-29-2016

ENVIRONMENTAL COMPLIANCE NOTES (CONTINUED)

2. NATURAL RESOURCE ISSUES:

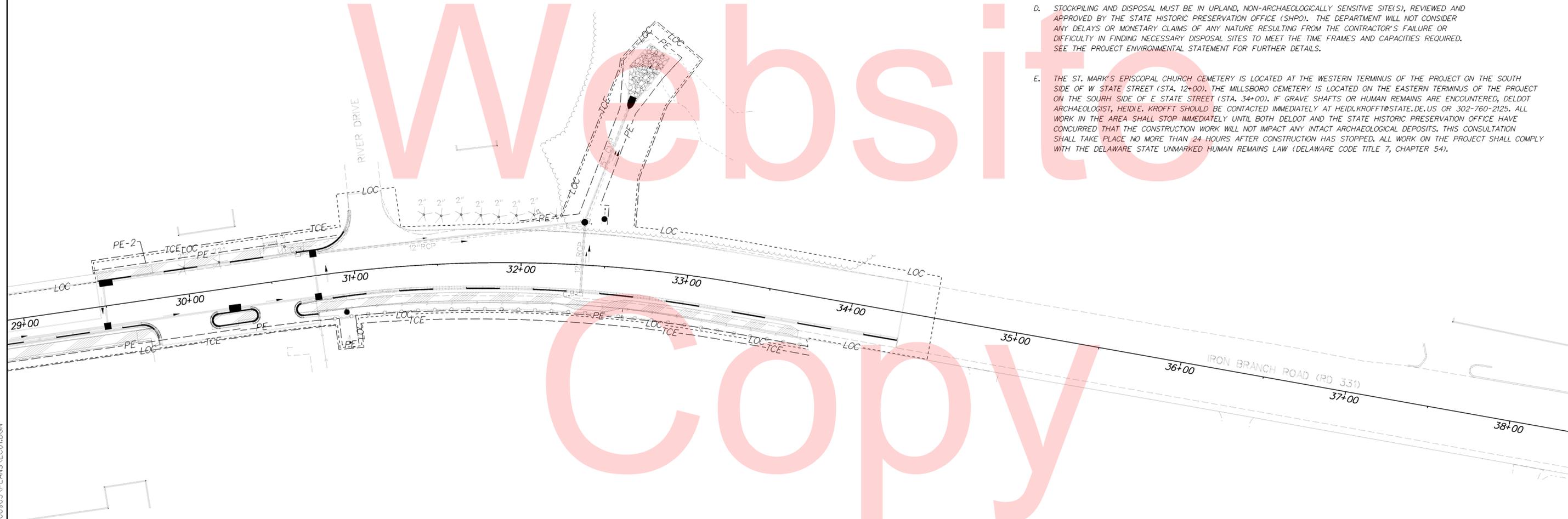
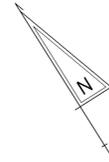
- A. PERMIT REQUIREMENTS/APPROVALS\*:
  - U.S. ARMY CORPS OF ENGINEERS (COE)..... NONE
  - DNREC - WETLANDS & SUBAQUEOUS LANDS (WLSL)..... NONE
  - DNREC - WATER QUALITY (WQC) & COASTAL ZONE CONSISTENCY (CZM)..... NONE
- B. CONSTRUCTION RESTRICTIONS:
  - FISHERIES..... NONE
  - ENDANGERED SPECIES..... NONE
  - MIGRATORY BIRDS..... NONE
- C. PROTECTION OF RESOURCES:
  - a. THE CONTRACTOR SHALL NOT UTILIZE THE AREA ON THE NORTH SIDE OF IRON BRANCH ROAD AFTER STATION 34+00 FOR CONSTRUCTION STOCKPILING AND STAGING FOR THIS PROJECT.
  - b. AT APPROXIMATE STA. 32+50 LEFT OFFSET THERE ARE WETLANDS JUST OUTSIDE OF THE LOC OF P19 AND ED1. IN ORDER TO PROTECT THESE WETLANDS SAND BAG SILT FENCE SHALL BE PLACED ALONG THE LOC IN THIS AREA. THE CONTRACTOR'S ACCESS BEYOND THE LOC IS STRICTLY PROHIBITED. SEE ALSO NOTES 2DA AND 3D.
- D. SILT FENCE INSTALLATION ADJACENT TO WOODED UPLANDS/WETLANDS:
  - a. SILT FENCE INSTALLATION ADJACENT TO WOODED UPLANDS/WETLANDS: SANDBAGS SHALL BE USED TO SECURE SILT FENCE IN LIEU OF TRENCHING PROVIDED PROPER EROSION & SEDIMENT CONTROL CAN BE MAINTAINED. SANDBAGS USED TO SECURE SILT FENCE SHALL BE INCIDENTAL TO ITEM NUMBER 905002 - REINFORCED SILT FENCE. THE ENVIRONMENTAL STUDIES SECTION (CAROL SULLIVAN, 302-760-2129) CAN PROVIDE FURTHER GUIDANCE REGARDING THIS METHOD OF INSTALLATION.
  - b. TREES TO BE REMOVED BETWEEN STA 32+00L AND 33+00L SHALL BE CLEARLY MARKED WITH PAINT PRIOR TO THE E&S SEDIMENT CONTROL/ENVIRONMENTAL MEETING.

ENVIRONMENTAL COMPLIANCE NOTES (CONTINUED)

3. CULTURAL RESOURCE ISSUES:

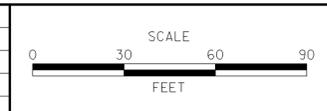
- A. THIS PROJECT TAKES PLACE WITHIN THE PROPOSED MILLSBORO DOWNTOWN HISTORIC DISTRICT. NO ADDITIONAL WORK BEYOND THE SCOPE INDICATED IN THESE PLANS SHALL BE PERFORMED. ANY PLAN MODIFICATIONS SIGNIFICANT IN NATURE OR NEW INFORMATION NOT INITIALLY KNOWN THAT ALTERS THE PLANS AND DESIGNS IS TO BE COORDINATED WITH THE DISTRICT ENGINEER AND THE ENVIRONMENTAL STUDIES OFFICE (CONTACT LAURA KEELEY (302) 760-2282 OR LAURAA.KEELEY@STATE.DE.US). IF IT IS DEEMED NECESSARY, COORDINATION WITH THE DELAWARE STATE HISTORIC PRESERVATION OFFICE (SHPO) AND FEDERAL HIGHWAY ADMINISTRATION (FHWA) WILL BE INITIATED BEFORE ANY ACTION IS IMPLEMENTED. PROPOSED OR NECESSARY MODIFICATIONS AND CHANGES WILL BE REVIEWED TO ENSURE PROPER TREATMENT AND CONSIDERATIONS TO HISTORIC PROPERTIES, PER DIALOGUE BETWEEN SHPO, FHWA, AFFECTED PROPERTY OWNER(S) AND THE CONTRACTOR.
- B. BEFORE CONSTRUCTION ACTIVITIES BEGIN, THE CONTRACTOR WILL RECORD AND MONITOR SITE CONDITIONS BY USE OF VIDEO OR OTHER DATA RECORDATION ACCORDING TO ITEM #211508. PAYMENT TO BE MADE UNDER ITEM #211508 - VIDEO MONITORING. AS PART OF THAT EFFORT, THE CONTRACTOR MAY ALSO UNDERTAKE FIELD MEASUREMENTS AND RECORD EXISTING CONDITIONS FOR DESIGN DETAILS AND ANY CONTRASTING SPECIFICATIONS. COPIES WILL BE PROVIDED TO THE APPROPRIATE PERSONNEL DURING THE PRE-CONSTRUCTION MEETING AND PRIOR TO CONSTRUCTION. THESE RECORDS WILL BE USED FOR DISPUTE RESOLUTION, SHOULD ANY DISPUTES ARISE.
- C. THE FOLLOWING PROPERTIES LOCATED WITHIN THE PROJECT LIMITS ARE CONSIDERED CONTRIBUTING TO THE MILLSBORO DOWNTOWN HISTORIC DISTRICT. SIX (6) OF THESE PROPERTIES ARE ELIGIBLE FOR THE NATIONAL REGISTER OF HISTORIC PLACES. ELIGIBLE PROPERTIES ARE LISTED BELOW AS SUCH AND ARE IDENTIFIED ON THE CONSTRUCTION PLAN SHEETS.
 

(14-L) 133-17.09-21.00 ELIGIBLE	(10-R) 133-17.13-173.00 ELIGIBLE
133-17.13-83.00 ELIGIBLE	(15-R) 133-17.13-72.00
(15-L) 133-17.13-129.00 ELIGIBLE	(17-R) 133-17.13-74.00
133-17.13-126.00 ELIGIBLE	(20-R) 133-17.14-18.00 ELIGIBLE
(25-L) 133-17.14-11.00	(21-R) 133-17.14-17.00
	(22-R) 133-17.14-27.00
- D. SHOULD ANY DAMAGE OCCUR TO ANY OF THESE CONTRIBUTING PROPERTIES AS A DIRECT RESULT OF THIS PROJECT UNDERTAKING, DELDOT'S ENVIRONMENTAL STUDIES PERSONNEL WILL BE CONTACTED PRIOR TO IMPLEMENTATION OF REPAIRS TO ENSURE REPAIRS WILL MEET THE SECRETARY OF INTERIOR STANDARDS FOR REHABILITATION AND/OR TREATMENT OF HISTORIC PROPERTIES. ANY REPAIR MEASURES WILL AVOID AND MINIMIZE POTENTIAL ADVERSE EFFECTS AS DETERMINED BY DELDOT, SHPO AND FHWA.
- E. STOCKPILING AND DISPOSAL MUST BE IN UPLAND, NON-ARCHAEOLOGICALLY SENSITIVE SITE(S), REVIEWED AND APPROVED BY THE STATE HISTORIC PRESERVATION OFFICE (SHPO). THE DEPARTMENT WILL NOT CONSIDER ANY DELAYS OR MONETARY CLAIMS OF ANY NATURE RESULTING FROM THE CONTRACTOR'S FAILURE OR DIFFICULTY IN FINDING NECESSARY DISPOSAL SITES TO MEET THE TIME FRAMES AND CAPACITIES REQUIRED. SEE THE PROJECT ENVIRONMENTAL STATEMENT FOR FURTHER DETAILS.
- F. THE ST. MARK'S EPISCOPAL CHURCH CEMETERY IS LOCATED AT THE WESTERN TERMINUS OF THE PROJECT ON THE SOUTH SIDE OF W STATE STREET (STA. 12+00). THE MILLSBORO CEMETERY IS LOCATED ON THE EASTERN TERMINUS OF THE PROJECT ON THE SOUTH SIDE OF E STATE STREET (STA. 34+00). IF GRAVE SHAFTS OR HUMAN REMAINS ARE ENCOUNTERED, DELDOT ARCHAEOLOGIST, HEIDIE KROFFT SHOULD BE CONTACTED IMMEDIATELY AT HEIDIE.KROFFT@STATE.DE.US OR 302-760-2125. ALL WORK IN THE AREA SHALL STOP IMMEDIATELY UNTIL BOTH DELDOT AND THE STATE HISTORIC PRESERVATION OFFICE HAVE CONCURRED THAT THE CONSTRUCTION WORK WILL NOT IMPACT ANY INTACT ARCHAEOLOGICAL DEPOSITS. THIS CONSULTATION SHALL TAKE PLACE NO MORE THAN 24 HOURS AFTER CONSTRUCTION HAS STOPPED. ALL WORK ON THE PROJECT SHALL COMPLY WITH THE DELAWARE STATE UNMARKED HUMAN REMAINS LAW (DELAWARE CODE TITLE 7, CHAPTER 54).



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ADDENDUMS / REVISIONS	



CONTRACT	BRIDGE NO.	<b>N/A</b>
T200800903	DESIGNED BY: CM	
COUNTY	CHECKED BY: BB	
SUSSEX		

# TRAFFIC CONTROL NOTES

1. THE CONTRACTOR SHALL NOT BEGIN WORK ON ANY SUBSEQUENT PHASE UNTIL ALL WORK FROM THE PREVIOUS PHASE IS COMPLETE WITH THE EXCEPTION OF THE FOLLOWING:

CONTRACTOR IS PROHIBITED FROM PERFORMING WORK IN PHASE 4 EXCEPT IN THE SUMMER MONTHS WHEN SCHOOL IS NOT IN SESSION. PHASE 4 MAY BE PERFORMED PRIOR TO PHASE 1 THRU PHASE 3, IN ORDER TO ACHIEVE THIS SCHEDULE.

IF PROJECT TIMING DICTATES THAT PHASE 4 CANNOT BEGIN BECAUSE SCHOOL IS IN SESSION, SUMMER BREAK IS MORE THAN A MONTH OUT AND PHASE 1 THROUGH 3 HAVE ALREADY BEEN COMPLETED, THE FOLLOWING MAY OCCUR TO ALLOW WORK TO BEGIN ON PHASE 5:

DRAINAGE WORK FOR PHASE 4 MAY BE COMPLETED OVER SPRING BREAK WITH LANE CLOSURES. IF SPRING BREAK HAS ALREADY PASSED OR IS MORE THAN A MONTH OUT, WEEKEND WORK MAY BE ALLOWED IF APPROVED BY THE ENGINEER. SEE NOTE 4 REGARDING MAINTENANCE OF TRAFFIC DURING LANE CLOSURES.

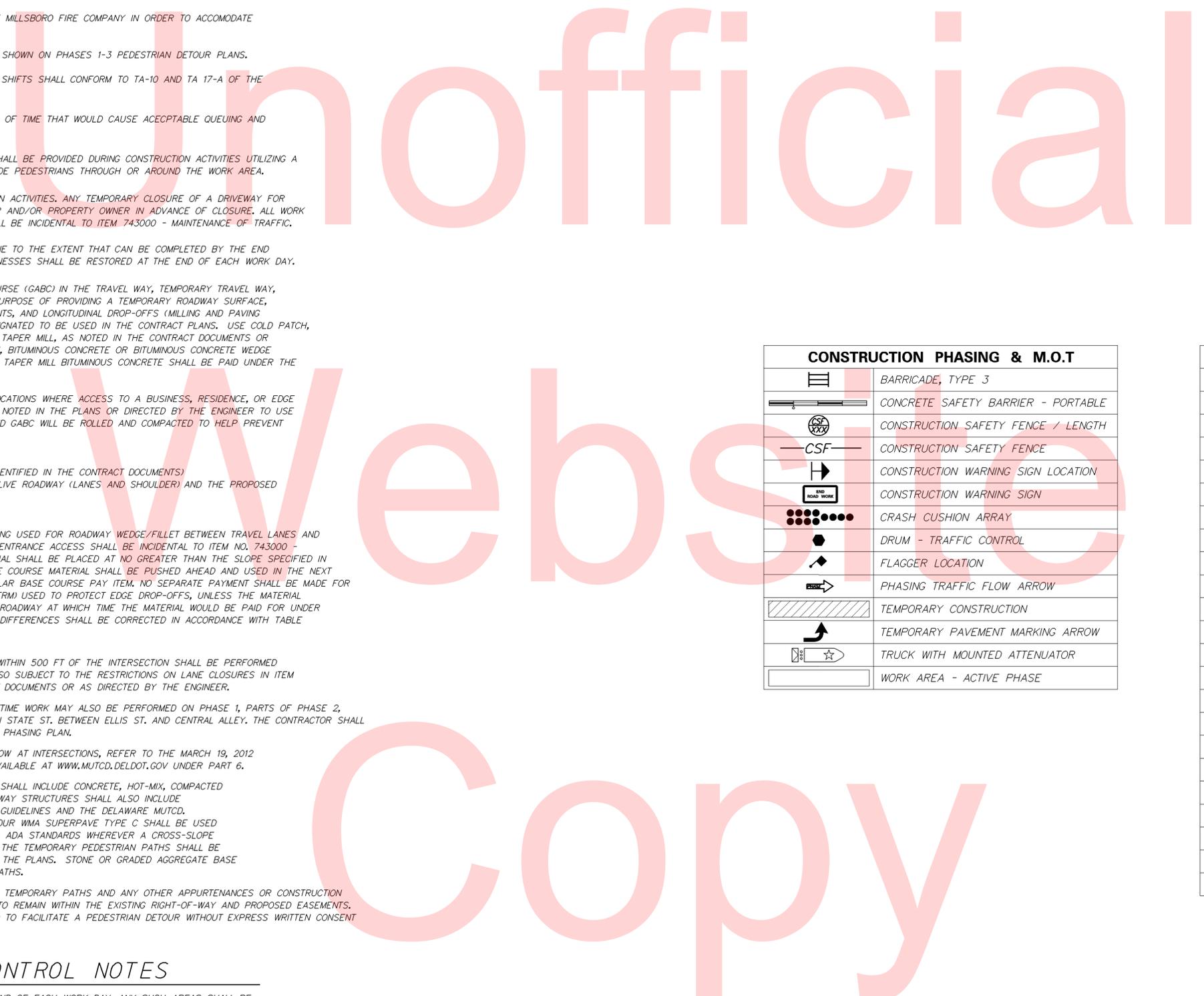
2. PHASES 3, 4, 5 & 7 WORK SHALL BE COORDINATED WITH THE MILLSBORO FIRE COMPANY IN ORDER TO ACCOMMODATE EMERGENCY RESPONSE SITUATIONS.
3. EXISTING PAVEMENT MARKINGS SHALL BE UTILIZED EXCEPT AS SHOWN ON PHASES 1-3 PEDESTRIAN DETOUR PLANS.
4. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL CONFORM TO TA-10 AND TA 17-A OF THE DELAWARE MUTCD.
5. FLAGGING OPERATIONS SHOULD ONLY BE DONE FOR A LENGTH OF TIME THAT WOULD CAUSE ACCEPTABLE QUEUING AND MINIMAL DELAYS IN BOTH DIRECTIONS.
6. PEDESTRIAN ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
7. VEHICULAR ACCESS SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES. ANY TEMPORARY CLOSURE OF A DRIVEWAY FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND/OR PROPERTY OWNER IN ADVANCE OF CLOSURE. ALL WORK AND MATERIALS TO PROVIDE AND MAINTAIN THIS ACCESS SHALL BE INCIDENTAL TO ITEM 743000 - MAINTENANCE OF TRAFFIC.
8. THE CONTRACTOR SHALL LIMIT THE LENGTH OF THE WORK ZONE TO THE EXTENT THAT CAN BE COMPLETED BY THE END OF EACH WORK DAY. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.
9. A. THE USE OF MILLINGS AND GRADED AGGREGATE BASE COURSE (GABC) IN THE TRAVEL WAY, TEMPORARY TRAVEL WAY, HIGH VOLUME ENTRANCES AND ACCESS RAMP FOR THE PURPOSE OF PROVIDING A TEMPORARY ROADWAY SURFACE, POT HOLE REPAIR, TAPERED EDGE FOR UTILITIES, BUTT JOINTS, AND LONGITUDINAL DROP-OFFS (MILLING AND PAVING OPERATIONS) IS PROHIBITED UNLESS IT IS OTHERWISE DESIGNATED TO BE USED IN THE CONTRACT PLANS. USE COLD PATCH, BITUMINOUS CONCRETE, BITUMINOUS CONCRETE WEDGE, OR TAPER MILL, AS NOTED IN THE CONTRACT DOCUMENTS OR APPROVED BY THE ENGINEER. PAYMENT FOR COLD PATCH, BITUMINOUS CONCRETE OR BITUMINOUS CONCRETE WEDGE SHALL BE PAID AS NOTED IN THE CONTRACT DOCUMENTS. TAPER MILL BITUMINOUS CONCRETE SHALL BE PAID UNDER THE BITUMINOUS CONCRETE MILLING ITEM.
- B. MILLINGS OR GABC SHALL BE USED AT THE FOLLOWING LOCATIONS WHERE ACCESS TO A BUSINESS, RESIDENCE, OR EDGE DROP OFF NEEDS TO BE MAINTAINED UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE ENGINEER TO USE BITUMINOUS CONCRETE OR COLD PATCH. ALL MILLINGS AND GABC WILL BE ROLLED AND COMPACTED TO HELP PREVENT THE MATERIAL FROM UNRAVELLING:
  - a. DRIVEWAYS
  - b. ENTRANCES
  - c. LOW VOLUME ACCESS RAMPS (IDENTIFIED IN THE CONTRACT DOCUMENTS)
  - d. EDGE DROP-OFFS ADJACENT TO LIVE ROADWAY (LANES AND SHOULDER) AND THE PROPOSED ROAD CONSTRUCTION
  - e. EDGE OF ROADWAY DROP-OFF

GRADING AND MAINTAINING BASE COURSE THAT IS BEING USED FOR ROADWAY WEDGE/FILLET BETWEEN TRAVEL LANES AND PAVEMENT BOX, EDGE OF TRAVELWAY, DRIVEWAY OR ENTRANCE ACCESS SHALL BE INCIDENTAL TO ITEM NO. 743000 - MAINTENANCE OF TRAFFIC. THE BASE COURSE MATERIAL SHALL BE PLACED AT NO GREATER THAN THE SLOPE SPECIFIED IN TABLE 6G-1 AND SHALL BE COMPACTED. EXCESS BASE COURSE MATERIAL SHALL BE PUSHED AHEAD AND USED IN THE NEXT SEGMENT AND SHALL BE INCIDENTAL TO THE PARTICULAR BASE COURSE PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR MILLINGS OR GABC TEMPORARY ROADWAY MATERIAL (TRM) USED TO PROTECT EDGE DROP-OFFS, UNLESS THE MATERIAL IS EVENTUALLY UTILIZED AS PART OF A PERMANENT ROADWAY AT WHICH TIME THE MATERIAL WOULD BE PAID FOR UNDER THE RESPECTIVE CONTRACT MATERIAL ITEM. VERTICAL DIFFERENCES SHALL BE CORRECTED IN ACCORDANCE WITH TABLE 6G-1 OF THE DELAWARE MUTCD.

10. WORK WHICH IMPACTS THE FLOW OF TRAFFIC ON SR 24 OR WITHIN 500 FT OF THE INTERSECTION SHALL BE PERFORMED BETWEEN THE HOURS OF 7:00 PM AND 6:00 A.M. AND IS ALSO SUBJECT TO THE RESTRICTIONS ON LANE CLOSURES IN ITEM 743000 OR OTHER RESTRICTIONS AS NOTED IN THE CONTRACT DOCUMENTS OR AS DIRECTED BY THE ENGINEER.
11. CONSTRUCTION PHASE 3 WILL REQUIRE NIGHTTIME WORK. NIGHTTIME WORK MAY ALSO BE PERFORMED ON PHASE 1, PARTS OF PHASE 2, AND PHASE 6. NIGHTTIME WORK MAY ONLY BE PERFORMED ON STATE ST. BETWEEN ELLIS ST. AND CENTRAL ALLEY. THE CONTRACTOR SHALL MAINTAIN TRAFFIC FLOW AS INDICATED ON THE CONSTRUCTION PHASING PLAN.
12. FOR CONSTRUCTION ACTIVITIES WHICH IMPACT THE TRAFFIC FLOW AT INTERSECTIONS, REFER TO THE MARCH 19, 2012 MEMO ON TEMPORARY TRAFFIC CONTROL AT INTERSECTIONS, AVAILABLE AT WWW.MUTCD.DELDOT.GOV UNDER PART 6.
13. ACCEPTABLE MATERIALS FOR TEMPORARY PEDESTRIAN PATHS SHALL INCLUDE CONCRETE, HOT-MIX, COMPACTED MILLINGS OR PLYWOOD WALKWAY STRUCTURE. PLYWOOD WALKWAY STRUCTURES SHALL ALSO INCLUDE DETECTABLE EDGING AND RAILINGS IN ACCORDANCE WITH ADA GUIDELINES AND THE DELAWARE MUTCD. DURING CONSTRUCTION, PRIOR TO INSTALLING PEDESTRIAN DETOUR WMA SUPERPAVE TYPE C SHALL BE USED TO BRING THE PEDESTRIAN DETOUR WALKWAY COMPLIANT WITH ADA STANDARDS WHEREVER A CROSS-SLOPE GREATER THAN 2% EXISTS ON THE SHOULDER, THIS AND ALL THE TEMPORARY PEDESTRIAN PATHS SHALL BE INCIDENTAL TO ITEM 743000 UNLESS OTHERWISE SPECIFIED IN THE PLANS. STONE OR GRADED AGGREGATE BASE COURSE SHALL NOT BE USED FOR TEMPORARY PEDESTRIAN PATHS.
14. ALL SIGNS, BARRICADES, FLAGGERS, TEMPORARY CURB RAMPS, TEMPORARY PATHS AND ANY OTHER APPURTENANCES OR CONSTRUCTION ACTIVITIES ASSOCIATED WITH THE PEDESTRIAN DETOURS ARE TO REMAIN WITHIN THE EXISTING RIGHT-OF-WAY AND PROPOSED EASEMENTS. NO TRESSPASSING ONTO PRIVATE PROPERTY WILL BE ALLOWED TO FACILITATE A PEDESTRIAN DETOUR WITHOUT EXPRESS WRITTEN CONSENT FROM THE PROPERTY OWNER.

