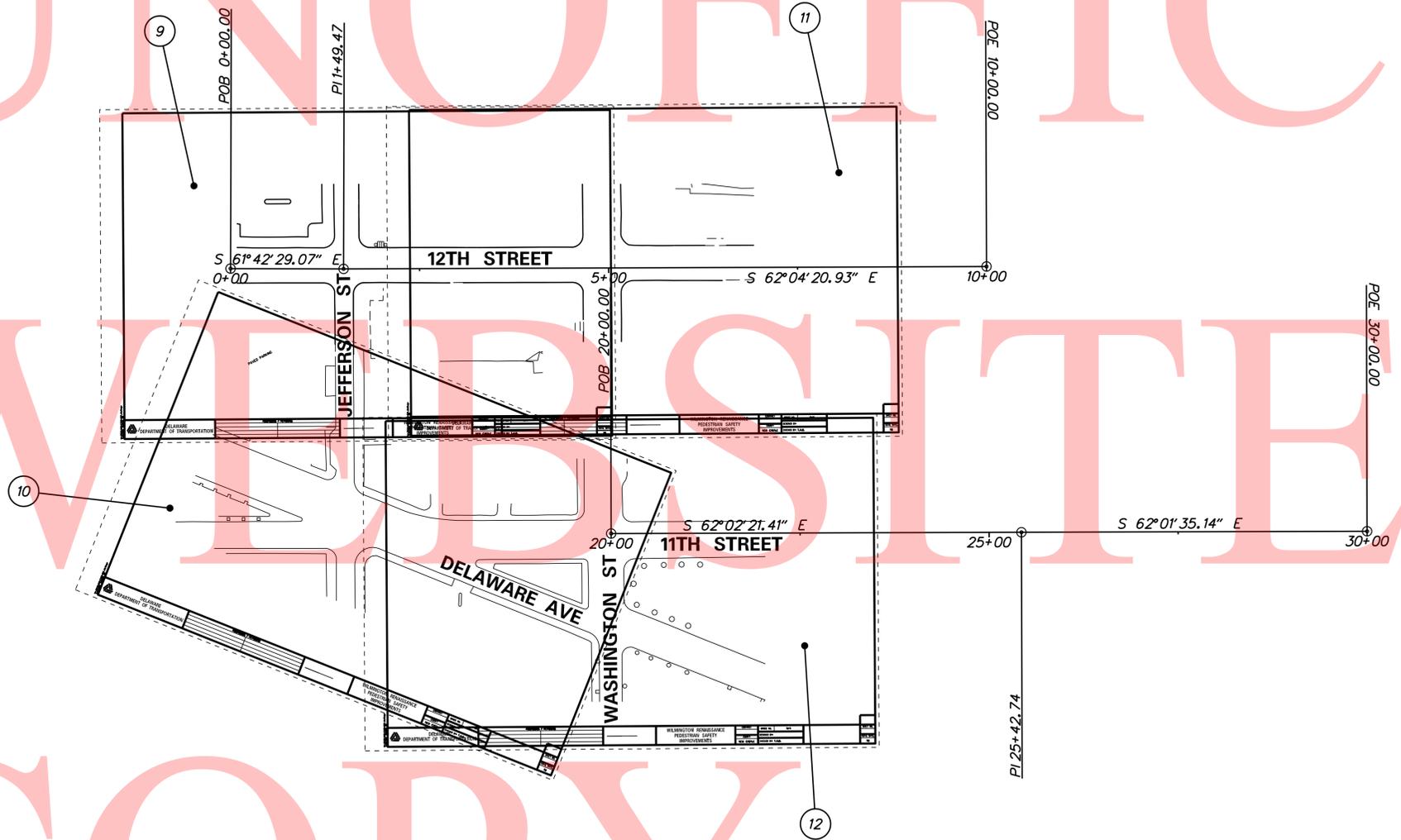
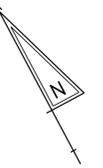


PLAN SHEET INDEX CROSS REFERENCE				
CONSTRUCTION PLAN	9	10	11	12
CONSTRUCTION PHASING, M.O.T. AND E&S PLAN	30	31, 32	33	34, 35
SIGNING, STRIPING AND CONDUIT PLAN			50	51
SIGNALIZATION PLAN	64			65
RIGHT OF WAY PLAN	73		74	75

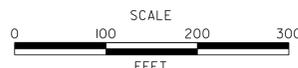


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ADDENDUMS / REVISIONS	



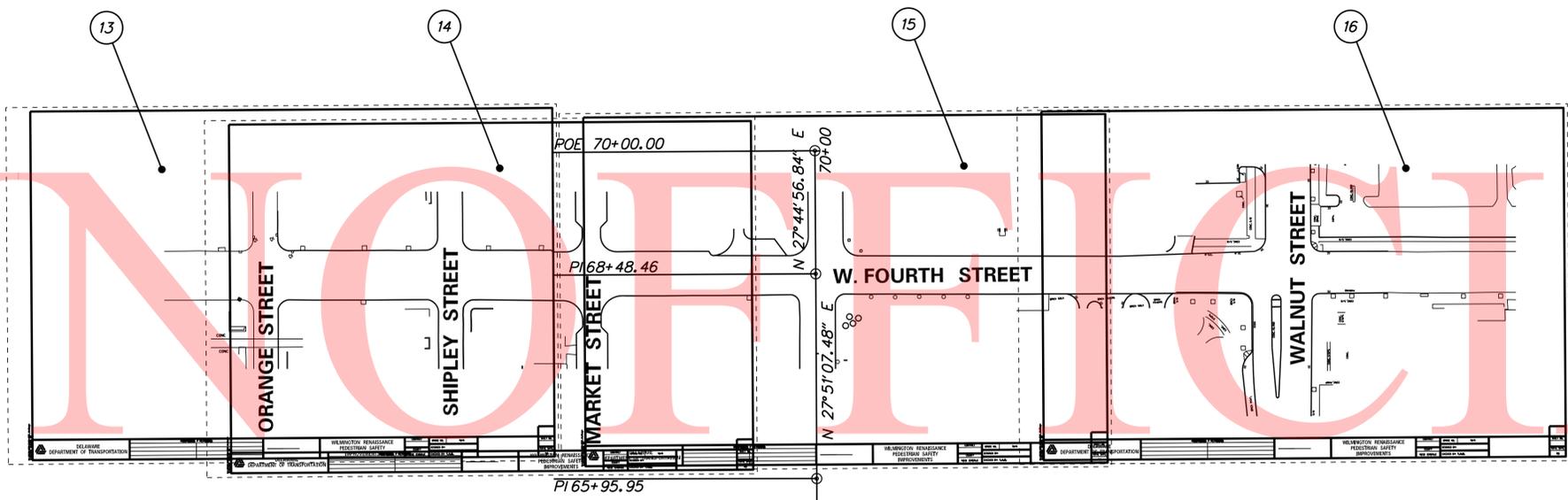
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

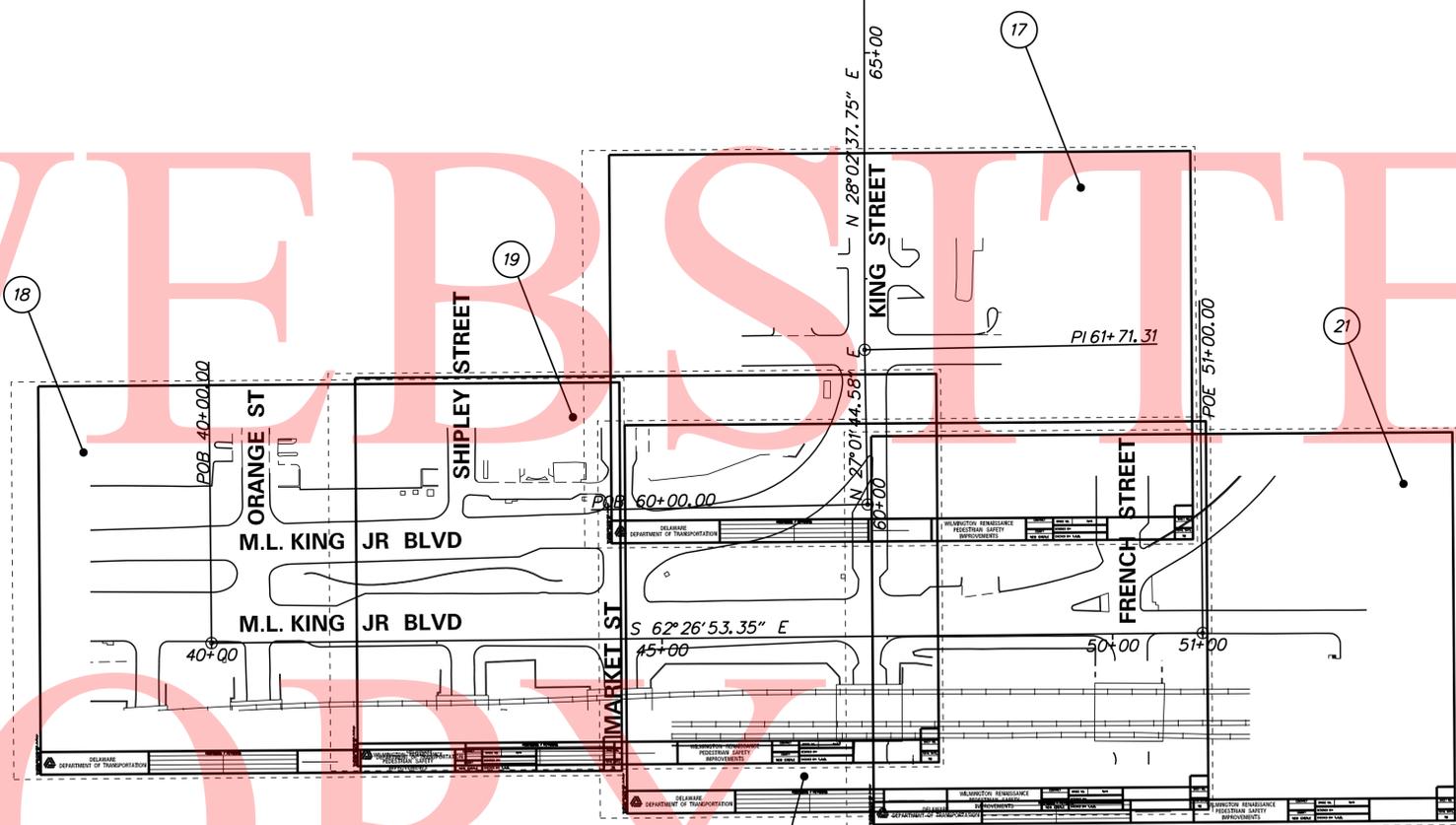
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SHEET NO.
2
TOTAL SHTS.
78

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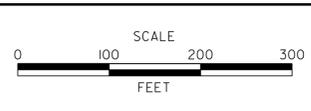
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PLAN SHEET INDEX CROSS REFERENCE									
CONSTRUCTION PLAN	13	14	15	16	17	18	19	20	21
CONSTRUCTION PHASING, M.O.T. AND E&S PLAN	29, 36, 37	38	39	40	41, 42	43, 44	45	46, 47	48, 49
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SIGNALIZATION PLAN				66	68	69	70	71	72
RIGHT OF WAY PLAN					76				

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ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

SHEET INDEX	SHEET NO. 3
	TOTAL SHTS. 78

IS-02

EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE CATCH BASIN
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER VENT OR CLEANOUT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE GUY WIRE ANCHOR
	UTILITY POLE
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD

MANMADE ROADSIDE FEATURES	
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE

UTILITY COMPANY FACILITIES	
	EXISTING CITY OF WILMINGTON SANITARY SEWER LINE
	EXISTING CITY OF WILMINGTON WATER LINE
	EXISTING DELMARVA POWER ELECTRIC LINE
	EXISTING DELMARVA POWER GAS LINE
	VERIZON DELAWARE LLC (FIBER OPTIC)

PROPOSED SYMBOLS

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BUTT JOINT
	CONSTRUCTION BASELINE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CLEAR ZONE
	DRAINAGE INLET
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPES 1 & 3
	GUARDRAIL, TYPE 2
	GUARDRAIL END TREATMENT - PARALLEL
	GUARDRAIL END TREATMENT - PARABOLIC
	HORIZONTAL CLEARANCE
	JUNCTION BOX - DRAINAGE
	LIMIT OF CONSTRUCTION
	MANHOLE
	PAVEMENT PATCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK @ 4"
	P.C.C. SIDEWALK @ 6"
	UNDERDRAIN
	UNDERDRAIN OUTLET

CONSTRUCTION PHASING SYMBOLS	
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	PHASING TRAFFIC FLOW ARROW

LANDSCAPING	
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

EROSION & SEDIMENT CONTROL	
	DEWATERING BASIN
	EROSION CONTROL BLANKET
	EARTH DIKE
	INLET SEDIMENT CONTROL
	PERIMETER DIKE/SWALE
	PORTABLE SEDIMENT TANK
	REINFORCED SILT FENCE
	SANDBAG DIKE
	SANDBAG DIVERSION
	STONE CHECK DAM
	STABILIZED CONSTRUCTION ENTRANCE
	SILT FENCE
	SUMP PIT, TYPE 1
	SUMP PIT, TYPE 2
	SEDIMENT TRAP
	SEDIMENT TRAP WITH INLET AS OUTLET
	SEDIMENT TRAP PIPE OUTLET
	STILLING WELL
	TEMPORARY SWALE
	TEMPORARY SLOPE DRAIN

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / SPECIAL
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	DRAINAGE INLET
	DO NOT DISTURB
	FLARED END SECTION
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	LANDSCAPE PLANTINGS
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY OTHERS
	SEDIMENT TRAP
	SILT FENCE
	UNDERDRAIN

IDENTIFIERS	
	CONDUIT RUN
	LIGHT STANDARD
	JUNCTION WELL

PAVEMENT SECTION(S)	
	2" SUPERPAVE, TYPE C HOT-MIX, PATCHING
	3" SUPERPAVE, TYPE B HOT-MIX, PATCHING
	4" SUPERPAVE, BCB, PATCHING
	8" GABC, TYPE B, PATCHING
	2" SUPERPAVE, TYPE C HOT-MIX, PATCHING
	3" SUPERPAVE, TYPE B HOT-MIX, PATCHING
	6" SUPERPAVE, BCB, PATCHING
	10" GABC, TYPE B, PATCHING

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING RIGHT-OF-WAY
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE
	HISTORIC RIGHT-OF-WAY BASELINE

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN
	DECORATIVE LUMINAIRE

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GENERAL NOTES

- THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS," DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS," DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
- THE CONTRACTOR SHALL GIVE TWO (2) WEEKS NOTICE TO THE PROPERTY OWNER WHEN ANY FIXTURE, SHRUB OR OTHER OBJECT MUST BE REMOVED FROM THE RIGHT OF WAY OR EASEMENT AREA. IF THE OWNER HAS NOT ATTEMPTED TO SALVAGE THIS PROPERTY, THE CONTRACTOR SHALL REMOVE IT WITHOUT OBLIGATION. COMPENSATION SHALL BE INCIDENTAL TO THE CONTRACT.
- THE ENDS OF ALL CURBS SHALL BE DEPRESSED FLUSH WITH THE PAVEMENT AT A RATIO OF TWELVE TO ONE (12:1) UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL PVC SLEEVES (4" INSIDE MINIMUM DIAMETER, 6" INSIDE MAXIMUM DIAMETER) IN PROPOSED CONCRETE SIDEWALKS, ISLANDS, AND MEDIANS FOR TRAFFIC SIGN POSTS AS DIRECTED BY THE ENGINEER. THE LOWER END OF THE SLEEVE SHALL SIT ON THE TOP OF THE SUBBASE MATERIAL. THE COST SHALL BE INCIDENTAL TO THE CONTRACT.
- SITE REVIEWER - AN EROSION CONTROL SITE REVIEWER SHALL BE A PERSON FROM THE CONTRACTOR'S STAFF ASSIGNED TO EROSION AND SEDIMENT CONTROL IMPLEMENTATION AND MAINTENANCE AND SHALL BE REQUIRED ON SPECIFIC PROJECTS. THE NAME AND DNREC CERTIFICATION NUMBER OF EACH SITE REVIEWER SO REQUIRED SHALL BE SUBMITTED TO THE DEPARTMENT AT THE TIME OF BID. THE NAME OF THE DELAWARE REGISTERED PROFESSIONAL ENGINEER PROVIDING DIRECTION AND SUPERVISION OF THE SITE REVIEWER, AS REQUIRED IN SECTION 12.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS, SHALL ALSO BE SUBMITTED TO THE DEPARTMENT AT THE TIME OF BID. THE SITE REVIEWER REQUIREMENTS IN EFFECT ON THIS PROJECT SHALL BE MARKED WITH AN "X" BELOW:

EROSION POTENTIAL FOR THIS PROJECT	SITE REVIEWER REQUIREMENT
(X) INSIGNIFICANT	NONE
() MINOR	CONTRACTOR CERTIFICATION COURSE TRAINING ONLY, AS DEFINED IN SECTION 13 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MEDIUM	AT THE TIME OF BID OF THE CONTRACT, EITHER THE SUPERINTENDENT OR A SEPARATE INDIVIDUAL FROM THE CONTRACTOR'S STAFF SHALL BE A CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 12 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MAJOR	SUPERINTENDENT AND AN INDIVIDUAL FROM CONTRACTOR'S STAFF SHALL BE CCR. ONE INDIVIDUAL FROM THE CONTRACTOR'S STAFF MUST BE A CCR AT THE TIME OF BID OF THE CONTRACT. THE SUPERINTENDENT MUST BECOME A CCR WITHIN ONE YEAR AFTER THE AWARD OF CONTRACT.

- DISTURBED AREA: 0.17 ACRES
- ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR INCLUDE:

()	NONE
()	ASCH/ DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
(X)	RASTER FILES, IN .CAL FILE FORMAT, FOR ALL PLAN SHEETS.
()	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

- AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

(X)	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
()	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR.
()	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR.

PROJECT NOTES

SECTION 100

- THE CONTRACTOR WILL BE REQUIRED TO COORDINATE HIS WORK WITH ANY CONSTRUCTION ACTIVITY BY UTILITY COMPANIES, OR ADJACENT CONSTRUCTION CONTRACTS - SEE PROJECT SPECIFICATIONS. PROPOSED UTILITY RELOCATIONS ARE SHOWN ON THE PLANS AND SHALL BE INSTALLED BY THE RESPECTIVE UTILITY COMPANY, UNLESS OTHERWISE NOTED. REFER TO UTILITY STATEMENT FOR DETAILED SCOPE AND TIMING OF UTILITY WORK.
- ANY DAMAGE TO ITEMS NOTED TO BE RELOCATED OR RESET BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN-KIND AT THE CONTRACTOR'S EXPENSE.
- UNLESS OTHERWISE NOTED, EXISTING FEATURES (I.E. STEPS, FENCES, RAILINGS, BILCO DOORS, WINDOW WELLS, ETC) SHALL NOT BE DISTURBED. IF THE EXISTING FEATURES ARE DISTURBED BY THE CONTRACTOR AT THE DISCRETION OF THE ENGINEER, THEY SHALL BE RESTORED TO THE ORIGINAL CONDITION OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- DUST SHALL BE CONTROLLED EXCLUSIVELY THROUGH THE USE OF WATER. COSTS ASSOCIATED WITH THE FURNISHING AND APPLICATION OF WATER FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT BID ITEMS INCLUDING SAW CUTTING OF BRICK, BLOCK, CONCRETE PIPE, CONCRETE SIDEWALKS.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE CONTRACTOR SHALL, AS PART OF HIS PROJECT SCHEDULE, SUBMIT TO THE ENGINEER AN ESTIMATE OF THE MONTHLY PAYMENTS EXPECTED TO BE RECEIVED ON THE CONTRACT. THIS WILL BE REFERENCED AS THE "MONTHLY SPEND PAYMENT CHART."

A CHART IN MICROSOFT EXCEL, MICROSOFT WORD, OR HAND WRITTEN FORMAT WILL BE ACCEPTABLE FOR THIS PURPOSE. THE CHART SHOULD INCLUDE, AS A MINIMUM, COLUMNS FOR THE MONTH, YEAR, AND ESTIMATED MONTHLY PAYMENTS. THE TOTAL OF ALL ESTIMATED MONTHLY PAYMENTS SHOULD EQUAL THE AWARDED CONTRACT TOTAL BID PRICE.

THE ENGINEER MAY REQUEST AN UPDATED "MONTHLY SPEND PAYMENT CHART" AT HIS DISCRETION, DEPENDING ON THE ACCURACY OF THE INITIAL ESTIMATES AND ACCORDING TO THE OVERALL NEEDS OF THE DEPARTMENT.

THE "MONTHLY SPEND PAYMENT CHART" WILL NOT BE CONSIDERED A BINDING DOCUMENT BY EITHER THE CONTRACTOR OR THE DEPARTMENT AND IS CONSIDERED SOLELY INFORMATIONAL.

ON PROJECTS REQUIRING CPM SCHEDULES, THE CONTRACTOR MAY, BUT IS NOT REQUIRED TO, "COST LOAD" THE CPM SCHEDULE IN ORDER TO GENERATE THE MONTHLY PAYMENT ESTIMATES.

COSTS TO PREPARE AND/OR UPDATE THE "MONTHLY SPEND PAYMENT CHART" ARE ADDRESSED AS FOLLOWS:

A. ON CONTRACTS REQUIRING CPM SCHEDULES AND UPDATES, PREPARATION OF THE INITIAL CHART SHALL BE INCIDENTAL TO ITEM 763508. UPDATES SHALL BE INCIDENTAL TO ITEM 763509.

B. ON CONTRACTS NOT REQUIRING CPM SCHEDULES, THE COST TO PREPARE AND UPDATE THE "MONTHLY SPEND PAYMENT CHART" SHALL BE INCLUDED IN ITEM 763000, INITIAL EXPENSE.

SECTION 200

- THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXCAVATE TEST PITS AT POINTS OF POSSIBLE UTILITY CONFLICTS TO DETERMINE IF A CONFLICT EXISTS. ANY CONFLICTS SHALL BE COORDINATED BY THE CONTRACTOR, WITH THE ENGINEER AND THE UTILITY COMPANY INVOLVED. THE ENGINEER SHALL ULTIMATELY DETERMINE THE SOLUTION TO THE UTILITY CONFLICT. TEST HOLES SHALL BE MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 202000 - EXCAVATION AND EMBANKMENT, BUT ONLY TO THE ACTUAL DEPTH EXCAVATED.
- THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE DAMAGE TO ALL EXISTING TREES INCLUDING THOSE NOTED AS DO NOT DISTURB (DND) ON THE PLANS. ONLY TREES AND SHRUBS NOTED ON THE PLANS FOR REMOVAL MAY BE DISTURBED. THE CONTRACTOR SHALL CONTACT DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE "CHIP" R. ROSAN, JR. (302) 760-2185 AND/OR HIS DESIGNEE, AND THE CITY OF WILMINGTON'S URBAN FOREST ADMINISTRATOR, AMANDA TOLINO, (302) 576-2582 OR ATOLINO@WILMINGTONDE.GOV AT LEAST TWO (2) DAYS PRIOR TO EXCAVATION IN THE VICINITY OF THE TREE IN ORDER THAT THE CONDITION OF ANY OF THE ROOT SYSTEM EXPOSED DURING THE EXCAVATION MAY BE EXAMINED. IF THE TREE IS TO BE REMOVED, AS DETERMINED BY DELDOT'S ROADSIDE ENVIRONMENTAL SPECIALIST, REMOVAL SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING. ALL ROOTS IMPACTED BY CONSTRUCTION SHALL BE PRUNED TO CORRECT ANY SUBSTANTIAL DAMAGE IN ACCORDANCE WITH I.S.A. STANDARDS BY AN I.S.A. CERTIFIED ARBORIST. UPON COMPLETION OF ALL CONSTRUCTION IN THE IMMEDIATE AREA OF THE TREE, THE AREA SHALL BE RESTORED TO ITS ORIGINAL APPEARANCE. THIS WORK SHALL BE DIRECTED BY DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR AND SHALL BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.
- IN AREAS WHERE TREES OR SHRUBS WILL BE OVERHANGING THE PROPOSED SIDEWALK, PRUNING MAY BE NECESSARY TO ACHIEVE A VERTICAL CLEAR SPACE OF 10 FEET ABOVE THE PROPOSED SIDEWALK ELEVATION. THE CONTRACTOR SHALL PRUNE EXISTING TREE AND SHRUB BRANCHES THAT OVERHANG THE SIDEWALK IN ACCORDANCE WITH I.S.A. STANDARDS. THE CONTRACTOR SHALL NOTIFY DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE "CHIP" ROSAN, JR. (302) 760-2185 AND/OR HIS DESIGNEE, AND THE CITY OF WILMINGTON'S URBAN FOREST ADMINISTRATOR, AMANDA TOLINO, (302) 576-2582 OR ATOLINO@WILMINGTONDE.GOV, AT LEAST TWO (2) DAYS PRIOR TO THE PRUNING OPERATION. ANY TOOL USED FOR PRUNING TREES SHALL BE CLEANED AND DISINFECTED PRIOR TO ITS USE ON ANOTHER TREE TO PREVENT THE SPREAD OF DISEASE. ALL COSTS ASSOCIATED WITH THE ABOVE WORK SHALL BE INCIDENTAL TO ITEM 201000 - CLEARING AND GRUBBING.

- ITEMS TO BE REMOVED UNDER ITEM 211000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:
 - CONCRETE SUPPORT FOUNDATIONS FOR MISCELLANEOUS SMALL STRUCTURES NOT COVERED UNDER OTHER PAY ITEMS - EXISTING PEDESTRIAN SIGNAL POLE BASES AT THE SOUTHEAST CORNER OF 12TH ST AND JEFFERSON ST AND THE SOUTHEAST CORNER OF 2ND ST AND MARKET ST
 - TRAFFIC SIGNALIZATION JUNCTION WELLS
 - EXISTING TREE GRATE, TREE STUMP, AND ROOTS AT THE NORTHEAST CORNER OF 2ND ST AND MLK JR BLVD

SECTION 700

- IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSITIONED IN 10 LINEAR FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL VERIFY ALL EXISTING CURB TYPES. PAYMENT FOR THIS WORK, INCLUDING SAW CUTTING EXISTING CURB, SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.
 - ALL HOT-MIX SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.
 - ALL PAVED AREAS (INCLUDING DRIVEWAYS) TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE IN.
 - WHERE NEW CONCRETE SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, SAWCUT THE EXISTING SIDEWALK TO MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAWCUTTING SHALL BE FULL DEPTH UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER. SAWCUTTING OF CONCRETE OR BRICK SHALL BE INCIDENTAL TO THE RESPECTIVE CURB RAMP ITEM AT LOCATIONS WHERE THE CURB RAMP IS BEING RECONSTRUCTED. SAWCUTTING FOR THE INSTALLATION OF TRUNCATED DOMES SHALL BE PAID FOR UNDER ITEM 762002 - SAWCUTTING, CONCRETE, FULL DEPTH.
 - THE CONTRACTOR SHALL COORDINATE WITH DTC TWO WEEKS PRIOR TO ANY ANTICIPATED DISRUPTION IN DART BUS STOP LOCATIONS. DTC'S CONTACT PERSON IS MIKE ELLER, CHIEF OF SCHEDULE DEVELOPMENT, LOCATED AT 119 LOWER BEECH STREET, WILMINGTON, DE 19804. HE MAY BE REACHED BY TELEPHONE AT (302) 576-6061.
 - THE CONTRACTOR SHALL ONLY EXCAVATE AN AREA FOR SIDEWALK CONSTRUCTION THAT CAN BE BACKFILLED WITH GABC AT THE END OF EACH WORK DAY. NO BARE SOIL WITHIN THE PROPOSED SIDEWALK LIMITS SHALL BE EXPOSED AT THE END OF EACH WORK DAY. PAYMENT FOR PLACEMENT AND/OR REMOVAL OF GABC WILL BE INCIDENTAL TO ITEM 302007 - GABC, TYPE B.
 - WHEN STEEL PLATES ARE REQUIRED TO COVER EXCAVATED AREAS IN OR NEAR TRAVEL WAYS OPEN TO TRAFFIC, THE CONTRACTOR MUST SUBMIT A PLAN SHOWING METHOD TO SUPPORT THE EXCAVATION, METHOD OF FASTENING THE PLATES AND PLATE THICKNESS TO THE ENGINEER FOR APPROVAL PRIOR TO INSTALLATION. STEEL PLATES ARE TO BE PAID UNDER ITEM 763500 - MOT.
 - DURING CONCRETE PLACEMENT, ALL ADJOINING SURFACES SHALL BE PROTECTED WITH PLASTIC SHEETING OR APPROVED EQUAL. COST TO BE INCLUDED IN CONCRETE ITEM.
 - THE CONTRACTOR SHALL REMOVE AND INSTALL ALL REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS WITHIN CONTRACT LIMITS UNLESS OTHERWISE NOTED. DELDOT TO FURNISH ALL REGULATORY, WARNING, AND SPECIAL SIGNS AND POSTS TO CONTRACTOR FOR INSTALLATION. FOR COORDINATION AND PICK UP OF SIGNS AND POSTS THE CONTRACTOR SHALL CONTACT DELDOT'S TRANSPORTATION SYSTEM SPECIALIST AT 302-760-2581. THE CONTRACTOR SHALL ALLOW FOUR (4) WEEKS FOR THE DELIVERY OF SIGNS.
 - ALL JUNCTION WELLS WITHIN THE LIMITS OF CURB RAMP RECONSTRUCTION SHALL BE ADJUSTED SO THAT THE LIDS AND FRAMES ARE FLUSH WITH THE ADJOINING SURFACE.
 - PRIOR TO THE CONSTRUCTION OF THE PROJECT AND WITHIN 20 DAYS OF EXECUTION OF THE CONTRACT, A VIDEO SHALL BE TAKEN OF THE SITE BY THE CONTRACTOR AND COPY (DVD) GIVEN TO THE DELDOT PROJECT ENGINEER. THE VIDEO SHALL CLEARLY SHOW THE PRECONSTRUCTION CONDITIONS OF THE WORKSITE AND ADJOINING AREAS. THE VIDEO WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE COST OF THE VIDEO SHALL BE INCIDENTAL TO ITEM 763501 - CONSTRUCTION ENGINEERING..
 - REMOVAL OF BRICK SHALL BE INCIDENTAL TO ITEM 758000 - REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.
- MISCELLANEOUS
- THE EXISTING TOPOGRAPHY SHOWN ON THESE PLANS IS BASED ON TAPE AND WHEEL SURVEY AND SUPPLEMENTED BY AERIAL PHOTOS. A GPS BASED FIELD SURVEY WAS COMPLETED FOR RIGHT OF WAY VERIFICATION ONLY. THERE IS NO VERTICAL DATUM ESTABLISHED FOR THIS PROJECT.
 - THE CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY ALL EXISTING CONDITIONS, LAYOUT DIMENSIONS AND CLEARANCES THAT ARE SHOWN ON THE DRAWINGS PRIOR TO ANY CONSTRUCTION WORK. THE COST OF THIS ITEM SHALL BE INCIDENTAL TO ITEM 763501 - CONSTRUCTION ENGINEERING.
 - THE CONTRACTOR SHALL MARK THEIR NAME IN THE SIDEWALK. EITHER BY STAMPING OR BY IN LAYING AN APPROVED METAL PLATE. THE STAMPED LETTERS SHALL BE ONE INCH AND ONE QUARTER INCH DEPTH. IF A METAL PLATE IS USED, THE TOP PLATE SHALL BE FLUSH WITH THE TOP OF THE SIDEWALK. THE CONTRACTORS NAME SHALL BE PLACED AT EACH END OF THE FINISHED SIDEWALK UP TO 100 FEET IN LENGTH. BEYOND THE 100 FEET LENGTH, THE CONTRACTOR'S NAME SHALL BE PLACED AT THE ENDS OF THE FINISHED SIDEWALK AND MIDDISTANT BETWEEN THE ENDS IN ACCORDANCE WITH THE WILMINGTON CODE OF ORDINANCES.