# EROSION & SEDIMENT CONTROL NOTES

1. NO BARE SOIL AREAS ARE TO BE LEFT UNCOVERED AT THE END OF EACH WORK DAY. ANY SUCH AREAS SHALL BE COVERED WITH GABC.



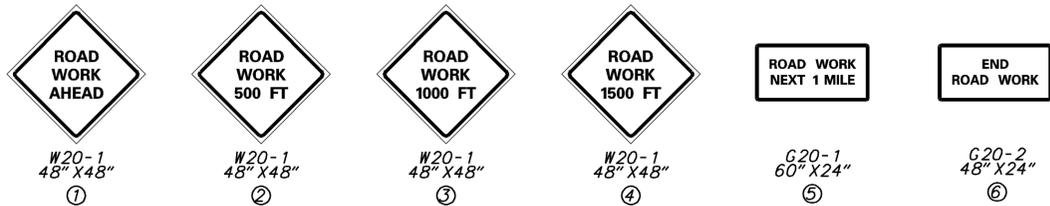
CONSTRUCTION PHASING & M.O.T	
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE

EROSION & SEDIMENT CONTROL	
	DEWATERING BAG
	DEWATERING BASIN
	EARTH DIKE
	INLET SEDIMENT CONTROL
	PERIMETER DIKE/SWALE
	PORTABLE SEDIMENT TANK
	SANDBAG DIKE
	SANDBAG DIVERSION
	STONE CHECK DAM
	STABILIZED CONSTRUCTION ENTRANCE
	SILT FENCE / LENGTH
	SILT FENCE
	SILT FENCE - REINFORCED
	SUMP PIT
	SEDIMENT TRAP / NUMBER
	SEDIMENT TRAP
	SEDIMENT TRAP WITH INLET AS OUTLET
	SEDIMENT TRAP PIPE OUTLET
	STILLING WELL
	TEMPORARY SWALE
	TEMPORARY SLOPE DRAIN
	TURBIDITY CURTAIN / LENGTH
	TURBIDITY CURTAIN

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<b>DELAWARE DEPARTMENT OF TRANSPORTATION</b>	ADDENDUMS / REVISIONS		NOT TO SCALE	<b>HSIP SC, IRON BRANCH ROAD/STATE STREET</b>	CONTRACT	BRIDGE NO.	N/A	<b>CONSTRUCTION PHASING, MOT AND EROSION CONTROL NOTES</b>	SHEET NO.
	T200800903				DESIGNED BY: CM	20			
	COUNTY	CHECKED BY: BB			TOTAL SHTS.				
	SUSSEX				48				

**WARNING SIGN LEGEND**

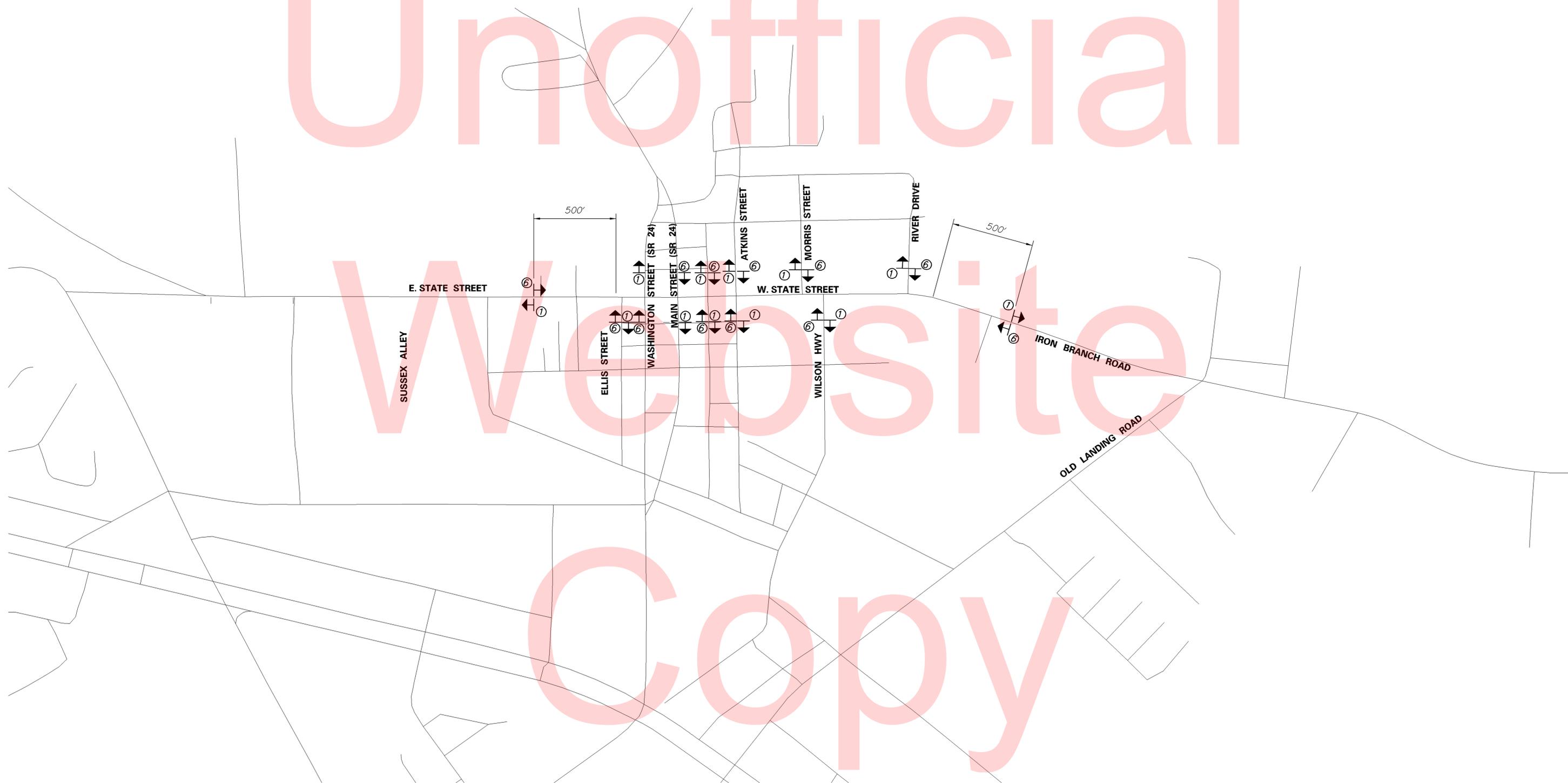


**NOTES:**

1. SEE INDIVIDUAL PHASING PLANS FOR PLACEMENT OF TEMPORARY WARNING SIGNS. FINAL LOCATION OF ALL PERMANENT WARNING SIGNS SHALL BE COORDINATED WITH THE ENGINEER.
2. ALL WARNING SIGNS SHOWN ON THIS SHEET ARE TO BE DISPLAYED THROUGHOUT THE DURATION OF THE CONTRACT AND ARE TO BE PAID FOR UNDER ITEM 743024 - TEMPORARY WARNING SIGNS AND PLAQUES.
3. ANY EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION WARNING SIGNS SHALL BE COVERED AS NEEDED, AS DIRECTED BY THE ENGINEER.



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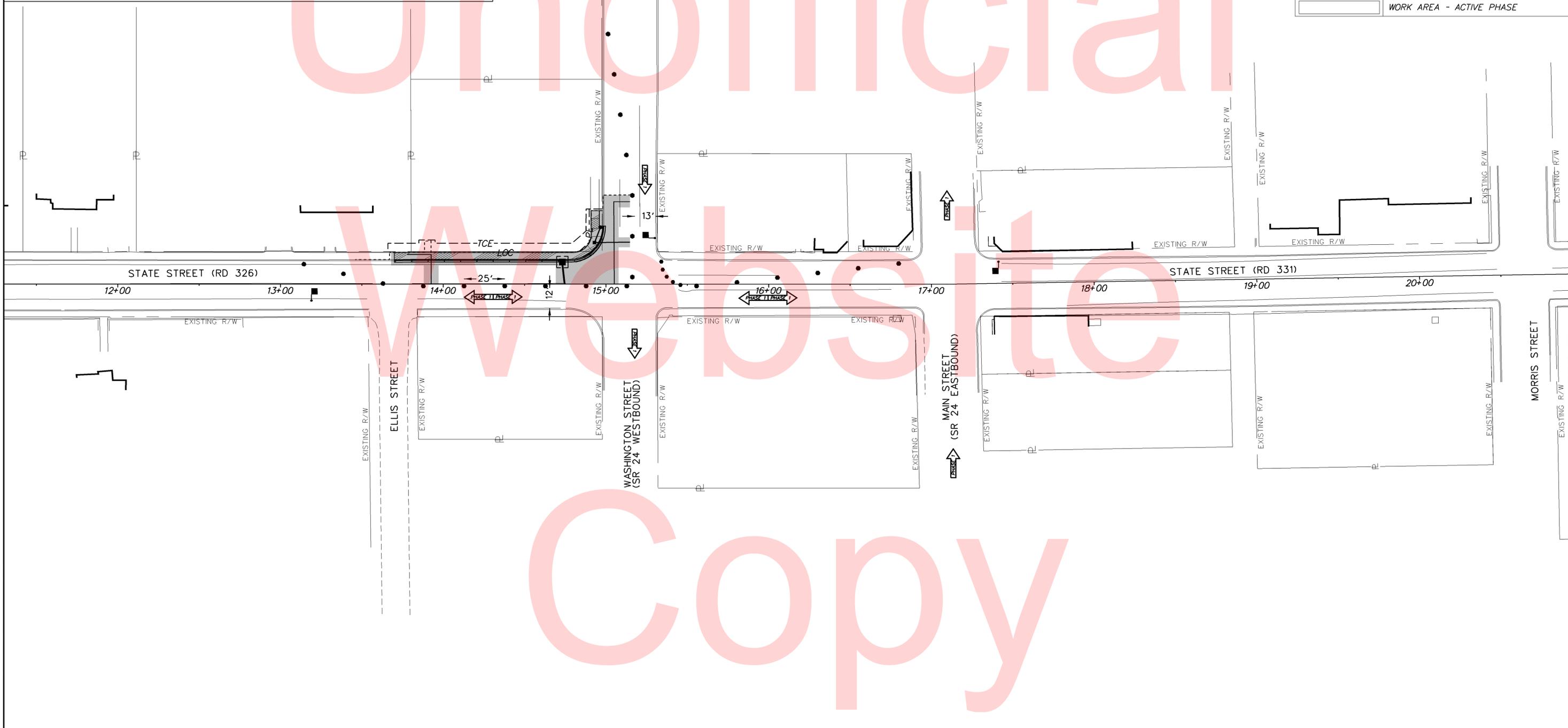
<p><b>DELAWARE</b> DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS		<p>SCALE</p> <p>FEET</p>	<p><b>HSIP SC,</b> <b>IRON BRANCH ROAD/STATE STREET</b></p>		<p>CONTRACT T200800903</p>	<p>BRIDGE NO. <b>N/A</b></p>	<p><b>PERMANENT WARNING</b> <b>SIGNS</b></p>	<p>SHEET NO. 21</p>
						<p>COUNTY SUSSEX</p>	<p>DESIGNED BY: CM</p>		<p>TOTAL SHTS. 48</p>
							<p>CHECKED BY: BB</p>		

**SEQUENCE OF CONSTRUCTION - PHASE 1**

1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES INCLUDING THOSE ON THE PEDESTRIAN DETOUR PLAN.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
3. PLACE TEMPORARY BLACK OUT STRIPING.
4. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL CONFORM TO TA-10 & TA-27 OF THE DELAWARE MUTCD. SHIFT WESTBOUND TRAFFIC TO THE EAST SIDE OF WASHINGTON STREET. FLAG TRAFFIC TO THE SOUTH SIDE OF STATE STREET. MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES.
5. REMOVE EXISTING CURB AND SIDEWALK. PLACE PROPOSED UTILITIES & SIGNAL CONDUIT.
6. EXCAVATE AND REPLACE PIPE ON NORTH SIDE OF STATE ST.
7. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK. PLACE PROPOSED WARM MIX PATCHING UP TO THE TOP OF TYPE B WARM MIX LAYER.
8. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
9. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
10. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND PEDESTRIAN DETOUR SIGNS.

**CONSTRUCTION PHASING & M.O.T**

	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



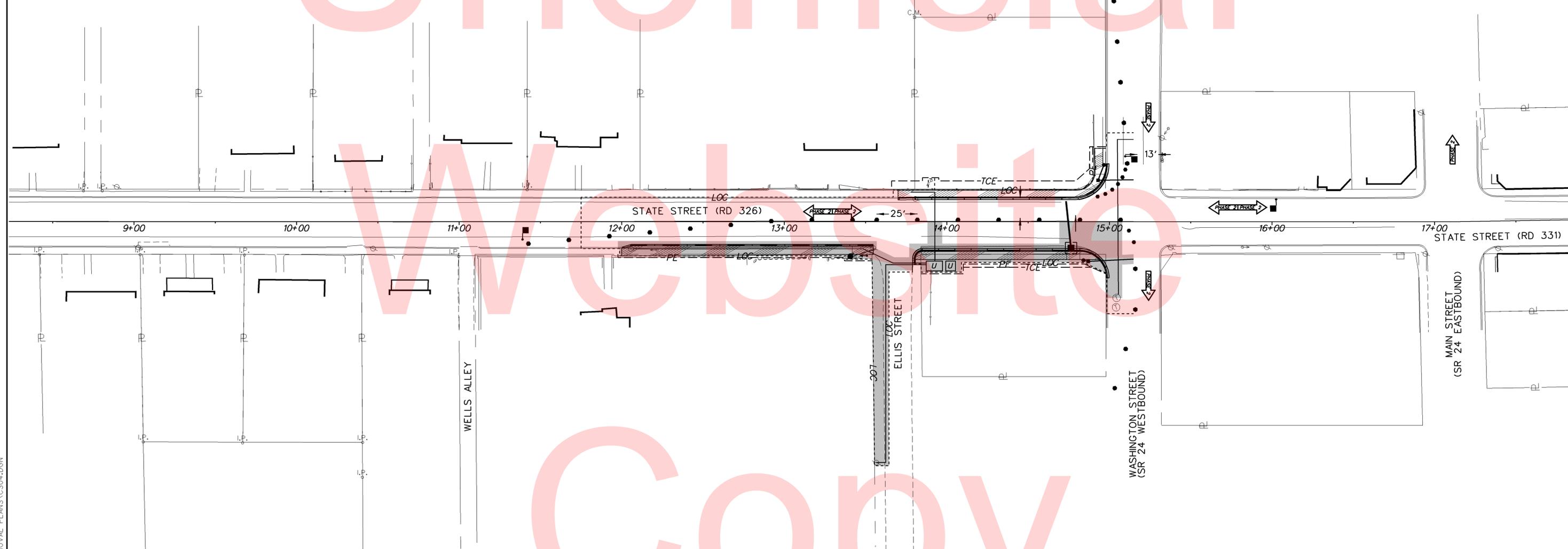
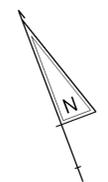
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**SEQUENCE OF CONSTRUCTION - PHASE 2**

1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES INCLUDING THOSE ON THE PEDESTRIAN DETOUR PLAN.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
3. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL CONFORM TO TA-27 & TA-10 OF THE DE MUTCD. SHIFT WESTBOUND TRAFFIC TO THE EAST SIDE OF WASHINGTON STREET. FLAG TRAFFIC TO THE NORTH SIDE OF STATE STREET MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES.
4. REMOVE EXISTING CURB AND SIDEWALK. PLACE PROPOSED UTILITIES & SIGNAL CONDUIT.
5. EXCAVATE AND REPLACE PIPE & DRAINAGE INLET ON SOUTH SIDE OF STATE ST.
6. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK. PLACE PROPOSED WARM MIX PATCHING UP TO THE TOP OF TYPE B WARM MIX LAYER.
7. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
8. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
9. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND PEDESTRIAN DETOUR SIGNS.

**CONSTRUCTION PHASING & M.O.T**

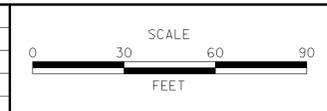
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



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**DELAWARE DEPARTMENT OF TRANSPORTATION**

ADDENDUMS / REVISIONS	



**HSIP SC,  
IRON BRANCH ROAD/STATE STREET**

CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

**CONSTRUCTION PHASING,  
M.O.T., AND EROSION  
CONTROL PLAN - PHASE 2**

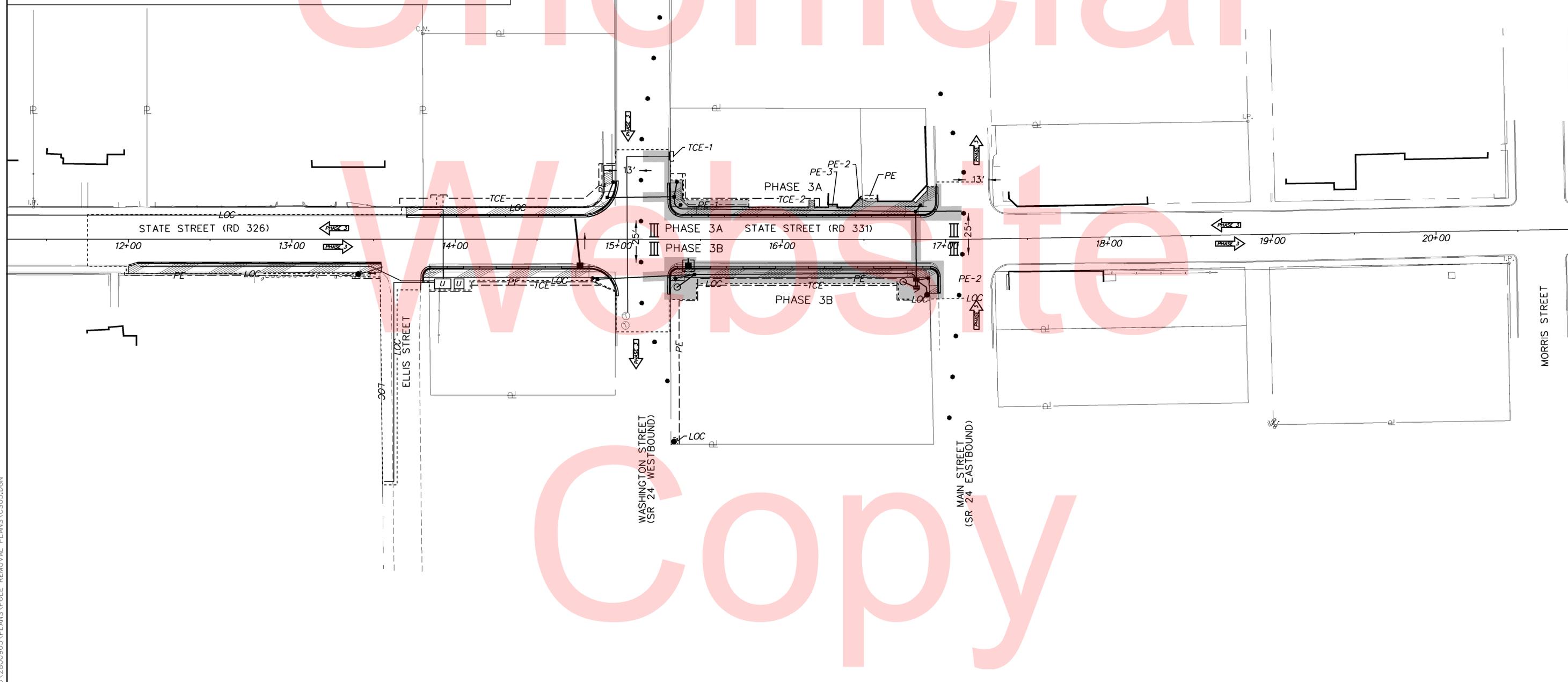
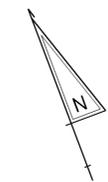
SHEET NO. 23
TOTAL SHTS. 48

**SEQUENCE OF CONSTRUCTION - PHASE 3**

1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES SHOWN ON THE ROUTE 24 DETOUR PLAN AND THE PHASE 3 PEDESTRIAN DETOUR PLAN.
2. BREAK PHASE 3 INTO TWO SECTIONS: PHASE 3A, NORTH SIDE OF STATE STREET AND PHASE 3B, SOUTH SIDE OF STATE STREET. THIS WILL FACILITATE DETOURING PEDESTRIANS AROUND THE WORK ZONE. WORK MAY NOT BEGIN IN PHASE 3B UNTIL CONSTRUCTION IN PHASE 3A IS COMPLETE.
3. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES AND LANE SHIFTS SHALL CONFORM TO TA-20 OF THE DE MUTCD. MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES.
4. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
5. CURB RAMP ON THE NORTHWEST CORNER OF STATE ST. AND MAIN ST. IN FRONT OF PARCEL 39-L TO BE BUILT AT NIGHT OR A MUTUALLY AGREEABLE TIME WITH THE OWNER OF THE BUSINESS SO THAT IT WILL NOT ADVERSELY AFFECT ACCESS TO THE BUSINESS.
6. REMOVE EXISTING CURB AND SIDEWALK.
7. PLACE PROPOSED UTILITIES & SIGNAL CONDUIT.
8. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK. PLACE PROPOSED WARM MIX PATCHING UP TO THE TOP OF TYPE B WARM MIX LAYER.
9. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
10. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
11. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND PEDESTRIAN DETOUR SIGNS FOR PHASE 3 AND OPEN STATE STREET TO TRAFFIC.