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 DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS	NOT TO SCALE	WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS	CONTRACT	BRIDGE NO.	N/A	NOTES	SHEET NO.	
					T200920013	DESIGNED BY: G.R.P.		5	
					COUNTY	CHECKED BY: T.A.O.		TOTAL SHTS.	
					NEW CASTLE			78	

MISCELLANEOUS

25. THERE ARE NO ENVIRONMENTAL PERMITS ASSOCIATED WITH THIS PROJECT. AS SUCH, AN ENVIRONMENTAL COMPLIANCE SHEET WAS NOT PREPARED.

26. ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON AT THE FOLLOWING LOCATIONS:

- * 12TH STREET AT JEFFERSON STREET
- * 11TH STREET/DELAWARE AVENUE AT JEFFERSON STREET
- * 12TH STREET AT WASHINGTON STREET
- * 11TH STREET AND DELAWARE AVENUE AT WASHINGTON STREET
- * 4TH STREET AT ORANGE STREET
- * 4TH STREET AT SHIPLEY STREET
- * 4TH STREET AT KING STREET
- * 4TH STREET AT WALNUT STREET
- * 2ND STREET AT KING STREET
- * MLK JR BLVD AT ORANGE STREET
- * MLK JR BLVD AT MARKET STREET
- * MLK JR BLVD AT KING STREET
- * FRONT STREET AT FRENCH STREET
- * 4TH STREET AT MARKET STREET
- * 2ND STREET AT MARKET STREET
- * 10TH STREET AT ORANGE STREET
- * 2ND STREET AT WALNUT STREET
- * FRONT STREET AT WALNUT STREET

27. ALL EXISTING GRANITE CURB REMOVED UNDER THIS CONTRACT SHALL BE SALVAGED AND DELIVERED TO THE CITY OF WILMINGTON'S DEPARTMENT OF PUBLIC WORKS MAINTENANCE YARD (WITHIN FIVE MILES OF PROJECT SITE). CONTACT DAVE BLANKENSHIP, P.E., DIRECTOR OF TRANSPORTATION (302) 576-3084 TO COORDINATE DELIVERIES. ALL COSTS SHALL BE INCIDENTAL TO ITEM 202000 EXCAVATION AND EMBANKMENT.

28. THE CONTRACTOR SHALL CONTACT THE WILMINGTON PARKING AUTHORITY (302) 655-4442 A MINIMUM OF TWO WEEKS PRIOR TO ANY CONSTRUCTION ACTIVITIES THAT IMPACT EXISTING ON-STREET PARKING.

29. WHEN INSTALLING THE PROPOSED CONDUIT AT THE SOUTHEAST CORNER OF 2ND ST AND MARKET ST, THE CONTRACTOR SHALL REMOVE THE EXISTING BRICK AND REPLACE IN KIND, AS DIRECTED BY THE ENGINEER. THE COST FOR THIS WORK SHALL BE INCIDENTAL TO ITEM 745543 - INSTALLATION OF CONDUIT UNDER EXISTING PAVEMENT - OPEN CUT.

NO OFFICIAL

WEBSITE

COPY

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ADDENDUMS / REVISIONS

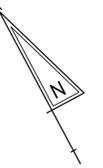
NOT TO SCALE

WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

NOTES

PN-02
SHEET NO.
6
TOTAL SHTS.
78



UNOFFICIAL

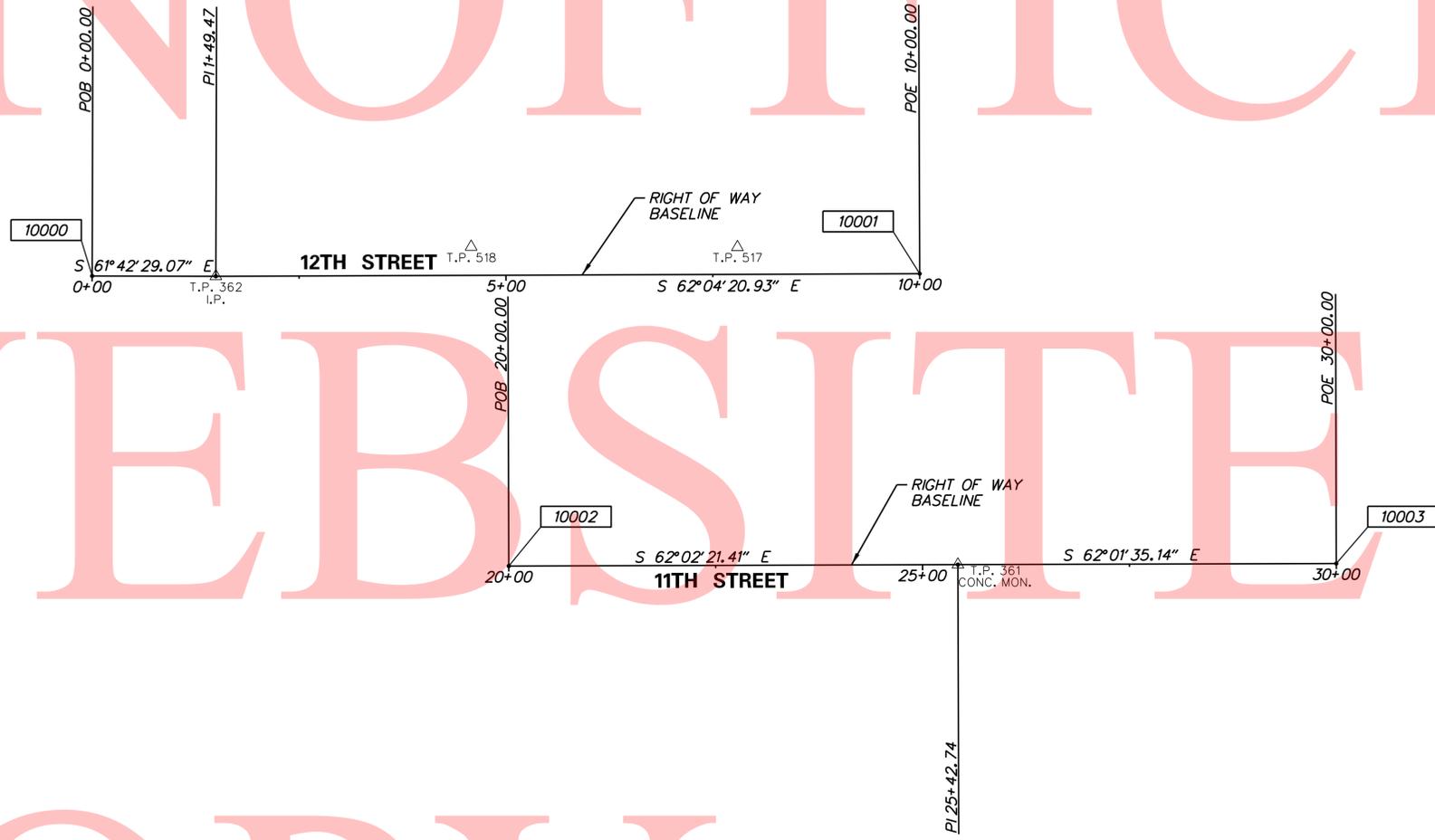
WEBSITE

COPY

HORIZONTAL CONTROL DATA				
POINT	STATION	OFFSET	NORTHING	EASTING
361	25+42.74	0.00	636450.08	618830.60
362	1+49.47	0.00	637179.53	618203.86
517	7+79.85	-32.70	636913.19	618776.14
518	4+58.08	-34.41	637065.40	618492.64

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
10000	0+00.00	0.00	637250.38	618072.24
10001	10+00.00	0.00	636781.19	618955.33
10002	20+00.00	0.00	636704.55	618351.22
10003	30+00.00	0.00	636235.59	619234.44

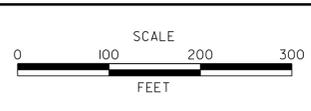
DATUM REFERENCE:
 HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83).
 VERTICAL - A VERTICAL DATUM IS NOT PROVIDED FOR THIS CONTRACT



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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**HORIZONTAL AND
 VERTICAL CONTROL**

HV-01
SHEET NO.
7
TOTAL SHTS.
78

UNOFFICIAL

WEBSITE

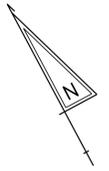
COPY



HORIZONTAL CONTROL DATA				
POINT	STATION	OFFSET	NORTHING	EASTING
351	68+48.46	0.00	633782.27	618549.54
352	65+95.95	0.00	633559.01	618431.57
508	40+28.18	-138.44	633349.39	617528.64
509	44+12.68	-144.97	633177.32	617872.56
511	61+40.80	253.12	633042.02	618443.53

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
10004	40+00.00	0.00	633239.68	617439.62
10005	51+00.00	0.00	632730.87	618414.87
10006	60+00.00	0.00	633031.63	618154.08
10007	61+71.31	0.00	633184.23	618231.93
10008	70+00.00	0.00	633916.38	618620.10

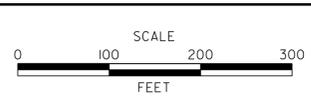
DATUM REFERENCE:
 HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83).
 VERTICAL - A VERTICAL DATUM IS NOT PROVIDED FOR THIS CONTRACT



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ADDENDUMS / REVISIONS	



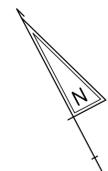
**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

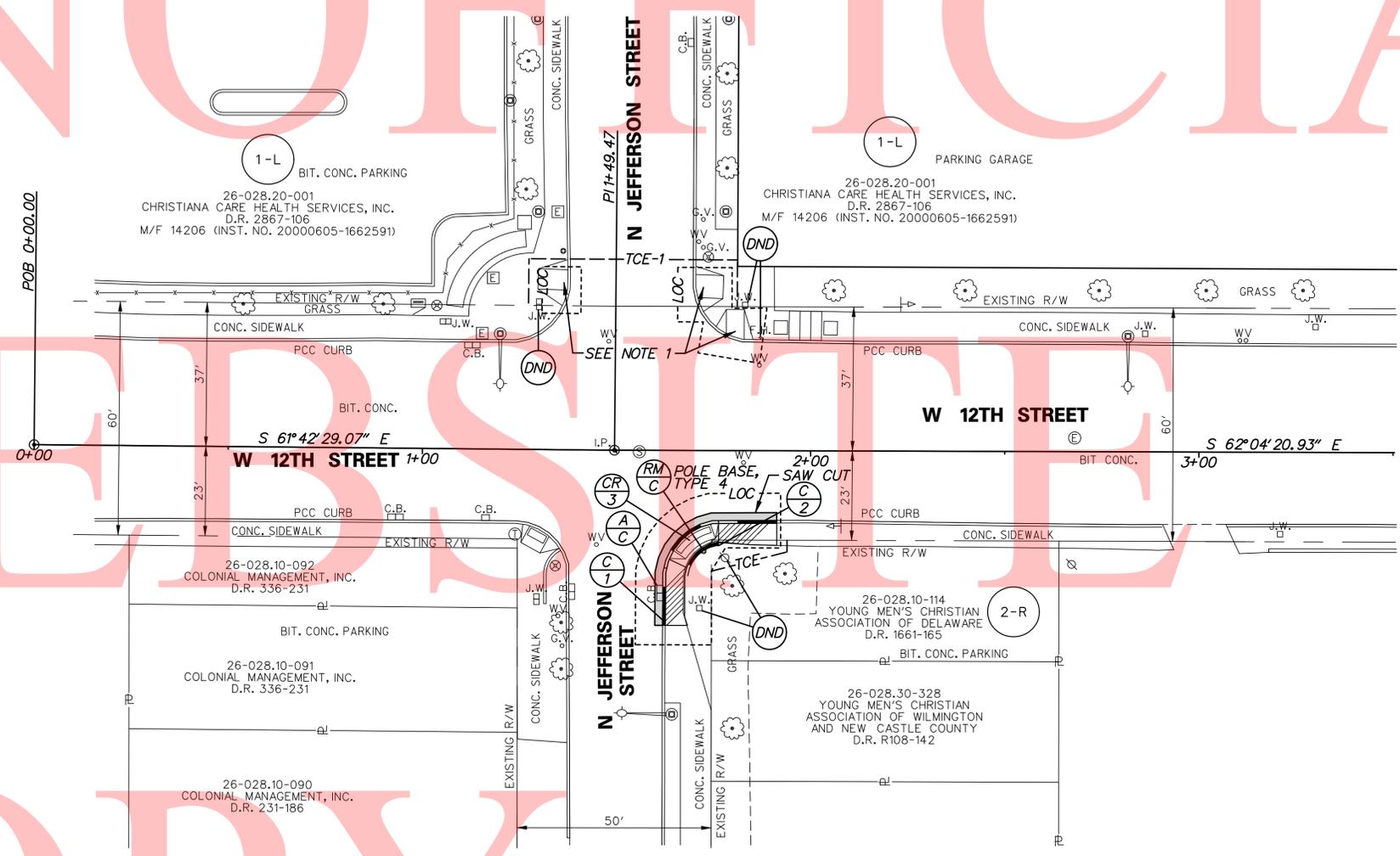
**HORIZONTAL AND
 VERTICAL CONTROL**

HV-02
SHEET NO. 8
TOTAL SHTS. 78

NOTES:
 1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CURB RAMP IN COMPLIANCE WITH ADA STANDARDS.



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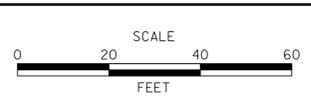
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	PCC CURB, TYPE 1*	50'
2	PCC CURB, TYPE 1	14'

* COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

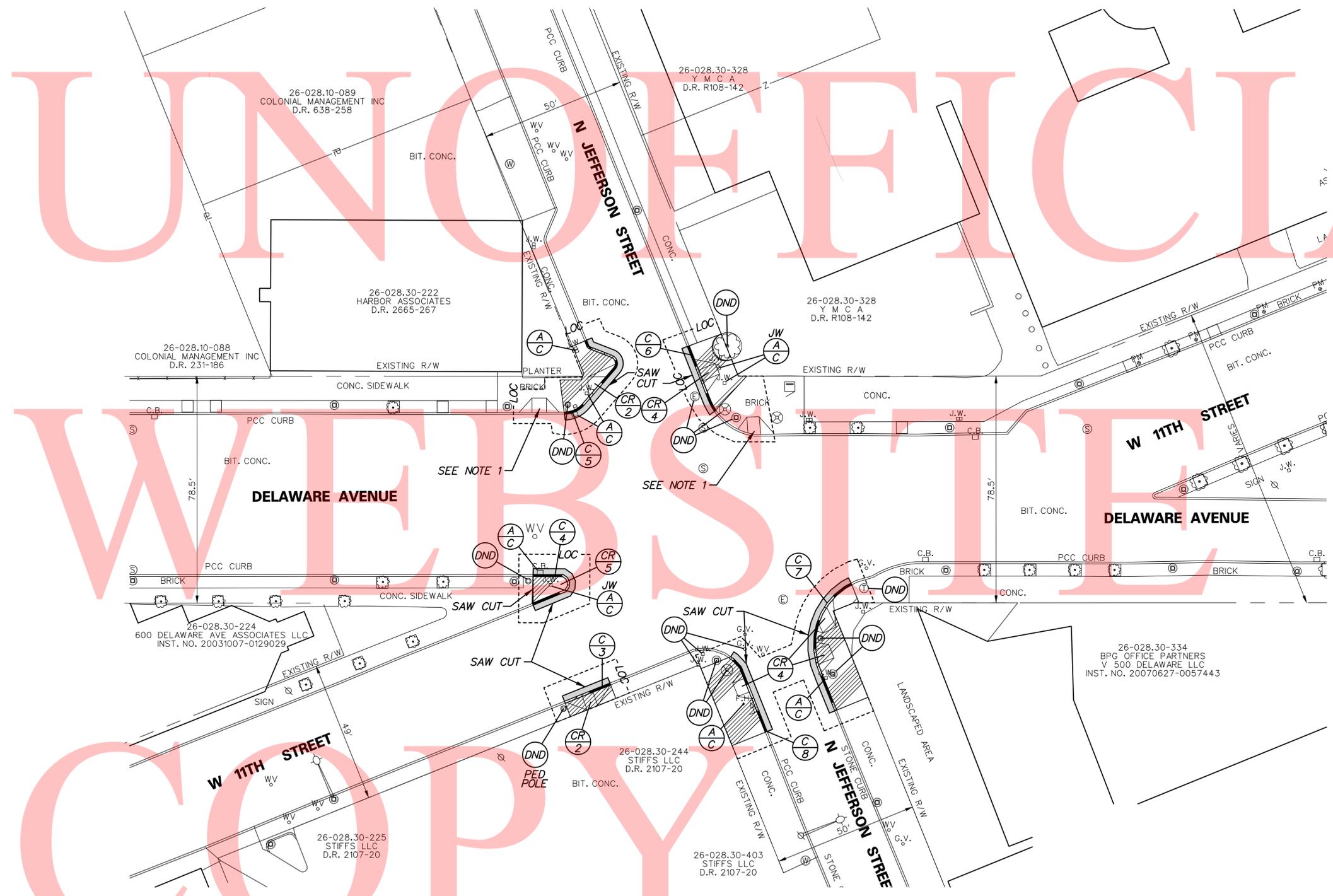
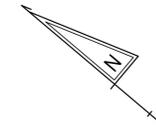
CONSTRUCTION PLAN

CP-01
SHEET NO. 9
TOTAL SHTS. 78

NOTES:

1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CURB RAMP IN COMPLIANCE WITH ADA STANDARDS.

2. SEE CONSTRUCTION DETAILS, SHEET DT-01, FOR ADDITIONAL CURB RAMP DETAILS.



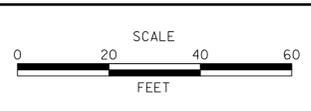
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
3	PCC CURB, TYPE 1*	17'
4	PCC CURB, TYPE 1*	30'
5	PCC CURB, TYPE 1*	41'
6	PCC CURB, TYPE 1*	25'
7	PCC CURB, TYPE 1*	51'
8	PCC CURB, TYPE 1*	30'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION PLAN

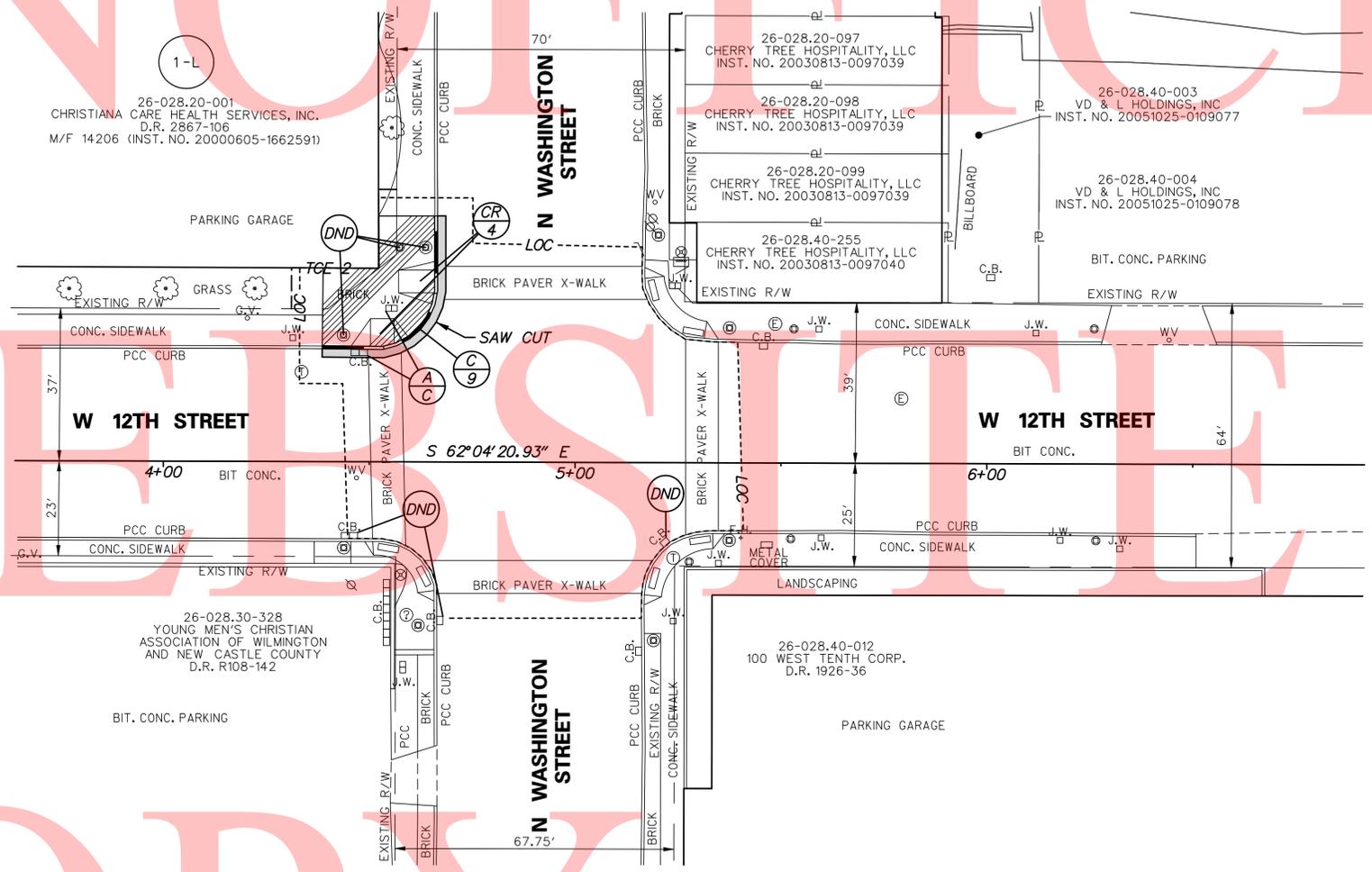
CP-02
SHEET NO.
10
TOTAL SHTS.
78



UNOFFICIAL

WILMINGTON

COPY



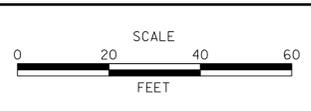
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
9	PCC CURB, TYPE 1*	54'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION PLAN

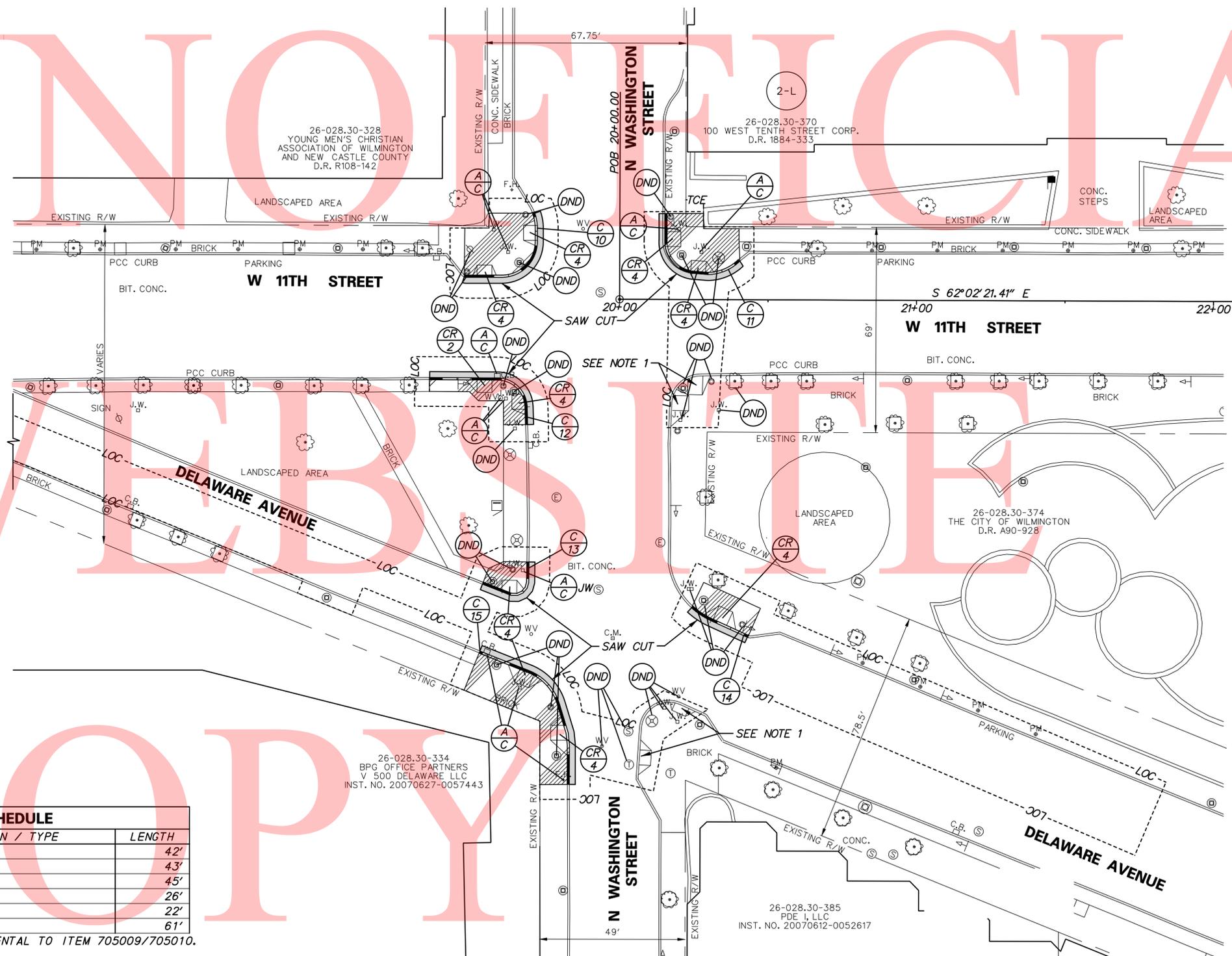
CP-03
SHEET NO.
11
TOTAL SHTS.
78

NOTES:

1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CURB RAMP IN COMPLIANCE WITH ADA STANDARDS.



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SEE SHEET SS-02 FOR FULL EXTENT OF LOC

SEE NOTE 1

SEE NOTE 1

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
10	PCC CURB, TYPE 1*	42'
11	PCC CURB, TYPE 1*	43'
12	PCC CURB, TYPE 1*	45'
13	PCC CURB, TYPE 1*	26'
14	PCC CURB, TYPE 1*	22'
15	PCC CURB, TYPE 1*	61'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

26-028.30-334
BPG OFFICE PARTNERS
V 500 DELAWARE LLC
INST. NO. 20070627-0057443

26-028.30-385
PDF I, LLC
INST. NO. 20070612-0052617



ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

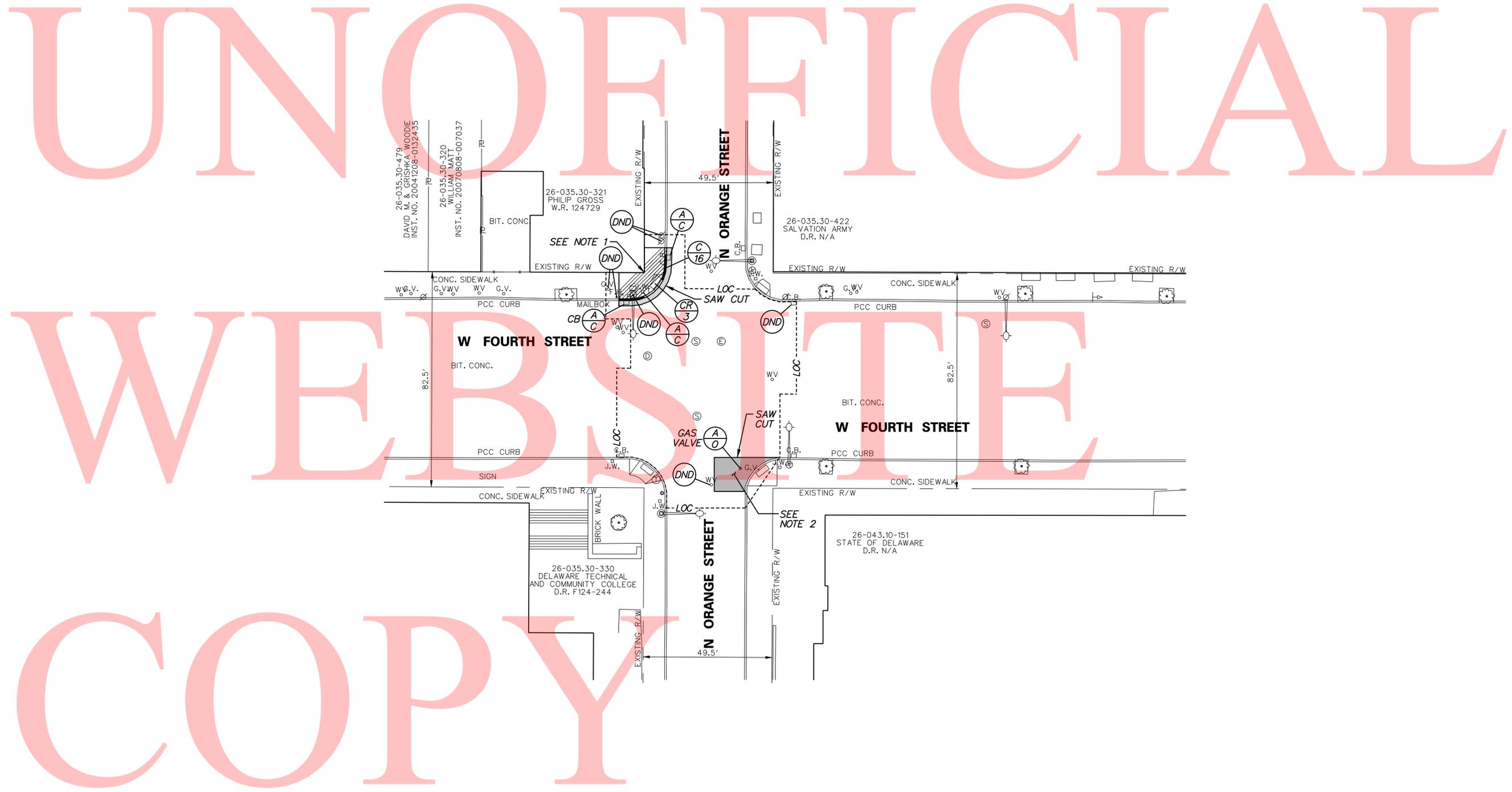
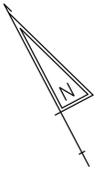
CONSTRUCTION PLAN

CP-04
SHEET NO.
12
TOTAL SHTS.
78

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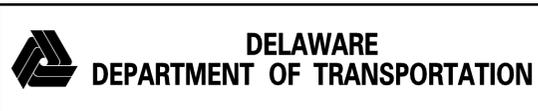
NOTES:

1. THE CONTRACTOR SHALL ADJUST THE CONCRETE STEPS AS NECESSARY TO COMPLY WITH ADA STANDARDS. PAYMENT FOR ADJUSTMENT TO CONCRETE STEPS SHALL BE INCIDENTAL TO ITEM 705009 - CURB RAMP, TYPE 2, 3 AND/OR 4.
2. SEE CONSTRUCTION DETAILS, SHEET DT-02, FOR ADDITIONAL CURB RAMP AND PAVEMENT PATCH DETAILS.

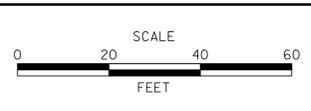


*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

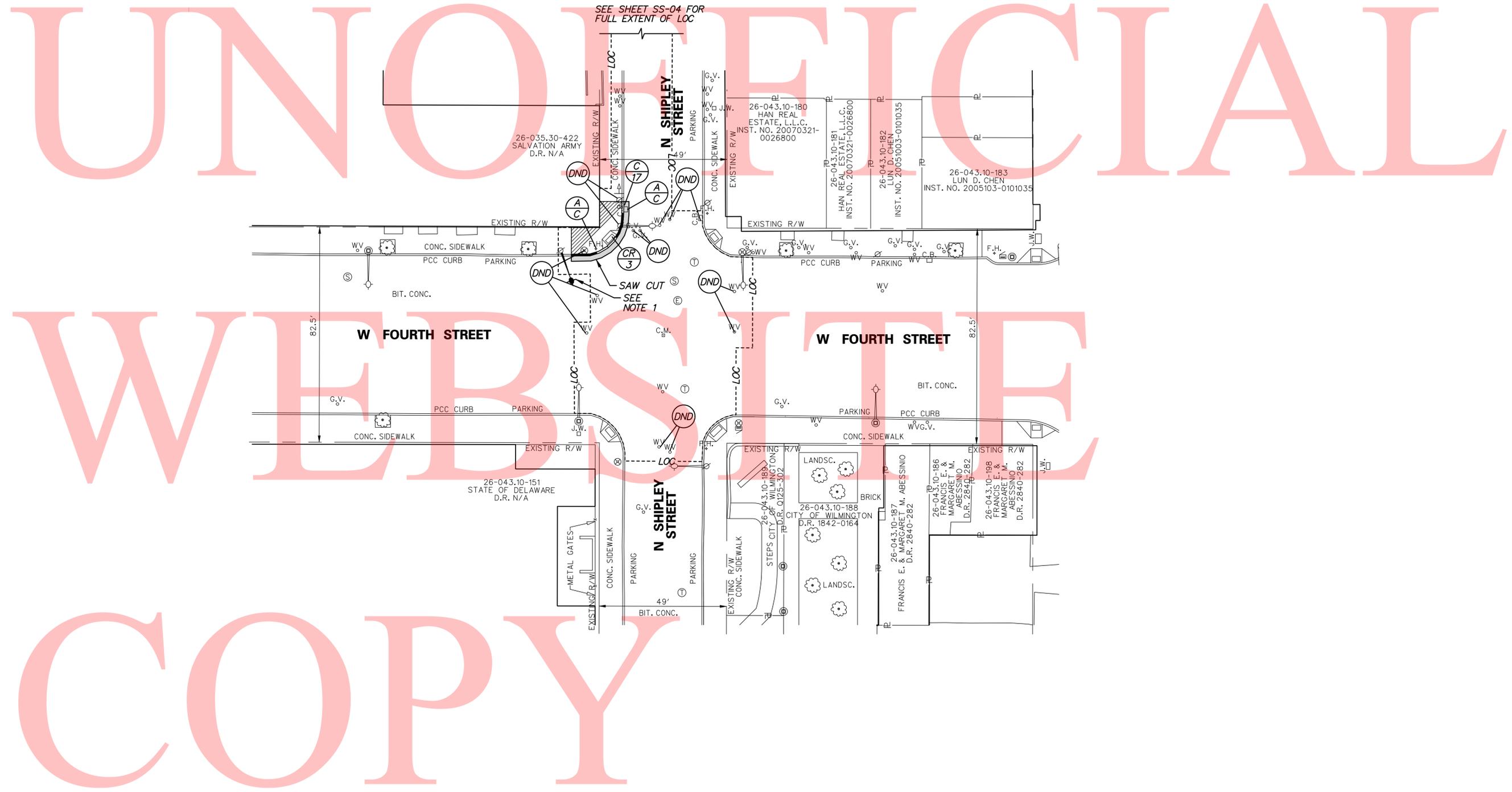
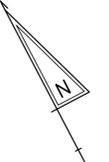
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

CONSTRUCTION PLAN

CP-05
SHEET NO. 13
TOTAL SHTS. 78

NOTES:

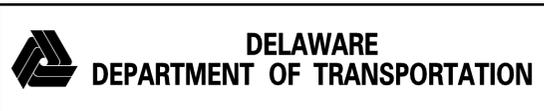
- 250 W HPS MONGOOSE STYLE LUMINAIRE WITH CUT-OFF TYPE III MOUNTED ON EXISTING WOOD UTILITY POLE AS SHOWN WITH 12' ARM LENGTH. ALL LUMINAIRES ATTACHED TO UTILITY POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.
- SEE CONSTRUCTION DETAILS, SHEET DT-02, FOR ADDITIONAL CURB RAMP DETAILS.



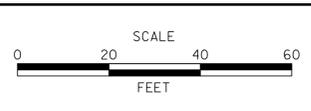
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
17	PCC CURB, TYPE I*	35'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



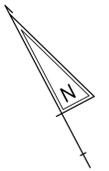
WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

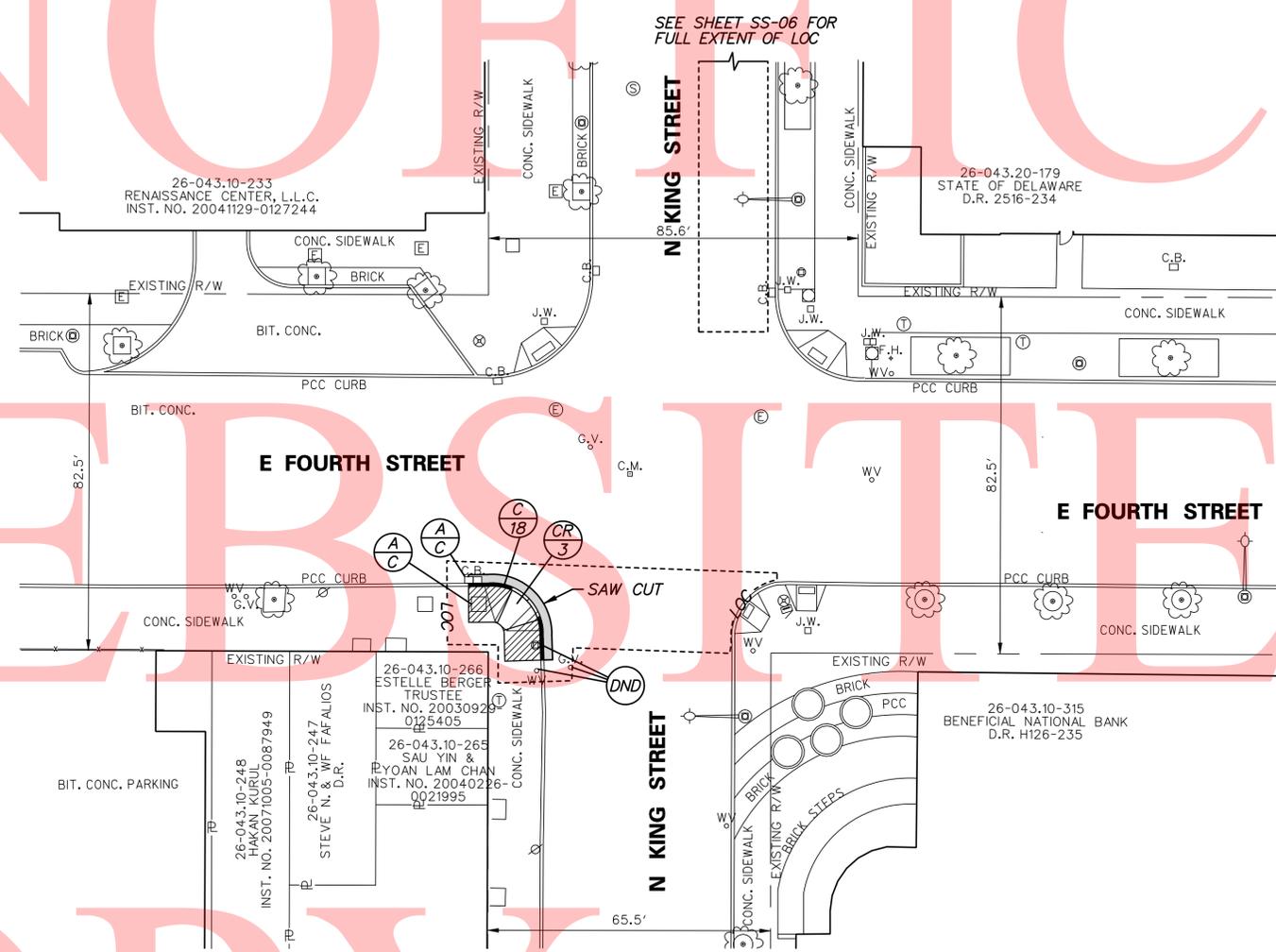
CONSTRUCTION PLAN

CP-06
SHEET NO.
14
TOTAL SHTS.
78

NOTES:
 1. SEE CONSTRUCTION DETAILS, SHEET DT-02, FOR ADDITIONAL CURB RAMP DETAILS.



UNOFFICIAL



WEBSITE

COPY

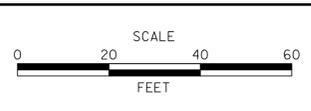
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
18	PCC CURB, TYPE I*	31'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS



WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

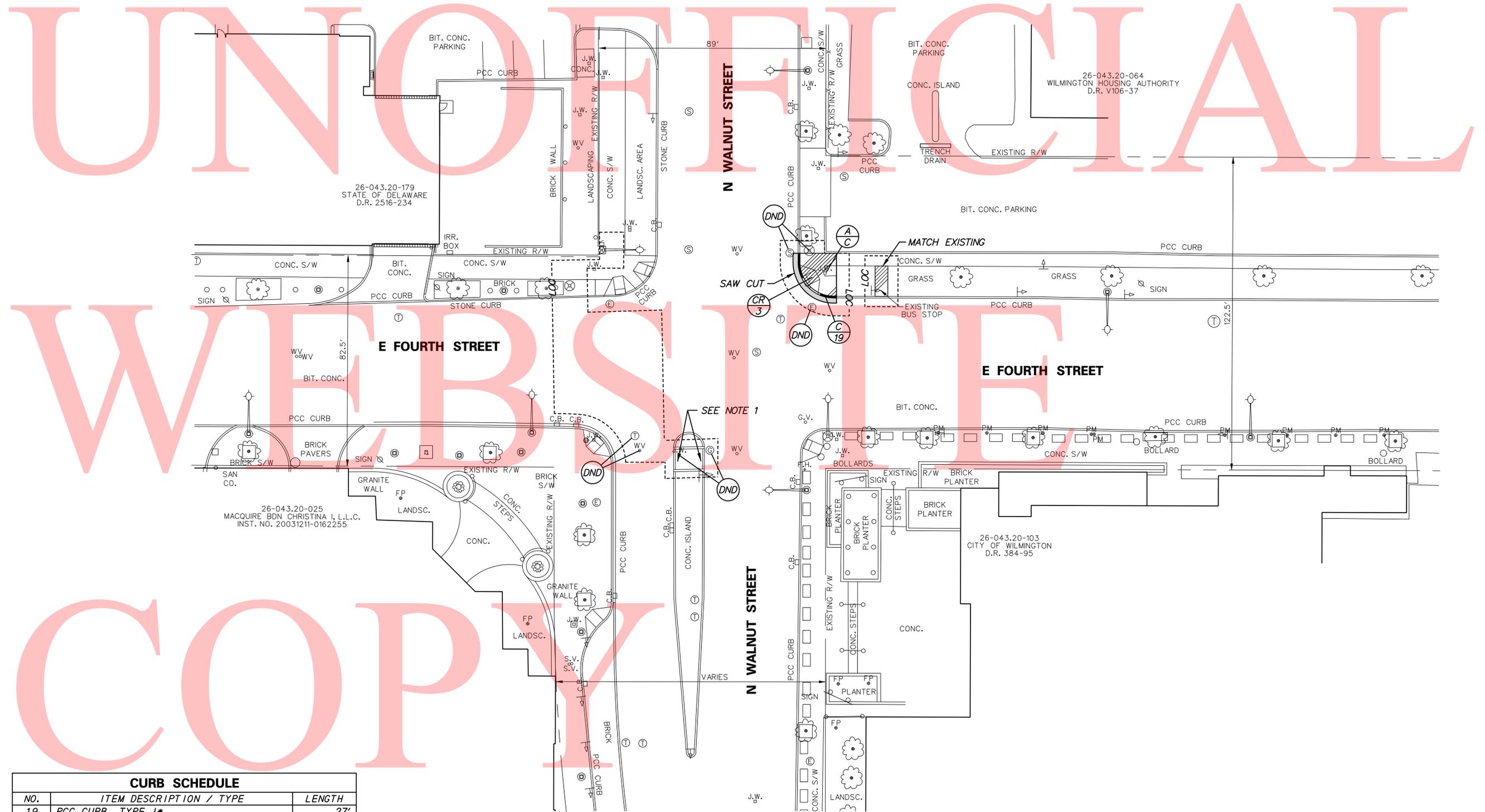
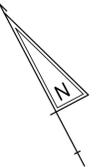
CONSTRUCTION PLAN

CP-07
SHEET NO.
15
TOTAL SHTS.
78

NOTES:

1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CUT-THROUGH IN THE MEDIAN IN COMPLIANCE WITH ADA STANDARDS.

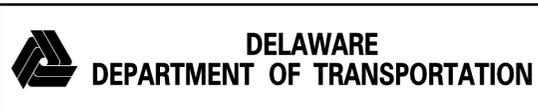
2. SEE CONSTRUCTION DETAILS, SHEET DT-02, FOR ADDITIONAL CURB RAMP DETAILS.



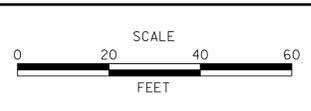
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
19	PCC CURB, TYPE 1*	27'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

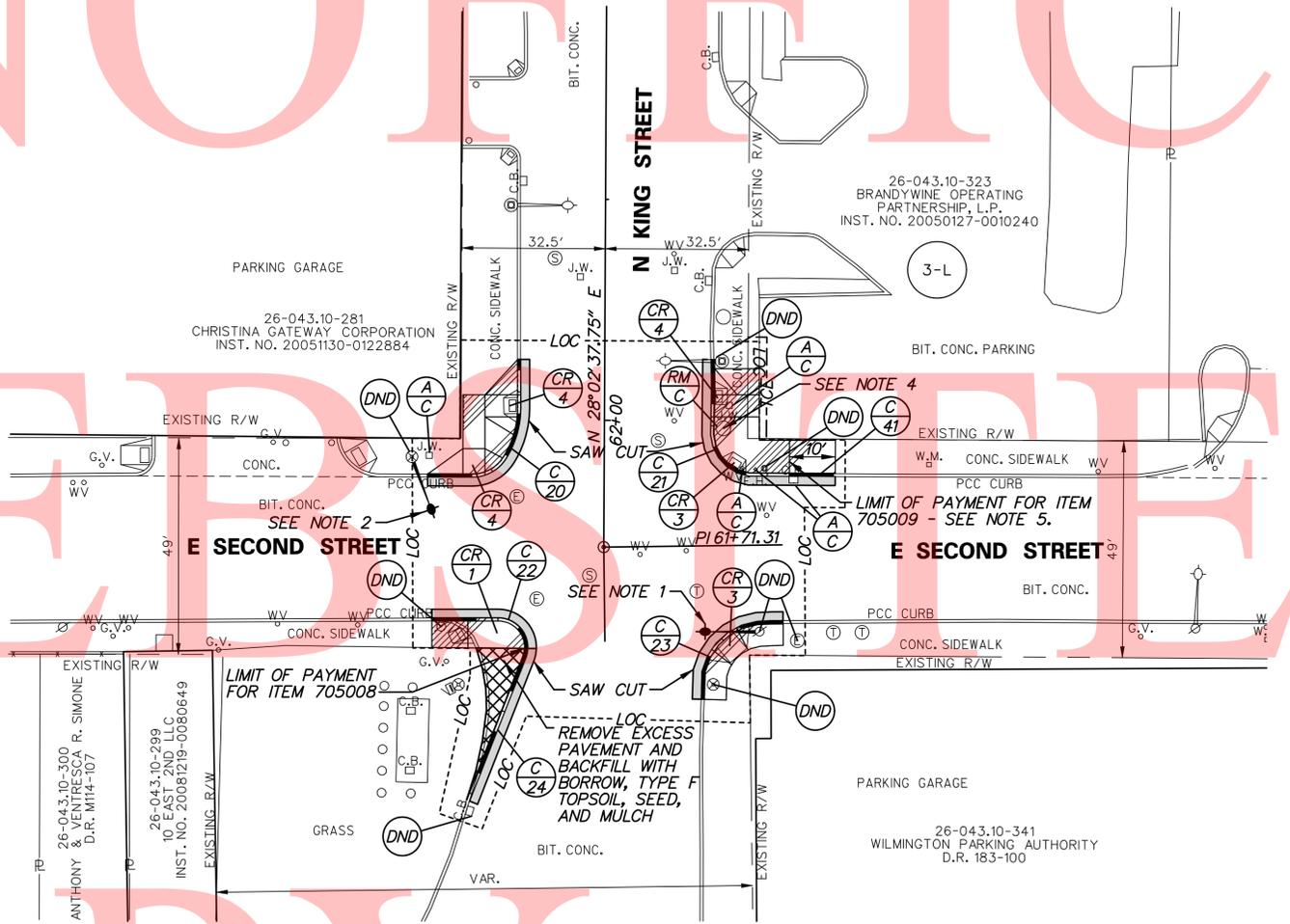
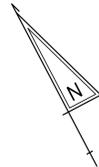
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

CONSTRUCTION PLAN

CP-08
SHEET NO. 16
TOTAL SHTS. 78

NOTES:

- 250 W HPS MONGOOSE STYLE LUMINAIRE WITH CUT-OFF TYPE III MOUNTED ON EXISTING WOOD UTILITY POLE WITH 8' ARM LENGTH. ALL LUMINAIRES ATTACHED TO UTILITY POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.
- 250 W HPS MONGOOSE STYLE LUMINAIRE WITH CUT-OFF TYPE III INSTALLED WITH SHORT BRACKET BANDED TO TRAFFIC SIGNAL POLE. ALL LUMINAIRES ATTACHED TO TRAFFIC SIGNAL POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. CONTRACTOR SHALL COORDINATE INSTALLATION OF LUMINAIRES WITH DELMARVA POWER ELECTRIC.
- SEE CONSTRUCTION DETAILS, SHEET DT-03, FOR ADDITIONAL CURB RAMP DETAILS.
- COST FOR REMOVAL OF EXISTING TREE GRATE, STUMP, AND ROOTS SHALL BE INCIDENTAL TO ITEM 211000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS.
- ADDITIONAL PCC SIDEWALK AND CURB SHALL BE INSTALLED BEYOND THE CURB RAMP LIMITS, AS SHOWN. PAYMENT FOR THIS WORK AND ASSOCIATED PAVEMENT PATCHING SHALL BE MADE IN ACCORDANCE WITH THE INDIVIDUAL PAY ITEMS.



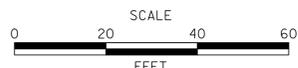
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
20	PCC CURB, TYPE 1*	43'
21	PCC CURB, TYPE 1*	41'
22	PCC CURB, TYPE 1*	25'
23	PCC CURB, TYPE 1*	31'
24	PCC CURB, TYPE 1	37'
41	PCC CURB, TYPE 1	10'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705008/705009/705010.

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ADDENDUMS / REVISIONS	



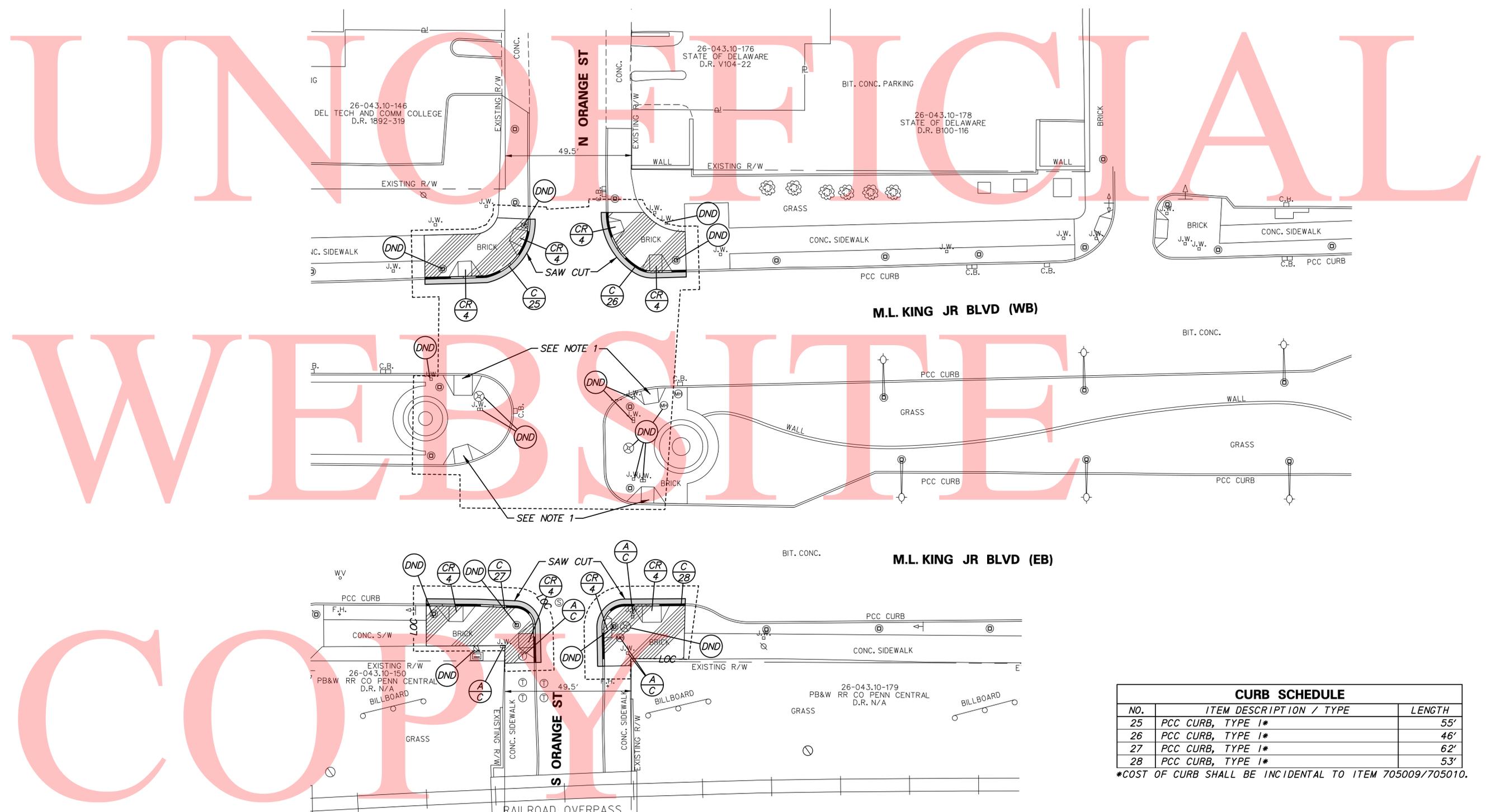
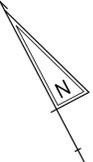
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION PLAN

CP-09
SHEET NO.
17
TOTAL SHTS.
78

- NOTES:**
1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CURB RAMPS IN COMPLIANCE WITH ADA STANDARDS.
 2. SEE CONSTRUCTION DETAILS, SHEET DT-04, FOR ADDITIONAL CURB RAMP DETAILS.



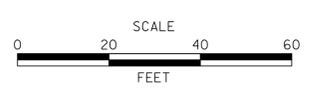
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
25	PCC CURB, TYPE 1*	55'
26	PCC CURB, TYPE 1*	46'
27	PCC CURB, TYPE 1*	62'
28	PCC CURB, TYPE 1*	53'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

No. 3/16/16-006.SCAD.DWG CP10 - WILM.dgn
 3/17/2011 8:59:52 AM



ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

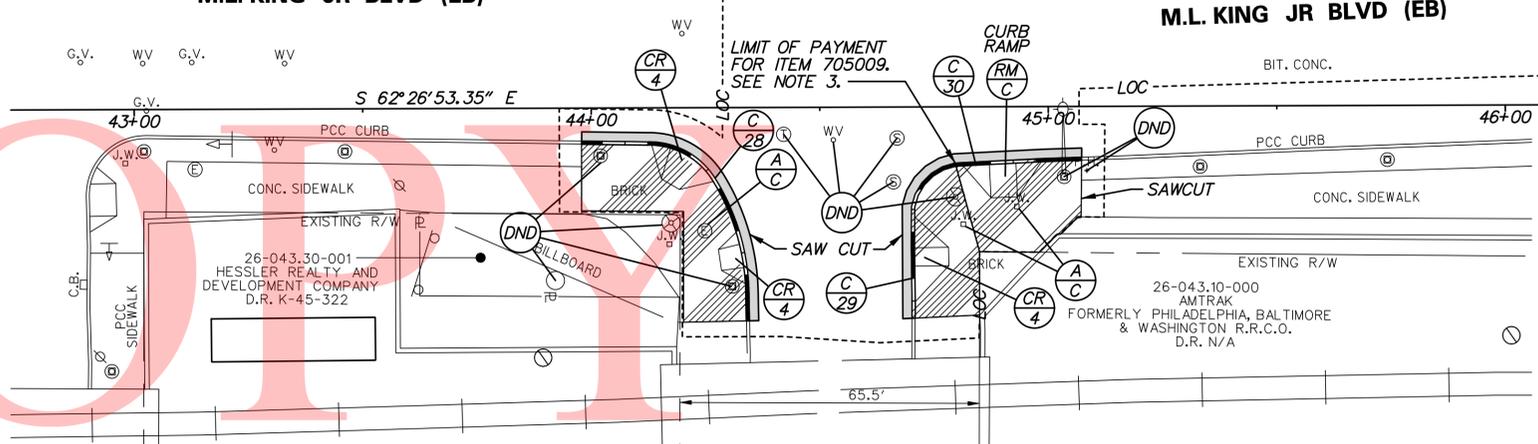
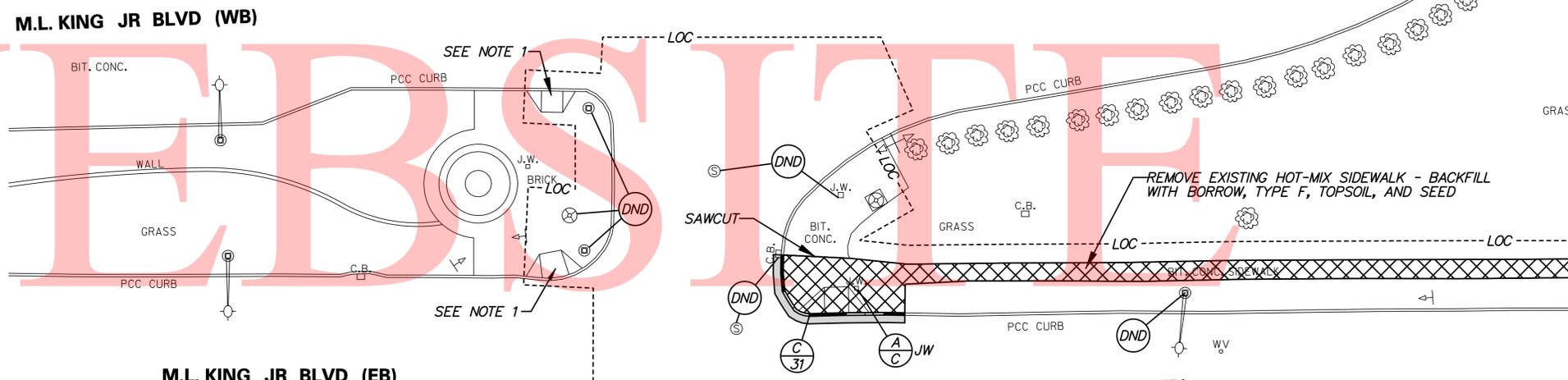
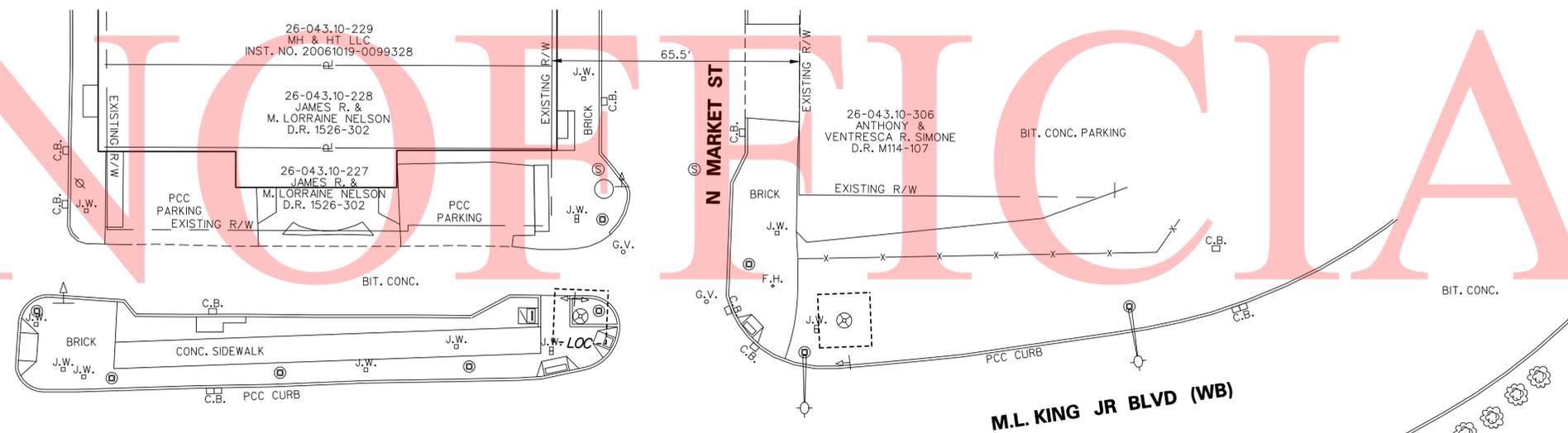
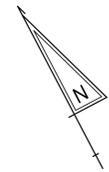
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

CONSTRUCTION PLAN

CP-10
SHEET NO. 18
TOTAL SHTS. 78

NOTES:

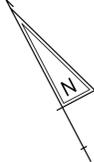
1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CURB RAMP IN COMPLIANCE WITH ADA STANDARDS.
2. SEE CONSTRUCTION DETAILS, SHEET DT-05, FOR ADDITIONAL CURB RAMP DETAILS.
3. THE CURB RAMP ON THE SOUTH EAST CORNER FOR THE EAST LEG CROSSING OF MLK JR. BLVD SHALL BE REMOVED AS SHOWN. REMOVAL OF THE EXISTING PCC SIDEWALK, BRICK, AND CURB ASSOCIATED WITH THE CURB RAMP SHALL BE PAID FOR UNDER ITEM 758000. RECONSTRUCTION OF THE PROPOSED PCC SIDEWALK AND CURB AND HOT-MIX PAVEMENT PATCHING, SHALL BE PAID FOR IN ACCORDANCE WITH THE INDIVIDUAL PAY ITEMS, NOT 705009.