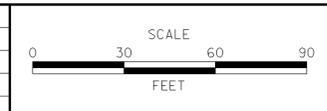
**CONSTRUCTION PHASING & M.O.T**

	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



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ADDENDUMS / REVISIONS



CONTRACT	BRIDGE NO.	<b>N/A</b>
T200800903	DESIGNED BY:	CM
COUNTY	CHECKED BY:	BB
SUSSEX		

**SEQUENCE OF CONSTRUCTION - PHASE 4**

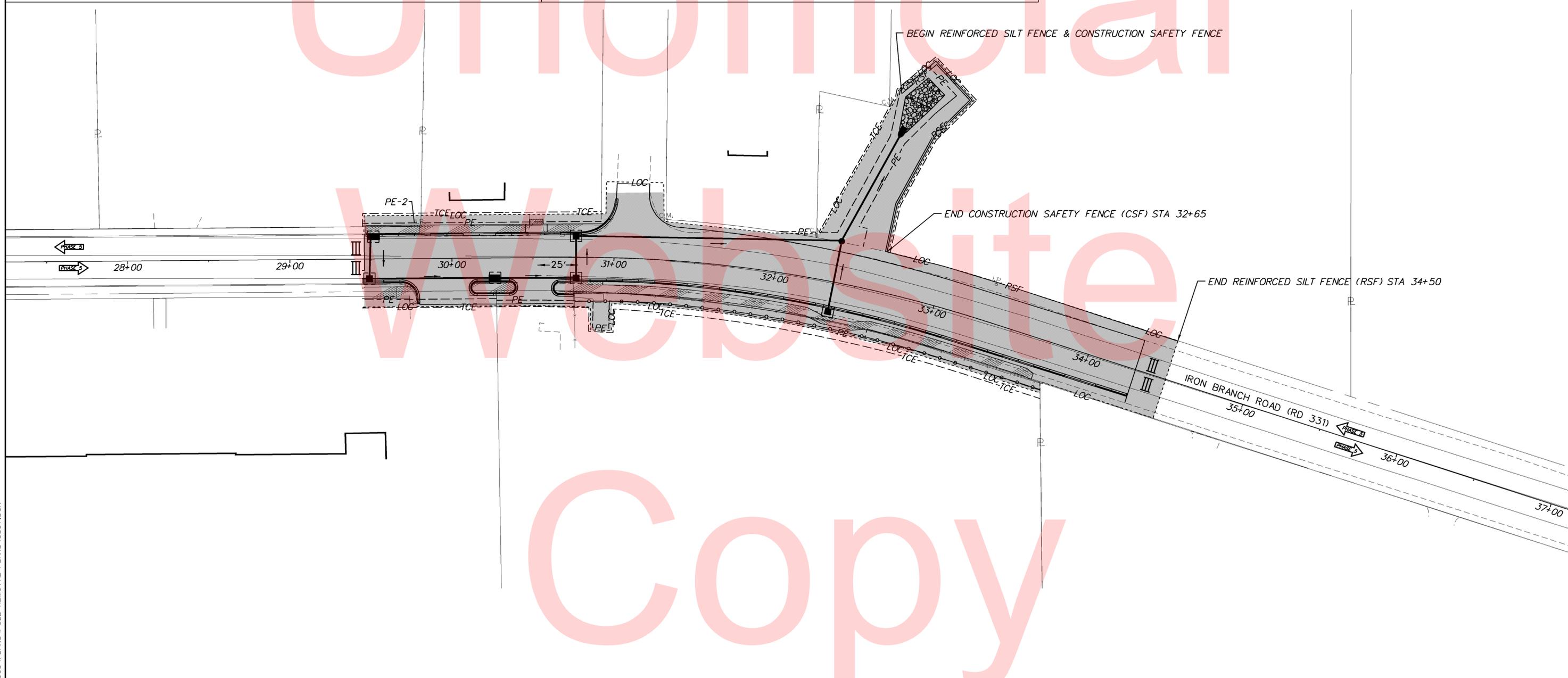
1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES AS SHOWN ON THE IRON BRANCH ROAD DETOUR PLAN AND PEDESTRIAN DETOUR PLAN. THIS DETOUR SHALL ONLY BE IMPLEMENTED DURING THE SUMMER TIME PERIOD WHEN SCHOOL IS NOT IN SESSION.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
3. EXCAVATE AND INSTALL RIPRAP, PIPE AND MANHOLE.
4. EXCAVATE AND INSTALL DRAINAGE SYSTEM UPSTREAM OF MANHOLE.
5. REMOVE EXISTING CURB AND SIDEWALK ON THE NORTH SIDE OF EAST STATE ST.
6. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK ON THE NORTH SIDE OF EAST STATE ST.
7. BEGIN RECONSTRUCTION OF THE ROAD INCLUDING THE SUPERELEVATION UP TO THE TOP OF THE TYPE B WARM MIX LAYER. LEAVE THE SIDEWALK ON THE SOUTH SIDE OF STATE STREET IN PLACE UNTIL THE ROADWORK IS COMPLETED TO PROVIDE A ROUTE THROUGH THE WORK ZONE FOR PEDESTRIANS. ITEM 5 & 6 MAY BE COMPLETED CONCURRENTLY WITH ITEM 7.
8. CONSTRUCT A TEMPORARY PATH, USING AN APPROVED BASE MATERIAL, AROUND THE PROPOSED SIDEWALK ON THE SOUTH SIDE OF STATE STREET TO ROUTE PEDESTRIANS AROUND THE WORK ZONE. ALTERNATIVELY, PEDESTRIANS MAY BE DIRECTED ONTO THE SHOULDER WITH THE USE OF FLAGGERS WHOSE SOLE RESPONSIBILITY IS TO DIRECT PEDESTRIANS SAFELY THROUGH THE WORK ZONE.

9. REMOVE EXISTING CURB AND SIDEWALK ON THE SOUTH SIDE OF STATE ST.
10. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK ON THE SOUTH SIDE OF STATE ST.
11. REMOVE THE TEMPORARY PATH (IF USED) TO DETOUR PEDESTRIANS AROUND THE WORK ZONE.
12. INSTALL PROPOSED FENCE ON THE SOUTH SIDE OF STATE ST.
13. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
14. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
15. OPEN IRON BRANCH ROAD TO TRAFFIC AND REMOVE ALL MOT DEVICES FOR PHASE 4.



**CONSTRUCTION PHASING & M.O.T**

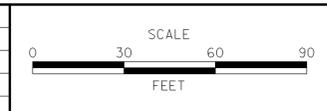
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



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ADDENDUMS / REVISIONS	



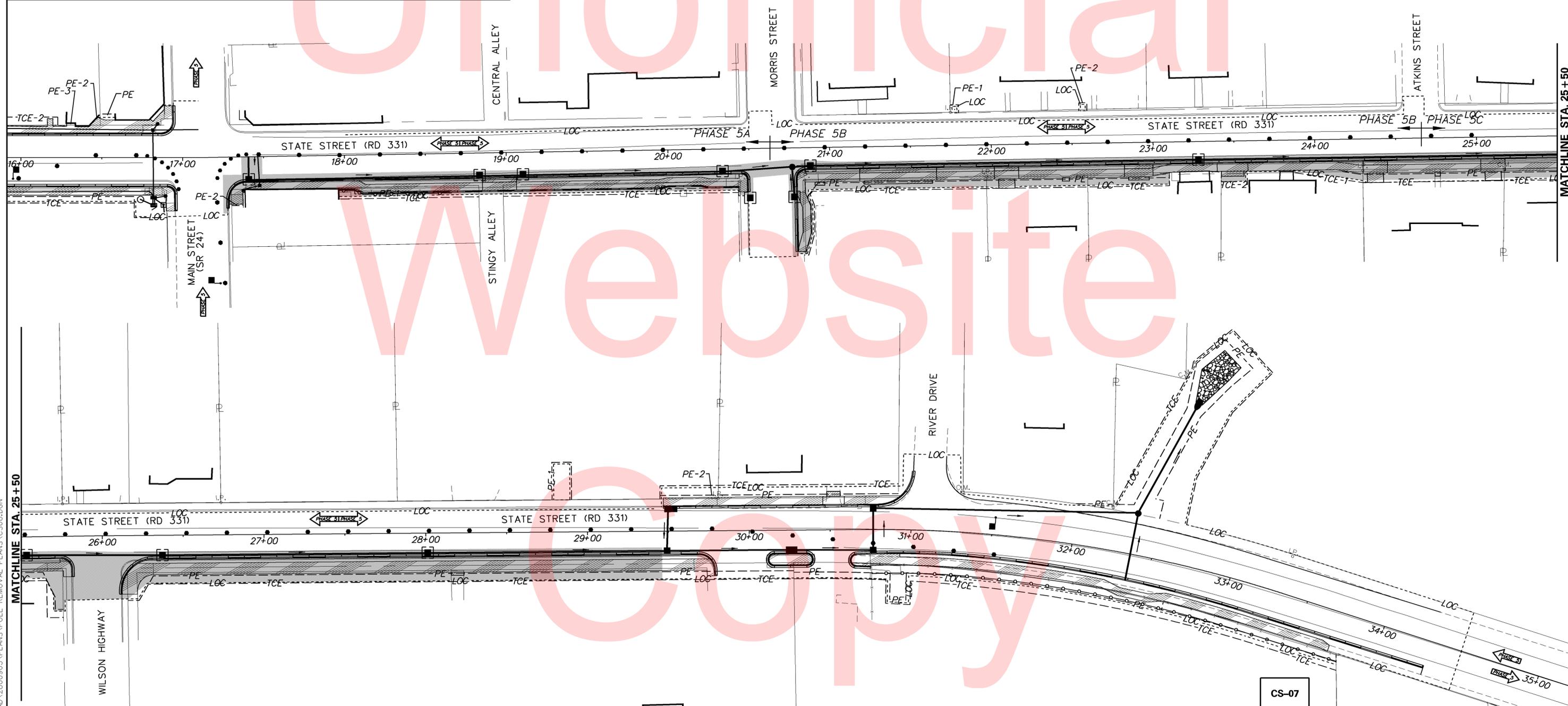
CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

**SEQUENCE OF CONSTRUCTION - PHASE 5**

1. WHERE FEASIBLE, ALL NECESSARY UTILITIES SHALL BE RELOCATED BEFORE CONSTRUCTION.
2. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES. BREAK PHASE 5 INTO THREE SECTIONS, PHASE 5A, 5B & 5C. WORK MAY NOT BEGIN IN A SUBSEQUENT SECTION UNTIL CONSTRUCTION IN THE PREVIOUS SECTION HAS BEEN COMPLETED.
3. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
4. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL CONFORM TO TA-10 & TA-27 OF THE DE MUTCD. FLAG TRAFFIC TO THE SOUTH SIDE OF STATE STREET. SHIFT EASTBOUND TRAFFIC TO THE WEST SIDE OF MAIN ST. UTILIZE TA-28 OF THE DE MUTCD TO DETOUR PEDESTRIANS AROUND THE WORK ZONE. MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES. SEE ALSO TRAFFIC CONTROL NOTE \*5.
5. REMOVE EXISTING CURB AND SIDEWALK.
6. PLACE PROPOSED UTILITIES AND SIGNAL CONDUIT.
7. EXCAVATE AND INSTALL DRAINAGE SYSTEM ON THE SOUTH SIDE OF STATE STREET.
8. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK.
9. PLACE PROPOSED WARM MIX PATCHING UP TO THE TOP OF TYPE B WARM MIX LAYER.
10. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
11. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.

**CONSTRUCTION PHASING & M.O.T**

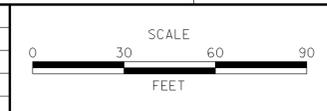
	BARRICADE, TYPE 3
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	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



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**DELAWARE DEPARTMENT OF TRANSPORTATION**

ADDENDUMS / REVISIONS



**HSIP SC, IRON BRANCH ROAD/STATE STREET**

CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN - PHASE 5**

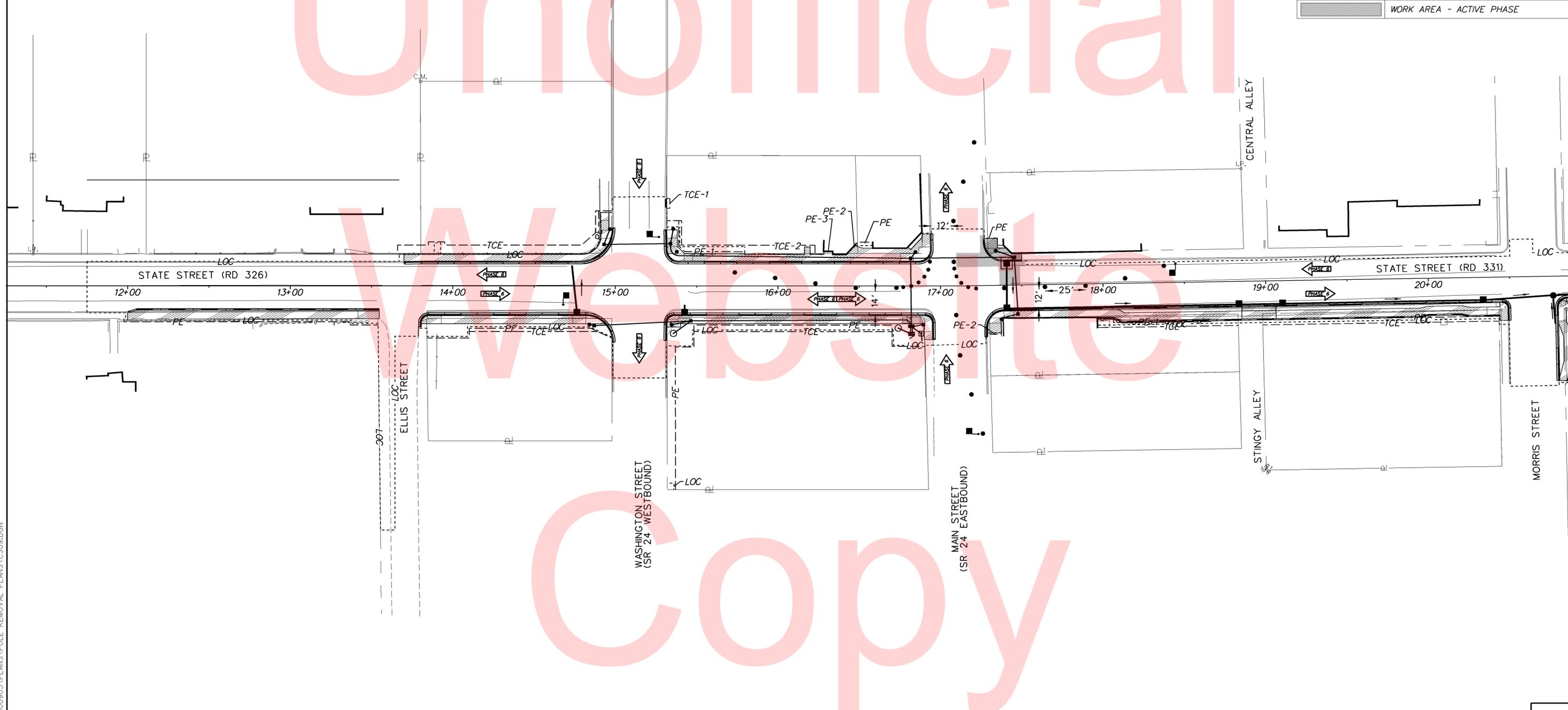
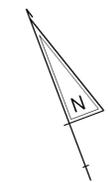
SHEET NO. 26
TOTAL SHTS. 48

**SEQUENCE OF CONSTRUCTION - PHASE 6**

1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES INCLUDING THOSE ON THE PEDESTRIAN DETOUR PLAN.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES.
3. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL CONFORM TO TA-10 & TA-27 OF THE DE MUTCD. SHIFT EASTBOUND TRAFFIC TO THE WEST SIDE OF MAIN STREET. FLAG TRAFFIC TO THE SOUTH SIDE OF STATE STREET. MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES.
4. REMOVE EXISTING CURB AND SIDEWALK. PLACE PROPOSED UTILITIES & SIGNAL CONDUIT.
5. EXCAVATE AND INSTALL DRAINAGE SYSTEM ON THE NORTH SIDE OF STATE ST.
6. CONSTRUCT PROPOSED CURB & GUTTER AND SIDEWALK.
7. PLACE PROPOSED WARM MIX PATCHING UP TO THE TOP OF TYPE B WARM MIX LAYER.
8. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
9. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
10. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND PEDESTRIAN DETOUR SIGNS.

**CONSTRUCTION PHASING & M.O.T**

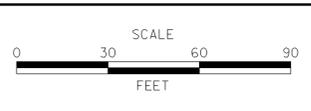
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



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Unofficial  
Working Site  
Copy

ADDENDUMS / REVISIONS	



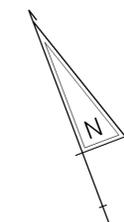
CONTRACT	BRIDGE NO.	N/A
T200800903	DESIGNED BY:	CM
COUNTY	CHECKED BY:	BB
SUSSEX		

**SEQUENCE OF CONSTRUCTION - PHASE 7**

1. INSTALL ALL ADVANCE WARNING SIGNS AND TEMPORARY TRAFFIC CONTROL DEVICES.
3. MAINTENANCE OF TRAFFIC DURING CONSTRUCTION SHALL CONFORM TO TA-10 & TA-17A OF THE DE MUTCD.
4. MILL AND OVERLAY ROADWAY USING TA-10 OF THE DEMUTCD.
5. INSTALL ALL PROPOSED SIGNING AND STRIPING USING TA-17A OF THE DE MUTCD.
8. STABILIZE WORK AREAS ACCORDING TO DIVISION 900 OF THE DELAWARE STANDARD SPECIFICATIONS.
9. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
10. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES.

**CONSTRUCTION PHASING & M.O.T**

	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE



Unofficial

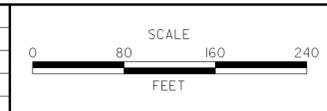
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ADDENDUMS / REVISIONS	

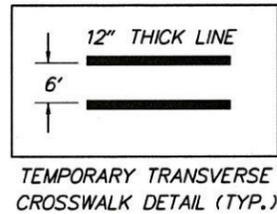


CONTRACT	BRIDGE NO.	N/A
T200800903	DESIGNED BY:	CM
COUNTY	CHECKED BY:	BB
SUSSEX		

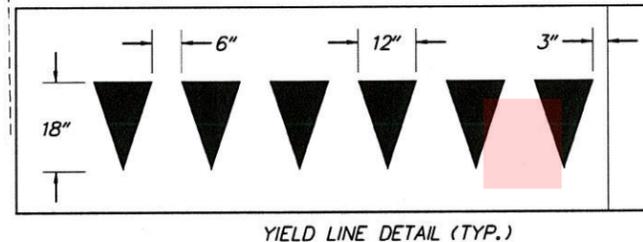
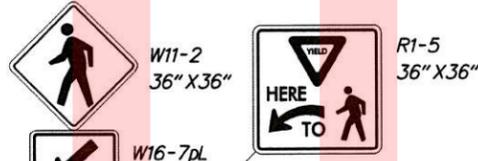
**SPECIAL SIGNS**

**PHASE 1 - STATE STREET @ WASHINGTON STREET**

1. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
2. WORK SHALL BE PERFORMED IN SECTIONS THAT CAN BE COMPLETED BY THE END OF THE WORK DAY. THE WORK AREA SHALL BE RESTORED AT THE END OF EACH WORK DAY USING AN ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.
3. CONTRACTOR TO SUPPLY DETAIL OR CATALOG CUT OUT FOR APPROVAL BY THE ENGINEER FOR THE TEMPORARY CURB RAMP.



TEMPORARY CURB RAMP



YIELD LINE DETAIL (TYP.)

**SPECIAL NOTES**

**PHASE 2 - STATE STREET @ WASHINGTON STREET**

1. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
2. WORK SHALL BE PERFORMED IN SECTIONS THAT CAN BE COMPLETED BY THE END OF THE WORK DAY. THE WORK AREA SHALL BE RESTORED AT THE END OF EACH WORK DAY USING AN ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.
3. CONTRACTOR TO SUPPLY DETAIL OR CATALOG CUT OUT FOR APPROVAL BY THE ENGINEER FOR THE TEMPORARY CURB RAMP.
4. WORK SHALL NOT BEGIN IN PHASE 2 UNTIL PHASE 1 HAS BEEN COMPLETED.

TEMPORARY CURB RAMP



TEMPORARY STOP LINE (TYP.)  
TEMPORARY CROSSWALK (TYP.)  
TEMPORARY CURB RAMP & WALKWAY

**LEGEND**

**GENERAL NOTES**

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
8. "O" BARRICADE SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK OR PEDESTRIAN PATH.
9. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.
10. PEDESTRIAN DETOUR TRAILBLAZERS NOT ATTACHED TO BARRICADES ARE TO EITHER BE GROUND MOUNTED OR ATTACHED TO AN EXISTING SIGN POST AT THE LOCATION SHOWN ON THE PLAN.
11. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON PEDESTRIAN CHANNELIZING BARRICADE AND TEMPORARY PEDESTRIAN PATH.
12. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON TEMPORARY PEDESTRIAN CURB RAMPS AND TEMPORARY PEDESTRIAN CROSSWALK.

NOTE: STORMWATER MANAGEMENT FACILITIES, INCLUDING BEST MANAGEMENT PRACTICE (BMP) FACILITIES, MAY BE PRESENT WITHIN THE LIMITS OF THIS PROJECT. PLEASE CONSULT THE CONSTRUCTION PLANS FOR THIS PROJECT TO DETERMINE THE TYPE AND LOCATION OF THESE FACILITIES.

RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_ RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_ RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_ APPROVED CHIEF SAFETY OFFICER \_\_\_\_\_ DATE: 8/9/2016 APPROVED TRAFFIC ENGINEER \_\_\_\_\_ DATE: 8/9/2016

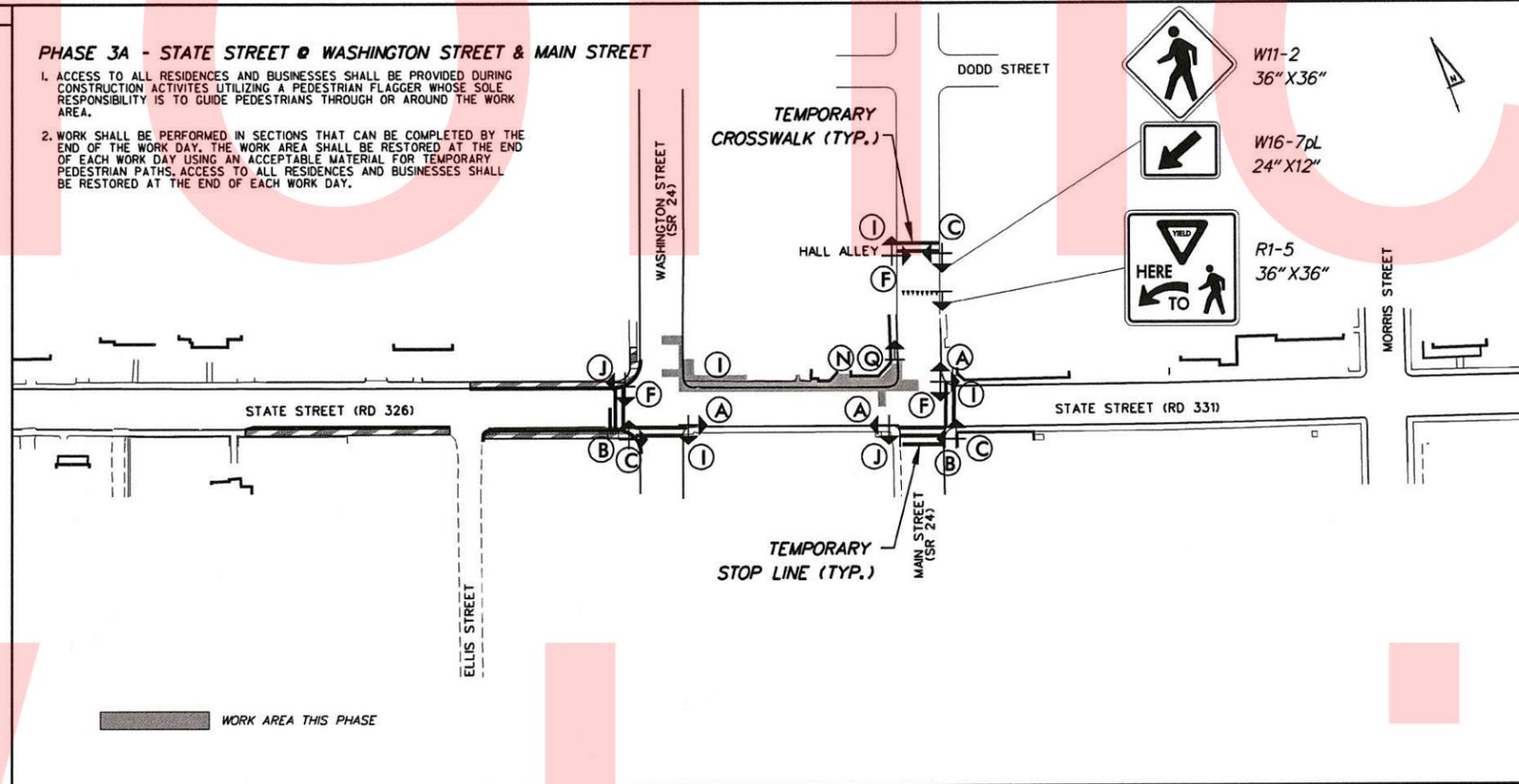
<p><b>DELAWARE DEPARTMENT OF TRANSPORTATION</b></p>	ADDENDUM / REVISIONS	NOT TO SCALE	<p>HSIP SC, IRON BRANCH ROAD/STATE STREET</p>	<p>CONTRACT: T200800903</p> <p>COUNTY: SUSSEX</p>	<p>BRIDGE NO.: NA</p> <p>DESIGNED BY: CM</p> <p>CHECKED BY: BB</p>	<p><b>PEDESTRIAN DETOUR PLAN</b></p>	<p>SHEET NO.: 29</p> <p>TOTAL SHTS.: 48</p>

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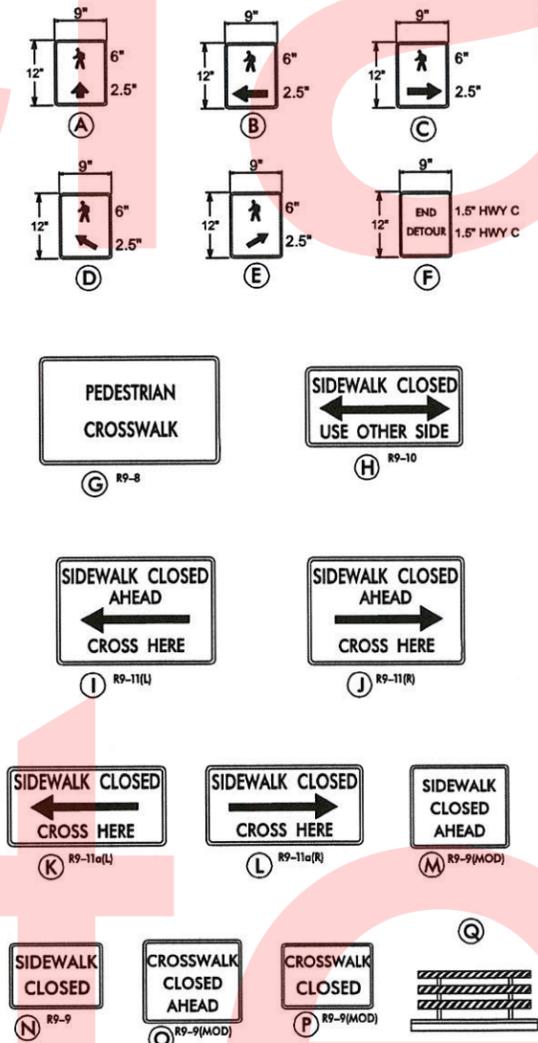
**SPECIAL SIGNS**

**PHASE 3A - STATE STREET @ WASHINGTON STREET & MAIN STREET**

1. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
2. WORK SHALL BE PERFORMED IN SECTIONS THAT CAN BE COMPLETED BY THE END OF THE WORK DAY. THE WORK AREA SHALL BE RESTORED AT THE END OF EACH WORK DAY USING AN ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.



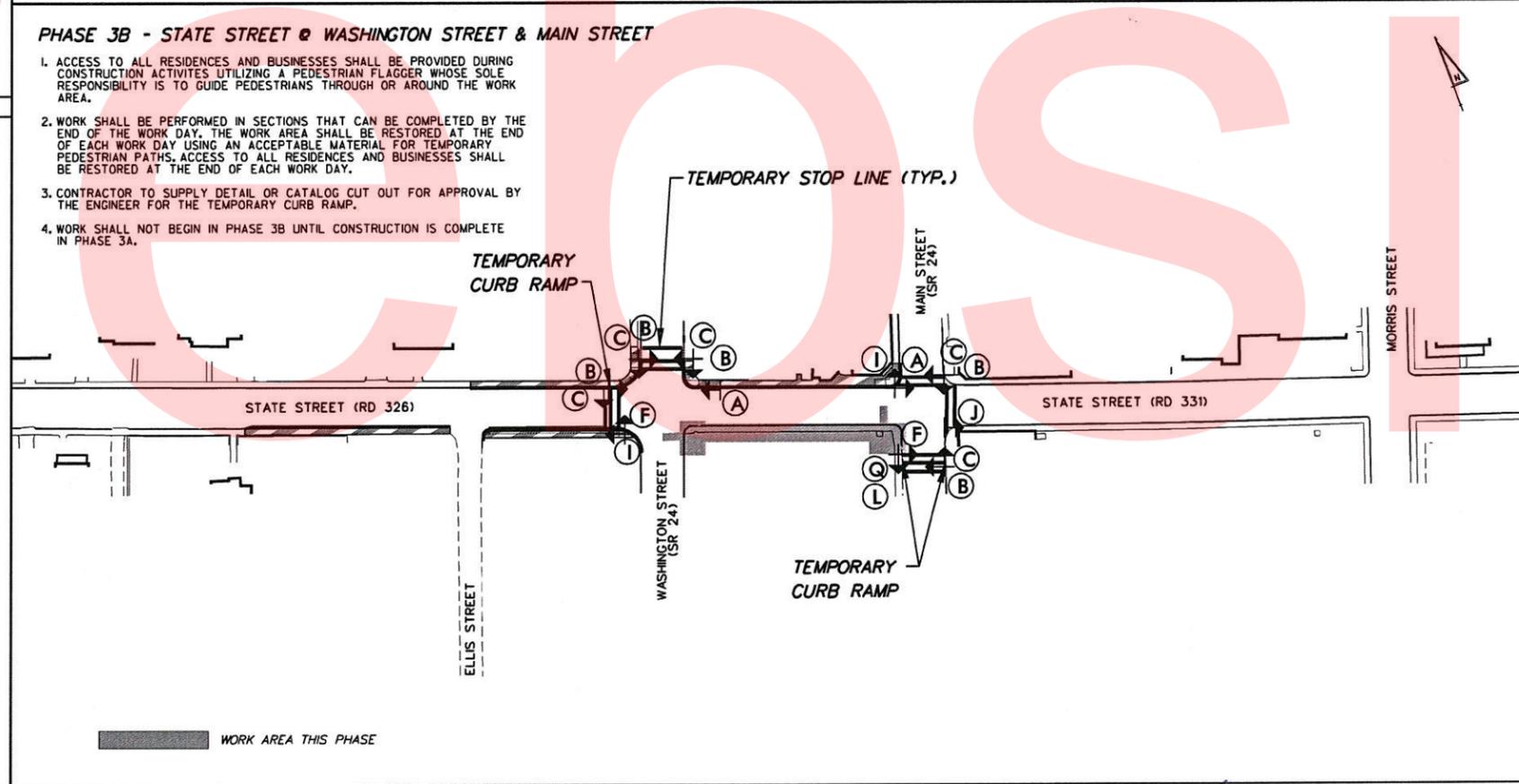
**LEGEND**



**SPECIAL NOTES**

**PHASE 3B - STATE STREET @ WASHINGTON STREET & MAIN STREET**

1. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
2. WORK SHALL BE PERFORMED IN SECTIONS THAT CAN BE COMPLETED BY THE END OF THE WORK DAY. THE WORK AREA SHALL BE RESTORED AT THE END OF EACH WORK DAY USING AN ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.
3. CONTRACTOR TO SUPPLY DETAIL OR CATALOG CUT OUT FOR APPROVAL BY THE ENGINEER FOR THE TEMPORARY CURB RAMP.
4. WORK SHALL NOT BEGIN IN PHASE 3B UNTIL CONSTRUCTION IS COMPLETE IN PHASE 3A.



**GENERAL NOTES**

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
8. "O" BARRICADE SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK OR PEDESTRIAN PATH.
9. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.
10. PEDESTRIAN DETOUR TRAILBLAZERS NOT ATTACHED TO BARRICADES ARE TO EITHER BE GROUND MOUNTED OR ATTACHED TO AN EXISTING SIGN POST AT THE LOCATION SHOWN ON THE PLAN.
11. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON PEDESTRIAN CHANNELIZING BARRICADE AND TEMPORARY PEDESTRIAN PATH.
12. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON TEMPORARY PEDESTRIAN CURB RAMP AND TEMPORARY PEDESTRIAN CROSSWALK.

RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_

RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_

RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_

APPROVED CHIEF SAFETY OFFICER *[Signature]* DATE: *8/19/2024*

APPROVED TRAFFIC ENGINEER *[Signature]* DATE: *8/19/2024*

ADDENDUM / REVISIONS	

NOT TO SCALE

HSIP SC,  
IRON BRANCH ROAD/STATE STREET

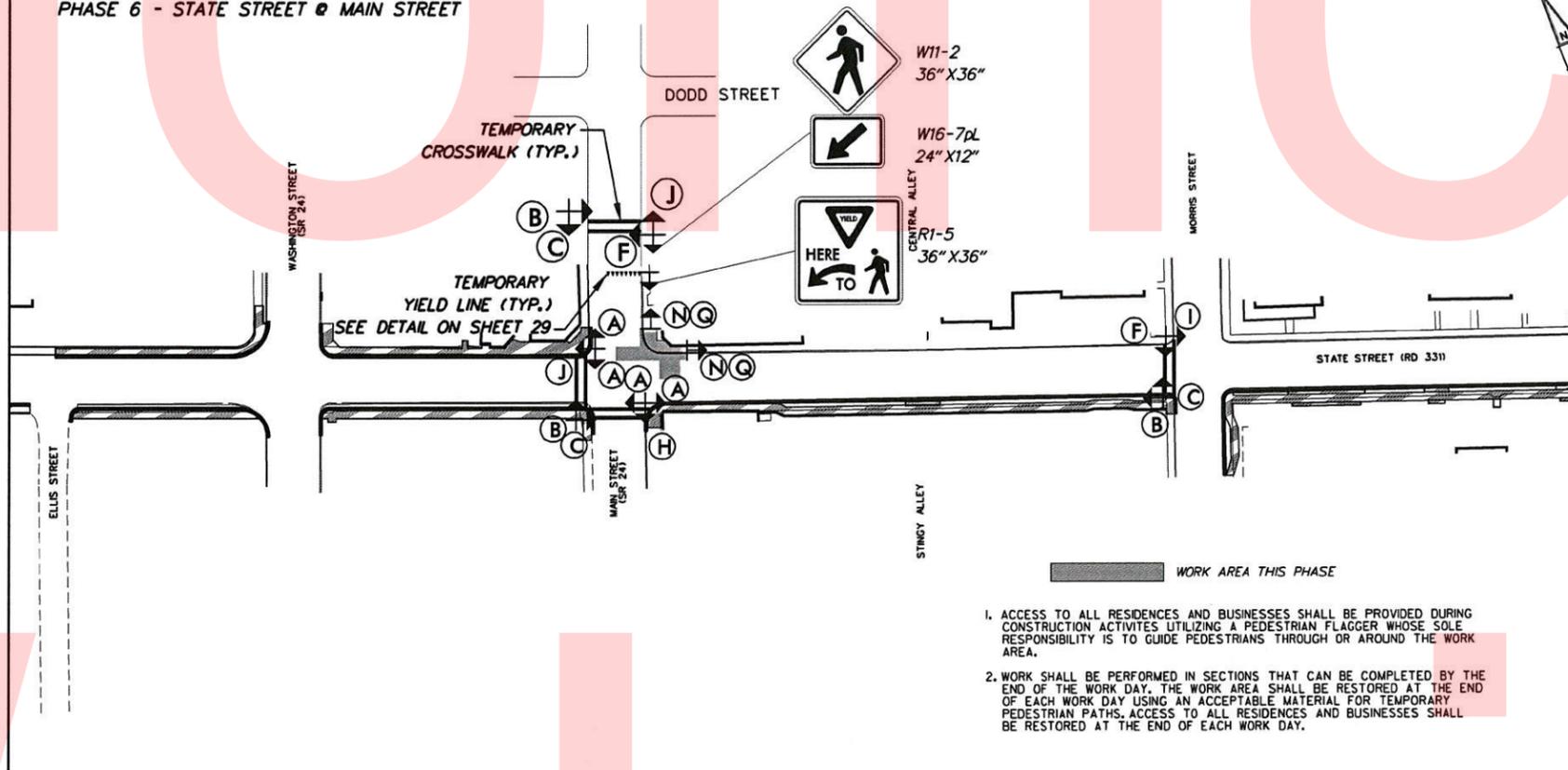
CONTRACT T200800903	BRIDGE NO. NA
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

<b>PEDESTRIAN DETOUR PLAN</b>	
SHEET NO. 30	TOTAL SHTS. 48

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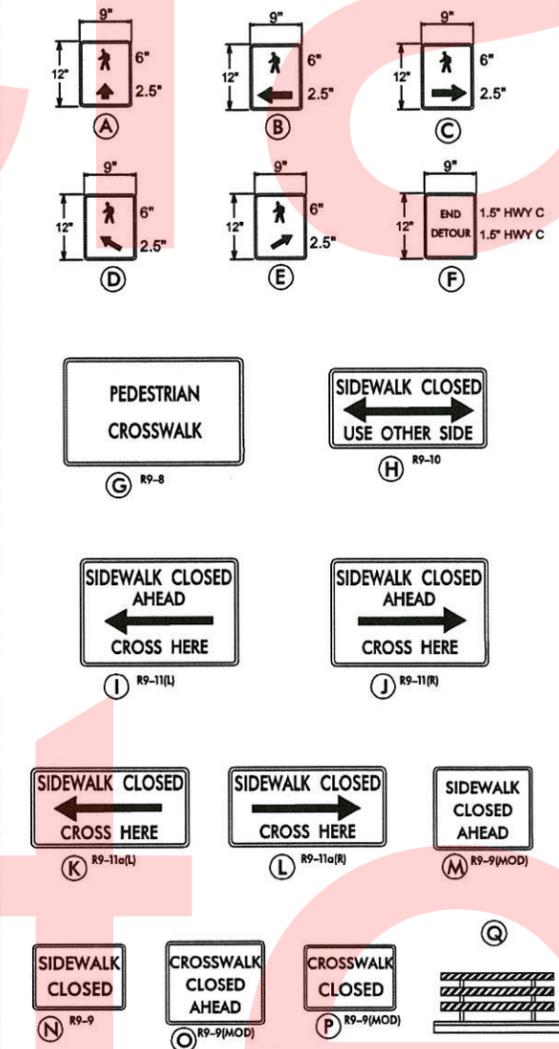
**SPECIAL SIGNS**

**PHASE 6 - STATE STREET @ MAIN STREET**



1. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE PROVIDED DURING CONSTRUCTION ACTIVITIES UTILIZING A PEDESTRIAN FLAGGER WHOSE SOLE RESPONSIBILITY IS TO GUIDE PEDESTRIANS THROUGH OR AROUND THE WORK AREA.
2. WORK SHALL BE PERFORMED IN SECTIONS THAT CAN BE COMPLETED BY THE END OF THE WORK DAY. THE WORK AREA SHALL BE RESTORED AT THE END OF EACH WORK DAY USING AN ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS. ACCESS TO ALL RESIDENCES AND BUSINESSES SHALL BE RESTORED AT THE END OF EACH WORK DAY.

**LEGEND**



**SPECIAL NOTES**

**GENERAL NOTES**

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
8. "Q" BARRICADE SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK OR PEDESTRIAN PATH.
9. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.
10. PEDESTRIAN DETOUR TRAILBLAZERS NOT ATTACHED TO BARRICADES ARE TO EITHER BE GROUND MOUNTED OR ATTACHED TO AN EXISTING SIGN POST AT THE LOCATION SHOWN ON THE PLAN.
11. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON PEDESTRIAN CHANNELIZING BARRICADE AND TEMPORARY PEDESTRIAN PATH.
12. REFER TO CONSTRUCTION PLAN SHEETS FOR DETAILS ON TEMPORARY PEDESTRIAN CURB RAMPS AND TEMPORARY PEDESTRIAN CROSSWALK.

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RECOMMENDED _____ DATE: _____	RECOMMENDED _____ DATE: _____	RECOMMENDED _____ DATE: _____	APPROVED CHIEF SAFETY OFFICER  DATE: 8/9/2016	APPROVED TRAFFIC ENGINEER  DATE: 8/1/2016
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<p><b>DELAWARE DEPARTMENT OF TRANSPORTATION</b></p>	ADDENDUM / REVISIONS		<p><b>NOT TO SCALE</b></p> <p><b>HSIP SC, IRON BRANCH ROAD/STATE STREET</b></p>	CONTRACT T200800903	BRIDGE NO. NA	<p><b>PEDESTRIAN DETOUR PLAN</b></p>	SHEET NO. 31
				COUNTY SUSSEX	DESIGNED BY: CM		TOTAL SHTS. 48
					CHECKED BY: BB		

**PORTABLE CHANGEABLE MESSAGE SIGNS**

**PRIOR TO DETOUR**  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

STATE ST AT MAIN TO CLOSE      STARTING XXXX/XX XXP-XXA

**DURING DETOUR**  
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-1

STATE ST AT MAIN CLOSED      FOLLOW DETOUR

**PRIOR TO DETOUR**  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-2

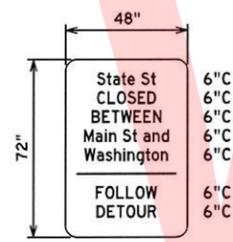
STATE ST AT WASHINGTON TO CLOSE      TO CLOSE XXXX/XX XXP-XXA

**DURING DETOUR**  
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

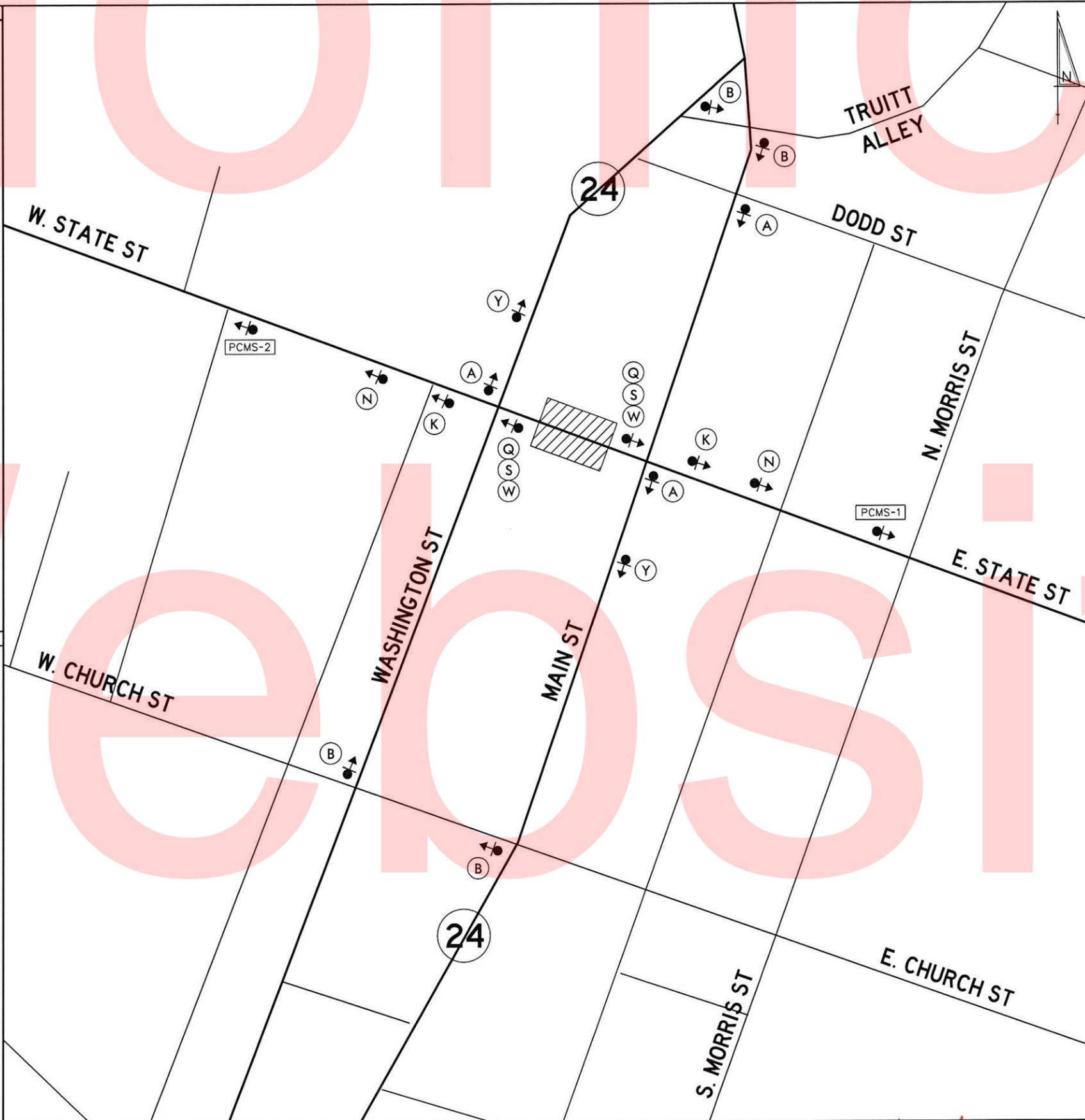
PCMS-2

STATE ST AT WASHINGTON      FOLLOW DETOUR

**SPECIAL SIGNS**



BLACK LEGEND ON D/G RETROREFLECTIVE FLUORESCENT ORANGE



**LEGEND**

(A) DETOUR M4-9-DE	(B) DETOUR M4-9	(C) DETOUR M4-9	(D) DETOUR M4-9	(E) DETOUR M4-9
(F) DETOUR M4-9	(G) DETOUR M4-9	(H) DETOUR M4-9	(I) DETOUR M4-9	(J) END DETOUR M4-8a
(K) DETOUR AHEAD W20-2	(L) DETOUR 1000 FT W20-2	(M) DETOUR 500 FT W20-2	(N) ROAD CLOSED AHEAD W20-3	(O) ROAD CLOSED 1000 FT W20-3
(P) ROAD CLOSED 500 FT W20-3	(Q) ROAD CLOSED R11-2	(R) DETOUR M4-10(I)	(S) DETOUR M4-10(R)	(T) ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY R11-3a
(U) BRIDGE OUT XX MILES AHEAD LOCAL TRAFFIC ONLY R11-3b	(V) ROAD CLOSED TO THRU TRAFFIC R11-4	(W) BARRICADE	(X) BARRICADE	

**GENERAL NOTES**

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. SIGNS "N" THROUGH "Q" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE.
8. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
9. "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
10. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      APPROVED CHIEF SAFETY OFFICER *[Signature]* DATE: 7-11-16      APPROVED TRAFFIC ENGINEER *[Signature]* DATE: 7/11/16

<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUM / REVISIONS	NOT TO SCALE	IRON BRANCH ROAD/STATE STREET ROADWAY IMPROVEMENTS	CONTRACT	ROAD NO.	S326	VEHICULAR DETOUR PLAN STATE STREET, MAIN ST TO WASHINGTON ST	SHEET NO.	
				T200800903	DESIGNED BY:			SWN	32
				SUSSEX	CHECKED BY:			ASW	TOTAL SHTS.