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
28	PCC CURB, TYPE 1*	65'
29	PCC CURB, TYPE 1*	39'
30	PCC CURB, TYPE 1	28'
31	PCC CURB, TYPE 1	46'

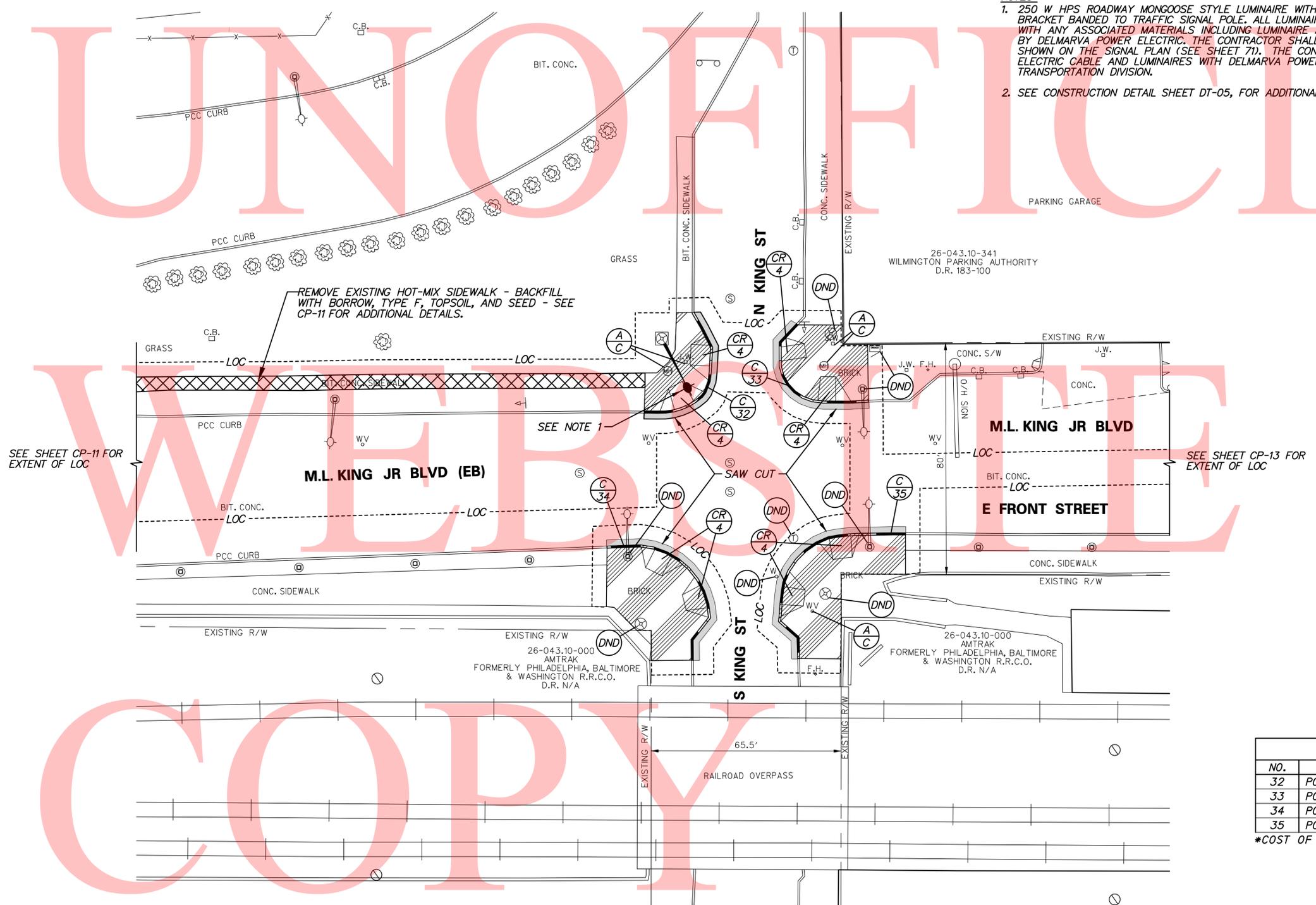
*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

No. 37678-006, SADD, CP11 - WILM.dgn
 3/17/2011 8:59:29 AM



NOTES:

- 250 W HPS ROADWAY MONGOOSE STYLE LUMINAIRE WITH CUT-OFF TYPE III INSTALLED WITH A SHORT BRACKET BANDED TO TRAFFIC SIGNAL POLE. ALL LUMINAIRES ATTACHED TO TRAFFIC SIGNAL POLES ALONG WITH ANY ASSOCIATED MATERIALS INCLUDING LUMINAIRE ARMS WILL BE SUPPLIED, MOUNTED AND INSTALLED BY DELMARVA POWER ELECTRIC. THE CONTRACTOR SHALL INSTALL THE PROPOSED ELECTRIC CABLE AS SHOWN ON THE SIGNAL PLAN (SEE SHEET 71). THE CONTRACTOR SHALL COORDINATE INSTALLATION OF ELECTRIC CABLE AND LUMINAIRES WITH DELMARVA POWER ELECTRIC AND THE CITY OF WILMINGTON TRANSPORTATION DIVISION.
- SEE CONSTRUCTION DETAIL SHEET DT-05, FOR ADDITIONAL CURB RAMP DETAILS.



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
32	PCC CURB, TYPE 1*	54'
33	PCC CURB, TYPE 1*	55'
34	PCC CURB, TYPE 1*	67'
35	PCC CURB, TYPE 1*	80'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

No. 331678-006, CAD, CP12 - WILM.dgn
 3/3/2011 8:50:18 AM



ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

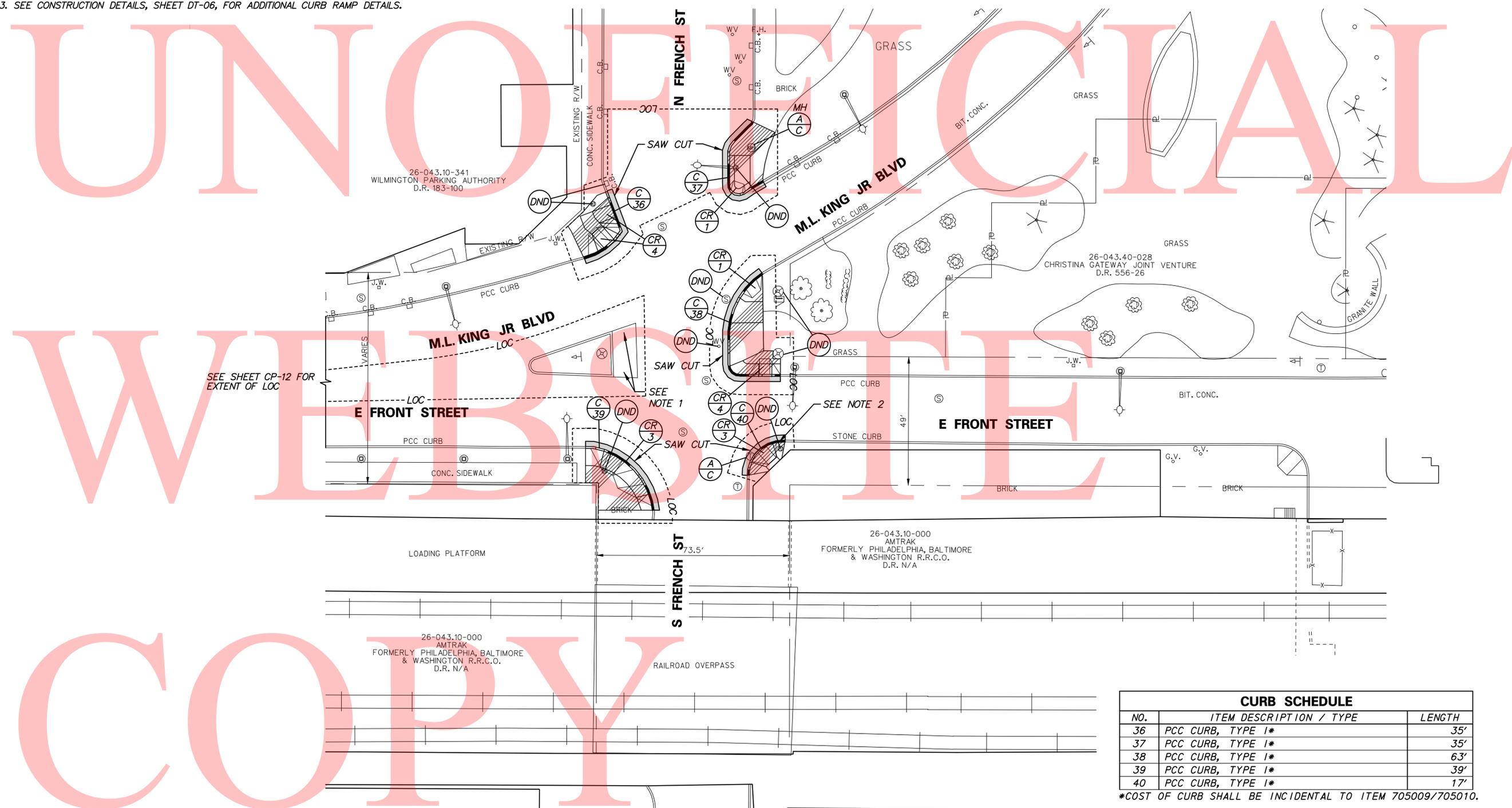
CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION PLAN

CP-12
SHEET NO.
20
TOTAL SHTS.
78

NOTES:

1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON THE EXISTING CUT-THROUGH IN THE MEDIAN. THE DETECTABLE WARNING TRUNCATED DOMES SHALL EXTEND THE FULL WIDTH OF THE CUT-THROUGH, AND THEY SHALL BE INSTALLED ADJACENT TO THE BACK OF CURB AND EXTEND BACK A MINIMUM OF 24 INCHES. WHERE THERE IS NO DEPRESSED CURB, THE DETECTABLE WARNING SHALL BE INSTALLED 3" FROM THE ROADWAY PAVEMENT.
2. THE CONTRACTOR SHALL SAWCUT AND REMOVE THE EXISTING GRANITE CURB TO THE EXTENT NECESSARY TO RECONSTRUCT THE CURB RAMP AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL INSTALL PCC CURB, TYPE 1 WITHIN THE LIMITS OF THE PROPOSED CURB RAMP.
3. SEE CONSTRUCTION DETAILS, SHEET DT-06, FOR ADDITIONAL CURB RAMP DETAILS.



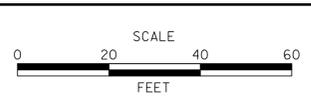
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
36	PCC CURB, TYPE 1*	35'
37	PCC CURB, TYPE 1*	35'
38	PCC CURB, TYPE 1*	63'
39	PCC CURB, TYPE 1*	39'
40	PCC CURB, TYPE 1*	17'

*COST OF CURB SHALL BE INCIDENTAL TO ITEM 705009/705010.

No. 31676-006, CAD, CP13 - WILM.dgn
 3/17/2011 8:50:38 AM



ADDENDUMS / REVISIONS



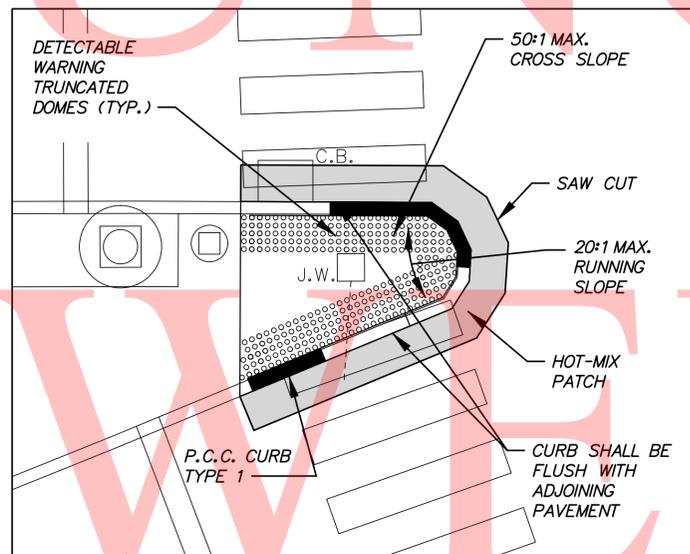
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

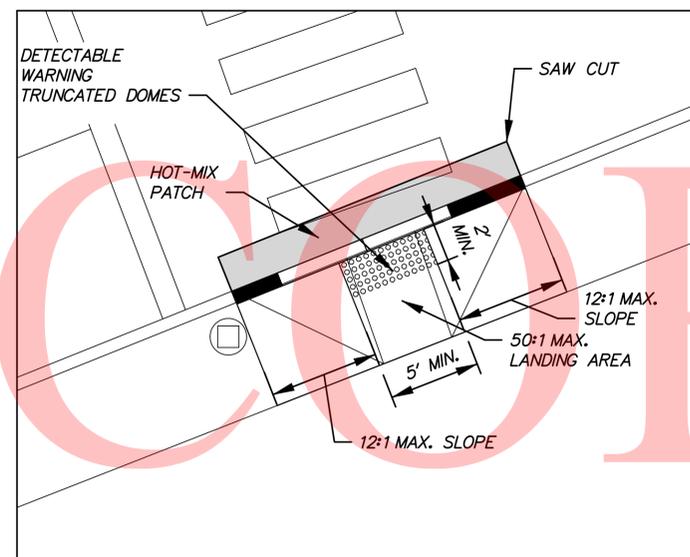
CONSTRUCTION PLAN

CP-13
SHEET NO. 21
TOTAL SHTS. 78

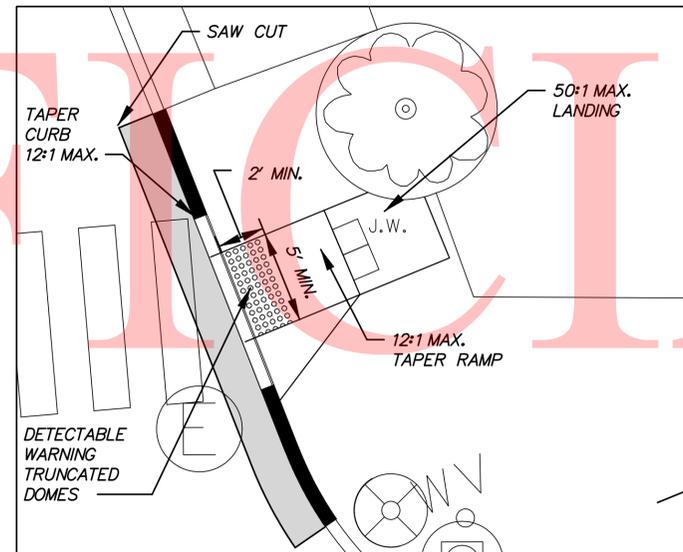
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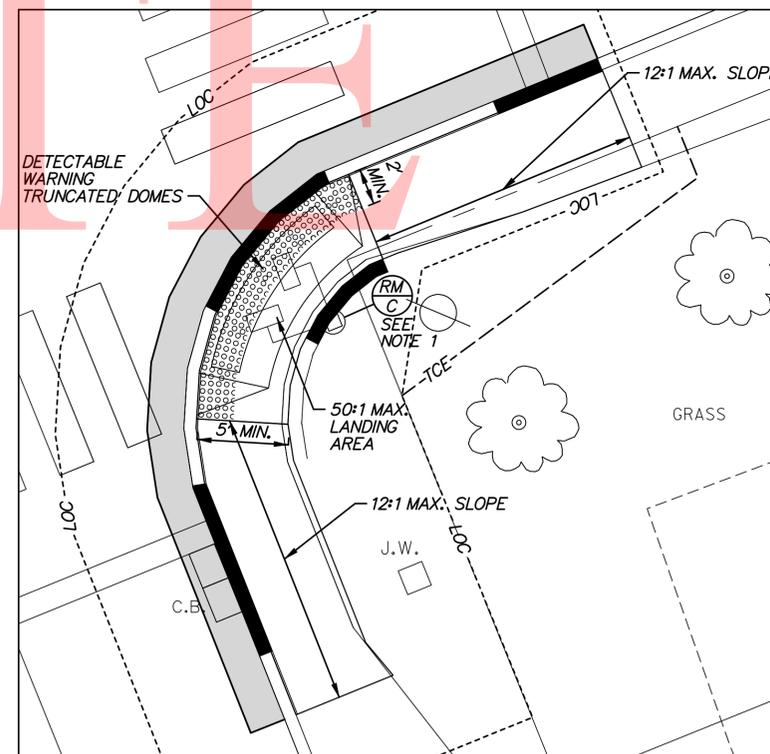
DELAWARE AVE, 11TH ST. AND JEFFERSON ST.
CURB RAMP, TYPE 5 - MEDIAN



DELAWARE AVE, 11TH ST. AND JEFFERSON ST.
CURB RAMP, TYPE 2 - SW CORNER



DELAWARE AVE, 11TH ST. AND JEFFERSON ST.
CURB RAMP, TYPE 4 - NE CORNER

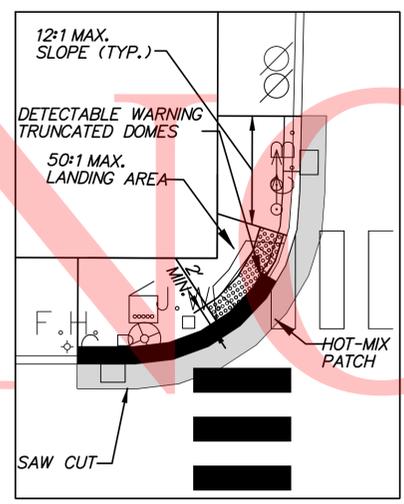


12TH ST. AND JEFFERSON ST.
CURB RAMP, TYPE 3 - SE CORNER

NOTES:
1. THE PROJECT CONTRACTOR SHALL REMOVE THE EXISTING POLE BASE, TYPE 4. COST OF THE REMOVAL SHALL BE INCIDENTAL TO ITEM 211000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS. THE PROJECT CONTRACTOR SHALL POSITION AND CONSTRUCT THE NEW POLE BASE, TYPE 4. THE PROPOSED PEDESTRIAN PEDESTAL AND THE PROPOSED PEDESTRIAN COUNTDOWN SIGNAL HEADS SHALL BE INSTALLED BY DELDOT TRAFFIC. SEE SHEET 64 FOR DETAILS.

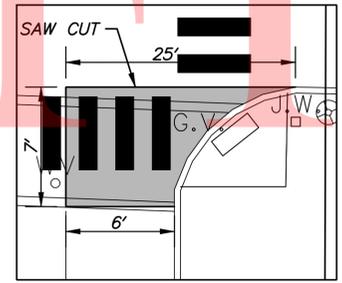
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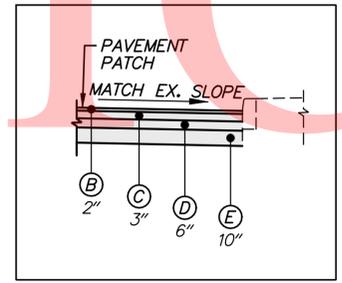


**FOURTH ST. AND ORANGE ST.
CURB RAMP, TYPE 3 - NW CORNER**

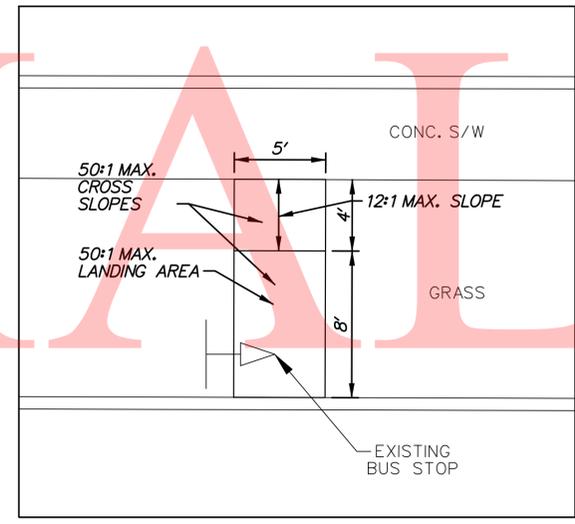
LEGEND	
(B)	ITEM 401665 - SUPERPAVE, TYPE C HOT-MIX, 160 GYRATIONS, PG 64-22, PATCHING (CARBONATE STONE)
(C)	ITEM 401666 - SUPERPAVE, TYPE B HOT-MIX, 160 GYRATIONS, PG 64-22, PATCHING
(D)	ITEM 401667 - SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING
(E)	ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING



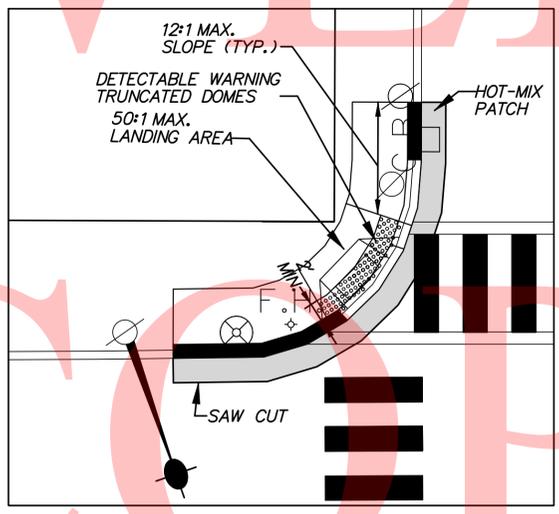
**FOURTH ST. AND ORANGE ST.
PAVEMENT PATCH - SE CORNER**



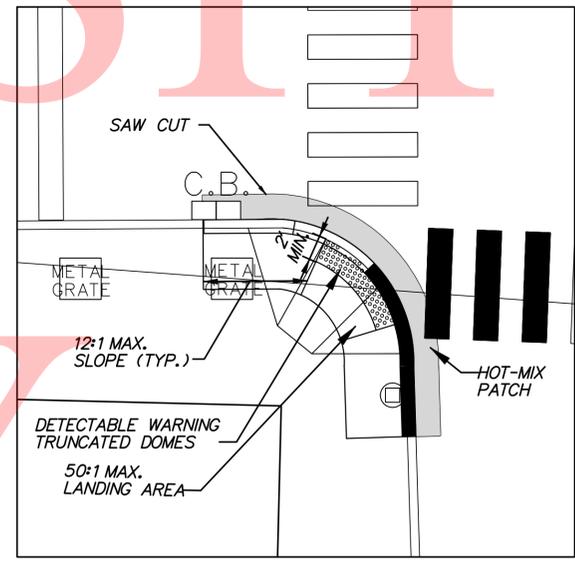
**FOURTH ST. AND ORANGE ST.
PAVEMENT PATCH, TYPICAL SECTION**



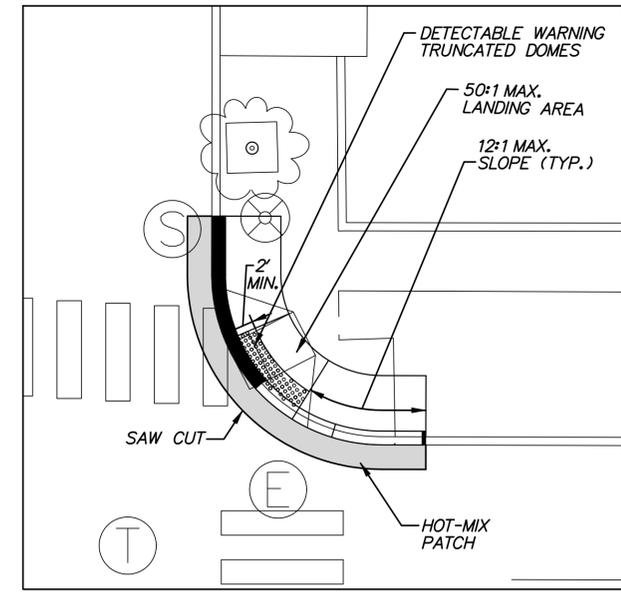
**FOURTH ST. AND WALNUT ST.
BUS STOP - LANDING PAD AND CONNECTION**



**FOURTH ST. AND SHIPLEY ST.
CURB RAMP, TYPE 3 - NW CORNER**



**FOURTH ST. AND KING ST.
CURB RAMP, TYPE 3 - SW CORNER**



**FOURTH ST. AND WALNUT ST.
CURB RAMP, TYPE 3 - NE CORNER**

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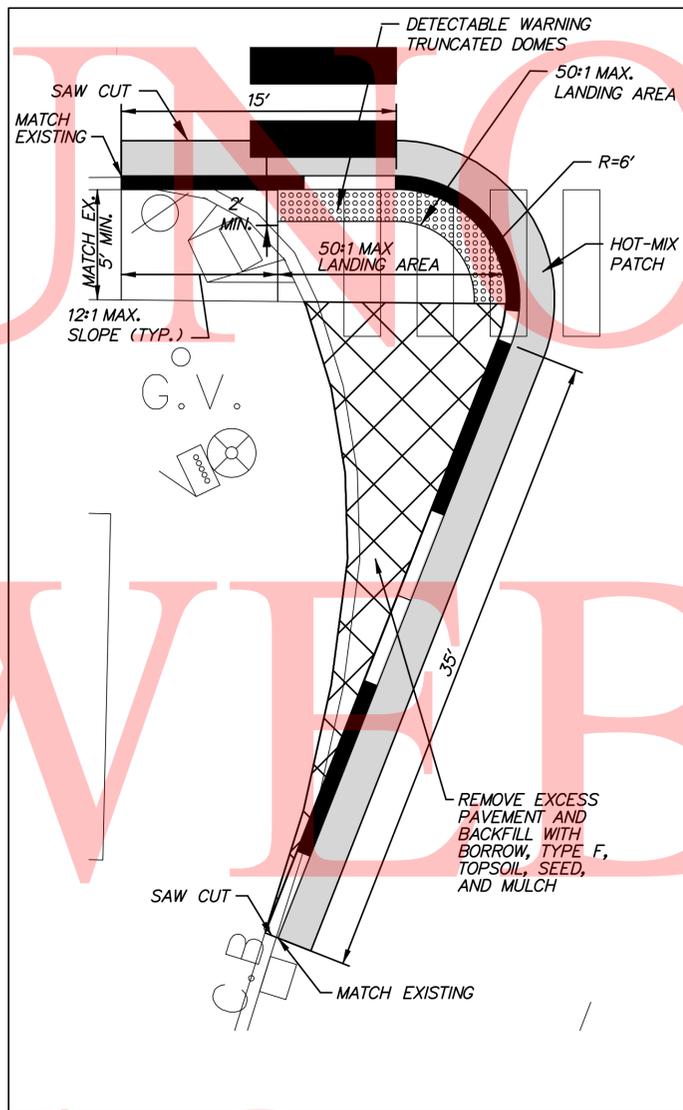
ADDENDUMS / REVISIONS	

NOT TO SCALE

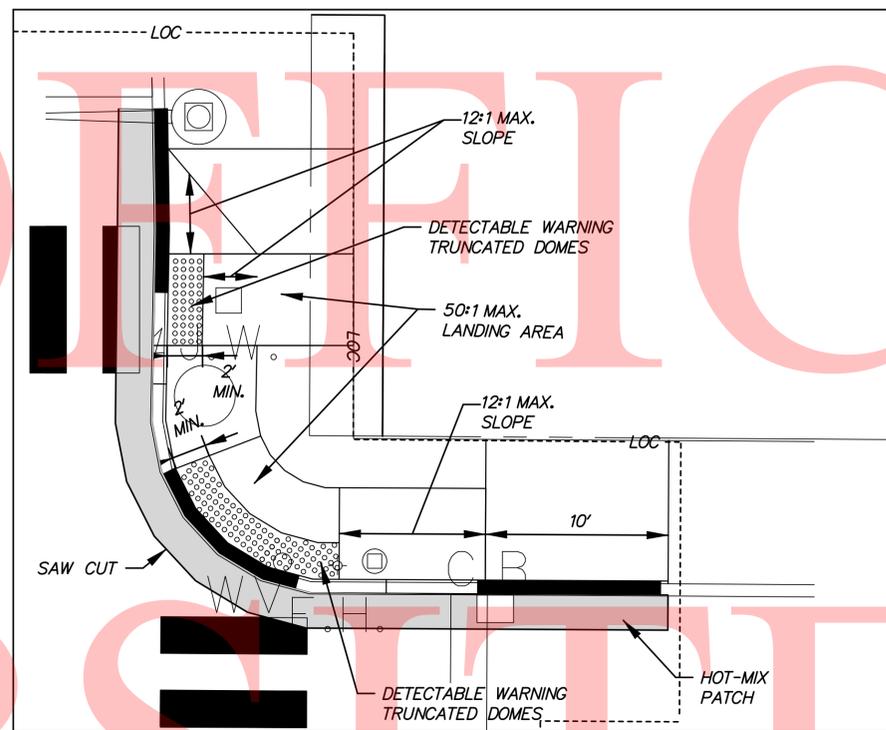
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

DT-02
SHEET NO. 23
TOTAL SHTS. 78

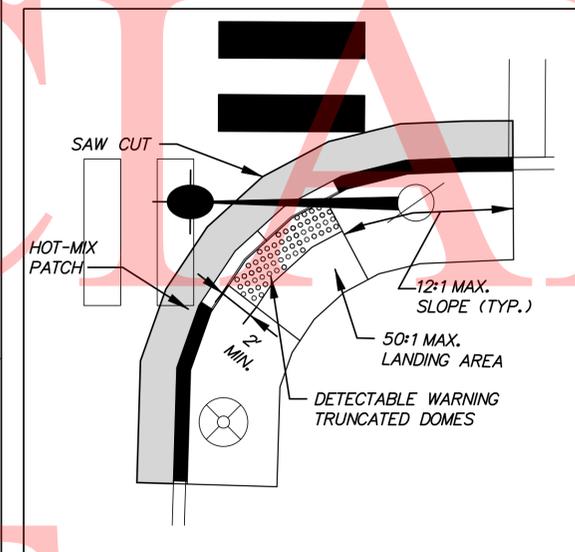
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WILMINGTON
COPY



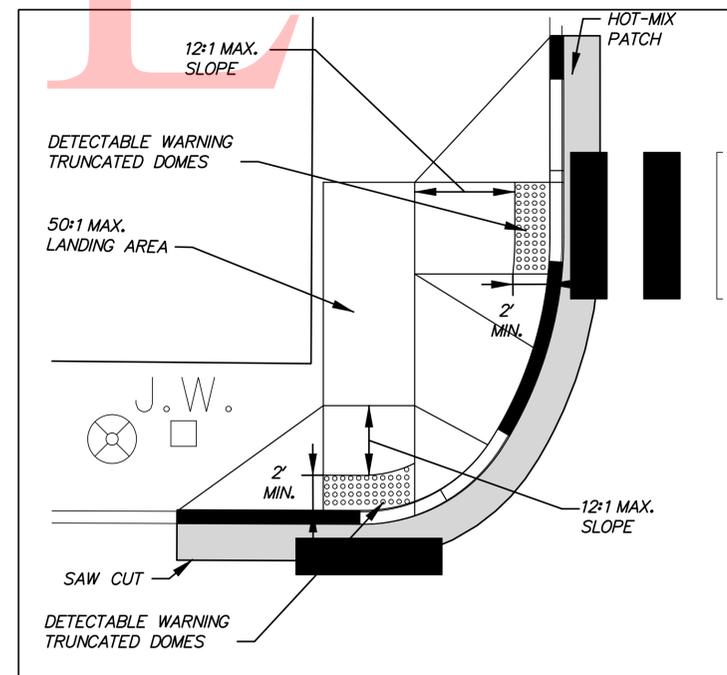
**2ND ST. AND KING ST.
CURB RAMP, TYPE 2 - SW CORNER**