W:\MSV\CELLS\PRODEV\SB.GEL

**PORTABLE CHANGEABLE MESSAGE SIGNS**

**PRIOR TO DETOUR**  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

STATE ST AT MAIN TO CLOSE      STARTING XXXXXX

**DURING DETOUR**  
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-1

STATE ST AT MAIN CLOSED      FOLLOW DETOUR SR 24 WB

**PRIOR TO DETOUR**  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-2

STATE ST AT OLD LANDING      TO CLOSE STARTING XXXXXX

**DURING DETOUR**  
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-2

STATE ST AT OLD LANDING      CLOSED FOLLOW DETOUR

**SPECIAL SIGNS**

BLACK LEGEND: D/G RETROREFLECTIVE FLUORESCENT ORANGE  
ROUTE SHIELD: BLACK LEGEND: WHITE BACKGROUND



**LEGEND**

A: DETOUR (up arrow) M4-9-DE  
B: DETOUR (left arrow) M4-9  
C: DETOUR (right arrow) M4-9  
D: DETOUR (up-left arrow) M4-9  
E: DETOUR (up-right arrow) M4-9  
F: DETOUR (down-left arrow) M4-9  
G: DETOUR (down-right arrow) M4-9  
H: DETOUR (down-left arrow) M4-9  
I: DETOUR (down-right arrow) M4-9  
J: END DETOUR M4-9a  
K: DETOUR AHEAD W20-2  
L: DETOUR 1000 FT W20-2  
M: DETOUR 500 FT W20-2  
N: ROAD CLOSED AHEAD W20-3  
O: ROAD CLOSED 1000 FT W20-3  
P: ROAD CLOSED 500 FT W20-3  
Q: ROAD CLOSED R11-2  
R: DETOUR (left arrow) M4-10(L)  
S: DETOUR (right arrow) M4-10(R)  
T: ROAD CLOSED 1/4 MILE AHEAD LOCAL TRAFFIC ONLY R11-3a  
U: BRIDGE OUT XX MILES AHEAD LOCAL TRAFFIC ONLY R11-3b  
V: ROAD CLOSED TO THRU TRAFFIC R11-4  
W: BARRICADE  
X: BARRICADE

**GENERAL NOTES**

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
- DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
- SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
- SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
- SIGNS "N" THROUGH "Q" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE.
- WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
- "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

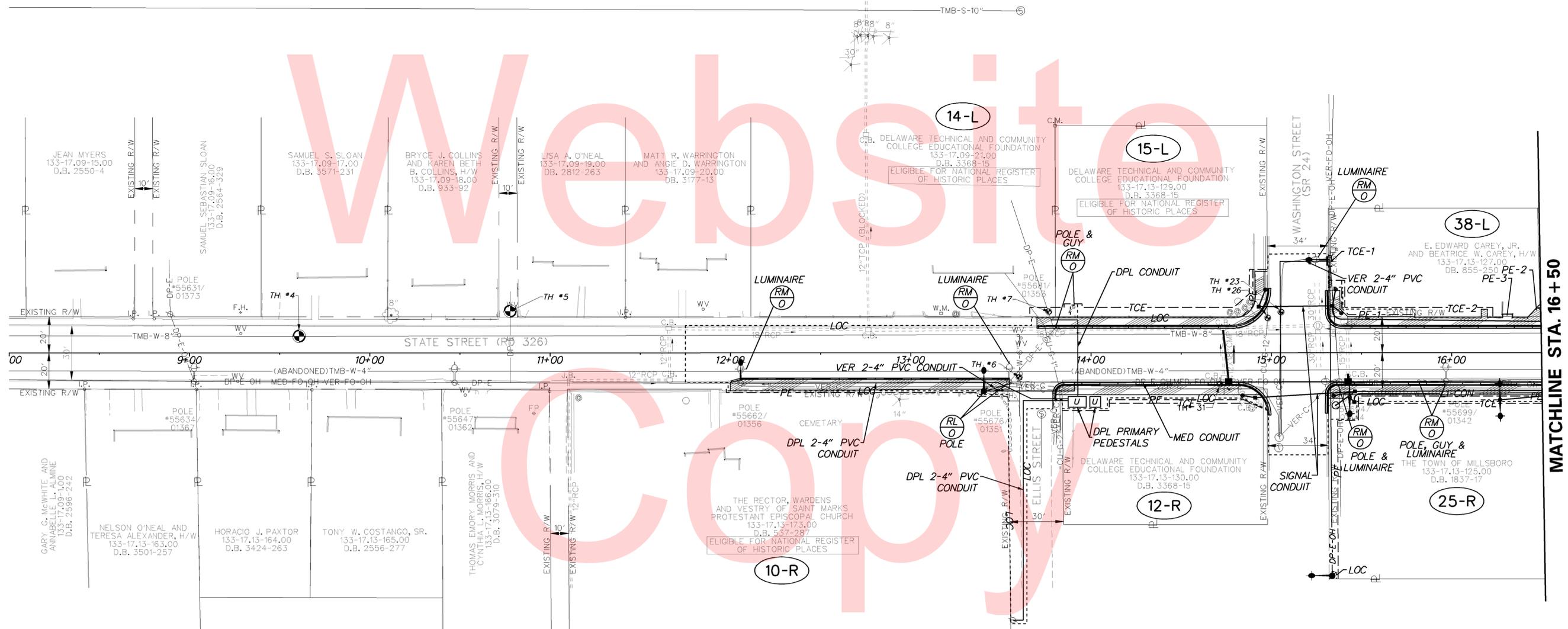
RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      APPROVED CHIEF SAFETY OFFICER: *[Signature]* DATE: 7-11-16      APPROVED TRAFFIC ENGINEER: *[Signature]* DATE: 7/11/16



Unofficial

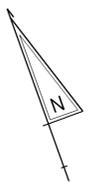
UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
4	TMB-W	9+60.82	-9.26	22.87	3.43	8" PLASTIC
5	DP-E	10+77.90	-23.09	22.87	3.41	2.5" STEEL
6	TMB-W	13+60.13	13.57	22.26	2.63	4" METALLIC
7	DP-E	13+76.53	-22.79	23.07	1.50	(4) 0.5" PLASTIC

NOTE: OFFSETS IN SCHEDULES WITH A MINUS SIGN ARE LEFT OF BASELINE



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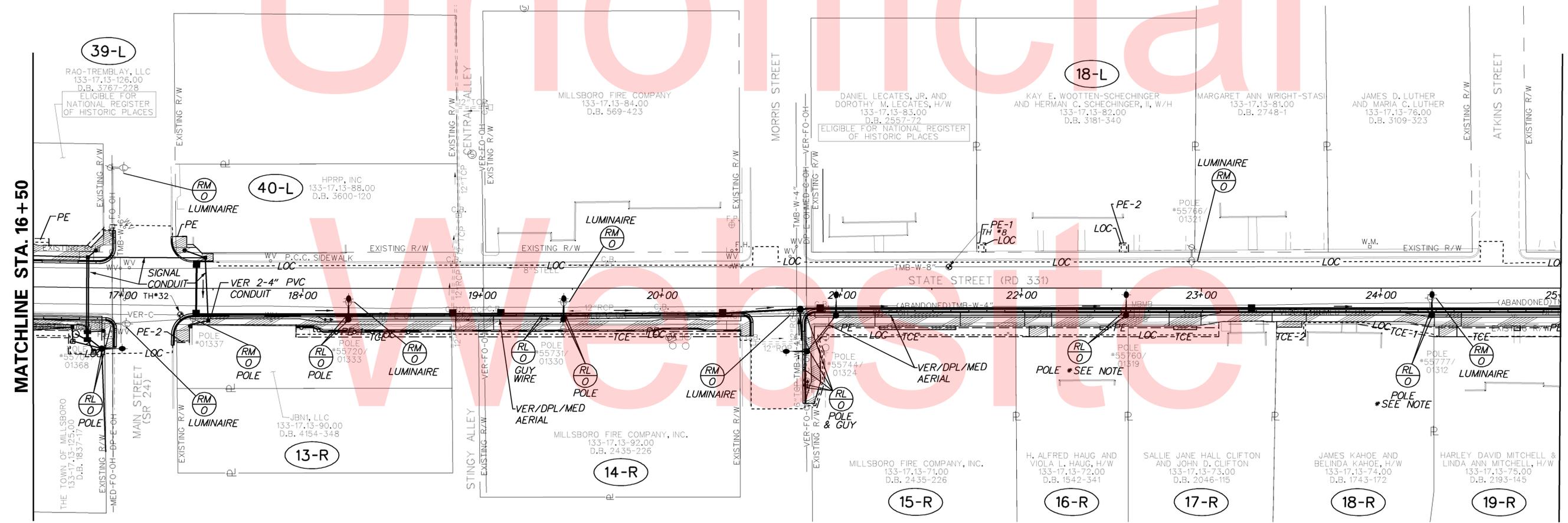
<b>DELAWARE DEPARTMENT OF TRANSPORTATION</b>	ADDENDUMS / REVISIONS	SCALE 	<b>HSIP SC, IRON BRANCH ROAD/STATE STREET</b>	CONTRACT T200800903 COUNTY SUSSEX	BRIDGE NO. DESIGNED BY: CM CHECKED BY: BB	<b>UTILITY RELOCATION PLAN</b>	SHEET NO. 34 TOTAL SHTS. 48



Unofficial

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UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
8	TMB-W	21+59.61	-11.74	22.62	4.03	8" PLASTIC
32	VER-C	17+31.92	14.54	23.58	2.94	(2)4" PLASTIC

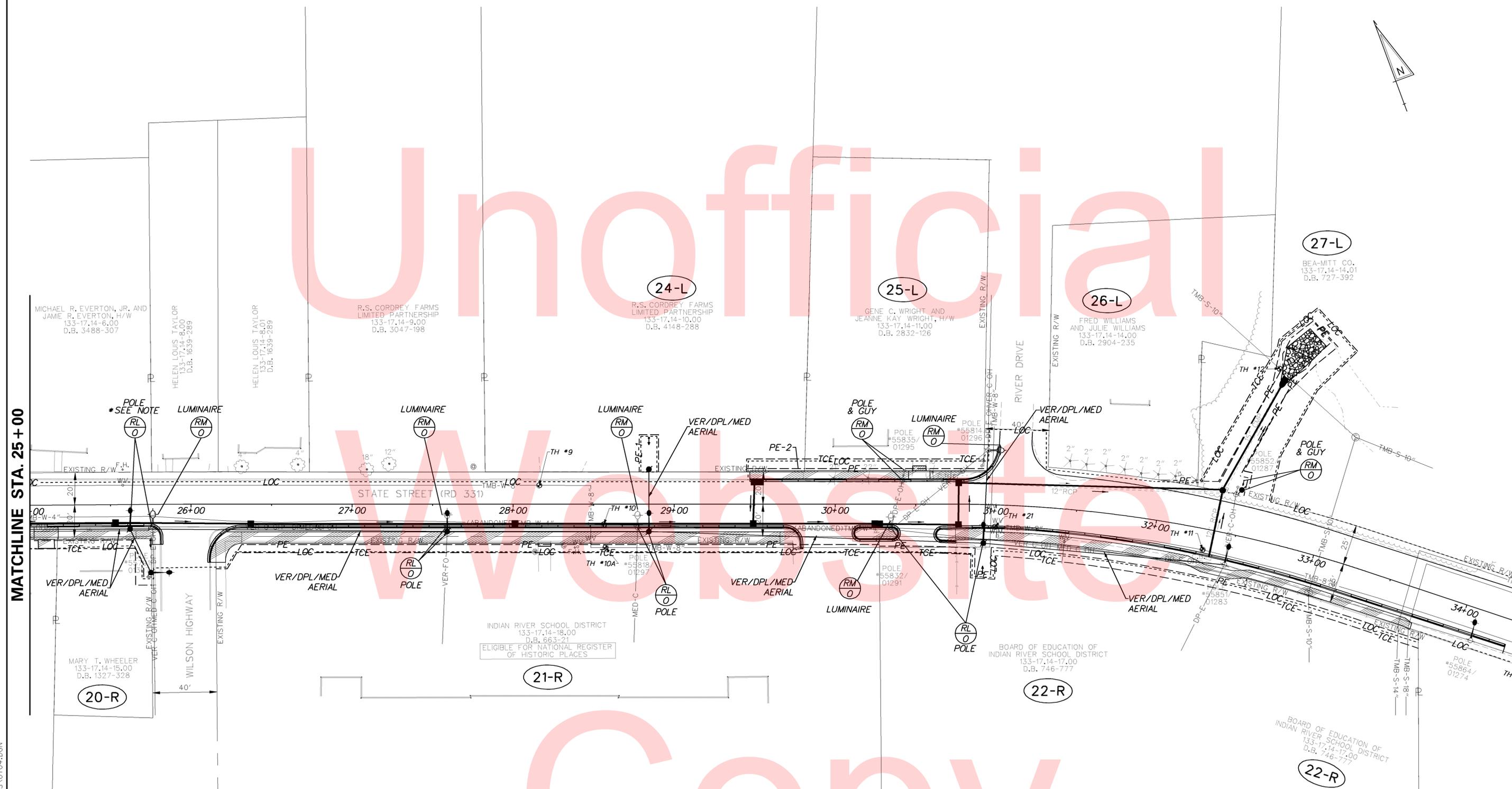
NOTE: OFFSETS IN SCHEDULE WITH A MINUS SIGN ARE LEFT OF BASELINE

\*UTILITY POLE RELOCATION NOTE:  
 PROPOSED UTILITY POLES AT STA. 22+58, OFFSET 15'R AND STA. 24+28, OFFSET 15'R MUST UTILIZE A VERTICAL SETTING CONFIGURATION TO MAINTAIN A MINIMUM 10' OF SEPARATION BETWEEN THE POWER LINES AND THE EXISTING STRUCTURES.

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<b>DELAWARE</b> DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS	SCALE 0      30      60      90 FEET	<b>HSIP SC,</b> <b>IRON BRANCH ROAD/STATE STREET</b>		CONTRACT T200800903	BRIDGE NO. <b>N/A</b>	<b>UTILITY RELOCATION PLAN</b>	SHEET NO. 35
				COUNTY SUSSEX	DESIGNED BY: CM	TOTAL SHTS. 48		
				CHECKED BY: BB				

# Unofficial



UTILITY TEST HOLE SCHEDULE						
NO.	UTILITY	STATION	OFFSET	GRND EL.	COVER	O. D. & MATERIAL
9	TMB-W	28+16.25	-11.66	21.30	3.59	8" PLASTIC
10	VER-C	28+56.40	12.54	21.25	2.78	2.5" STEEL
10A	TMB-W	28+57.44	26.74	21.92	3.10	8" AC
11	TMB-W	32+31.63	12.90	21.08	3.83	8" AC
12	TMB-S	32+54.00	-106.6	13.10	4.60	8" UNKNOWN
21	TMB-W	30+99.66	13.60	21.10	3.94	8" DIP

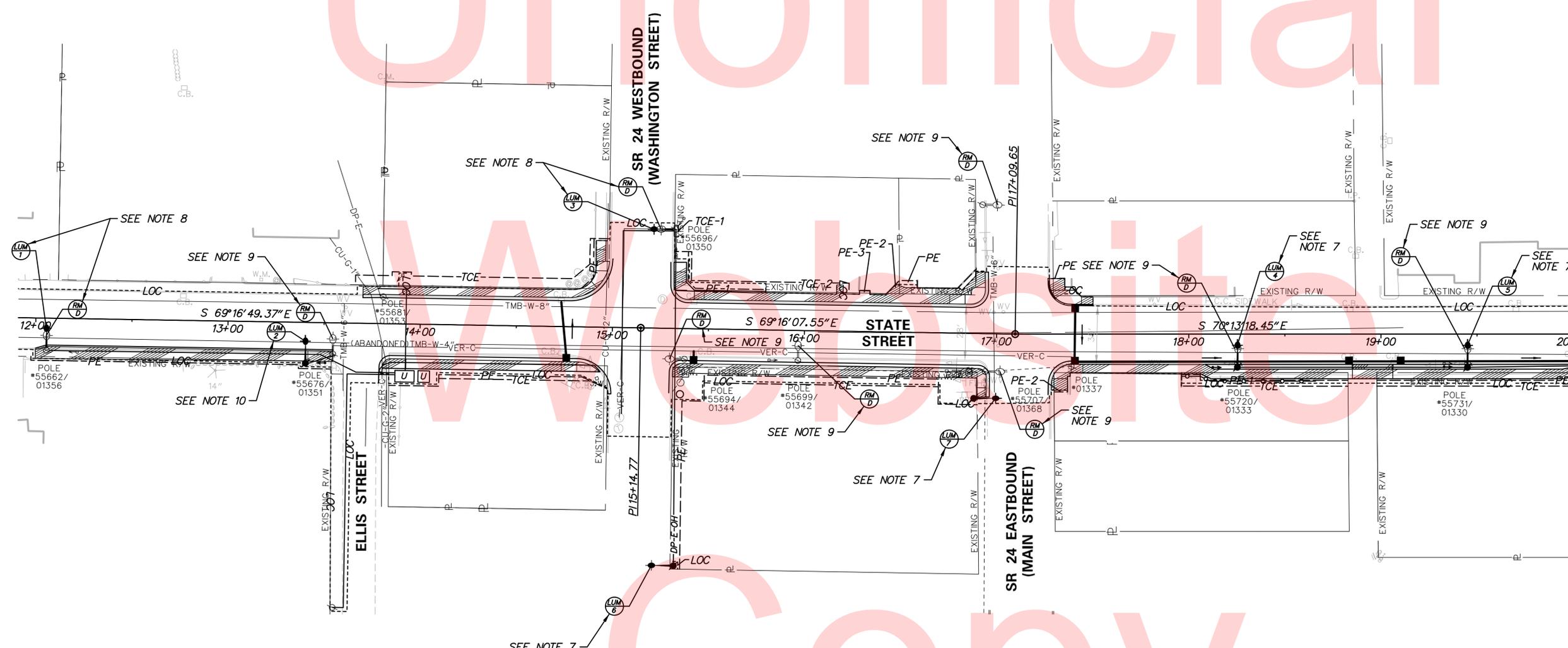
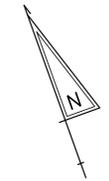
NOTE: OFFSETS IN SCHEDULE WITH A MINUS SIGN ARE LEFT OF BASELINE

\*UTILITY POLE RELOCATION NOTE:  
 PROPOSED UTILITY POLE AT STA. 25+62, OFFSET 15'R MUST UTILIZE A VERTICAL SETTING CONFIGURATION TO MAINTAIN A MINIMUM 10' OF SEPARATION BETWEEN THE POWER LINES AND THE EXISTING STRUCTURES.

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PROPOSED LUMINAIRE SCHEDULE						
LUM. #	DESCRIPTION	LAMP	PREF. MOUNTING HEIGHT	MOUNTING ARM LENGTH	VERTICAL TILT	POLE NO.
1	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	*55662/01356
2	COBRA HEAD (CUTOFF)	250 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
3	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	*55696/01350
4	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
5	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
6	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
7	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED

- NOTES:**
- ALL PROPOSED LUMINAIRES SHALL BE 150 OR 250 WATT EQUIVALENT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 2 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE ON A 8-FOOT BRACKET ARM MOUNTED TO THE PROPOSED UTILITY POLE, AS SHOWN.
  - THE PROPOSED LEASED LIGHTS SHALL BE ADDED TO THE TOWN OF MILLSBORO LIGHTING ACCOUNT.
  - THE LUMINAIRES SHALL BE MOUNTED AT A MINIMUM MOUNTING HEIGHT OF 25 FEET AND A MAXIMUM MOUNTING HEIGHT OF 35 FEET, WITH A PREFERRED MOUNTING HEIGHT OF 30 FEET.
  - DELMARVA POWER SHALL INSTALL A 150 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE PROPOSED UTILITY POLE, AS SHOWN.
  - DELMARVA POWER SHALL REMOVE THE EXISTING HPS LUMINAIRE AND INSTALL A 150 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE EXISTING UTILITY POLE, AS SHOWN.
  - DELMARVA POWER SHALL REMOVE THE EXISTING HPS LUMINAIRE.
  - DELMARVA POWER SHALL INSTALL A 250 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE PROPOSED UTILITY POLE, AS SHOWN.



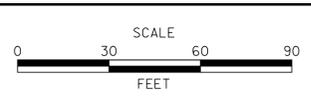
MATCH LINE STA. 20+00 (SEE SHEET 38)

LEGEND	
	REMOVE BY CONTRACTOR
	REMOVE BY DELMARVA
	ABANDON
	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED LIGHTING STANDARD IDENTIFIER (* OF LIGHTING SERVICE)
	EXISTING LIGHTING STANDARD
	PROPOSED SPAN WIRE
	EXISTING SPAN WIRE
	RIGHT-OF-WAY OR PROPERTY LINE
	SERVICE PEDESTAL
	PROPOSED LIGHTING STANDARD
	EXISTING LIGHTING STANDARD
	PROPOSED SIGNAL CABINET
	EXISTING SIGNAL CABINET
	PROPOSED SIGNAL POLE BASE
	EXISTING SIGNAL POLE BASE
	PROPOSED PEDESTRIAN POLE BASE
	EXISTING PEDESTRIAN POLE BASE
	PROPOSED WOOD POLE
	EXISTING UTILITY POLE
	PROPOSED JUNCTION WELL
	EXISTING JUNCTION WELL
	PROPOSED SIGNAL HEAD
	EXISTING SIGNAL HEAD

- GENERAL NOTES**
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
  - LUMINAIRE AND MOUNTING BRACKET ARM TO BE INSTALLED, OWNED, AND MAINTAINED BY UTILITY COMPANY.
  - DELMARVA POWER CONSTRUCTION ACTIVITIES PERFORMED OUTSIDE DELDOT'S RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH DELMARVA POWER POLICIES AND STANDARDS FOR WORKING WITHIN PRIVATE UTILITY EASEMENTS.

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ADDENDUM / REVISIONS



CONTRACT	PERMIT NO.	<b>N/A</b>
T200800903	DESIGNED BY:	B.S.S. (WRA)
COUNTY	CHECKED BY:	M.J.B. (WRA)
SUSSEX		

<b>LEASED LIGHTING PLAN</b>		SHEET NO.
		37
		TOTAL SHTS.
		48

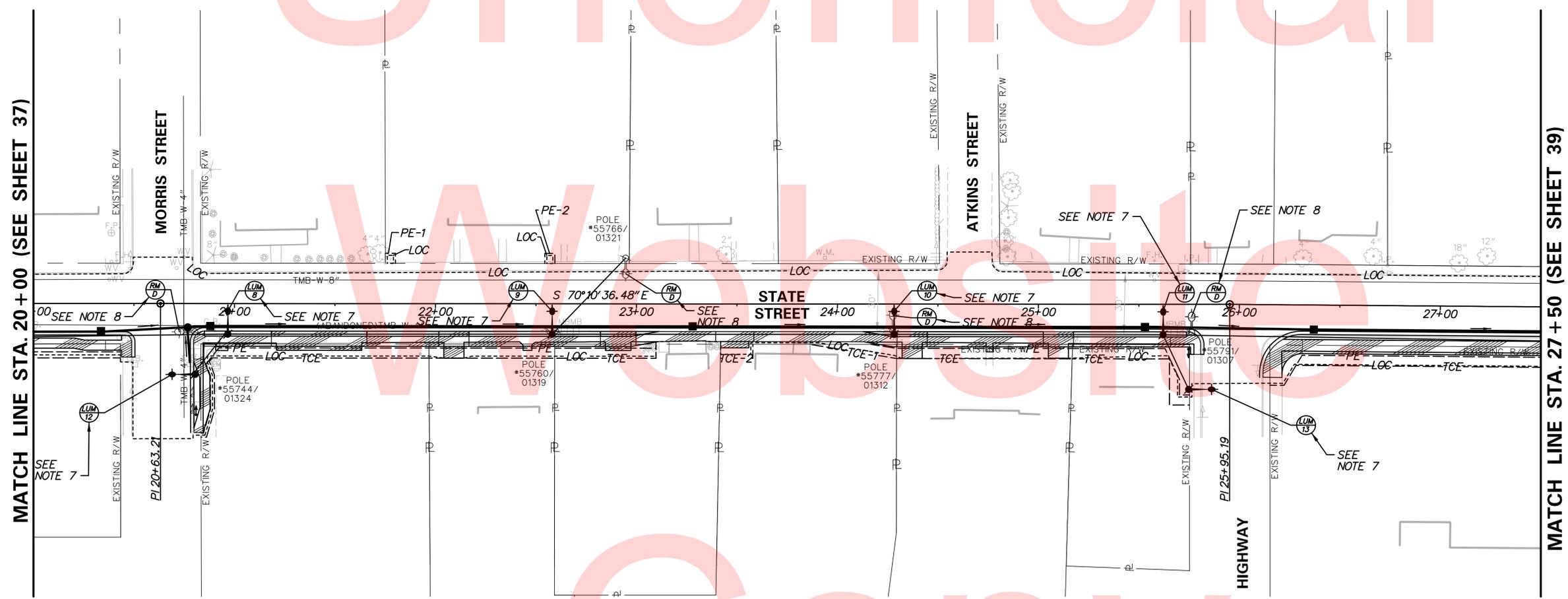
PROPOSED LUMINAIRE SCHEDULE						
LUM. #	DESCRIPTION	LAMP	PREF. MOUNTING HEIGHT	MOUNTING ARM LENGTH	VERTICAL TILT	POLE NO.
8	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
9	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
10	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
11	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
12	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
13	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED

- NOTES:**
- ALL PROPOSED LUMINAIRES SHALL BE 150 WATT EQUIVALENT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 2 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE ON A 8-FOOT BRACKET ARM MOUNTED TO THE PROPOSED UTILITY POLE, AS SHOWN.
  - THE PROPOSED LEASED LIGHTS SHALL BE ADDED TO THE TOWN OF MILLSBORO LIGHTING ACCOUNT.
  - THE LUMINAIRES SHALL BE MOUNTED AT A MINIMUM MOUNTING HEIGHT OF 25 FEET AND A MAXIMUM MOUNTING HEIGHT OF 35 FEET, WITH A PREFERRED MOUNTING HEIGHT OF 30 FEET.
  - DELMARVA POWER SHALL INSTALL A 150 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE PROPOSED UTILITY POLE, AS SHOWN.
  - DELMARVA POWER SHALL REMOVE THE EXISTING HPS LUMINAIRE.



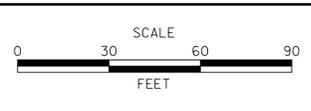
LEGEND	
	REMOVE BY CONTRACTOR
	REMOVE BY DELMARVA
	ABANDON
	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED LIGHTING STANDARD IDENTIFIER (* OF LIGHTING SERVICE)
	PROPOSED SPAN WIRE
	EXISTING SPAN WIRE
	RIGHT-OF-WAY OR PROPERTY LINE
	SERVICE PEDESTAL
	PROPOSED LIGHTING STANDARD
	EXISTING LIGHTING STANDARD
	PROPOSED SIGNAL CABINET
	EXISTING SIGNAL CABINET
	PROPOSED SIGNAL POLE BASE
	EXISTING SIGNAL POLE BASE
	PROPOSED PEDESTRIAN POLE BASE
	EXISTING PEDESTRIAN POLE BASE
	PROPOSED WOOD POLE
	EXISTING UTILITY POLE
	PROPOSED JUNCTION WELL
	EXISTING JUNCTION WELL
	PROPOSED SIGNAL HEAD
	EXISTING SIGNAL HEAD

- GENERAL NOTES**
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
  - LUMINAIRE AND MOUNTING BRACKET ARM TO BE INSTALLED, OWNED, AND MAINTAINED BY UTILITY COMPANY.
  - DELMARVA POWER CONSTRUCTION ACTIVITIES PERFORMED OUTSIDE DELDOT'S RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH DELMARVA POWER POLICIES AND STANDARDS FOR WORKING WITHIN PRIVATE UTILITY EASEMENTS.



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ADDENDUM / REVISIONS	

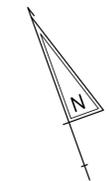


CONTRACT	T200800903	PERMIT NO.	N/A
COUNTY	SUSSEX	DESIGNED BY:	B.S.S. (WRA)
		CHECKED BY:	M.J.B. (WRA)

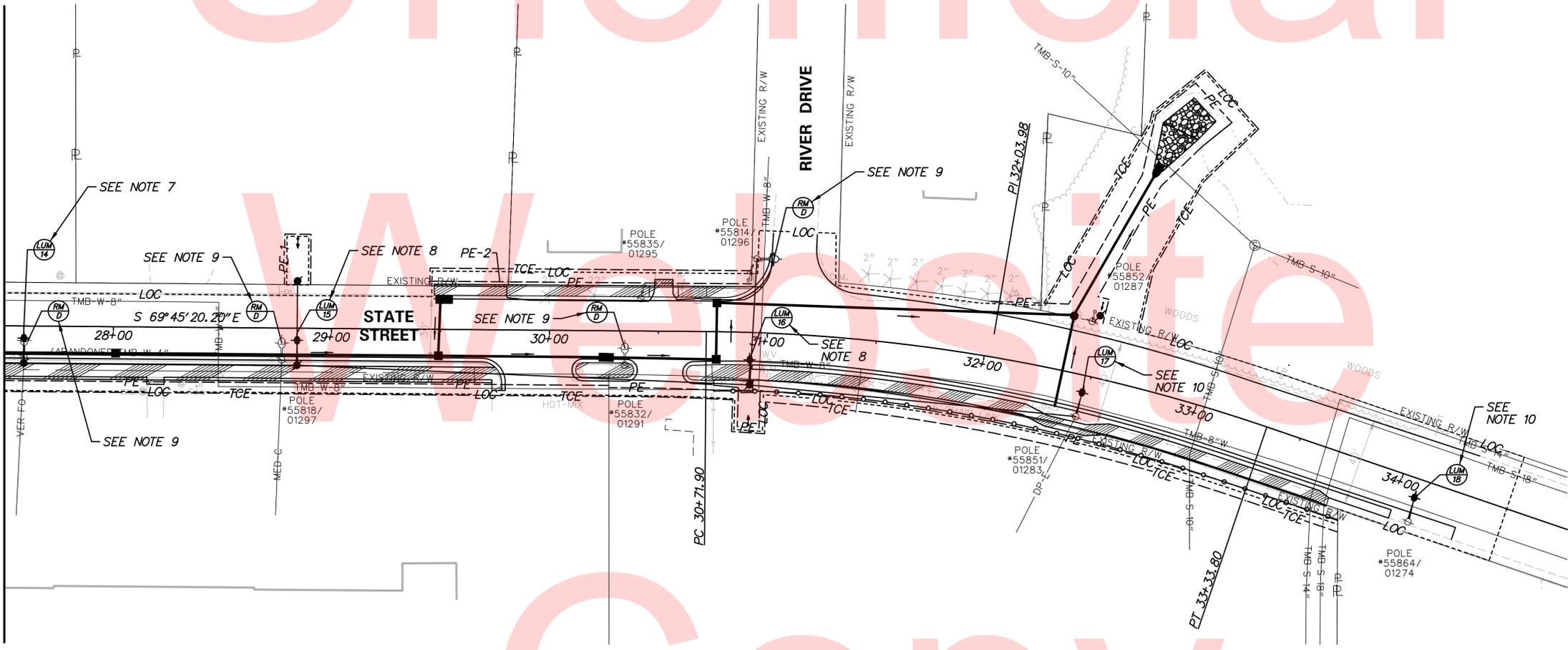
PROPOSED LUMINAIRE SCHEDULE						
LUM. #	DESCRIPTION	LAMP	PREF. MOUNTING HEIGHT	MOUNTING ARM LENGTH	VERTICAL TILT	POLE NO.
14	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
15	COBRA HEAD (CUTOFF)	250 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
16	COBRA HEAD (CUTOFF)	250 W EQ. LED	30 FT.	8 FT.	0 DEGREES	PROPOSED
17	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	*55851/ 01283
18	COBRA HEAD (CUTOFF)	150 W EQ. LED	30 FT.	8 FT.	0 DEGREES	*55864/ 01274

**NOTES:**

- ALL PROPOSED LUMINAIRES SHALL BE 150 OR 250 WATT EQUIVALENT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 2 HORIZONTAL DISTRIBUTION, COBRA-HEAD STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE ON A 8-FOOT BRACKET ARM MOUNTED TO THE PROPOSED UTILITY POLE, AS SHOWN.
- THE PROPOSED LEASED LIGHTS SHALL BE ADDED TO THE TOWN OF MILLSBORO LIGHTING ACCOUNT.
- THE LUMINAIRES SHALL BE MOUNTED AT A MINIMUM MOUNTING HEIGHT OF 25 FEET AND A MAXIMUM MOUNTING HEIGHT OF 35 FEET, WITH A PREFERRED MOUNTING HEIGHT OF 30 FEET.
- DELMARVA POWER INSTALL A 150 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE PROPOSED UTILITY POLE, AS SHOWN.
- DELMARVA POWER INSTALL A 250 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE PROPOSED UTILITY POLE, AS SHOWN.
- DELMARVA POWER SHALL REMOVE THE EXISTING HPS LUMINAIRE.
- DELMARVA POWER INSTALL A 150 WATT EQUIVALENT LED LUMINAIRE WITH AN 8-FT BRACKET ARM MOUNTED ON THE EXISTING UTILITY POLE, AS SHOWN.



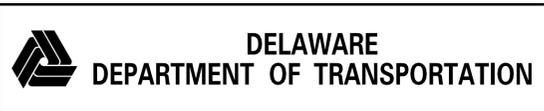
MATCH LINE STA. 27+50 (SEE SHEET 38)



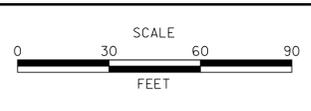
LEGEND	
	REMOVE BY CONTRACTOR
	REMOVE BY DELMARVA
	ABANDON
	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
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	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED LIGHTING STANDARD IDENTIFIER (* OF LIGHTING SERVICE)
	PROPOSED SPAN WIRE
	EXISTING SPAN WIRE
	RIGHT-OF-WAY OR PROPERTY LINE
	SERVICE PEDESTAL
	PROPOSED LIGHTING STANDARD
	EXISTING LIGHTING STANDARD
	PROPOSED SIGNAL CABINET
	EXISTING SIGNAL CABINET
	PROPOSED SIGNAL POLE BASE
	EXISTING SIGNAL POLE BASE
	PROPOSED PEDESTRIAN POLE BASE
	EXISTING PEDESTRIAN POLE BASE
	PROPOSED WOOD POLE
	EXISTING UTILITY POLE
	PROPOSED JUNCTION WELL
	EXISTING JUNCTION WELL
	PROPOSED SIGNAL HEAD
	EXISTING SIGNAL HEAD

- GENERAL NOTES**
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
  - LUMINAIRE AND MOUNTING BRACKET ARM TO BE INSTALLED, OWNED, AND MAINTAINED BY UTILITY COMPANY.
  - DELMARVA POWER CONSTRUCTION ACTIVITIES PERFORMED OUTSIDE DELDOT'S RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH DELMARVA POWER POLICIES AND STANDARDS FOR WORKING WITHIN PRIVATE UTILITY EASEMENTS.

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ADDENDUM / REVISIONS



HSIP SC,  
IRON BRANCH ROAD / STATE STREET

CONTRACT	T200800903	PERMIT NO.	N/A
COUNTY	SUSSEX	DESIGNED BY:	B.S.S. (WRA)
		CHECKED BY:	M.J.B. (WRA)

LEASED LIGHTING PLAN		SHEET NO.	39
		TOTAL SHTS.	48

**NOTES:**

1. ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS.
2. ALL CONDUIT SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING, UNLESS OTHERWISE NOTED.
3. FURNISH AND INSTALL ELECTRICAL UTILITY SERVICE PEDESTAL (100 AMP) PER DELDOT STANDARD NO.T-17 WITH (1) 30 AMP INLINE FUSE KIT IN TYPE 'C' CONDUIT.
4. THE PROPOSED 250 WATT EQUIVALENT LED LUMINAIRE SHALL BE INSTALLED BY OTHERS (DELMARVA POWER).
5. THE PROPOSED 150 WATT EQUIVALENT LED LUMINAIRE SHALL BE INSTALLED BY OTHERS (DELMARVA POWER).
6. THE PROPOSED TWIN LUMINAIRE SHALL BE (2) 83 WATT 4000K CCT LED, WITH MEDIUM VERTICAL DISTRIBUTION, IES TYPE 3 HORIZONTAL DISTRIBUTION, TEAR DROP STYLE FIXTURES WITH CUT-OFF OPTICS MOUNTED WITH 0° TILT ANGLE.
7. PHOTOELECTRIC CONTROL SHALL BE AT EACH PROPOSED LUMINAIRE.
8. THE CONTRACTOR SHALL STAKEOUT ALL PROPOSED LIGHT POLE LOCATIONS PRIOR TO INSTALLATION OF THE POLE BASE. IF THE CONTRACTOR PERCEIVES THAT AN UNDERGROUND OR OVERHEAD UTILITY CONFLICT EXISTS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR A RESOLUTION.
9. TOWN OF MILLSBORO STANDARDS SHALL BE INSTALLED ON BREAKAWAY BASES.
10. THE CONTRACTOR SHALL PROVIDE (4) SETS OF RED-LINED AS-BUILT PLANS TO THE ENGINEER SHOWING OFFSETS FROM THE CENTERLINE OF THE ROAD AND DEPTH OF THE CONDUIT FROM THE CENTERLINE OF THE ROAD.
11. THE PROPOSED LUMINAIRES SHALL HAVE A QUICK LOCK STEM MOUNT, 4 INCH DIAMETER FLUTED SHAFT, AND BLACK HOUSING.

**LIGHTING SYMBOL LEGEND**

SYMBOL	DESCRIPTION
	- PROPOSED TWIN LIGHTING STANDARD AND POLE BASE
	- PROPOSED LUMINAIRE
	- PROPOSED LIGHTING JUNCTION WELL
	- PROPOSED LIGHTING STANDARD IDENTIFIER
	- LI-CON - PROPOSED LIGHTING SERVICE RUN
	- PROPOSED SERVICE RUN IDENTIFIER
	- PROPOSED LIGHTING JUNCTION WELL IDENTIFIER (TYPE)
	- PROPOSED LIGHTING POLE BASE IDENTIFIER (TYPE)
	- PROPOSED SERVICE PEDESTAL
	- PROPOSED UTILITY POLE
	- EXISTING UTILITY POLE
	- EXISTING LUMINAIRE

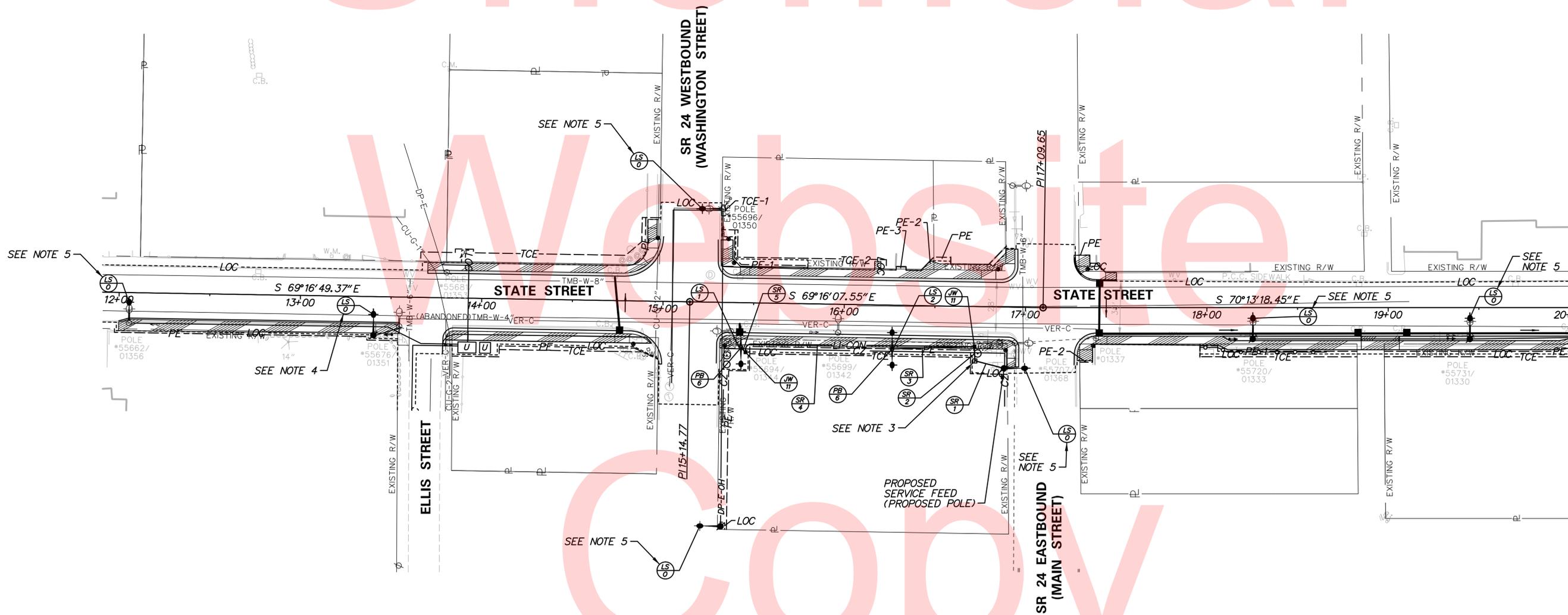
**LIGHTING SERVICE SCHEDULE**

SERVICE RUN	# OF CONDUITS	SIZE	DISTANCE	DESCRIPTION	INSTALLATION
1*	1	2.0"	17 FT	(3)#2, (1)#2 GROUND - LINE SIDE	TRENCH
2*	1	2.0"	8 FT	(2)#6, (1)#6 GROUND	TRENCH
3	1	3.0"	45 FT	(2)#6, (1)#6 GROUND	TRENCH
4	1	4.0"	126 FT	(2)#6, (1)#6 GROUND	TRENCH
5	1	3.0"	4 FT	(2)#6, (1)#6 GROUND	TRENCH

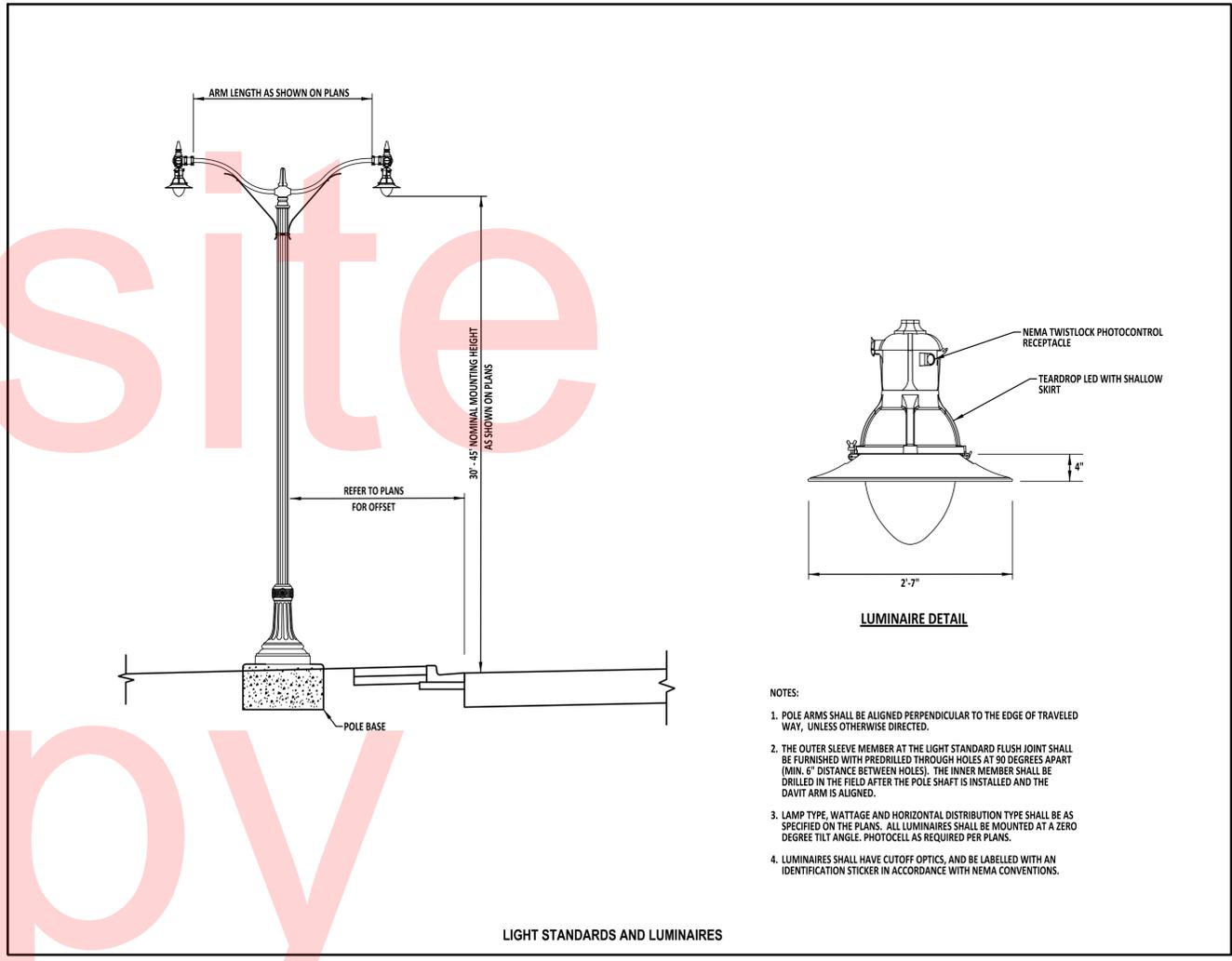
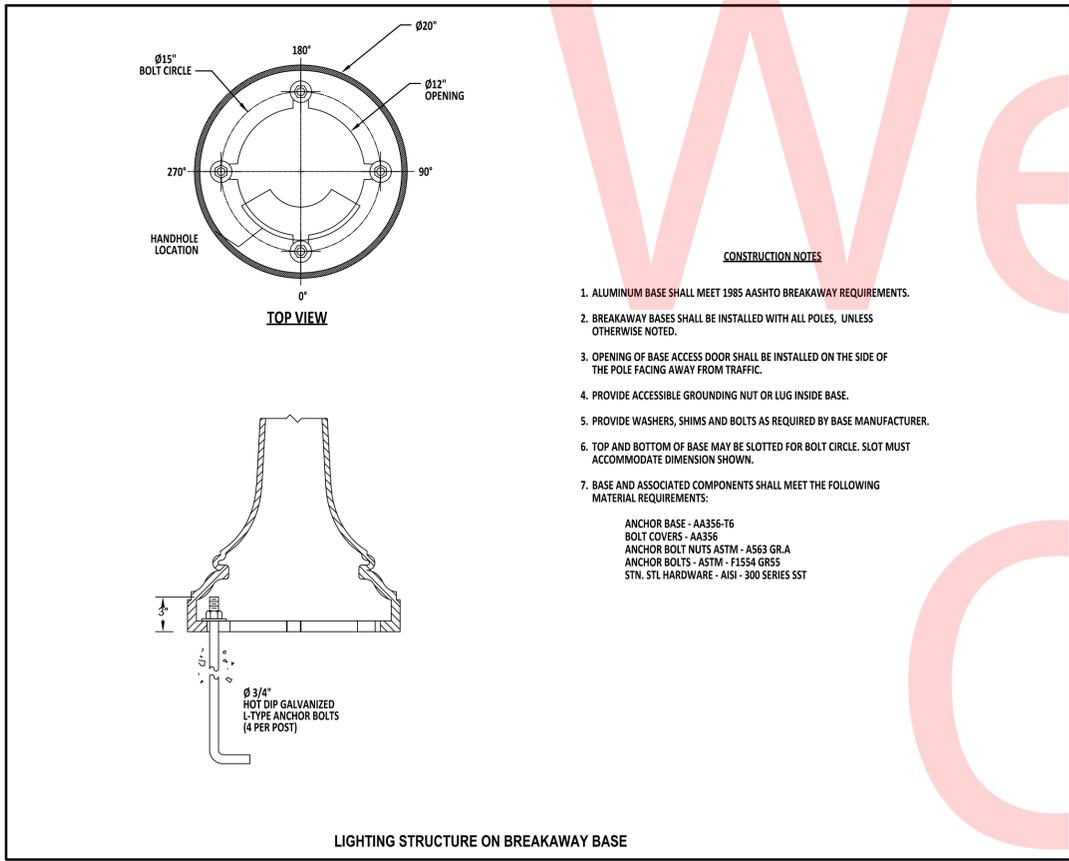
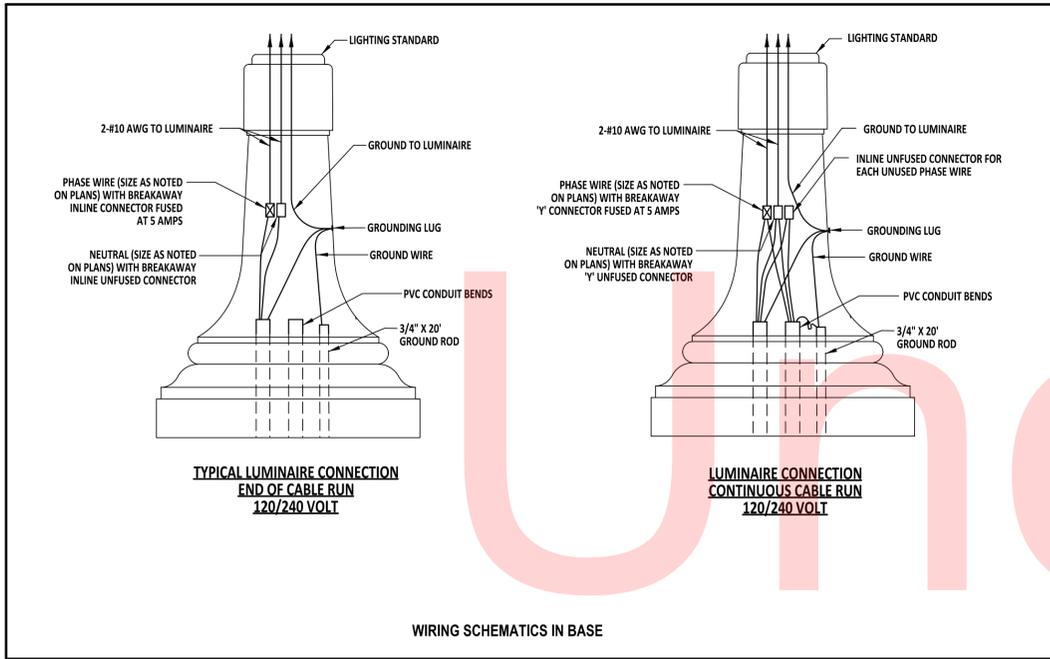
NOTE: SERVICE RUN DISTANCE REFLECTS CONDUIT LENGTH ONLY.  
\*RIGID GALVANIZED STEEL CONDUIT

**LIGHTING STANDARD SCHEDULE**

NO.	CIRCUIT NO.	STATION	OFFSET	HEIGHT	ARM	LIGHT STANDARD
LS-1	1	15+43.4	24.8' RT.	30 FT	12 FT DOUBLE	(2) 83 W LED, IES TYPE 3 DISTRIBUTION
LS-2	1	16+27.0	24.0' RT.	30 FT	12 FT DOUBLE	(2) 83 W LED, IES TYPE 3 DISTRIBUTION