**2ND ST. AND KING ST.
CURB RAMPS, TYPE 3 AND 4 - NE CORNER**



**2ND ST. AND KING ST.
CURB RAMP, TYPE 3 - SE CORNER**



**2ND ST. AND KING ST.
CURB RAMPS, TYPE 4 - NW CORNER**

ADDENDUMS / REVISIONS

NO.	DATE	DESCRIPTION

NOT TO SCALE

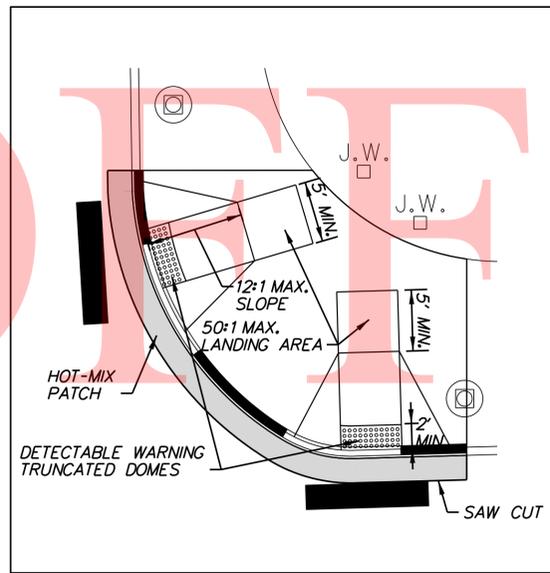
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

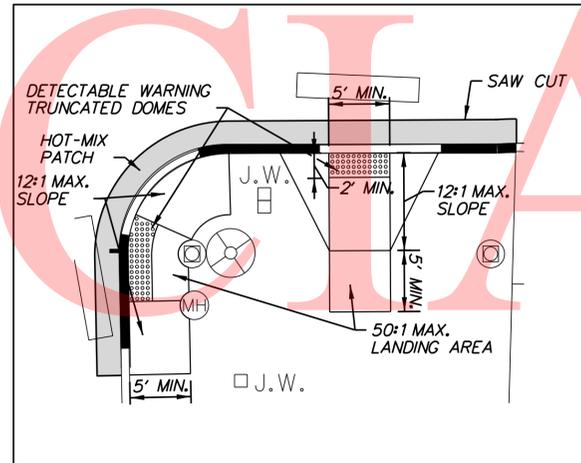
CONSTRUCTION DETAILS

DT-03
SHEET NO.
24
TOTAL SHTS.
78

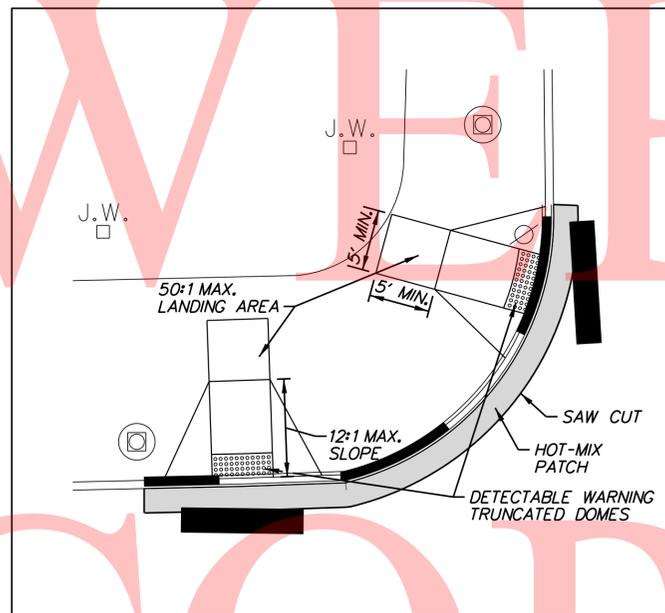
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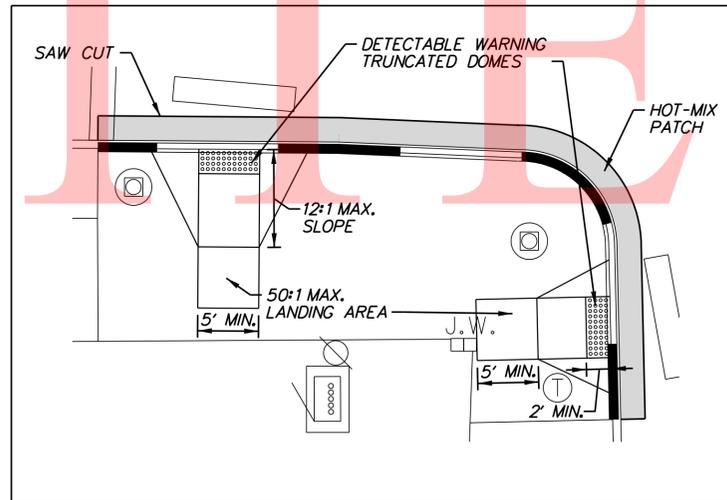
M.L. KING JR BLVD AND ORANGE STREET CURB RAMPS, TYPE 4 - NE CORNER



M.L. KING JR BLVD AND ORANGE STREET CURB RAMPS, TYPES 3 AND 4 - SE CORNER



M.L. KING JR BLVD AND ORANGE STREET CURB RAMPS, TYPE 4 - NW CORNER



M.L. KING JR BLVD AND ORANGE STREET CURB RAMPS, TYPE 4 - SW CORNER

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ADDENDUMS / REVISIONS	

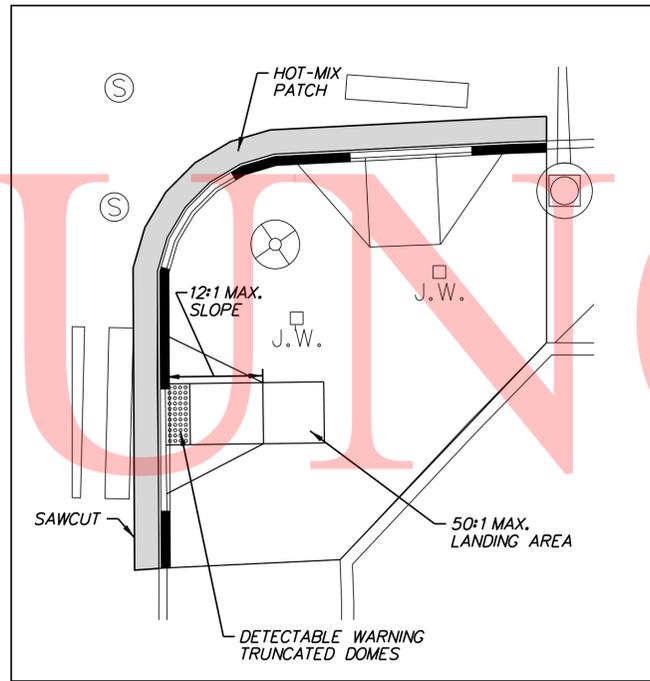
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WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

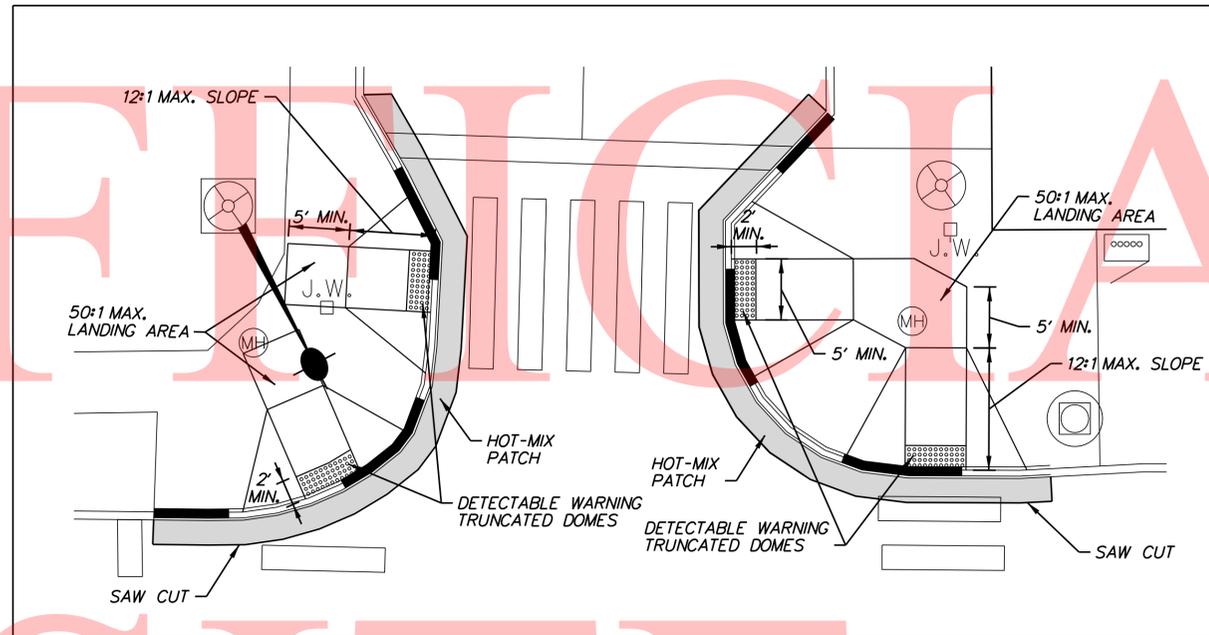
CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION DETAILS

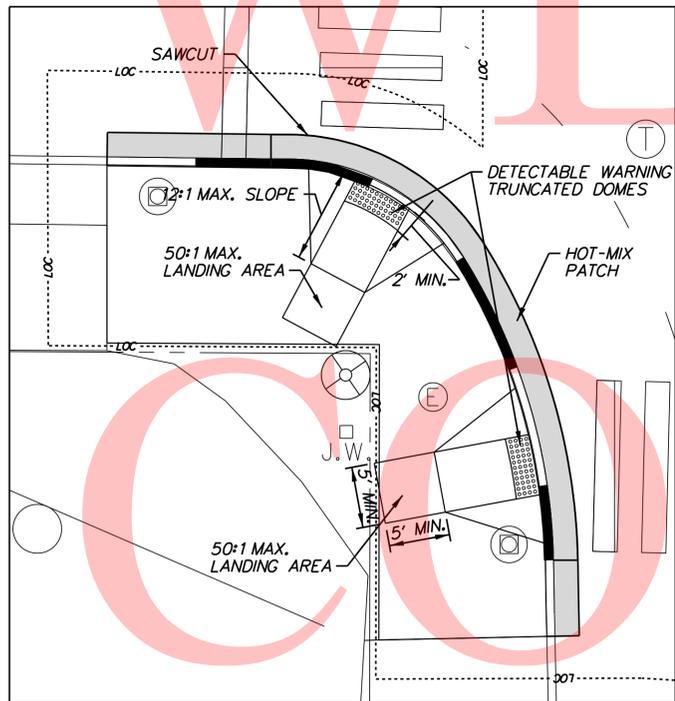
DT-04
SHEET NO.
25
TOTAL SHTS.
78



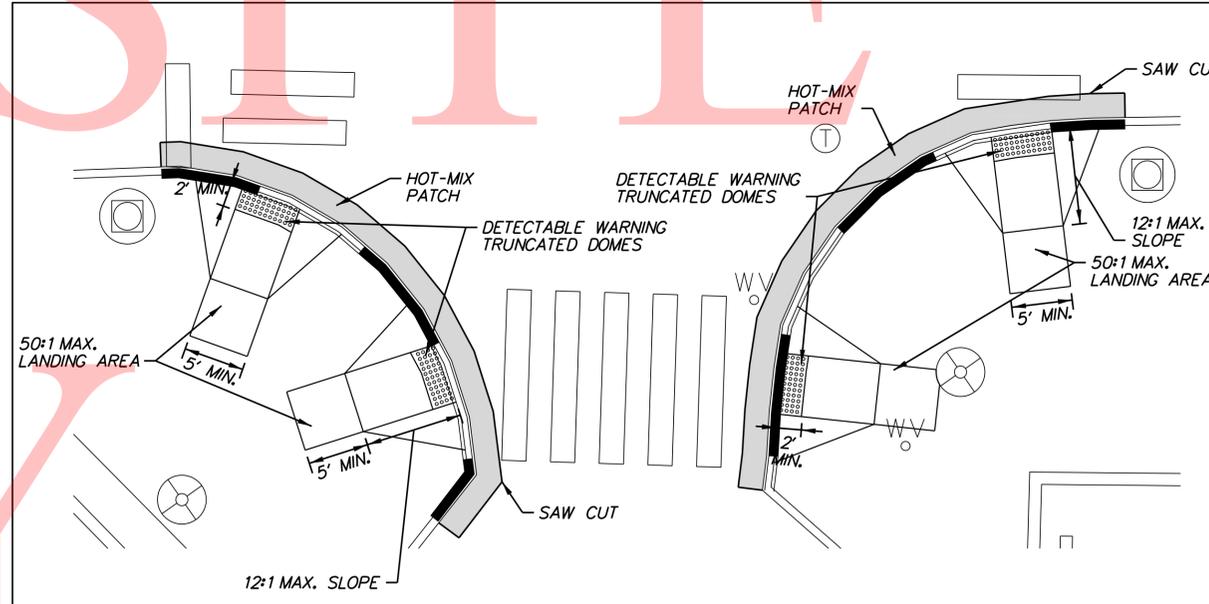
M.L. KING JR BLVD. AND MARKET ST.
CURB RAMP, TYPE 4 - SE CORNER



M.L. KING JR BLVD. AND KING ST.
CURB RAMPS, TYPE 4 - NE & NW CORNER



M.L. KING JR BLVD. AND MARKET ST.
CURB RAMPS, TYPE 4 - SW CORNER



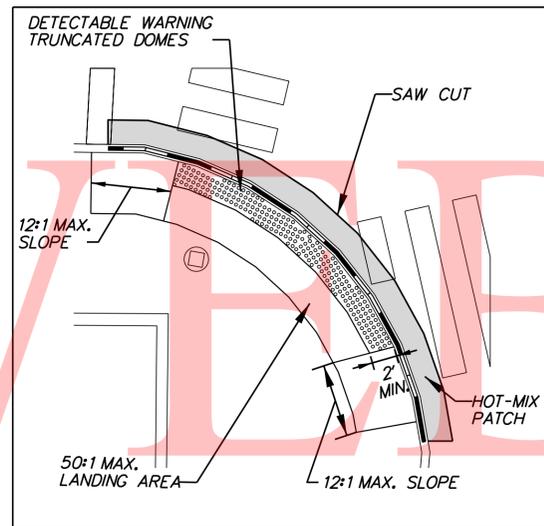
M.L. KING JR BLVD. AND KING ST.
CURB RAMPS, TYPE 4 - SE & SW CORNER

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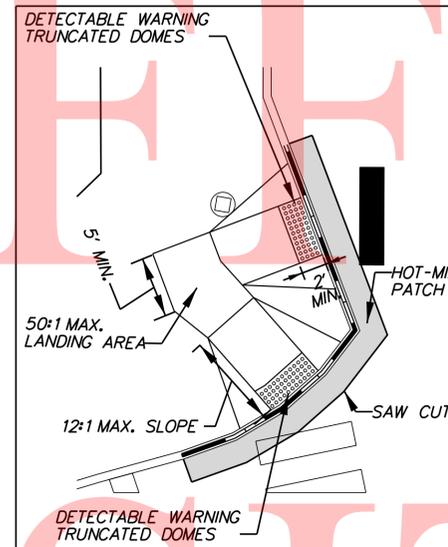
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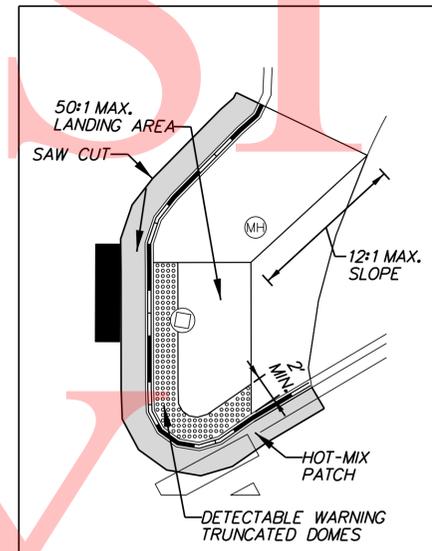
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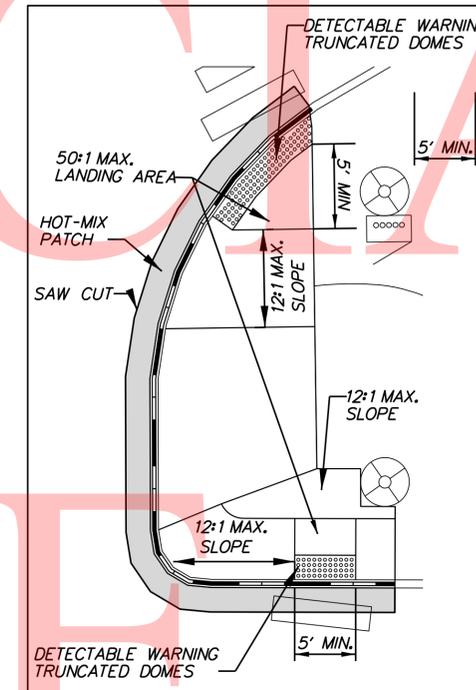
M.L. KING JR BLVD. AND E. FRONT ST. AND S. FRENCH ST.
CURB RAMP, TYPE 3 - SW CORNER



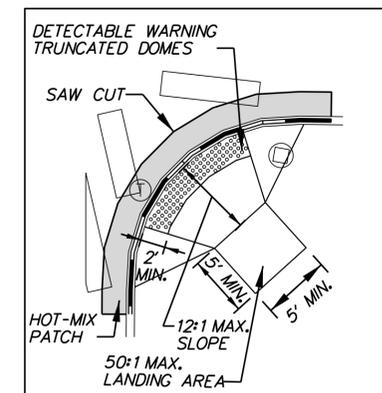
M.L. KING JR BLVD. AND E. FRONT ST. AND S. FRENCH ST.
CURB RAMP, TYPE 4 - NW CORNER



M.L. KING JR BLVD. AND E. FRONT ST. AND S. FRENCH ST.
CURB RAMP, TYPE 1 - NE CORNER



M.L. KING JR BLVD. AND E. FRONT ST. AND S. FRENCH ST.
CURB RAMP, TYPE 1 AND 3 - MEDIAN



M.L. KING JR BLVD. AND E. FRONT ST. AND S. FRENCH ST.
CURB RAMP, TYPE 3 - SE CORNER

ADDENDUMS / REVISIONS

NO.	DATE	DESCRIPTION

NOT TO SCALE

WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

CONSTRUCTION DETAILS

DT-06
SHEET NO.
27
TOTAL SHTS.
78

GENERAL TRAFFIC CONTROL NOTES

1. MAINTENANCE OF TRAFFIC DURING SHOULDER AND LANE CLOSURES AND LANE SHIFTS SHALL CONFORM TO:
 CASE 2 - TWO-LANE, TWO-WAY TRAFFIC WITH DAYTIME SHOULDER CLOSURE
 CASE 3 - MULTILANE, DIVIDED HIGHWAYS AND INTERSTATES, SHOULDER CLOSURE
 CASE 6 - TWO-LANE, TWO-WAY TRAFFIC WITH LANE CLOSURE
 CASE 7 - MULTILANE, DIVIDED HIGHWAYS AND INTERSTATES, LANE CLOSURE
 CASE 10 - TWO-LANE, TWO-WAY TRAFFIC - 25 MPH OR LESS DAYLIGHT OPERATIONS LANE DIVERSION
 CASE 16 - SIDEWALK DETOUR (APPLIES TO LOCATIONS WHERE PED DETOURS ARE NOT PROVIDED) OF THE DELAWARE MUTCD.
2. ON ALL INTERSECTING STREETS APPROACHING THE WORK AREA, PERMANENT SIGNS SHALL BE PLACED AS SHOWN ON THESE PLANS OR AS DIRECTED BY THE ENGINEER. AN "END ROAD WORK" SIGN SHALL BE PLACED VISIBLE TO TRAFFIC EXITING THE WORK ZONE.
3. GRADING AND MAINTAINING HOT MIX TRM THAT IS BEING USED AS A TRAVELWAY, DRIVEWAY, ACCESS RAMP, ETC. SHALL BE INCIDENTAL TO ITEM 763500 - MAINTENANCE OF TRAFFIC. EXCESS HOT MIX TRM MATERIAL SHALL BE PUSHED AHEAD AND USED IN THE NEXT SEGMENT AND SHALL BE INCIDENTAL TO THE PARTICULAR HOT MIX TRM PAY ITEM. NO SEPARATE PAYMENT SHALL BE MADE FOR TEMPORARY ROADWAY MATERIAL (TRM) USED TO PROTECT EDGE DROP-OFFS, UNLESS THE MATERIAL IS EVENTUALLY UTILIZED AS PART OF A PERMANENT ROADWAY AT WHICH TIME THE MATERIAL WOULD BE PAID FOR UNDER THE RESPECTIVE CONTRACT MATERIAL ITEM. CONSTRUCTION OF A PLANNED RUNAROUND OR DETOUR WOULD BE ELIGIBLE FOR PAYMENT AS SPECIFIED IN THE CONTRACT.
4. WHERE A TEMPORARY STOP SIGN IS USED, THE CONTRACTOR SHALL COVER THE EXISTING STOP SIGN. TEMPORARY STOP SIGN SHALL BE 48" X 48".
5. THE CONTRACTOR SHALL MAINTAIN A MINIMUM LANE WIDTH OF TEN FEET AT ALL TIMES.
6. R9-9 (SIDEWALK CLOSED) SIGNS SHALL BE MOUNTED ON A TYPE III BARRICADE. THE TYPE III BARRICADE SHALL EXTEND ACROSS THE ENTIRE WIDTH OF THE SIDEWALK.
7. ALL FIRE HYDRANTS SHALL BE CLEAR OF ALL EQUIPMENT AT ALL TIMES.
8. ALL WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 9:00 A.M. AND 4:00 P.M., MONDAY THROUGH FRIDAY, EXCEPT AS NOTED BELOW. WORK MAY BE PERFORMED AT MULTIPLE INTERSECTIONS SIMULTANEOUSLY. WORK SHALL NOT BE PERFORMED AT NIGHT OR WEEKENDS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
9. TRAFFIC OFFICERS, WHEN USED, MUST BE UNIFORMED AND EQUIPPED WITH MARKED CITY OF WILMINGTON POLICE VEHICLES WITH FULL LIGHT BAR.
10. THE CONTRACTOR SHALL COORDINATE PLACEMENT OF TEMPORARY AND PERMANENT WARNING SIGNS WITH THE ENGINEER TO ENSURE THAT SIGNS ARE PLACED IN AN APPROPRIATE LOCATION. TEMPORARY TRAFFIC CONTROL DEVICES, INCLUDING SIGNS, SHALL NOT OBSTRUCT THE SIDEWALK, UNLESS SPECIFIED ON THE CONSTRUCTION PHASING OR DETOUR PLANS.
11. THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER FOR PLACEMENT OF TEMPORARY WARNING SIGNS AND TRAFFIC CONTROL DEVICES. TEMPORARY TRAFFIC CONTROL SHALL BE PLACED AS NOTED BELOW:

DELAWARE AVENUE AT 11TH STREET/JEFFERSON STREET (CP-02):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CONSTRUCTION SHALL BE PHASED TO ENSURE THAT A MINIMUM OF TWO TRAVEL LANES REMAIN OPEN AT ALL TIMES ALONG THE WEST LEG OF JEFFERSON STREET. CONSTRUCTION ON THE SOUTH LEG OF JEFFERSON STREET WILL REQUIRE SHOULDER CLOSURES IN ACCORDANCE WITH CASE 3 OF THE DELAWARE MUTCD.

FOURTH STREET AT ORANGE STREET (CP-05):
 CONSTRUCTION OF THE PAVEMENT PATCH ON THE SOUTHEAST CORNER WILL REQUIRE A LANE CLOSURE ON ORANGE STREET AND SHALL CONFORM TO THE MAINTENANCE OF TRAFFIC PLAN PROVIDED (SEE CS-02). CURB RAMP CONSTRUCTION AT THE NORTHWEST CORNER WILL REQUIRE CLOSURE OF THE PARKING LANES ON FOURTH STREET AND ORANGE STREET AND SHALL CONFORM TO CASE 3 OF THE DELAWARE MUTCD. THE CONTRACTOR SHALL COORDINATE WITH THE WILMINGTON PARKING AUTHORITY 2 WEEKS PRIOR TO ANY LANE AND/OR SHOULDER CLOSURE THAT AFFECTS ON STREET PARKING.

FOURTH STREET AT SHIPLEY STREET (CP-06):
 CONSTRUCTION OF THE CURB RAMP ON THE NORTHWEST CORNER WILL REQUIRE CLOSURE OF THE PARKING LANE ALONG FOURTH STREET AND A LANE CLOSURE ALONG SHIPLEY STREET. TRAFFIC CONTROL SHALL CONFORM TO CASE 3 AND CASE 7 OF THE DELAWARE MUTCD RESPECTIVELY. THE CONTRACTOR SHALL COORDINATE WITH THE WILMINGTON PARKING AUTHORITY 2 WEEKS PRIOR TO ANY LANE AND/OR SHOULDER CLOSURE THAT AFFECTS ON STREET PARKING.

FOURTH STREET AT KING STREET (CP-07):
 CONSTRUCTION OF THE CURB RAMP ON THE SOUTHWEST CORNER WILL REQUIRE A LANE CLOSURE ON FOURTH STREET AND KING STREET. THE LANE CLOSURES SHALL CONFORM TO CASE 7 OF THE DELAWARE MUTCD. ALL WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 8:00 AM AND 3:00 PM.

FOURTH STREET AT WALNUT STREET (CP-08):
 CONSTRUCTION OF THE CURB RAMP AND SIDEWALK ON THE NORTHEAST CORNER WILL REQUIRE A LANE CLOSURE ON FOURTH STREET AND WALNUT STREET. THE LANE CLOSURES SHALL CONFORM TO CASE 7 OF THE DELAWARE MUTCD.

GENERAL TRAFFIC CONTROL NOTES (CONT.)

SECOND STREET AND KING STREET (CP-09):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CONSTRUCTION SHALL BE PHASED TO ENSURE THAT A MINIMUM OF TWO TRAVEL LANES REMAIN OPEN AT ALL TIMES ALONG SECOND STREET AND KING STREET. ACCESS TO MLK JR BOULEVARD SHALL BE MAINTAINED AT ALL TIMES. ALL WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 8:00 AM AND 3:00 PM.

MLK JR BOULEVARD AT ORANGE STREET (CP-10):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES ALONG MLK JR BLVD IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG THE SOUTH LEG OF ORANGE STREET SHALL BE IN ACCORDANCE WITH CASE 6 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG THE NORTH LEG OF ORANGE STREET SHALL BE IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CITY OF WILMINGTON POLICE OFFICERS SHOULD BE PRESENT FOR SIGNALIZED INTERSECTION FLAGGING OPERATIONS.

MLK JR BOULEVARD AT MARKET STREET (CP-11):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CONSTRUCTION SHALL BE PHASED TO ENSURE THAT A MINIMUM OF TWO TRAVEL LANES REMAIN OPEN AT ALL TIMES ALONG MLK JR BOULEVARD AND MARKET STREET.

MLK JR BOULEVARD AT KING STREET (CP-12):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES ALONG MLK JR BLVD IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG THE SOUTH LEG OF KING STREET SHALL BE IN ACCORDANCE WITH CASE 6 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG THE NORTH LEG OF KING STREET SHALL BE IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CITY OF WILMINGTON TRAFFIC OFFICERS SHOULD BE PRESENT FOR SIGNALIZED INTERSECTION FLAGGING OPERATIONS.

MLK JR BOULEVARD AT FRENCH STREET/FRONT STREET (CP-13):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES ALONG MLK JR BLVD AND FRONT STREET IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG FRENCH STREET SHALL BE IN ACCORDANCE WITH CASE 6 OF THE DELAWARE MUTCD. CITY OF WILMINGTON TRAFFIC OFFICERS SHOULD BE PRESENT FOR SIGNALIZED INTERSECTION FLAGGING OPERATIONS.

W. 12TH STREET AT N. WASHINGTON STREET (CP-03):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES ALONG N. WASHINGTON STREET IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. LANE CLOSURES ALONG W. 12TH STREET SHALL BE IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD.

12TH STREET AT N. JEFFERSON STREET (CP-01):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES ON 12TH STREET IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD. CONSTRUCTION ON THE SOUTH LEG OF JEFFERSON STREET WILL REQUIRE SHOULDER CLOSURES IN ACCORDANCE WITH CASE 3 OF THE DELAWARE MUTCD.

DELAWARE AVENUE AT 11TH STREET/WASHINGTON STREET (CP-04):
 CONSTRUCTION OF THE CURB RAMPS WILL REQUIRE LANE CLOSURES IN ACCORDANCE WITH CASE 7 OF THE DELAWARE MUTCD FOR DELAWARE AVENUE AND WASHINGTON STREET. CONSTRUCTION ON JEFFERSON STREET WILL REQUIRE SHOULDER CLOSURES IN ACCORDANCE WITH CASE 3 OF THE DELAWARE MUTCD.

DETOUR NOTES

PEDESTRIAN DETOUR PLANS

1. INTERSECTIONS WHERE MULTIPLE CURB RAMPS ARE BEING RECONSTRUCTED SHALL BE CONSTRUCTED SUCH THAT PEDESTRIAN ACCESS AT THE INTERSECTION IS MAINTAINED THROUGH AT LEAST ONE CROSSWALK OF SAID INTERSECTION. THE PEDESTRIAN DETOUR PLANS PROVIDE GUIDANCE FOR MAINTAINING ACCESS, BUT THEY DO NOT SPECIFY THE ORDER IN WHICH THE CURB RAMPS SHALL BE CONSTRUCTED.