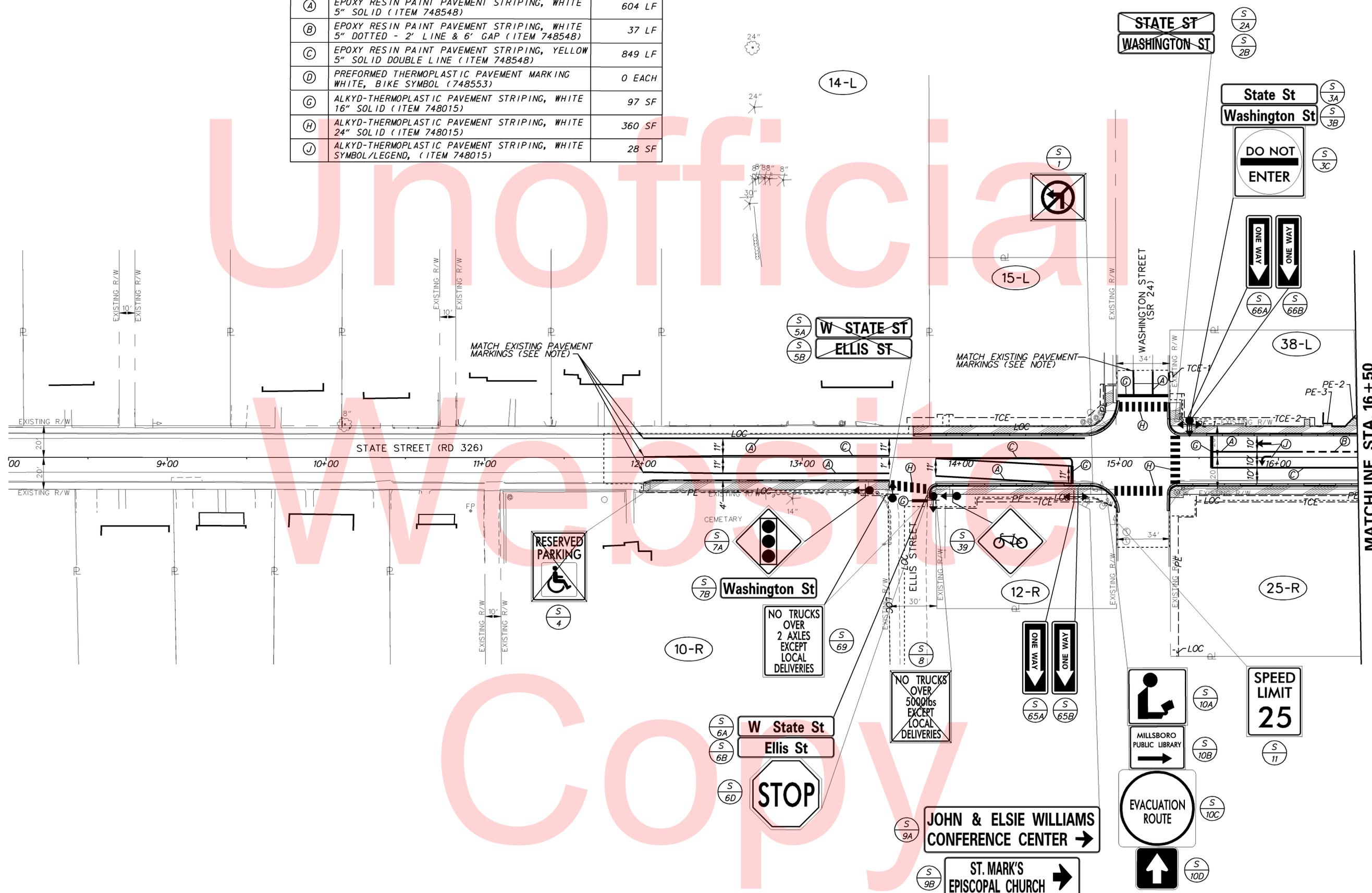
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 3/19/2016 9:15:58 AM



No. 19606-002-CADD Traffic\_CADD.L102 - DelDOT.dgn 10/17/2016 2:27:09 PM

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	604 LF
(B)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	37 LF
(C)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	849 LF
(D)	PREFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	0 EACH
(G)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 16" SOLID (ITEM 748015)	97 SF
(H)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 24" SOLID (ITEM 748015)	360 SF
(J)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	28 SF

NOTE: ADDITIONAL STRIPING BEYOND CONTRACT LIMITS MAY BE REQUIRED TO MATCH THE EXISTING STRIPING REMOVED FOR MAINTENANCE OF TRAFFIC. ACTUAL LIMITS TO BE DETERMINED BY THE ENGINEER.

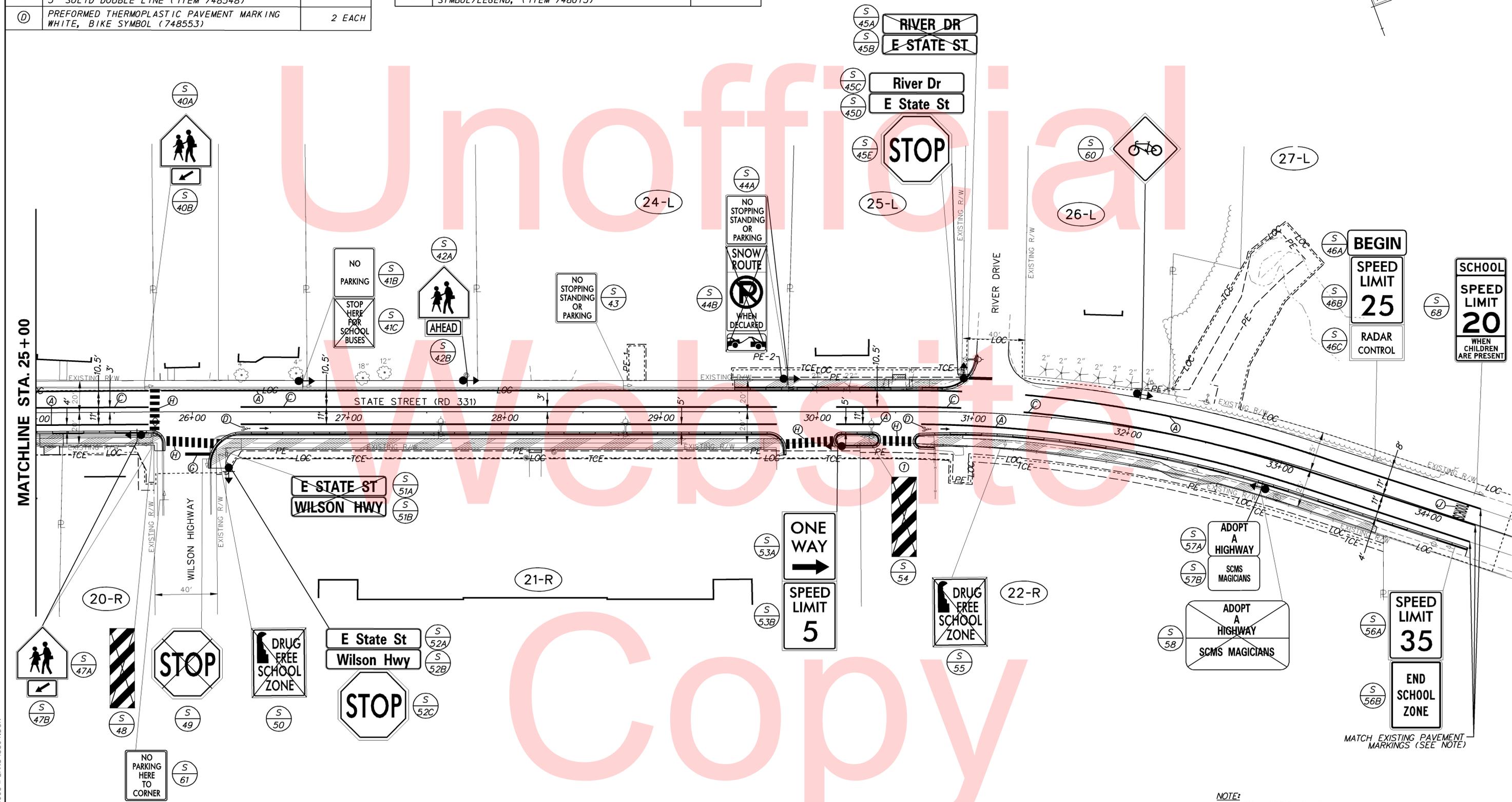


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PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	1689 LF
(B)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	0 LF
(C)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID DOUBLE LINE (ITEM 748548)	1588 LF
(D)	PREFORMED THERMOPLASTIC PAVEMENT MARKING WHITE, BIKE SYMBOL (748553)	2 EACH

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(G)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 16" SOLID (ITEM 748015)	41 SF
(H)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE 24" SOLID (ITEM 748015)	339 SF
(J)	ALKYD-THERMOPLASTIC PAVEMENT STRIPING, WHITE SYMBOL/LEGEND, (ITEM 748015)	35 SF



MATCH EXISTING PAVEMENT MARKINGS (SEE NOTE)

**NOTE:**  
ADDITIONAL STRIPING BEYOND CONTRACT LIMITS MAY BE REQUIRED TO MATCH THE EXISTING STRIPING REMOVED FOR MAINTENANCE OF TRAFFIC. ACTUAL LIMITS TO BE DETERMINED BY THE ENGINEER.



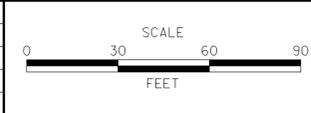
**SIGN SCHEDULE**

NO.	SHEET NO.	PLAN INDICATOR	CODE	QTY.	DISCRIPTION	ASSEMBLY NO.	SIGN WIDTH (IN)	SIGN HEIGHT (IN)	SIGN AREA (SF)	ITEM 749687 SINGLE POST (EACH)			ITEM 749690 MULTI POST (POST NEEDED)			POST INSTALLATION TYPE	Code X11 12' Post (W/ Basepost)	ITEM 749688 4" HOLE, 0-6" (EACH)	ITEM 749689 4" HOLE, >6" (EACH)	REMARKS					
										SIGN DISPOSITION	REMOVE	INSTALL	SIGN DISPOSITION	REMOVE	INSTALL										
424	44	S-44A	R7-4-DE(18)	1	NO STOPPING, STANDING, OR PARKING - 18x24	S-44	18"	24"	3	RENEW	1	1				SOIL	1	0	0	PR SW					
567	43	S-27A	R10-7-DE	1	DO NOT BLOCK DRIVEWAY	S-27	24"	30"	5	NEW						SOIL	1	0	0						
1197	43	S-27B	W11-8(30)	1	EMERGENCY VEHICLE (Symbol) - 30x30	S-27	30"	30"	6.25	NEW						SOIL	1	0	0						
42	43	S-28A	R2-1-25(24)	1	SPEED LIMIT (25 MPH - 24x30)	S-28				REPOSITION	1	1				CONCRETE	1	0	0	PR SW					
424	43	S-28B	R7-4-DE(18)	1	NO STOPPING, STANDING, OR PARKING - 18x24	S-28				REPOSITION	1	1													
1963	43	S-29	OM3-R	1	TYPE 3 OBJECT MARKER	S-29				REMOVE	1						0	0	0						
424	43	S-30	R7-4-DE(18)	1	NO STOPPING, STANDING, OR PARKING - 18x24	S-30	18"	24"	3	NEW						SOIL	1	0	0						
1963	43	S-31	OM3-R	1	TYPE 3 OBJECT MARKER	S-31				REMOVE	1						0	0	0						
1671	43	S-32A	D3-1(6)	1	STREET NAME (1 Line)	S-32				REMOVE	1						0	0	0	E STATE ST					
1671	43	S-32B	D3-1(6)	1	STREET NAME (1 Line)	S-32				REMOVE	1						0	0	0	MORRIS ST					
4	43	S-33	R1-1(36)	1	STOP	S-33				REMOVE	1						0	0	0						
1671	43	S-34A	D3-1(6)	1	STREET NAME (1 Line)	S-34	36	12	3	NEW										Morris St (Back-to-Back) 6" & 4.5" C-Series					
1671	43	S-34B	D3-1(6)	1	STREET NAME (1 Line)	S-34	42	12	3.5	NEW						SOIL	1	0	0	E State St (Back-to-Back) 6" & 4.5" C-Series					
4	43	S-34C	R1-1(36)	1	STOP	S-34	36"	36"	9	NEW															
623	43	S-23	R12-3-DE	1	NO TRUCKS OVER 2 AXLES EXCEPT LOCAL SERVICES	S-23	24"	36"	6	NEW						CONCRETE	1	0	0	PR SW					
1966	43	S-35A	S1-1(36)	1	SCHOOL SIGN (Symbol) - 36x36	S-35	36"	36"	9	NEW															
1369	43	S-35B	W16-9P(24)	1	AHEAD (plaque) - 24x12	S-35	24"	12"	2	NEW						SOIL	1	0	0						
1963	43	S-36	OM3-R	1	TYPE 3 OBJECT MARKER	S-36				REMOVE	1						0	0	0						
1997	43	S-37	S5-1-DE(24)	1	SCHOOL SPEED LIMIT XX WHEN CHILDREN ARE PRESENT - 24x54	S-37	24"	54"	9				NEW			CONCRETE	2	2	0						
1963	43	S-38		1	TYPE 3 OBJECT MARKER	S-38				REMOVE	1						0	0	0						
585	43	S-62	R10-14b(24)	1	STOP HERE ON FLASHING RED - 24x36	S-62	24"	36"	6	NEW						SOIL	1	0	0						
440	43	S-64		1	NO PARKING (Blank) - 12"x18"	S-64				REMAIN							0	0	0	NO PARKING HERE TO CORNER					
1966	44	S-40A	S1-1(36)	1	SCHOOL SIGN (Symbol) - 36x36	S-40				REMAIN															
1360	44	S-40B	W16-7P_L(30)	1	LEFT DOWNWARD DIAGONAL ARROW (plaque) - 30x18	S-40				REMAIN															
441	44	S-41B	R7-107-DE(18)	1	NO PARKING (Blank) - 18"x24"	S-41				REPOSITION	1	1				CONCRETE	1	1	0						
44	44	S-41C		1	STOP HERE FOR SCHOOL BUSES	S-41				REMOVE	1														
1966	44	S-42A	S1-1(36)	1	SCHOOL SIGN (Symbol) - 36x36	S-42	36"	36"	9	NEW						CONCRETE	1	1	0						
1369	44	S-42B	W16-9P(24)	1	AHEAD (plaque) - 24x12	S-42	24"	12"	2	NEW															
424	44	S-43	R7-4-DE(18)	1	NO STOPPING, STANDING, OR PARKING - 18x24	S-43	18"	24"	3	RENEW	1	1				CONCRETE	1	1	0						
44	44	S-44B		1	CUSTOM SIGN	S-44				REMOVE	1						0	0	0	SNOW ROUTE/WHEN DECLARED					
1671	44	S-45A	D3-1(6)	1	STREET NAME (1 Line)	S-45				REMOVE	1						0	0	0	RIVER DR					
1671	44	S-45B	D3-1(6)	1	STREET NAME (1 Line)	S-45				REMOVE	1						0	0	0	E STATE ST					
1671	44	S-45C	D3-1(6)	1	STREET NAME (1 Line)	S-45	30	12	2.5	NEW										River Dr (Back-to-Back) 6" & 4.5" C-Series					
1671	44	S-45D	D3-1(6)	1	STREET NAME (1 Line)	S-45	42	12	3.5	NEW						SOIL	1	0	0	E State St (Back-to-Back) 6" & 4.5" C-Series					
4	44	S-45E	R1-1(36)	1	STOP	S-45	36"	36"	9	NEW															
1576	44	S-46A	M4-14(24)	1	BEGIN - 24x12	S-46				REMAIN															
41	44	S-46B	R2-1-25(18)	1	SPEED LIMIT (25 MPH - 18x24)	S-46				REMAIN															
1376	44	S-46C		1	RADAR ENFORCED (plaque) - 24x18	S-46				REMAIN															
1966	44	S-47A	S1-1(36)	1	SCHOOL SIGN (Symbol) - 36x36	S-47				REPOSITION	1	1													
1360	44	S-47B	W16-7P_L(30)	1	LEFT DOWNWARD DIAGONAL ARROW (plaque) - 30x18	S-47				REPOSITION	1	1				SOIL	0	0	0						
1963	44	S-48	OM3-R	1	TYPE 3 OBJECT MARKER	S-48				REMOVE	1						0	0	0						
4	44	S-49	R1-1(36)	1	STOP	S-49				REMOVE	1						0	0	0						
44	44	S-50		1	DRUG FREE SCHOOL ZONE	S-50				REMOVE	1						0	0	0	DRUG FREE SCHOOL ZONE					
1671	44	S-51A	D3-1(6)	1	STREET NAME (1 Line)	S-51				REMOVE	1						0	0	0	E STATE ST					
1671	44	S-51B	D3-1(6)	1	STREET NAME (1 Line)	S-51				REMOVE	1						0	0	0	WILSON HWY					
1671	44	S-52A	D3-1(6)	1	STREET NAME (1 Line)	S-52	42	12	3.5	NEW										E State St (Back-to-Back) 6" & 4.5" C-Series					
1671	44	S-52B	D3-1(6)	1	STREET NAME (1 Line)	S-52	42	12	3.5	NEW						SOIL	1	0	0	Wilson Hwy (Back-to-Back) 6" & 4.5" C-Series					
4	44	S-52C	R1-1(36)	1	STOP	S-52	36"	36"	9	NEW															
396	44	S-53A	R6-2_R(18)	1	ONE WAY (RIGHT ARROW) - 18x24	S-53	18"	24"	3	NEW						CONCRETE	1	1	0						
38	44	S-53B	R2-1(24)	1	SPEED LIMIT (Blank - 24x30)	S-53	24"	30"	5	NEW										Speed Limit 5					
1963	44	S-54	OM3-R	1	TYPE 3 OBJECT MARKER	S-54				REMOVE	1						0	0	0						
44	44	S-55		1	DRUG FREE SCHOOL ZONE	S-55				REMOVE	1						0	0	0	DRUG FREE SCHOOL ZONE					
50	44	S-56A	R2-1-35(24)	1	SPEED LIMIT (35 MPH - 24x30)	S-56				REMAIN							0	0	0	REPOSITION BELOW SIGN S-59					
1999	44	S-56B	S5-2(24)	1	END SCHOOL ZONE - 24x30	S-59				REMAIN							0	0	0						
1872	44	S-57A	D14-3-DE	1	ADOPT A HIGHWAY	S-57				NEW						SOIL	1	0	0						
1872	44	S-57B	D14-3-DE	1	ADOPT A HIGHWAY	S-57				NEW										SCMS MAGICIANS					
1872	44	S-58	D14-3-DE	1	ADOPT A HIGHWAY	S-58				REMOVE	1						0	0	0	ADOPT A HIGHWAY / SCMS MAGICIANS					
1164	44	S-60		1	BICYCLE (Symbol) - 30x30	S-60	30"	30"	6.25	NEW						SOIL	1	0	0						
1997	44	S-68	S5-1-DE(24)	1	SCHOOL SPEED LIMIT XX WHEN CHILDREN ARE PRESENT - 24x54	S-68				REMAIN							0	0	0						
44	44	S-61		1	NO PARKING HERE TO CORNER	S-61				REMAIN							0	0	0						
47	47	S-101	R3-1/2	1	No Left/Right Turn - back-2-back		36	36	9								0	0	0	Mast arm signs - Traffic Contractor					
47	47	S-102	R3-1/2	1	No Left/Right Turn - back-2-back		36	36	9								0	0	0	Mast arm signs - Traffic Contractor					
<b>PAGE TOTALS</b>									<b>142</b>																
<b>JOB TOTALS</b>									<b>241.375</b>	<b>60</b>	<b>72</b>				<b>0</b>	<b>9</b>		<b>18</b>	<b>7</b>	<b>2</b>					

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ADDENDUMS / REVISIONS	



**HSIP SC,  
IRON BRANCH ROAD/STATE STREET**

CONTRACT T200800903	BRIDGE NO. N/A
COUNTY SUSSEX	DESIGNED BY: CM
	CHECKED BY: BB

**SIGNING, STRIPING  
AND CONDUIT PLAN**

<b>SS-06</b>
SHEET NO. 46
TOTAL SHTS. 48

CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.0 IN	5 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2*	1	2.0 IN	8 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	3	4.0 IN	5 FT	T	[NEW (2) 9/*14, (1) 4/*14, (14) 5/*14, (2) 4/*18, (1) CAT5e CABLE, (3) *6 GRD.]
4	2	4.0 IN	136 FT	T	[NEW (2) 9/*14, (3) 5/*14, (2) 4/*18, (1) CAT5e CABLE, (2) *6 GRD.]
5	1	3.0 IN	13 FT	T	[NEW (2) 9/*14, (2) 4/*18, (1) CAT5e CABLE, (1) *6 GRD.]
6	1	2.5 IN	12 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
7	1	4.0 IN	60 FT	O	[NEW (1) 5/*14, (1) *6 GRD.]
8	1	2.5 IN	3 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
9	2	4.0 IN	42 FT	B	[NEW (1) 4/*14, (9) 5/*14, (2) *6 GRD.]
10	1	2.5 IN	5 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
11	1	4.0 IN	149 FT	T	[NEW (3) 5/*14, (1) *6 GRD.]
12	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
13	1	2.5 IN	9 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
14	1	4.0 IN	41 FT	O	[NEW (1) 5/*14, (1) *6 GRD.]
15	1	2.5 IN	10 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
17**	1	AERIAL	125 FT	-	<REMOVE EX. (1) 4/*18>
18	1	4.0 IN	64 FT	O	[NEW (1) 4/*14, (4) 5/*14, (1) *6 GRD.]
19	1	2.5 IN	12 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
20	1	4.0 IN	36 FT	O	[NEW (1) 4/*14, (2) 5/*14, (1) *6 GRD.]
21	1	2.5 IN	11 FT	T	[NEW (1) 4/*14, (2) 5/*14, (1) *6 GRD.]
22	1	3.0 IN	8 FT	T	[NEW (2) 9/*14, (2) 4/*18, (1) CAT5e CABLE, (1) *6 GRD.]
23	1	2.5 IN	9 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
24	1	3.0 IN	9 FT	T	[NEW (1) 4/*18, (1) *6 GRD.]
25	1	AERIAL	131 FT	-	[NEW (1) 4/*18]
26**	1	AERIAL	750 FT	-	TO REMAIN - EX. (1) 4/*18
50**	1	AERIAL	68 FT	-	<REMOVE EX. (2) 4/*18>
51**	1	AERIAL	45 FT	-	<REMOVE EX. (1) 4/*18, EX. (1) 9/*14>
52**	1	AERIAL	152 FT	-	<REMOVE EX. (2) 4/*18>

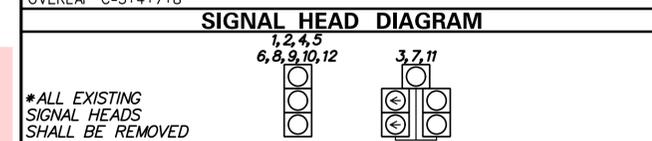
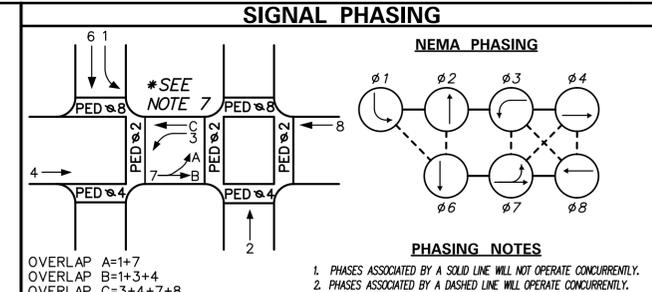
\* RIGID GALVANIZED STEEL CONDUIT  
 \*\*EXISTING AERIAL CABLE  
 \*\*\*ALL EXISTING CONDUIT TO BE ABANDONED IN PLACE  
 B = BORE, T = TRENCH, O = OPEN CUT

NO.	TYPE	STATION	OFFSET
1	PEDESTRIAN POLE W/ TYPE 4 BASE	14+84.0	23.7' RT.
2	PEDESTRIAN POLE W/ TYPE 4 BASE	14+96.6	34.9' LT.
3	PEDESTRIAN POLE W/ TYPE 4 BASE	15+34.5	23.8' RT.
4	PEDESTRIAN POLE W/ TYPE 4 BASE	15+35.6	35.0' LT.
5	55' MAST ARM W/ TYPE 3B BASE	15+35.7	29.1' RT.
6*	MAST ARM UPRIGHT POLE W/ TYPE 3 BASE - REMOVE	15+36.0	22.1' RT.
7	PEDESTRIAN POLE W/ TYPE 4 BASE	15+38.9	21.9' LT.
8	60' MAST ARM W/ TYPE 3B BASE	16+74.0	25.8' RT.
9	PEDESTRIAN POLE W/ TYPE 4 BASE	16+84.5	20.8' LT.
10*	MAST ARM UPRIGHT POLE W/ TYPE 3 BASE - REMOVE	16+86.0	20.8' RT.
11	PEDESTRIAN POLE W/ TYPE 4 BASE	16+89.4	23.8' RT.
12	PEDESTRIAN POLE W/ TYPE 4 BASE	17+34.3	21.3' LT.
13	PEDESTRIAN POLE W/ TYPE 4 BASE	17+37.2	20.8' RT.

\* EXISTING MAST ARMS (TO BE REMOVED BY TRAFFIC CONTRACTOR)

MA#	HEIGHT OF POLE	LENGTH OF ARM	* OF HEADS	* OPTICOM RECEIVERS	SF. OF SIGNING	ARM MOUNT HEIGHT
*1	21'-6"	60 FT	5	2	9.00 SF	**18'-0"
*2	21'-6"	55 FT	6	2	9.00 SF	**18'-0"

\*FISHEYE WITH 84" MOUNTING ARM  
 \*\*FIELD ADJUST, AS REQUIRED

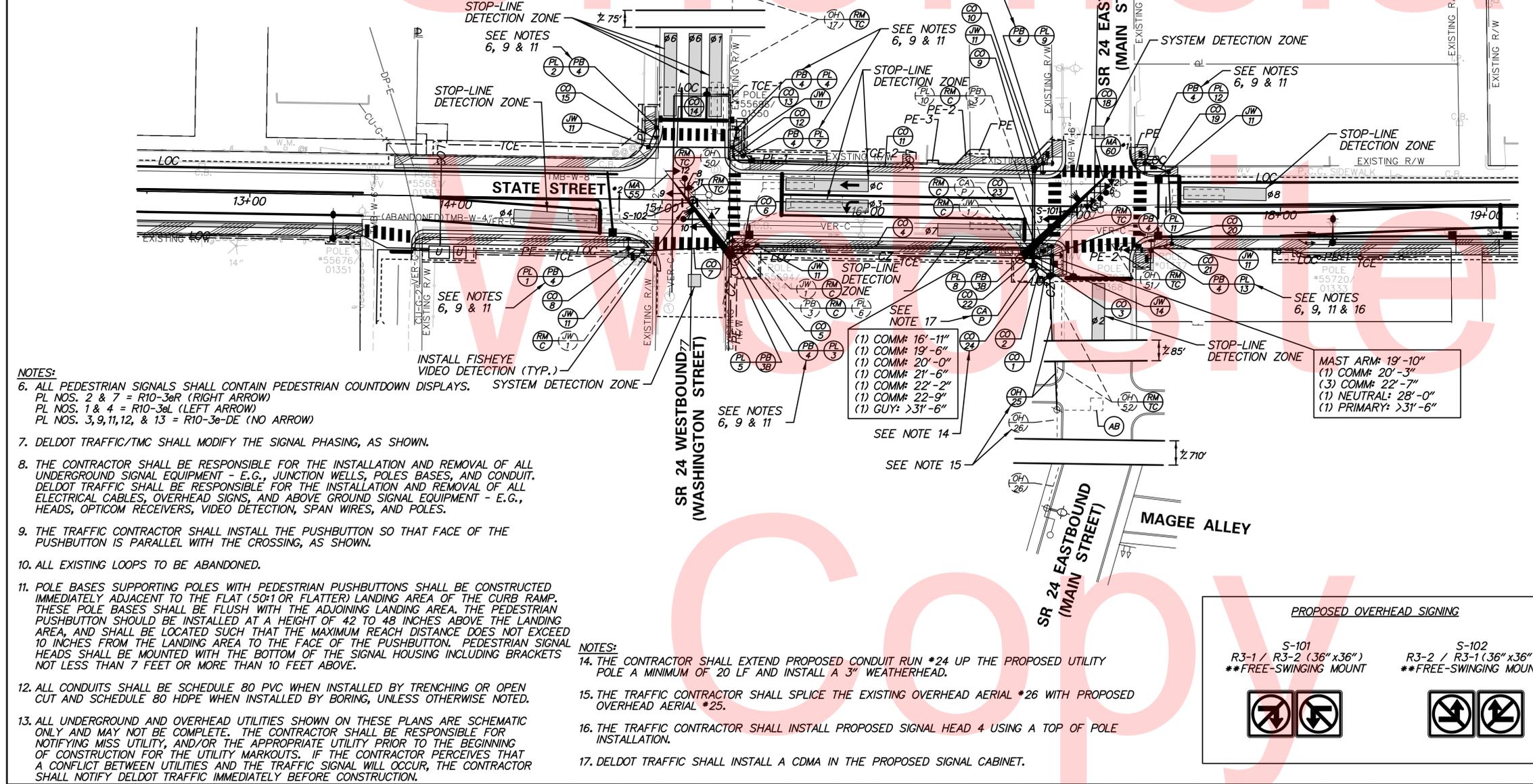


EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.
LOOP DETECTOR, TYPE 1	[Symbol]
LOOP DETECTOR, TYPE 2	[Symbol]
LUMINAIRE	[Symbol]
MAST ARM	[Symbol]
MICROWAVE DETECTION	[Symbol]
OPTICOM RECEIVER	[Symbol]
OVERHEAD SIGNING	[Symbol]
PEDESTRIAN POLE/BASE	[Symbol]
PEDESTRIAN PUSHBUTTON	[Symbol]
PEDESTRIAN SIGNAL HEAD	[Symbol]
RIGHT-OF-WAY	[Symbol]
SERVICE PEDESTAL	[Symbol]
SIGNAL CABINET	[Symbol]
SIGNAL HEAD	[Symbol]
SIGNAL POLE/BASE	[Symbol]
SPAN INSULATOR	[Symbol]
SPAN WIRE	[Symbol]
UTILITY POLE	[Symbol]
VIDEO DETECTION (FISHEYE)	[Symbol]
FISHEYE DETECTION ZONE	[Symbol]

EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.
LOOP DETECTOR, TYPE 1	[Symbol]
LOOP DETECTOR, TYPE 2	[Symbol]
LUMINAIRE	[Symbol]
MAST ARM	[Symbol]
MICROWAVE DETECTION	[Symbol]
OPTICOM RECEIVER	[Symbol]
OVERHEAD SIGNING	[Symbol]
PEDESTRIAN POLE/BASE	[Symbol]
PEDESTRIAN PUSHBUTTON	[Symbol]
PEDESTRIAN SIGNAL HEAD	[Symbol]
RIGHT-OF-WAY	[Symbol]
SERVICE PEDESTAL	[Symbol]
SIGNAL CABINET	[Symbol]
SIGNAL HEAD	[Symbol]
SIGNAL POLE/BASE	[Symbol]
SPAN INSULATOR	[Symbol]
SPAN WIRE	[Symbol]
UTILITY POLE	[Symbol]
VIDEO DETECTION (FISHEYE)	[Symbol]
FISHEYE DETECTION ZONE	[Symbol]

**GENERAL SIGNAL NOTES**

- PROPOSED FISHEYE DETECTION ZONES (TO INSTALL):  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET RIGHT-TURN/THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET THROUGH MOVEMENT (MEDIAN)  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET LEFT-TURN MOVEMENT (MEDIAN)  
 TYPE #2 - 6' x 40' - SR 24 WESTBOUND LEFT-TURN MOVEMENT  
 TYPE #2 - 6' x 40' - SR 24 EASTBOUND THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SR 24 WESTBOUND THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SOUTHBOUND STATE STREET RIGHT-TURN/THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SOUTHBOUND STATE STREET LEFT-TURN/THROUGH MOVEMENT (MEDIAN)  
 SYSTEM - 6' x 6' - SR 24 WESTBOUND THROUGH LANE MOVEMENT
- ALL PROPOSED SIGNAL POLES ARE DELDOT MAST ARMS (ORNAMENTAL).
- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
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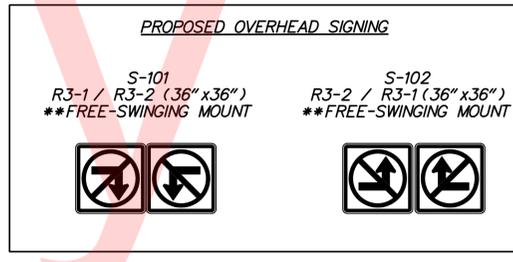


**NOTES:**

- ALL PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS. SYSTEM DETECTION ZONE  
 PL NOS. 2 & 7 = R10-3eR (RIGHT ARROW)  
 PL NOS. 1 & 4 = R10-3eL (LEFT ARROW)  
 PL NOS. 3, 9, 11, 12, & 13 = R10-3e-DE (NO ARROW)
- DELDOT TRAFFIC/TMC SHALL MODIFY THE SIGNAL PHASING, AS SHOWN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLES BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES, OVERHEAD SIGNS, AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OPTICOM RECEIVERS, VIDEO DETECTION, SPAN WIRES, AND POLES.
- THE TRAFFIC CONTRACTOR SHALL INSTALL THE PUSHBUTTON SO THAT FACE OF THE PUSHBUTTON IS PARALLEL WITH THE CROSSING, AS SHOWN.
- ALL EXISTING LOOPS TO BE ABANDONED.
- POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50:1 OR FLATTER) LANDING AREA OF THE CURB RAMP. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE.
- ALL CONDUITS SHALL BE SCHEDULE 80 PVC WHEN INSTALLED BY TRENCHING OR OPEN CUT AND SCHEDULE 80 HDPE WHEN INSTALLED BY BORING, UNLESS OTHERWISE NOTED.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

**NOTES:**

- THE CONTRACTOR SHALL EXTEND PROPOSED CONDUIT RUN #24 UP THE PROPOSED UTILITY POLE A MINIMUM OF 20 LF AND INSTALL A 3" WEATHERHEAD.
- THE TRAFFIC CONTRACTOR SHALL SPLICE THE EXISTING OVERHEAD AERIAL #26 WITH PROPOSED OVERHEAD AERIAL #25.
- THE TRAFFIC CONTRACTOR SHALL INSTALL PROPOSED SIGNAL HEAD 4 USING A TOP OF POLE INSTALLATION.
- DELDOT TRAFFIC SHALL INSTALL A CDMA IN THE PROPOSED SIGNAL CABINET.



RECOMMENDED _____ DATE: _____	RECOMMENDED _____ DATE: _____	RECOMMENDED <i>[Signature]</i> DATE: <i>8/17/16</i>	APPROVED TRAFFIC ENGINEER _____ DATE: _____	APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER <i>[Signature]</i> DATE: <i>8/18/16</i>
ADDENDUM / REVISIONS		SCALE 0 30 60 90 FEET		CONTRACT T200800903
DELAWARE DEPARTMENT OF TRANSPORTATION		HSIP SC, IRON BRANCH ROAD /STATE STREET		PERMIT NO. S151
				DESIGNED BY: B.S.S. (WRA)
				CHECKED BY: M.J.B. (WRA)
				SIGNAL PLAN SR 24 (WASHINGTON STREET/ MAIN STREET) @ STATE STREET
				SHEET NO. 47
				TOTAL SHTS. 48

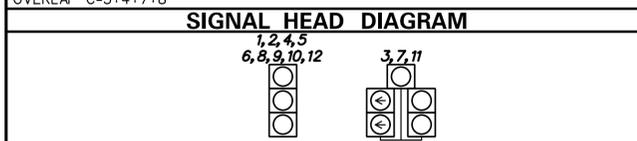
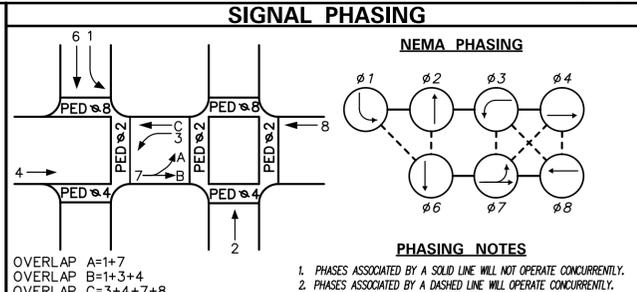
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CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.0 IN	5 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LINE SIDE]
2*	1	2.0 IN	8 FT	T	[NEW (1) 2/*8 U.F. W/GROUND - LOAD SIDE]
3	3	4.0 IN	5 FT	T	[NEW (2) 9/*14, (1) 4/*14, (14) 5/*14, (2) 4/*18, (1) CAT5e CABLE, (3) *6 GRD.]
4	2	4.0 IN	136 FT	T	[NEW (2) 9/*14, (3) 5/*14, (2) 4/*18, (1) CAT5e CABLE, (2) *6 GRD.]
5	1	3.0 IN	13 FT	T	[NEW (2) 9/*14, (2) 4/*18, (1) CAT5e CABLE, (1) *6 GRD.]
6	1	2.5 IN	12 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
7	1	4.0 IN	60 FT	O	[NEW (1) 5/*14, (1) *6 GRD.]
8	1	2.5 IN	3 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
9	2	4.0 IN	42 FT	B	[NEW (1) 4/*14, (9) 5/*14, (2) *6 GRD.]
10	1	2.5 IN	5 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
11	1	4.0 IN	149 FT	T	[NEW (3) 5/*14, (1) *6 GRD.]
12	1	2.5 IN	6 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
13	1	2.5 IN	9 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
14	1	4.0 IN	41 FT	O	[NEW (1) 5/*14, (1) *6 GRD.]
15	1	2.5 IN	10 FT	T	[NEW (1) 5/*14, (1) *6 GRD.]
17**	1	AERIAL	125 FT	-	<REMOVE EX. (1) 4/*18>
18	1	4.0 IN	64 FT	O	[NEW (1) 4/*14, (4) 5/*14, (1) *6 GRD.]
19	1	2.5 IN	12 FT	T	[NEW (2) 5/*14, (1) *6 GRD.]
20	1	4.0 IN	36 FT	O	[NEW (1) 4/*14, (2) 5/*14, (1) *6 GRD.]
21	1	2.5 IN	11 FT	T	[NEW (1) 4/*14, (2) 5/*14, (1) *6 GRD.]
22	1	3.0 IN	8 FT	T	[NEW (2) 9/*14, (2) 4/*18, (1) CAT5e CABLE, (1) *6 GRD.]
23	1	2.5 IN	9 FT	T	[NEW (2) 9/*14, (2) 4/*18, (1) CAT5e CABLE, (1) *6 GRD.]
24	1	3.0 IN	9 FT	T	[NEW (1) 4/*18, (1) *6 GRD.]
25	1	3.0 IN	20 FT	BAND	[NEW (1) 4/*18, (1) *6 GRD.]
26**	1	AERIAL	131 FT	-	[NEW (1) 4/*18]
26**	1	AERIAL	750 FT	-	TO REMAIN - EX. (1) 4/*18
50**	1	AERIAL	68 FT	-	<REMOVE EX. (2) 4/*18>
51**	1	AERIAL	45 FT	-	<REMOVE EX. (1) 4/*18, EX. (1) 9/*14>
52**	1	AERIAL	152 FT	-	<REMOVE EX. (2) 4/*18>

\* RIGID GALVANIZED STEEL CONDUIT  
 \*\*EXISTING AERIAL CABLE  
 \*\*\*ALL EXISTING CONDUIT TO BE ABANDONED IN PLACE

- NOTES:**
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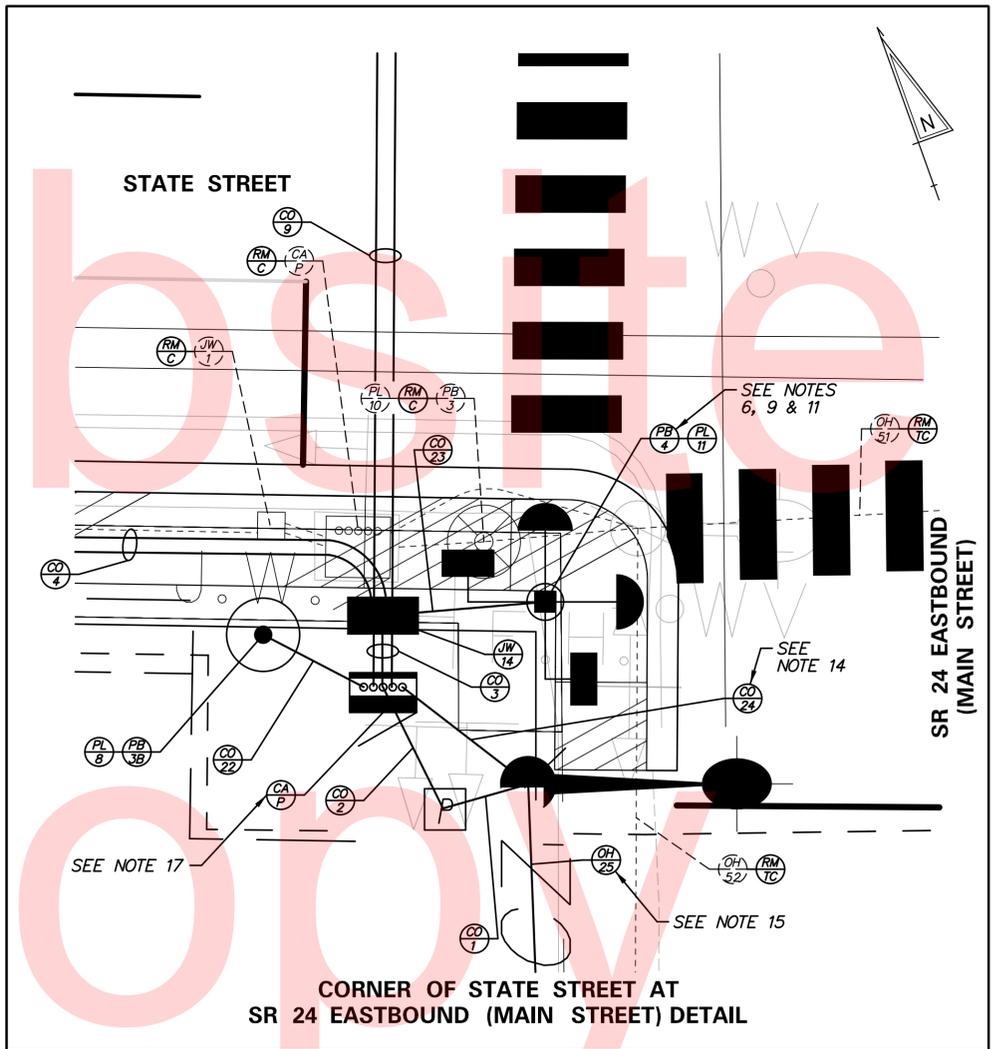
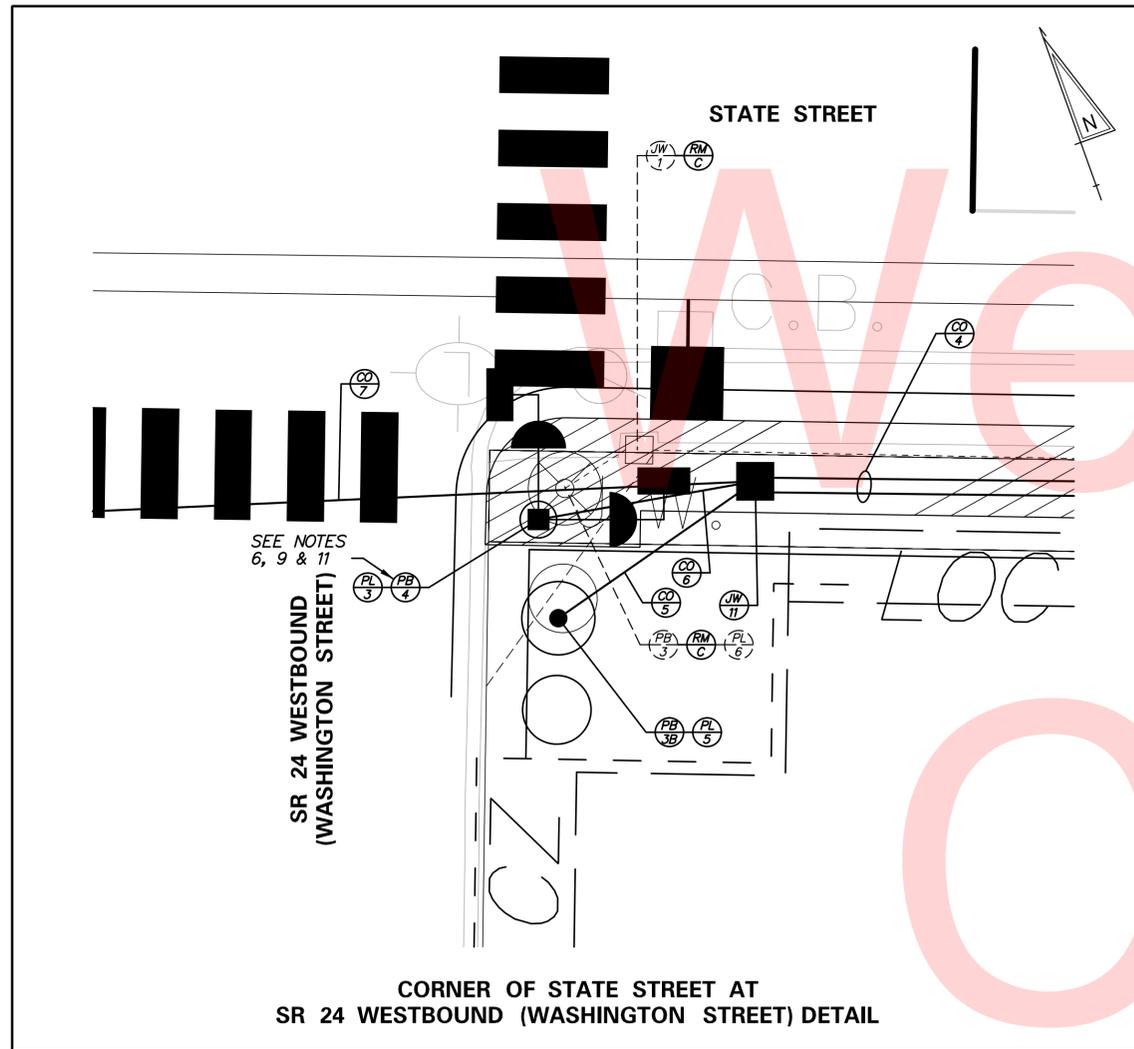
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LEGEND	
(AB)	ABANDON
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
(CA)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(CO)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
(OH)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(OH)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(PL)	EXISTING POLE IDENTIFIER (* OF POLE)
(PL)	PROPOSED POLE IDENTIFIER (* OF POLE)
(RM)	REMOVE BY CONTRACTOR
(RM)	REMOVE BY OTHERS
(RM)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	○	○
MAST ARM	∧	∧
MICROWAVE DETECTION	◄	◄
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	-	-
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	⊙	⊙
PEDESTRIAN SIGNAL HEAD	⊙	⊙
RIGHT-OF-WAY	---	---R/W---
SERVICE PEDESTAL	□	□
SIGNAL CABINET	□	□
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	—XX—	—XX—
UTILITY POLE	⊙	⊙
VIDEO DETECTION (FISHEYE)	⊙	⊙
FISHEYE DETECTION ZONE	□	□

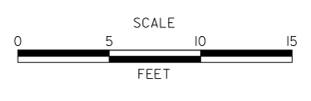
- GENERAL SIGNAL NOTES**
- PROPOSED FISHEYE DETECTION ZONES (TO INSTALL):  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET RIGHT-TURN/THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET THROUGH MOVEMENT (MEDIAN)  
 TYPE #2 - 6' x 40' - NORTHBOUND STATE STREET LEFT-TURN MOVEMENT (MEDIAN)  
 TYPE #2 - 6' x 40' - SR 24 WESTBOUND LEFT-TURN MOVEMENT  
 TYPE #2 - 6' x 40' - SR 24 EASTBOUND THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SR 24 WESTBOUND THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SOUTHBOUND STATE STREET RIGHT-TURN/THROUGH MOVEMENT  
 TYPE #2 - 6' x 40' - SOUTHBOUND STATE STREET LEFT-TURN/THROUGH MOVEMENT (MEDIAN)  
 SYSTEM - 6' x 6' - SR 24 WESTBOUND THROUGH LANE MOVEMENT  
 SYSTEM - 6' x 6' - SR 24 EASTBOUND THROUGH LANE MOVEMENT
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ADDENDUM / REVISIONS



HSIP SC,  
 IRON BRANCH ROAD /STATE STREET

CONTRACT	T200800903
COUNTY	SUSSEX
PERMIT NO.	S151
DESIGNED BY:	B.S.S. (WRA)
CHECKED BY:	M.J.B. (WRA)

SIGNAL PLAN		SHEET NO.
SR 24 (WASHINGTON STREET/		48
MAIN STREET) @		TOTAL SHTS.
STATE STREET		48

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