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ADDENDUMS / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY: J.J.O.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

GENERAL TRAFFIC
 CONTROL NOTES

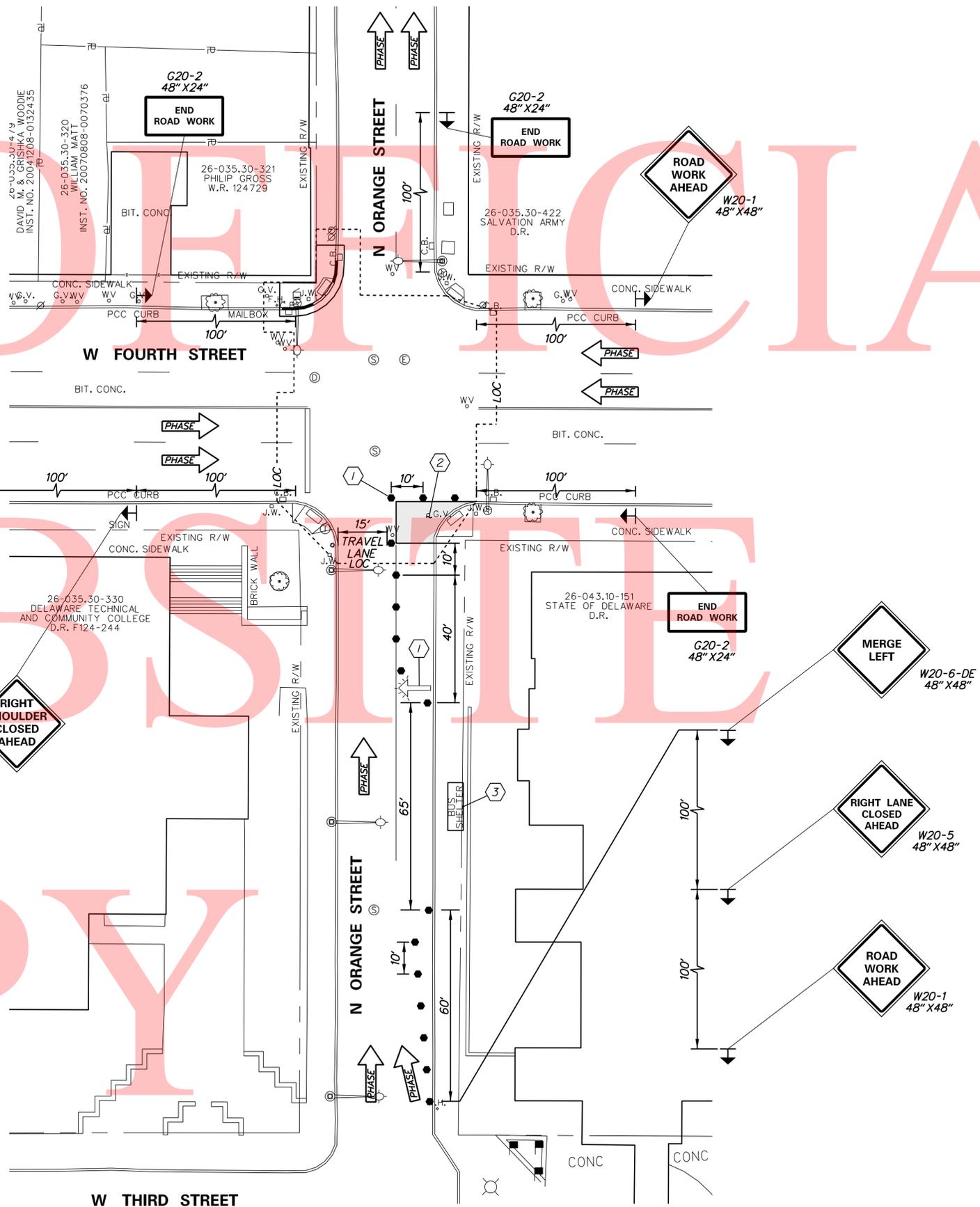
CS-01
SHEET NO.
28
TOTAL SHTS.
78

SEQUENCE OF CONSTRUCTION

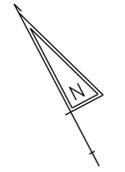
- 1 PLACE ALL PERTINENT TRAFFIC CONTROL DEVICES AS SHOWN ON THIS PLAN.
- 2 EXCAVATE FOR AND CONSTRUCT PROPOSED PAVEMENT PATCH. SEE CONSTRUCTION DETAILS, SHEET DT-02, FOR ADDITIONAL DETAILS.
- 3 THE EXISTING BUS SHELTER SHALL REMAIN ACCESSIBLE AT ALL TIMES.

CONSTRUCTION PHASING LEGEND

	WORK AREA THIS PHASE
	BARRICADE, TYPE 3
	CONSTRUCTION WARNING SIGN LOCATION
	CONSTRUCTION WARNING SIGN
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	ARROW BOARD

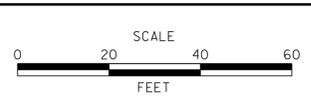


COPY



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ADDENDUMS / REVISIONS	



WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	D.N.P.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

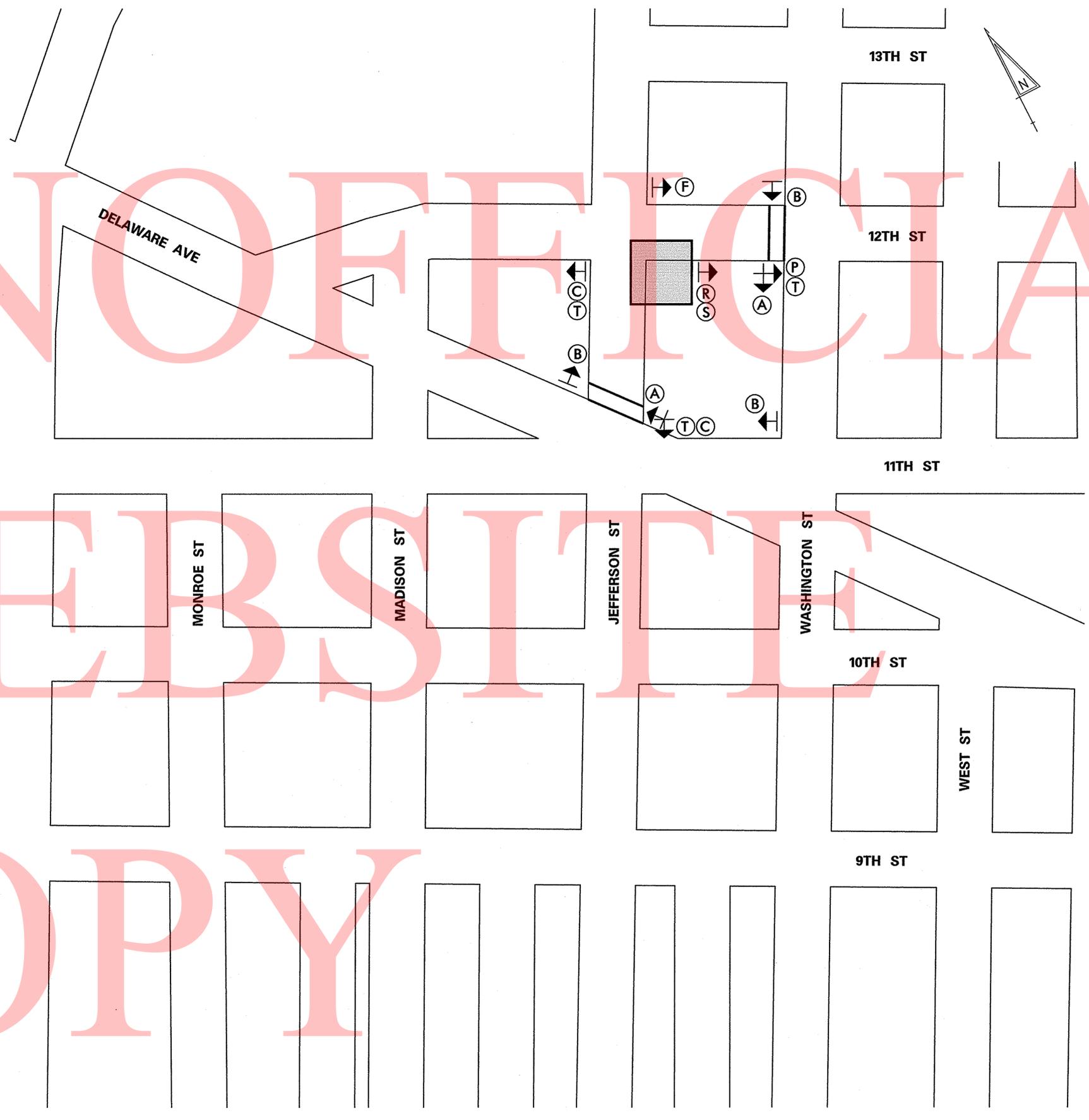
CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN

CS-02
SHEET NO.
29
TOTAL SHTS.
78

UNOFFICIAL

WEBSTER

COPY



(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) ← DETOUR	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) 		(T)

- GENERAL NOTES:**
1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
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 5. WARNING SIGNS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
 6. *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
 7. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Todd A. Di... DATE: 3/18/2011 RECOMMENDED Brian Mitchell DATE: 3-21-11 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013 COUNTY NEW CASTLE PERMIT NO. N/A DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

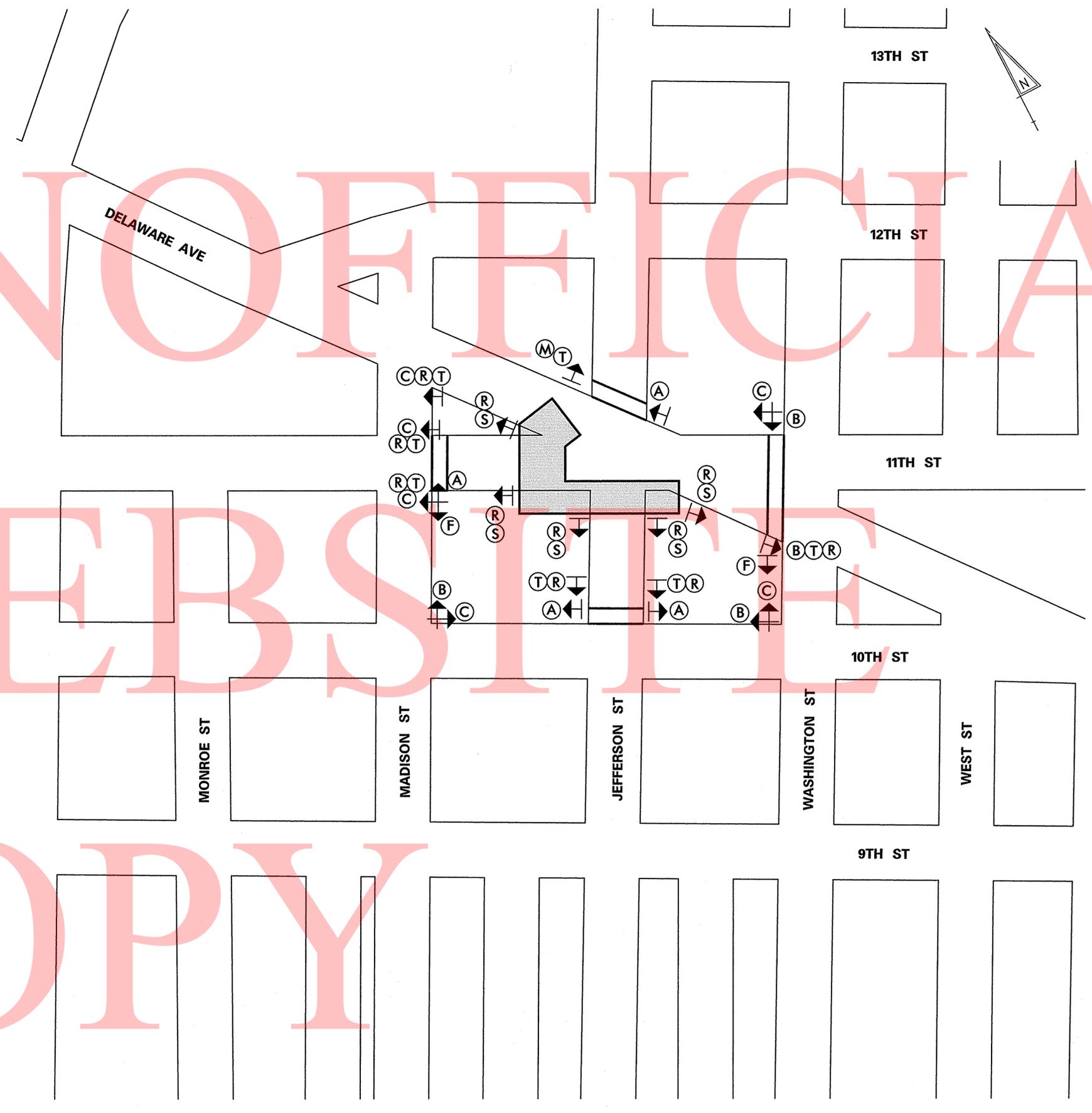
PEDESTRIAN DETOUR PLAN 12TH STREET @ JEFFERSON STREET

SHEET NO. 30 TOTAL SHTS. 78

UNOFFICIAL

WEBSTER

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

SPECIAL SIGNS

GENERAL NOTES

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RECOMMENDED Todd A. Ali DATE: 3/18/2011
 RECOMMENDED Brian Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P.
	CHECKED BY: T.A.O.

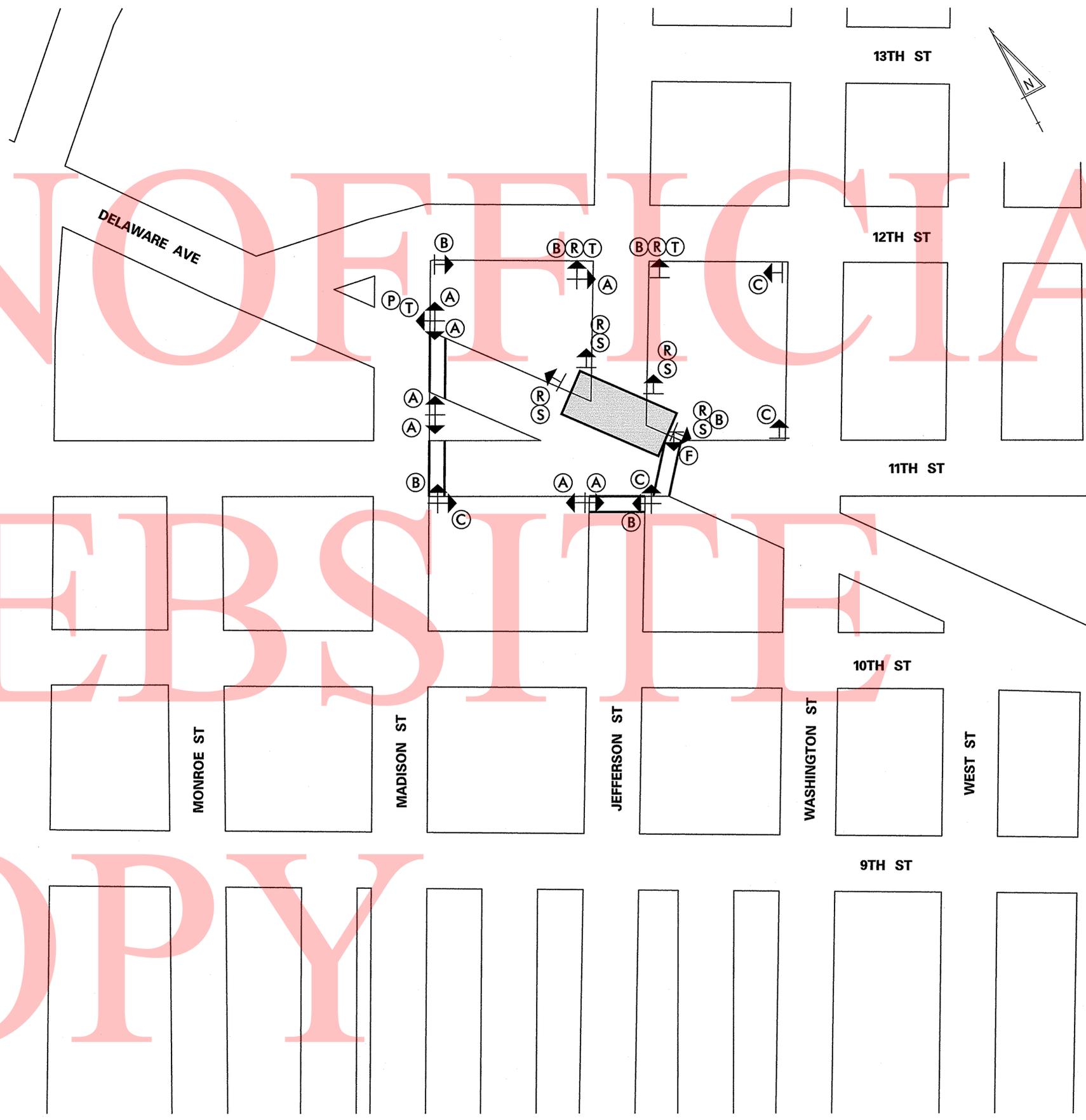
PEDESTRIAN DETOUR PLAN DELAWARE AVENUE AND 11TH STREET @ JEFFERSON STREET

SHEET NO. 31
TOTAL SHTS. 78

UNOFFICIAL

WEB SITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

SPECIAL SIGNS

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RECOMMENDED Todd A. Ali DATE: 3/18/2011

RECOMMENDED Brian A. Mitchell DATE: 3-21-11

RECOMMENDED _____ DATE: _____

APPROVED CHIEF SAFETY OFFICER _____ DATE: _____

APPROVED TRAFFIC ENGINEER _____ DATE: _____



ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

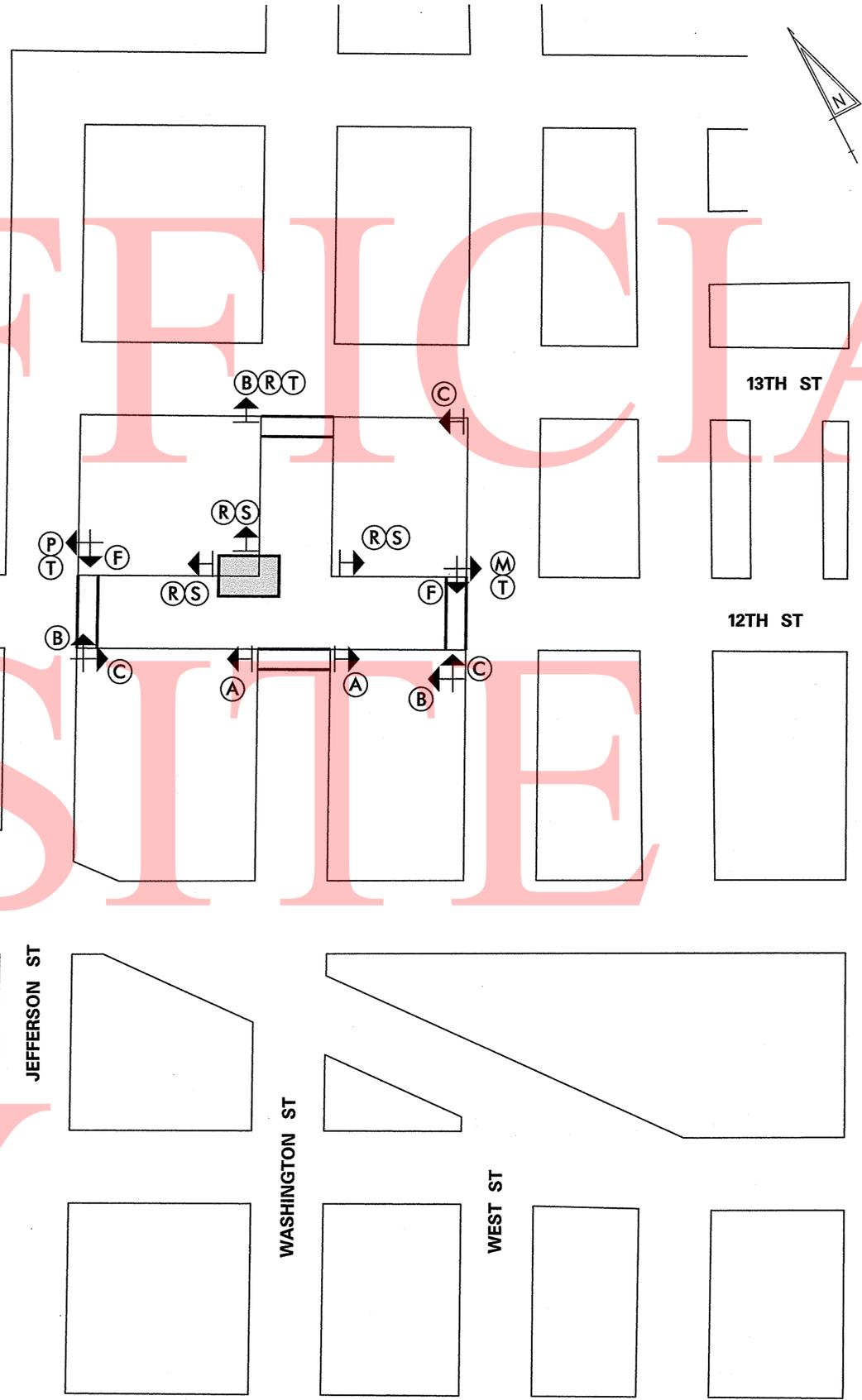
PEDESTRIAN DETOUR PLAN DELAWARE AVENUE AND 11TH STREET @ JEFFERSON STREET

SHEET NO.	32
TOTAL SHTS.	78

UNOFFICIAL

WEBSITE

COPY



(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) ← DETOUR	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) BARRICADE	(T) BARRICADE	

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RECOMMENDED Todd A. Di DATE: 3/18/2011 RECOMMENDED Brian A. Mitchell DATE: 3-21-11 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

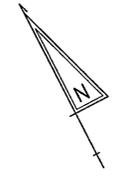
WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013
COUNTY NEW CASTLE
PERMIT NO. N/A
DESIGNED BY: G.R.P.
CHECKED BY: T.A.O.

PEDESTRIAN DETOUR PLAN 12TH STREET @ WASHINGTON STREET

SHEET NO. 33
TOTAL SHTS. 78

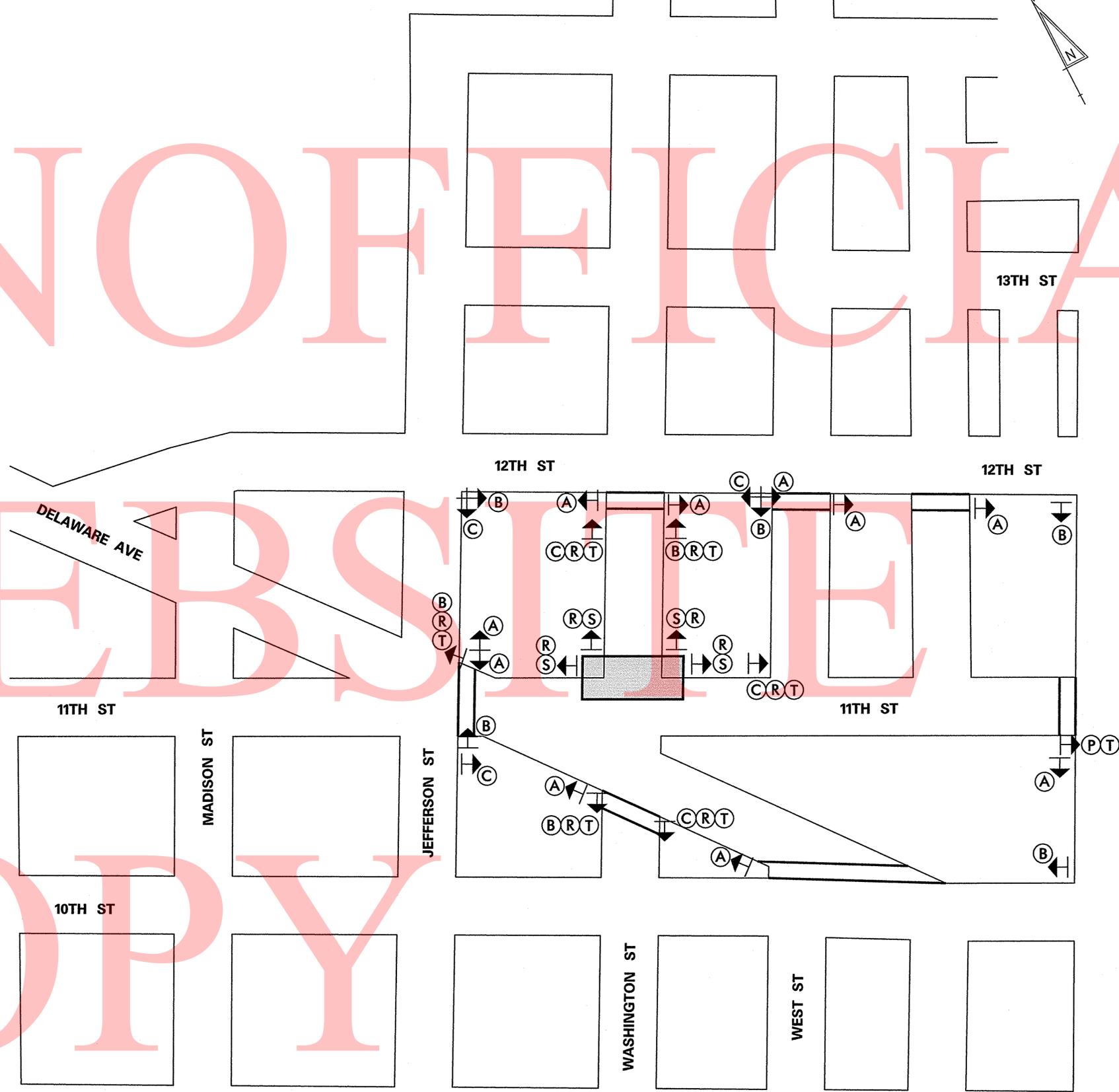
UNOFFICIAL COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

SPECIAL SIGNS



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RECOMMENDED Todd A. O'Neil DATE: 3/18/2011
 RECOMMENDED Brian Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P.
	CHECKED BY: T.A.O.

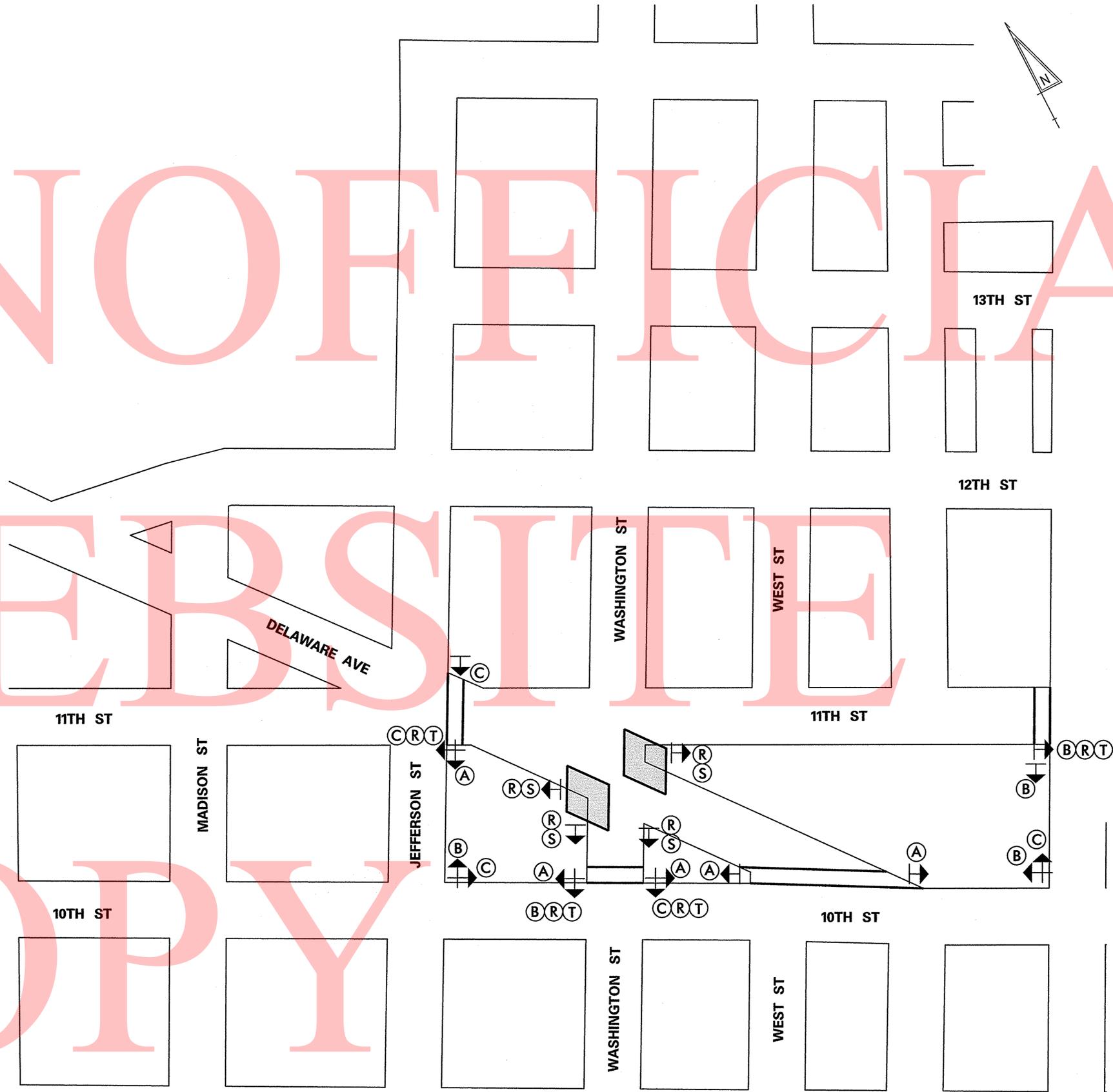
PEDESTRIAN DETOUR PLAN DELAWARE AVENUE AND 11TH STREET @ WASHINGTON STREET

SHEET NO.	34
TOTAL SHTS.	78

UNOFFICIAL

WEB SITE

COPY



LEGEND

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(D)	(E)	(F)
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(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

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RECOMMENDED *Todd A. O'Neil* DATE: 3/18/2011
 RECOMMENDED *Brian Mitchell* DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

PEDESTRIAN DETOUR PLAN DELAWARE AVENUE AND 11TH STREET @ WASHINGTON STREET

SHEET NO.	35
TOTAL SHTS.	78

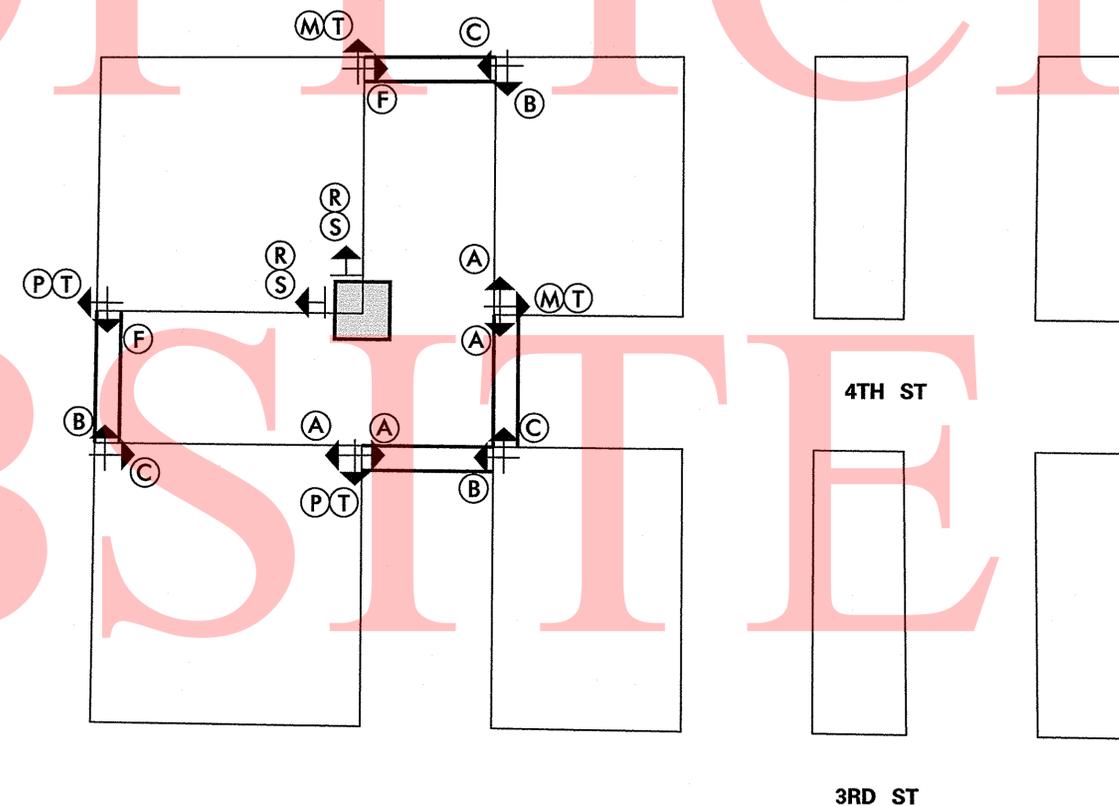
CHANGEABLE MESSAGE BOARDS

LEGEND

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)		(T)

SPECIAL SIGNS

GENERAL NOTES:

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 RECOMMENDED Brian A. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

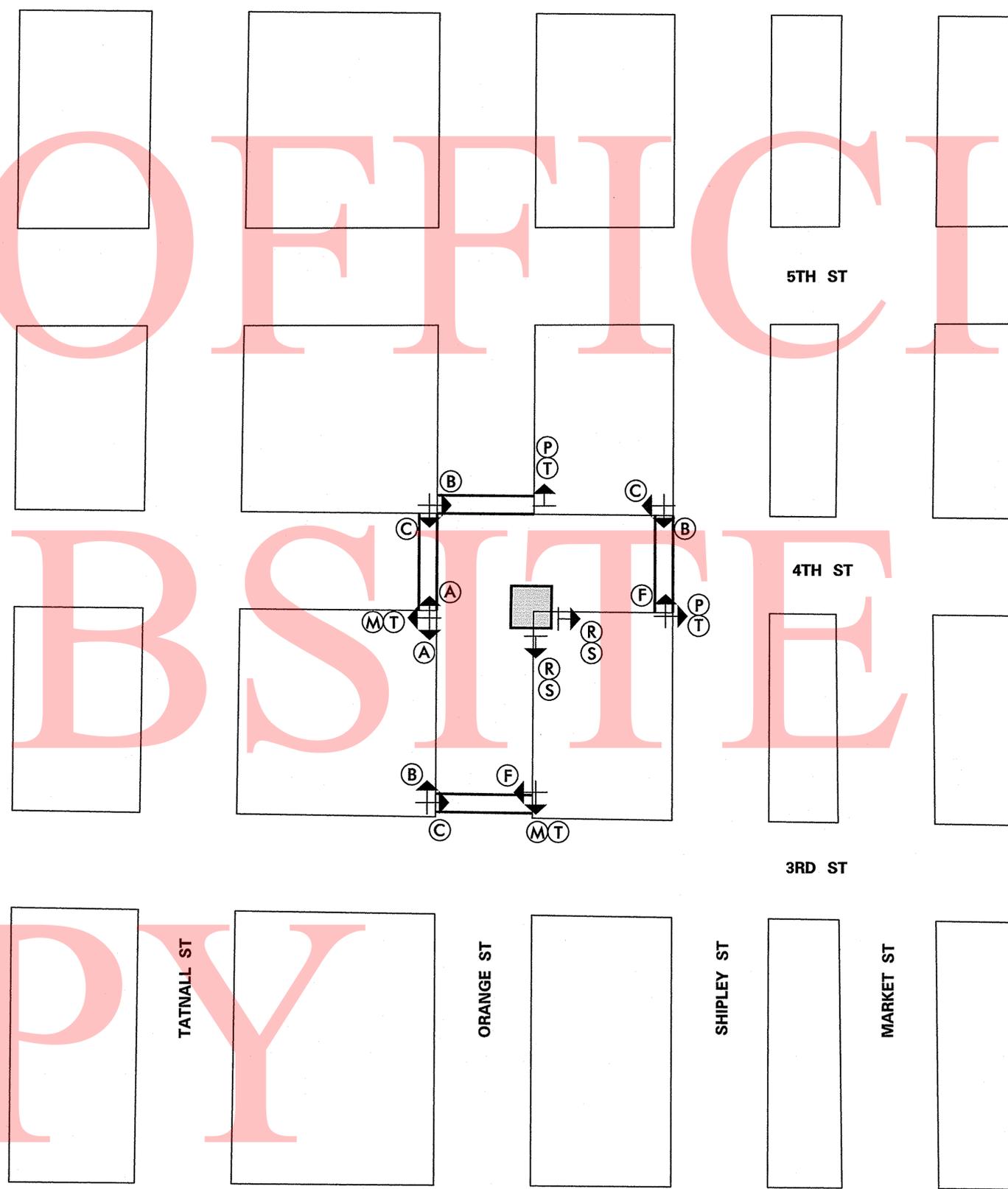
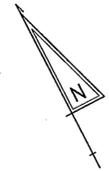
PEDESTRIAN DETOUR PLAN
4TH STREET @ ORANGE STREET

SHEET NO.	36
TOTAL SHTS.	78

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

SPECIAL SIGNS

GENERAL NOTES

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6) FOR LIGHTS, BARRICADES AND SIGNS. (AS PER LATEST REVISION)
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- WARNING SIGNS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
- *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Todd A. Ovi DATE: 3/18/2011 RECOMMENDED Brian J. Mitchell DATE: 3-21-11 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

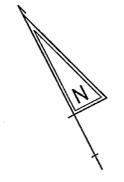
PEDESTRIAN DETOUR PLAN
4TH STREET @ ORANGE STREET

SHEET NO. 37
TOTAL SHTS. 78

UNOFFICIAL

WEBSITES

COPY



5TH ST

4TH ST

3RD ST

TATNALL ST

ORANGE ST

SHIPLEY ST

MARKET ST

LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

SPECIAL SIGNS

GENERAL NOTES

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RECOMMENDED Todd A. Ovi DATE: 3/18/2011
 RECOMMENDED Brian J. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013 COUNTY NEW CASTLE PERMIT NO. N/A DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

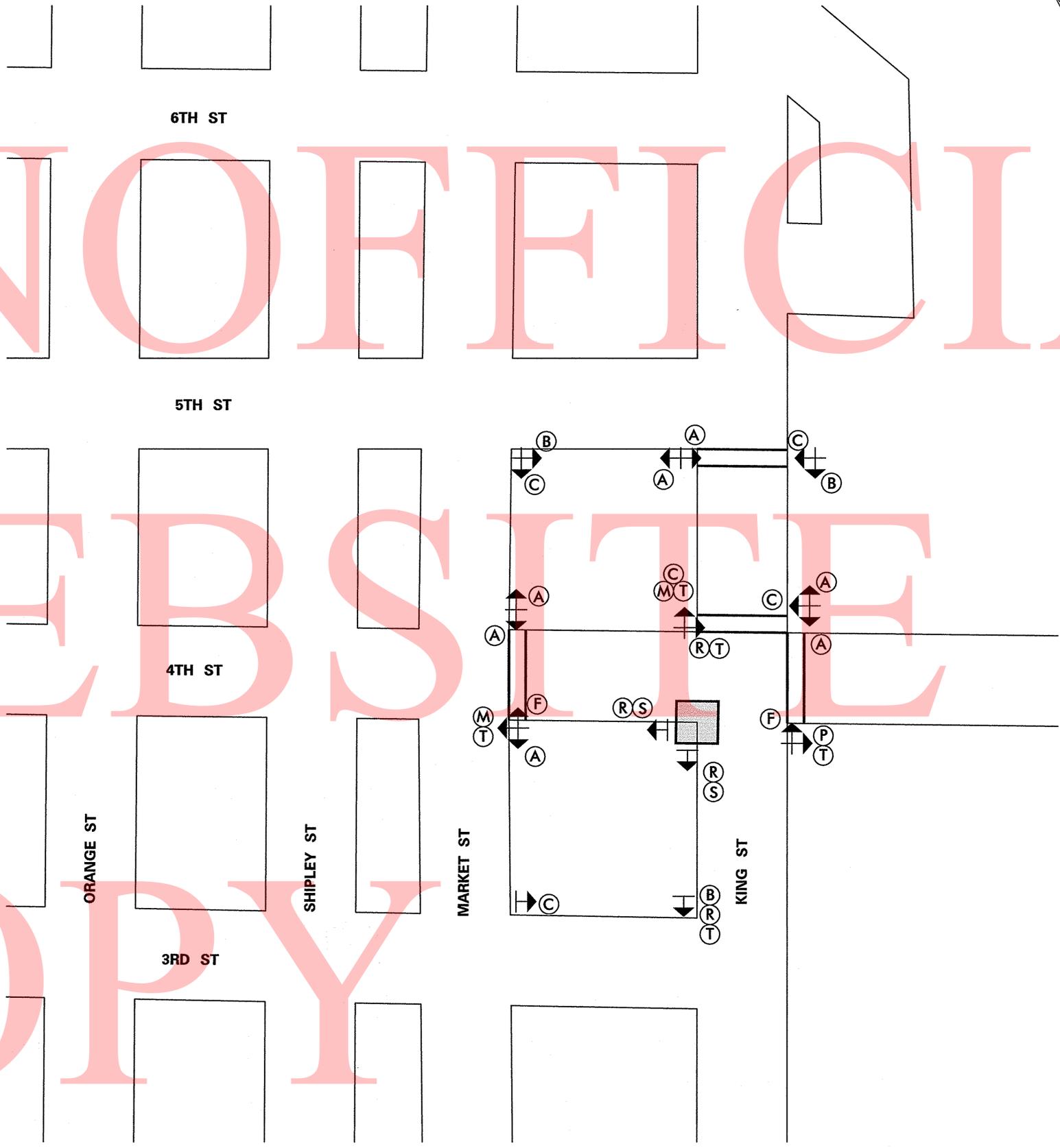
PEDESTRIAN DETOUR PLAN 4TH STREET @ SHIPLEY STREET

SHEET NO. 38 TOTAL SHTS. 78

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)		(T)

SPECIAL SIGNS

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RECOMMENDED Todd A. Ovi DATE: 3/18/2011
 RECOMMENDED Brian A. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

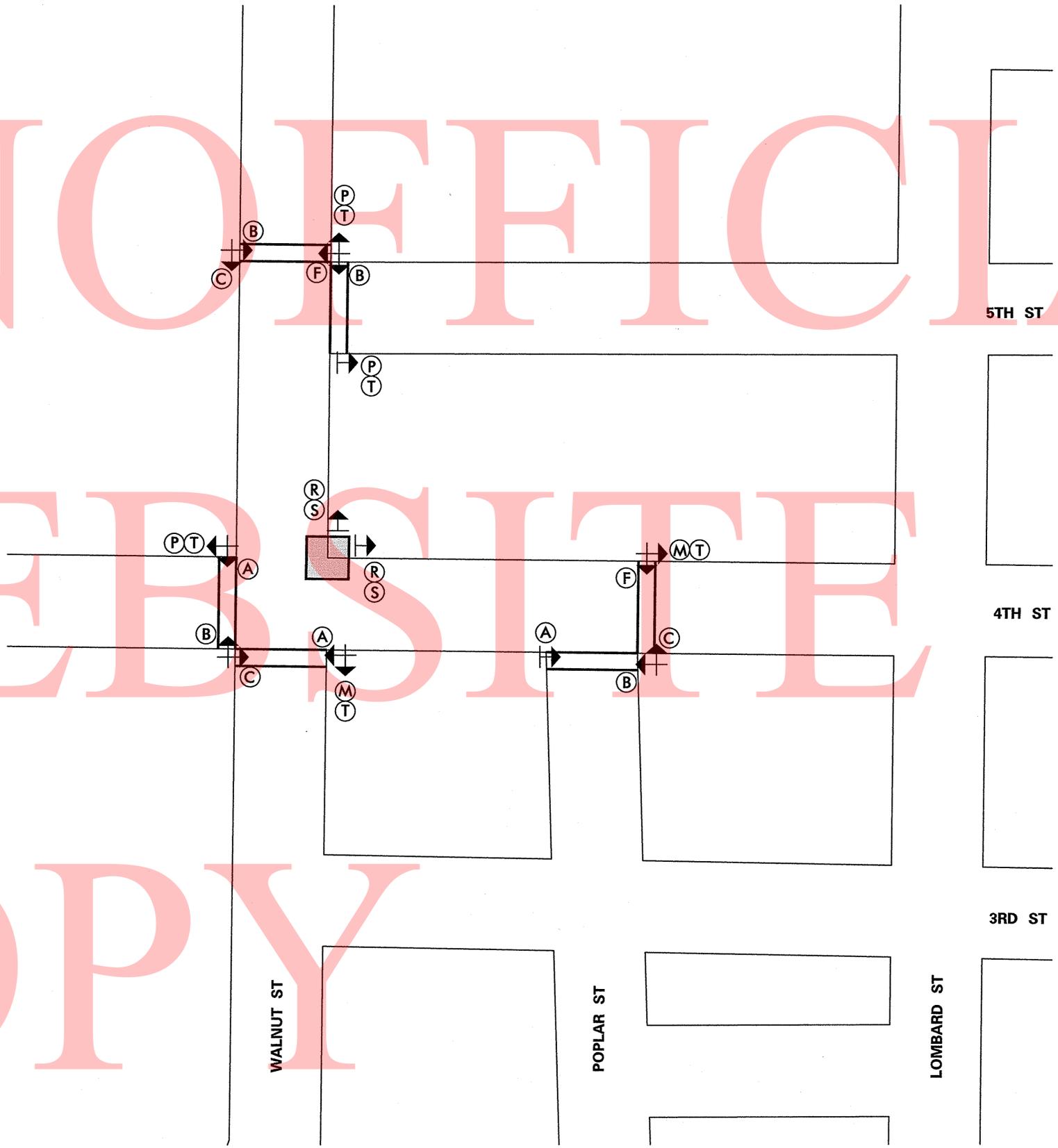
PEDESTRIAN DETOUR PLAN
4TH STREET @ KING STREET

SHEET NO.	39
TOTAL SHTS.	78

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)		(T)

- GENERAL NOTES**
- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
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RECOMMENDED Todd A. Di... DATE: 3/18/2011
 RECOMMENDED Brian P. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

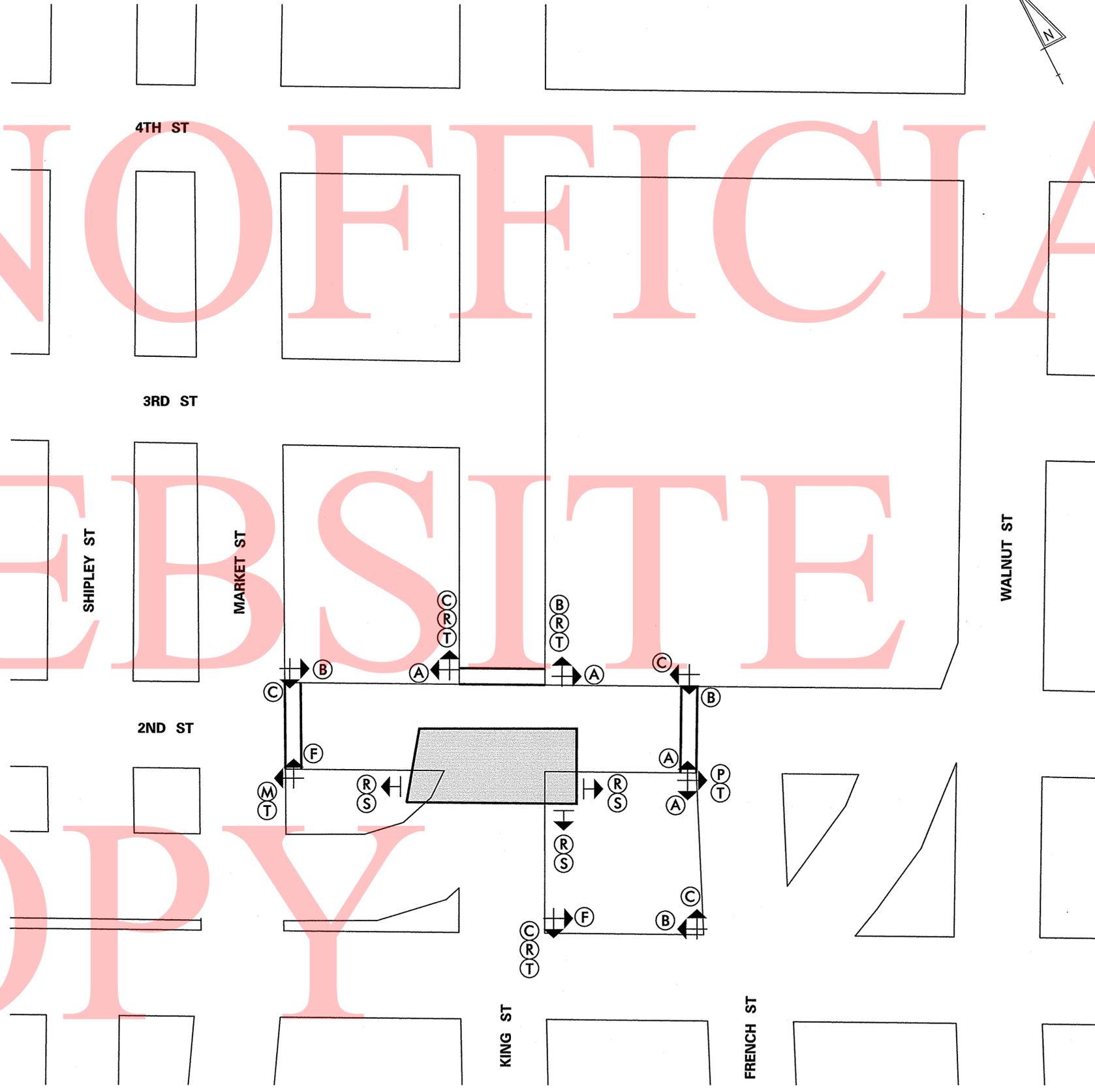
PEDESTRIAN DETOUR PLAN
4TH STREET @ WALNUT STREET

SHEET NO.	40
TOTAL SHTS.	78

UNOFFICIAL

WEBSITE

COPY



(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) ← DETOUR	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) BARRICADE	(T) BARRICADE	

GENERAL NOTES

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
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RECOMMENDED Todd A. O. DATE: 3/18/2011 RECOMMENDED Brian A. Mitchell DATE: 3-21-11 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

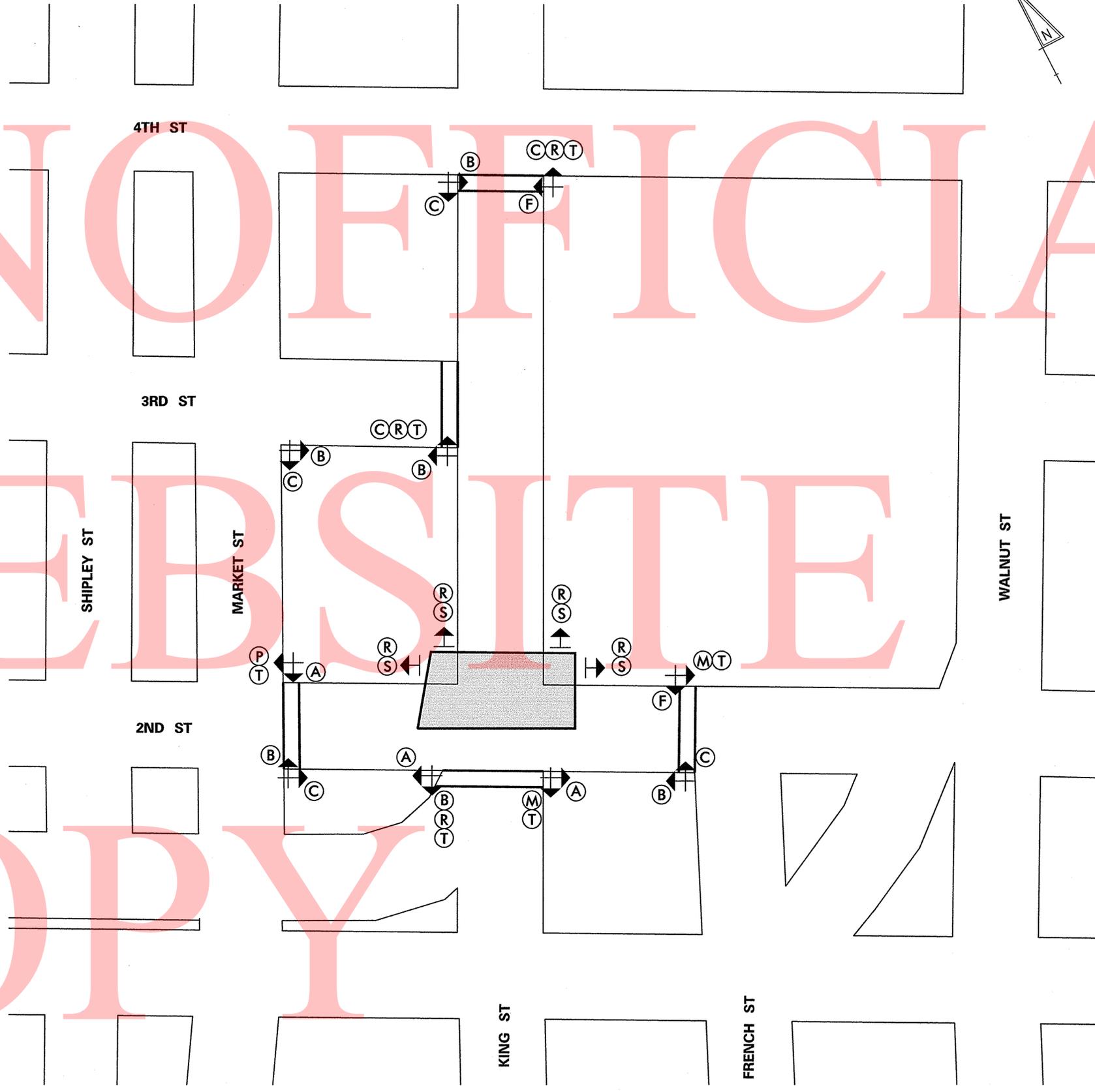
**PEDESTRIAN DETOUR PLAN
2ND STREET @
KING STREET**

SHEET NO. 41
TOTAL SHTS. 78

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

- GENERAL NOTES:**
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RECOMMENDED Todd A. Ovi DATE: 3/18/2011
 RECOMMENDED Bevin A. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013 COUNTY NEW CASTLE PERMIT NO. N/A DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

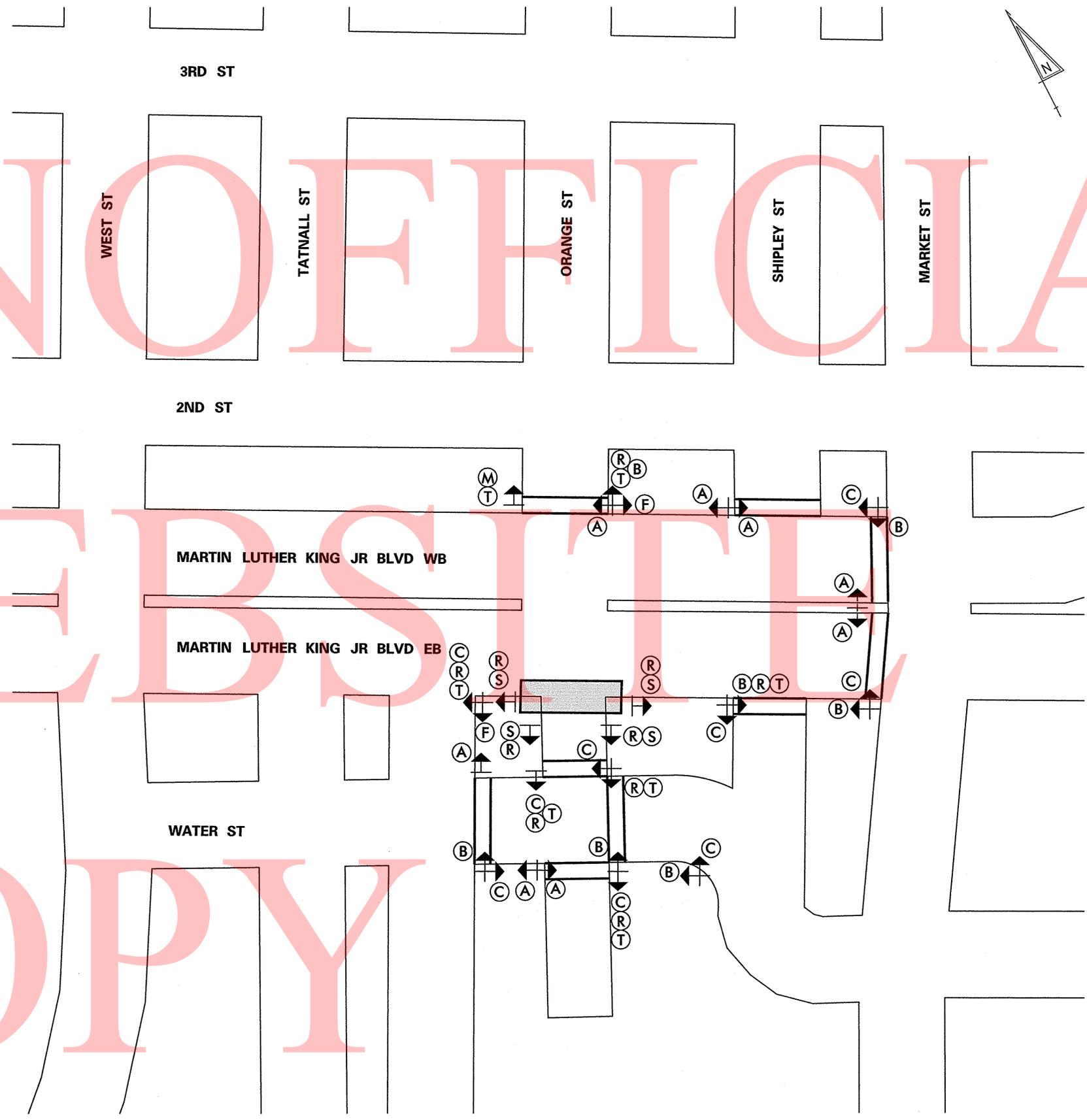
PEDESTRIAN DETOUR PLAN 2ND STREET @ KING STREET

SHEET NO. 42 TOTAL SHTS. 78

UNOFFICIAL

WEBSITES

COPY



LEGEND

(A) DETOUR (up arrow)

(B) DETOUR (left arrow)

(C) DETOUR (right arrow)

(D) DETOUR (up-left arrow)

(E) DETOUR (up-right arrow)

(F) END DETOUR

(G) DETOUR AHEAD

(H) DETOUR 1000 FT

(I) DETOUR 500 FT

(J) ROAD CLOSED AHEAD

(K) ROAD CLOSED 1000 FT

(L) ROAD CLOSED 500 FT

(M) SIDEWALK CLOSED AHEAD CROSS HERE (left arrow)

(N) DETOUR (left arrow)

(O) DETOUR (right arrow)

(P) SIDEWALK CLOSED AHEAD CROSS HERE (right arrow)

(Q) ROAD CLOSED

(R) SIDEWALK CLOSED

(S) BARRICADE (left)

(T) BARRICADE (right)

GENERAL NOTES:

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
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6. *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
7. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Todd A. Ovi DATE: 3/18/2011
 RECOMMENDED Brian Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

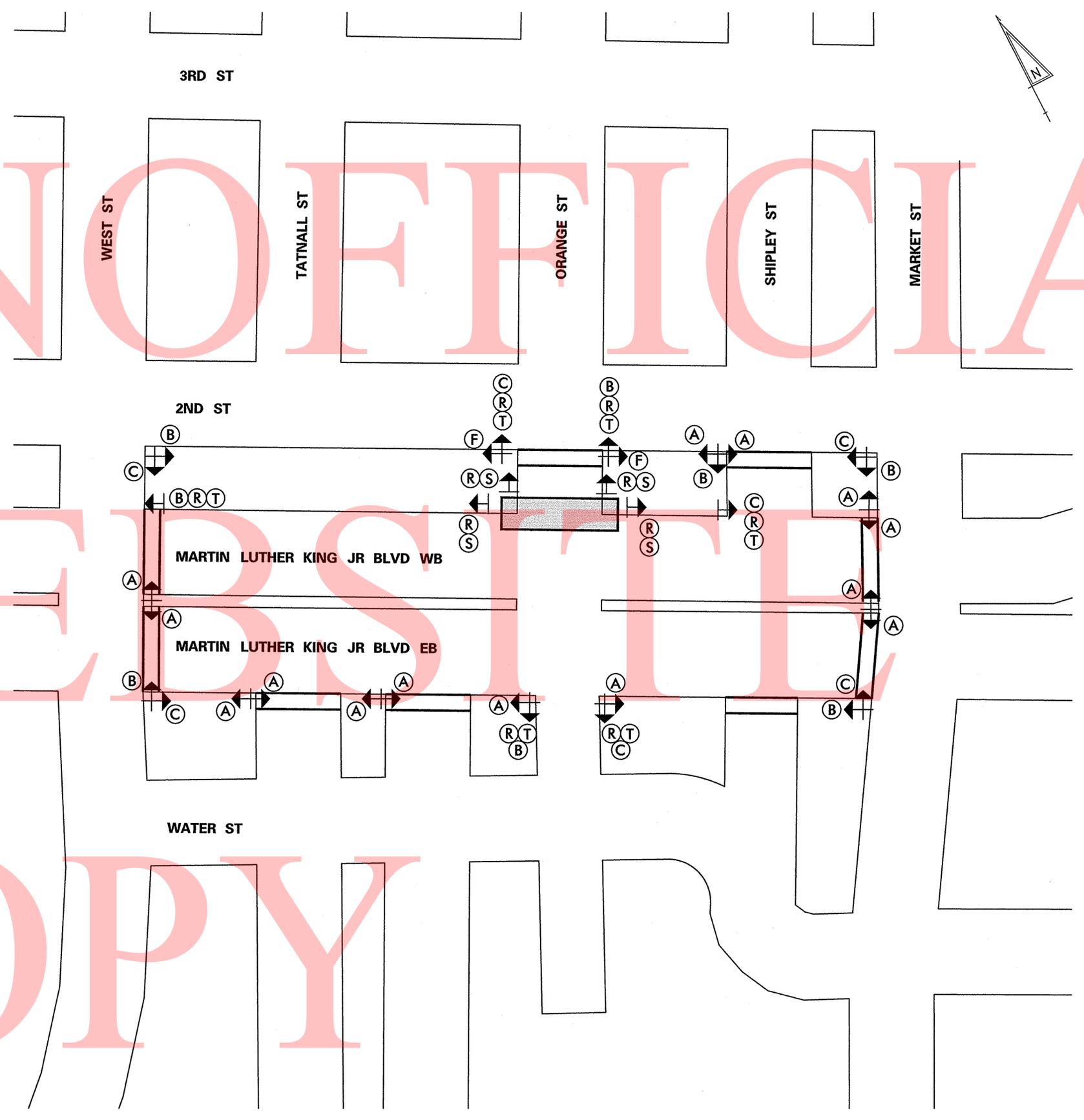
PEDESTRIAN DETOUR PLAN M.L. KING JR. BLVD @ ORANGE STREET

SHEET NO. 43
TOTAL SHTS. 78

UNOFFICIAL

WEBSITES

COPY



LEGEND

(A) DETOUR (up arrow)

(B) DETOUR (left arrow)

(C) DETOUR (right arrow)

(D) DETOUR (down arrow)

(E) DETOUR (up arrow)

(F) END DETOUR

(G) DETOUR AHEAD

(H) DETOUR 1000 FT

(I) DETOUR 500 FT

(J) ROAD CLOSED AHEAD

(K) ROAD CLOSED 1000 FT

(L) ROAD CLOSED 500 FT

(M) SIDEWALK CLOSED AHEAD CROSS HERE (left arrow)

(N) DETOUR (left arrow)

(O) DETOUR (right arrow)

(P) SIDEWALK CLOSED AHEAD CROSS HERE (right arrow)

(Q) ROAD CLOSED

(R) SIDEWALK CLOSED

(S) BARRICADE (left)

(T) BARRICADE (right)

GENERAL NOTES

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RECOMMENDED Todd A. Ovi DATE: 3/18/2011

RECOMMENDED Brian J. Mitchell DATE: 3-21-11

RECOMMENDED _____ DATE: _____

APPROVED CHIEF SAFETY OFFICER _____ DATE: _____

APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

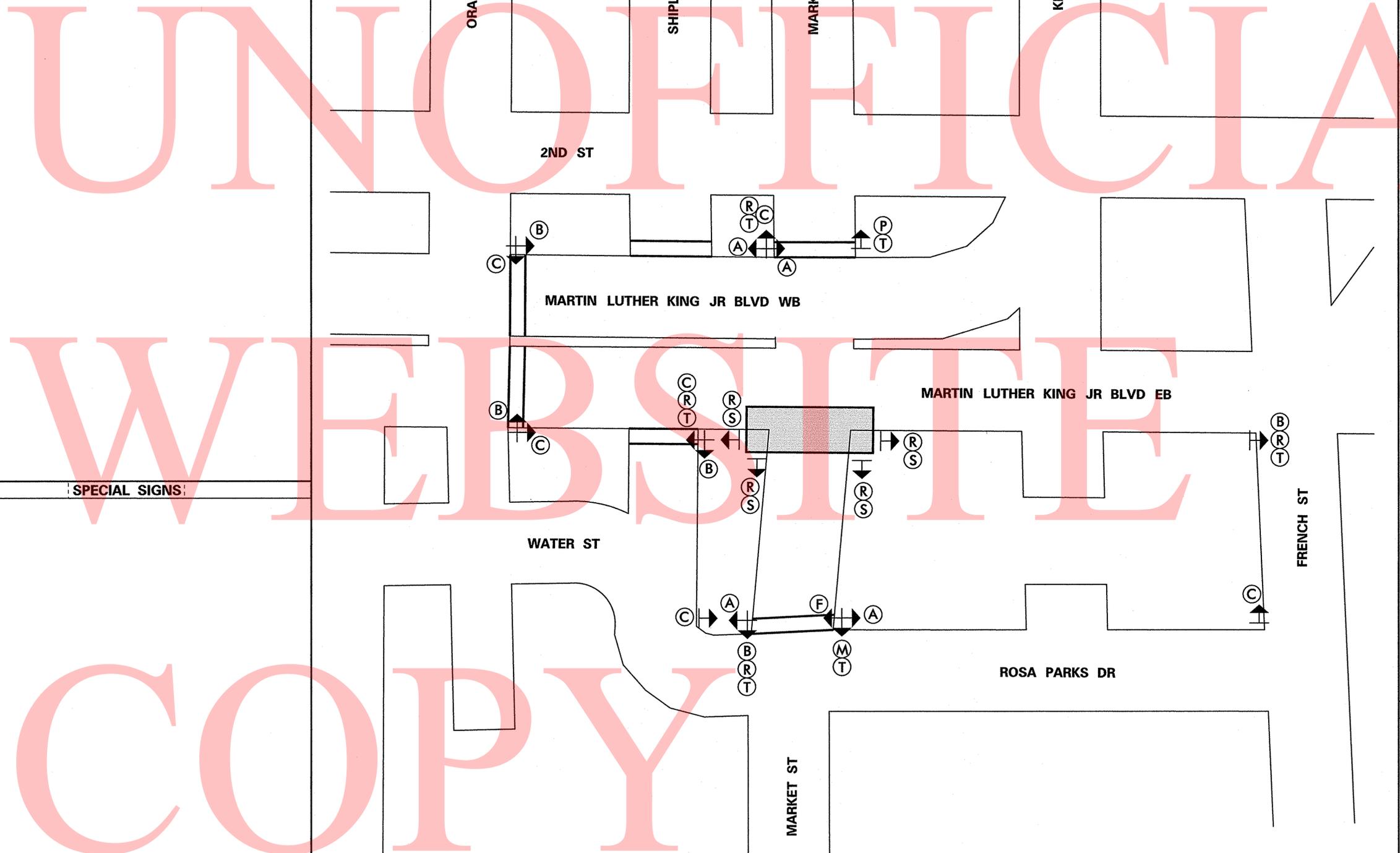
NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

PEDESTRIAN DETOUR PLAN M.L. KING JR. BLVD @ ORANGE STREET

SHEET NO.	44
TOTAL SHTS.	78



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

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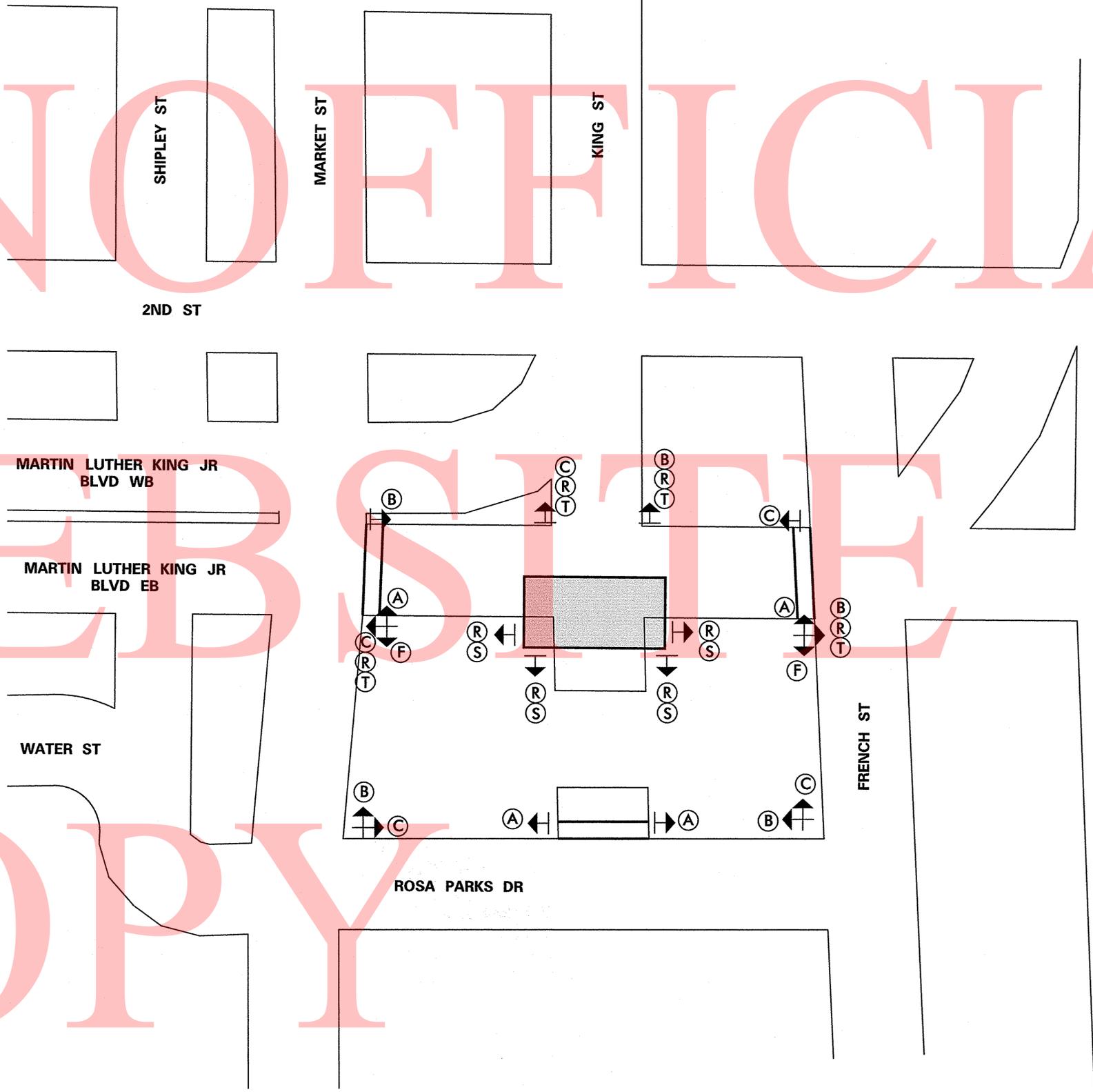
RECOMMENDED Todd A. Ali DATE: 3/18/2011
 RECOMMENDED Brian Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	<p>ADDENDUM / REVISIONS</p>	<p>NOT TO SCALE</p>	<p>WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS</p>	<p>CONTRACT T200920013</p>	<p>PERMIT NO. N/A</p>	<p>PEDESTRIAN DETOUR PLAN M.L. KING JR. BLVD @ MARKET STREET</p>	<p>SHEET NO. 45</p>	
					<p>COUNTY</p>		<p>DESIGNED BY: G.R.P.</p>	<p>TOTAL SHTS. 78</p>
					<p>NEW CASTLE</p>		<p>CHECKED BY: T.A.O.</p>	

UNOFFICIAL

WEBSITE

COPY



LEGEND

(A)	(B)	(C)
(D)	(E)	(F)
(G)	(H)	(I)
(J)	(K)	(L)
(M)	(N)	(O)
(P)	(Q)	(R)
(S)	(T)	

- GENERAL NOTES:**
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RECOMMENDED Todd A. Ovi DATE: 3/18/2011
 RECOMMENDED Prin. Mitchell DATE: 3-21-11
 RECOMMENDED _____ DATE: _____
 APPROVED CHIEF SAFETY OFFICER _____ DATE: _____
 APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P.
	CHECKED BY: T.A.O.

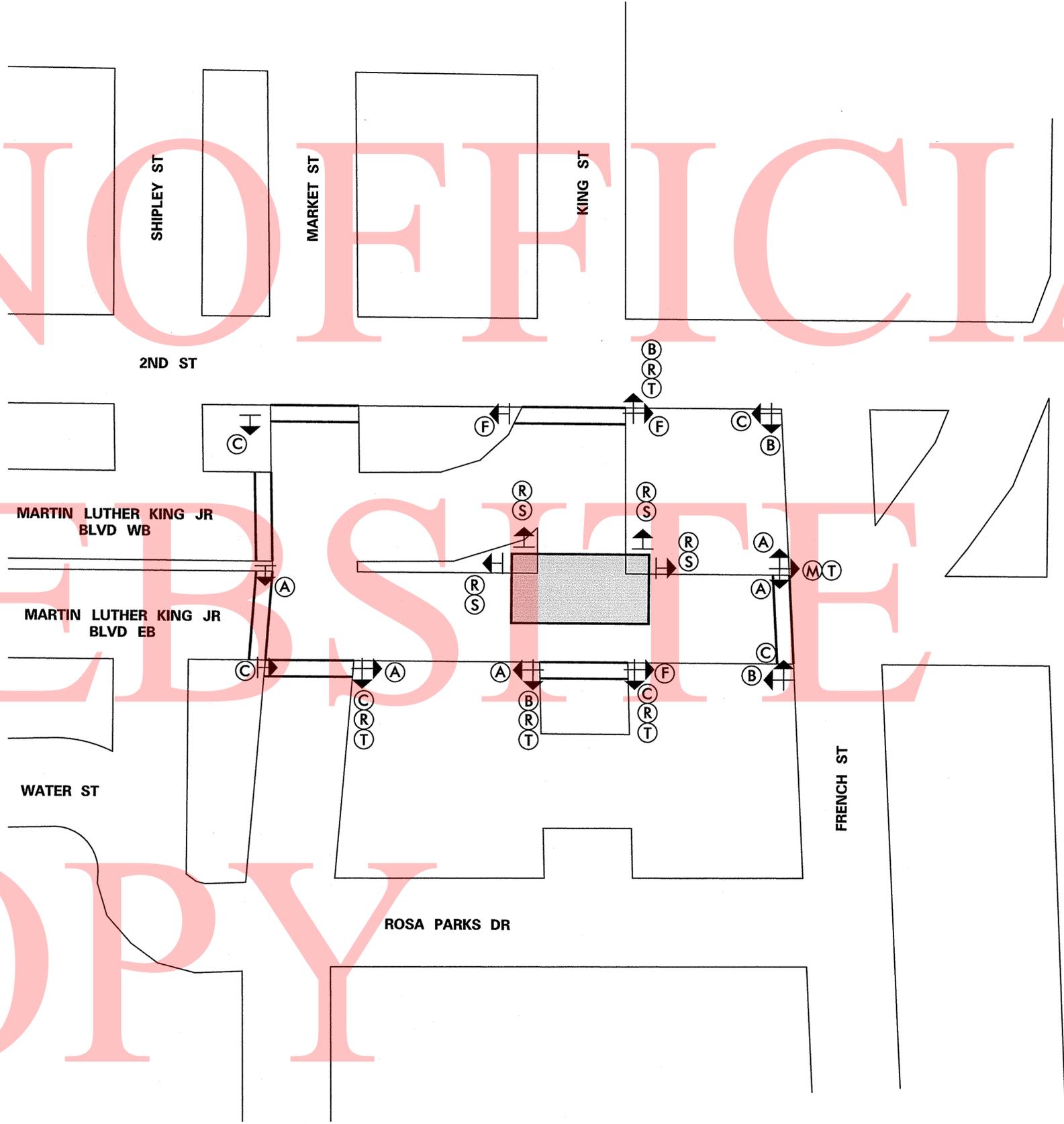
**PEDESTRIAN DETOUR PLAN
M.L. KING JR. BLVD @
KING STREET**

SHEET NO. 46
TOTAL SHTS. 78

UNOFFICIAL

WEBSITE

COPY



(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) ← DETOUR	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) BARRICADE	(T) BARRICADE	

GENERAL NOTES

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6) FOR LIGHTS, BARRICADES AND SIGNS. (AS PER LATEST REVISION)
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES WILL PREVAIL.
- SIGNS J THROUGH L AND P THROUGH R (THE WORD (ROAD) SHOULD BE CHANGED TO RAMP, R/R OR BRIDGE WHERE APPLICABLE).
- WARNING SIGNS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
- *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Tom A. O. DATE: 3/18/2011 RECOMMENDED Ben Mitchell DATE: 3-21-11 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

NOT TO SCALE

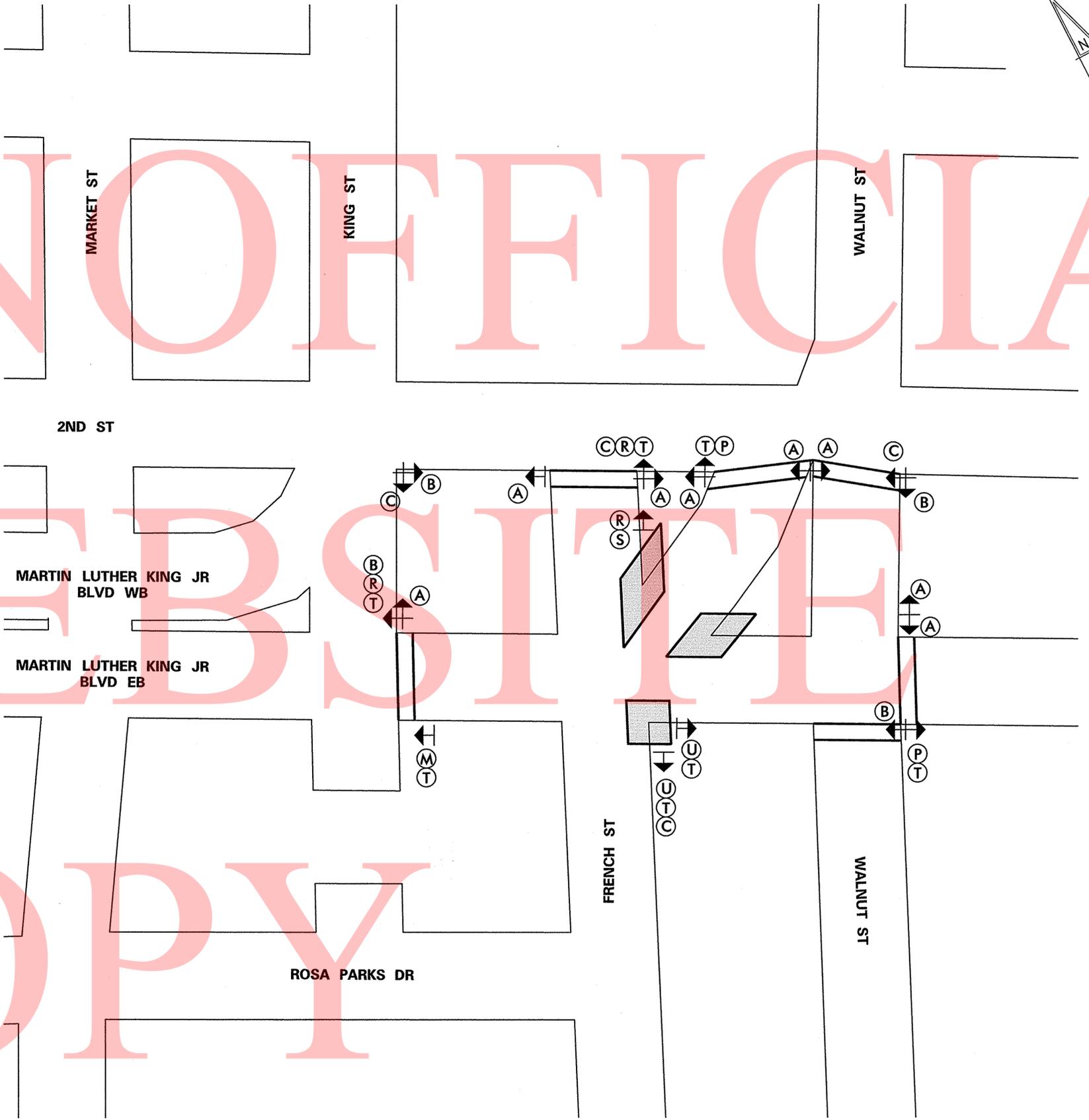
WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT	PERMIT NO.	N/A
T200920013	DESIGNED BY: G.R.P.	
COUNTY	CHECKED BY: T.A.O.	
NEW CASTLE		

PEDESTRIAN DETOUR PLAN M.L. KING JR. BLVD @ KING STREET

SHEET NO.	47
TOTAL SHTS.	78

UNOFFICIAL



SPECIAL SIGNS:



(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) ← DETOUR	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) BARRICADE	(T) BARRICADE	

GENERAL NOTES:

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6) FOR LIGHTS, BARRICADES AND SIGNS. (AS PER LATEST REVISION)
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES WILL PREVAIL.
- SIGNS J THROUGH L AND P THROUGH R, THE WORD (ROAD) SHOULD BE CHANGED TO RAMP, R/R OR BRIDGE WHERE APPLICABLE.
- WARNING SIGNS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
- *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Todd A. O'Neil DATE: 3/18/2011 RECOMMENDED Brian Mitchell DATE: 3-21-11 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: _____

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

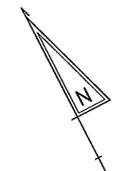
NOT TO SCALE

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

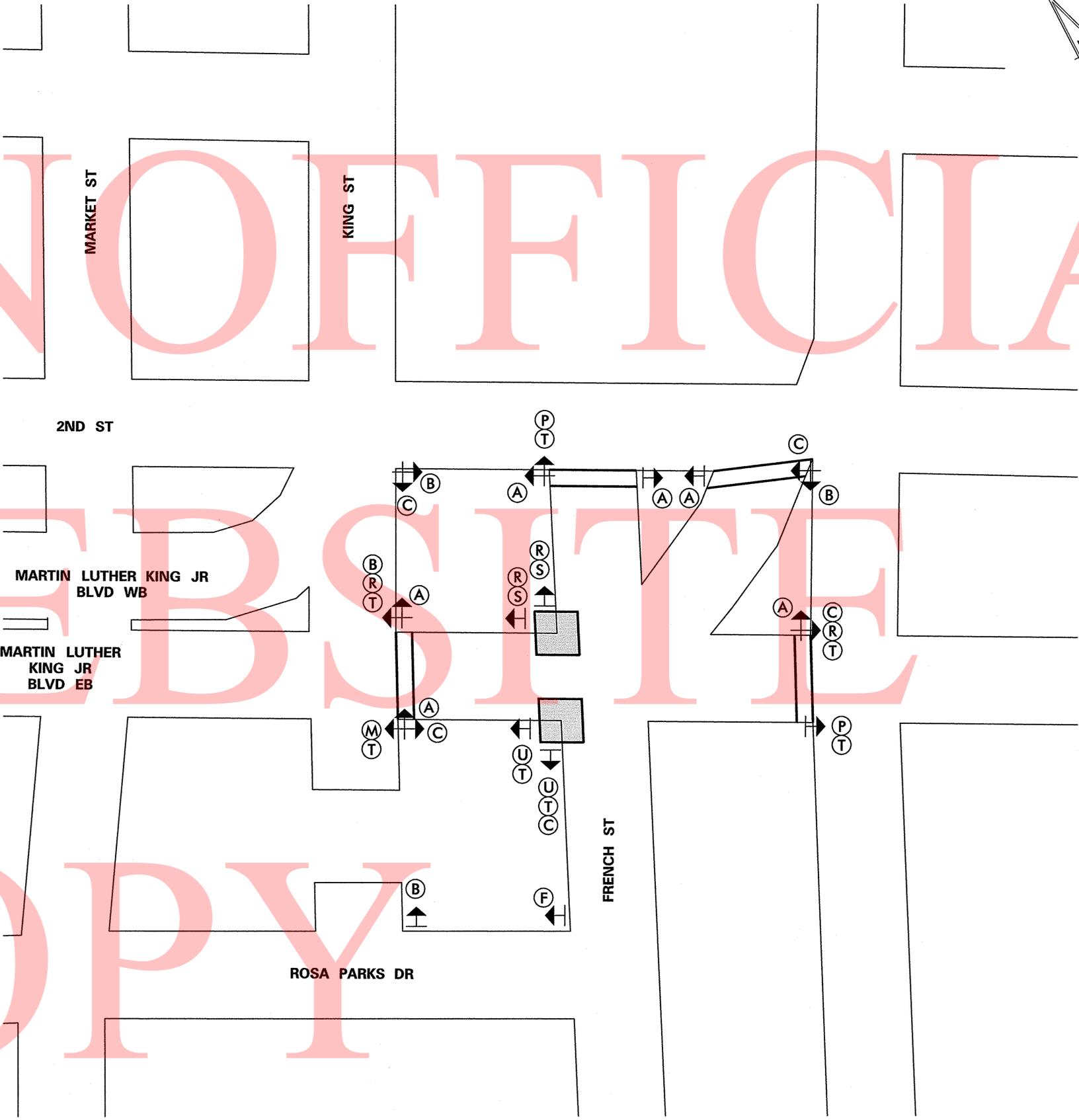
CONTRACT T200920013 COUNTY NEW CASTLE PERMIT NO. N/A DESIGNED BY: G.R.P. CHECKED BY: T.A.O.

PEDESTRIAN DETOUR PLAN M.L. KING JR. BLVD @ FRENCH STREET

SHEET NO. 48 TOTAL SHTS. 78



UNOFFICIAL



SPECIAL SIGNS



SIGN DETAIL

D/G WHITE - BLACK LEGEND

(A) DETOUR ↑	(B) DETOUR ←	(C) DETOUR →
(D) DETOUR ↙	(E) DETOUR ↘	(F) END DETOUR
(G) DETOUR AHEAD	(H) DETOUR 1000 FT	(I) DETOUR 500 FT
(J) ROAD CLOSED AHEAD	(K) ROAD CLOSED 1000 FT	(L) ROAD CLOSED 500 FT
(M) SIDEWALK CLOSED AHEAD ← CROSS HERE	(N) DETOUR ←	(O) DETOUR →
(P) SIDEWALK CLOSED AHEAD → CROSS HERE	(Q) ROAD CLOSED	(R) SIDEWALK CLOSED
(S) BARRICADE	(T) BARRICADE	

GENERAL NOTES

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6).
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6) FOR LIGHTS, BARRICADES AND SIGNS, (AS PER LATEST REVISION)
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES WILL PREVAIL.
- SIGNS J THROUGH L AND P THROUGH R, THE WORD (ROAD) SHOULD BE CHANGED TO RAMP, R/R OR BRIDGE WHERE APPLICABLE.
- WARNING SIGNS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
- *S* BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Todd A. Ovi DATE: 3/18/2011

RECOMMENDED Ben A. Mitchell DATE: 3-21-11

RECOMMENDED _____ DATE: _____

APPROVED CHIEF SAFETY OFFICER _____ DATE: _____

APPROVED TRAFFIC ENGINEER _____ DATE: _____



ADDENDUM / REVISIONS

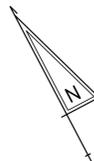
NOT TO SCALE

WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS

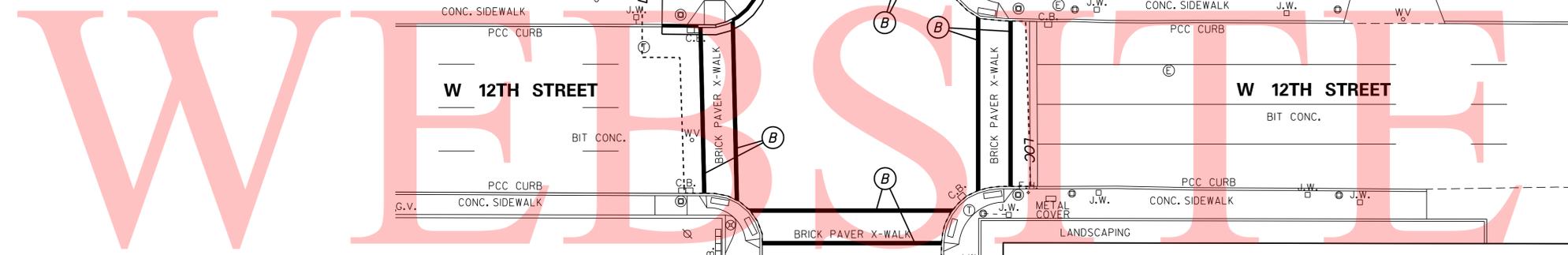
CONTRACT T200920013	PERMIT NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: G.R.P.
	CHECKED BY: T.A.O.

PEDESTRIAN DETOUR PLAN
M.L. KING JR. BLVD @
FRENCH STREET

SHEET NO. 49
TOTAL SHTS. 78



UNOFFICIAL



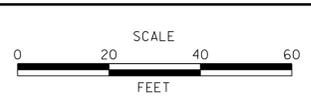
COPY

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(B)	12" WHITE RETROFLECTIVE PREFORMED PATTERNED MARKINGS, 12" (ITEM 748513)	415 LF

No. 21678-006-CADD-SS01-WILM.dgn
 7/22/2011 9:00:00 AM



ADDENDUMS / REVISIONS



WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

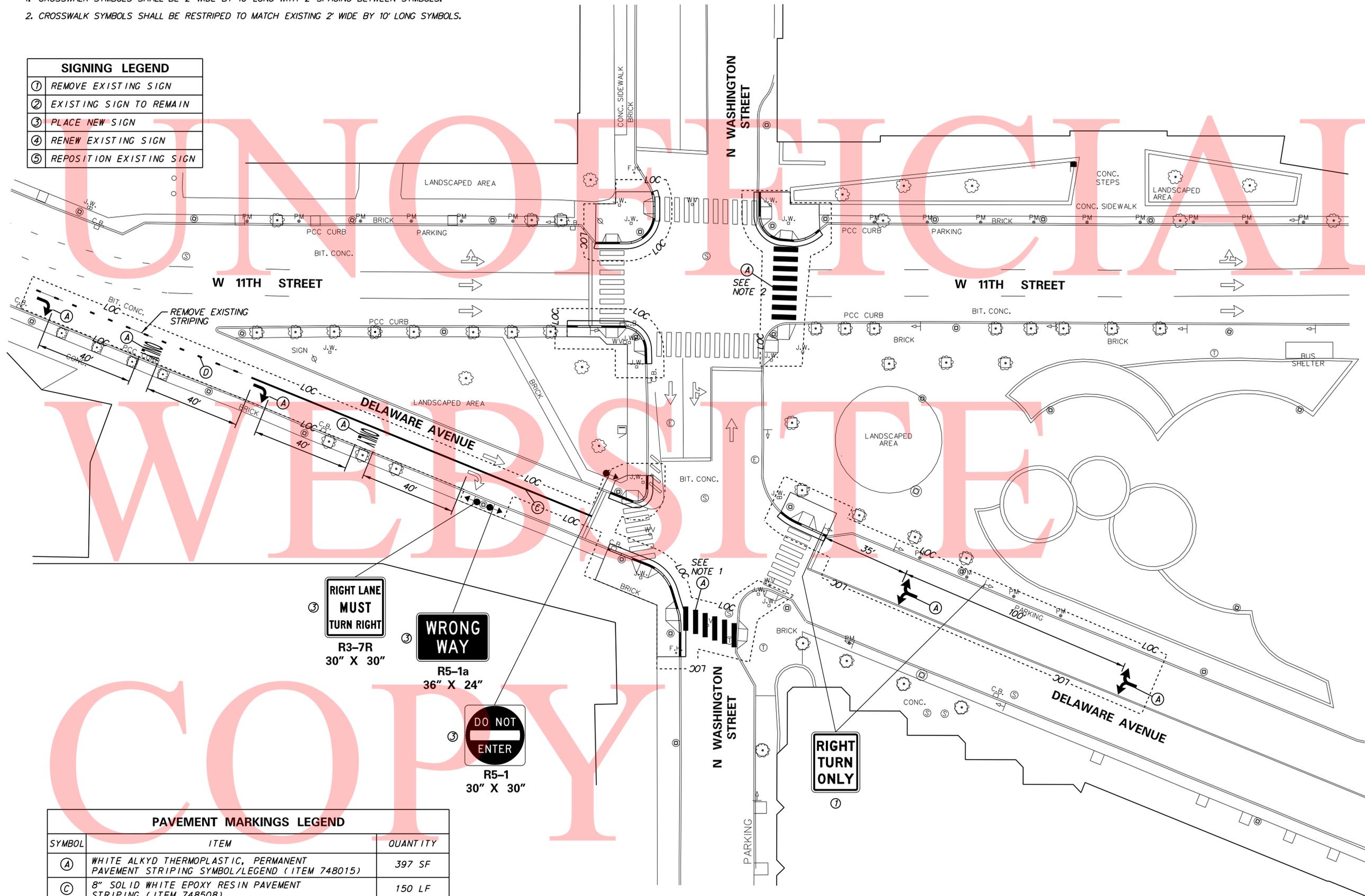
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

SIGNING AND STRIPING PLAN

SS-01
SHEET NO. 50
TOTAL SHTS. 78

- NOTES:**
 1. CROSSWALK SYMBOLS SHALL BE 2' WIDE BY 10' LONG WITH 2' SPACING BETWEEN SYMBOLS.
 2. CROSSWALK SYMBOLS SHALL BE RESTRIPIED TO MATCH EXISTING 2' WIDE BY 10' LONG SYMBOLS.

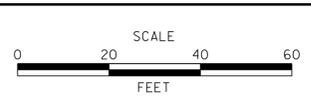
SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	397 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	150 LF
(D)	8" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748508)	100 LF

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ADDENDUMS / REVISIONS

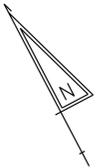


CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

SS-02
SHEET NO. 51
TOTAL SHTS. 78

NOTES:

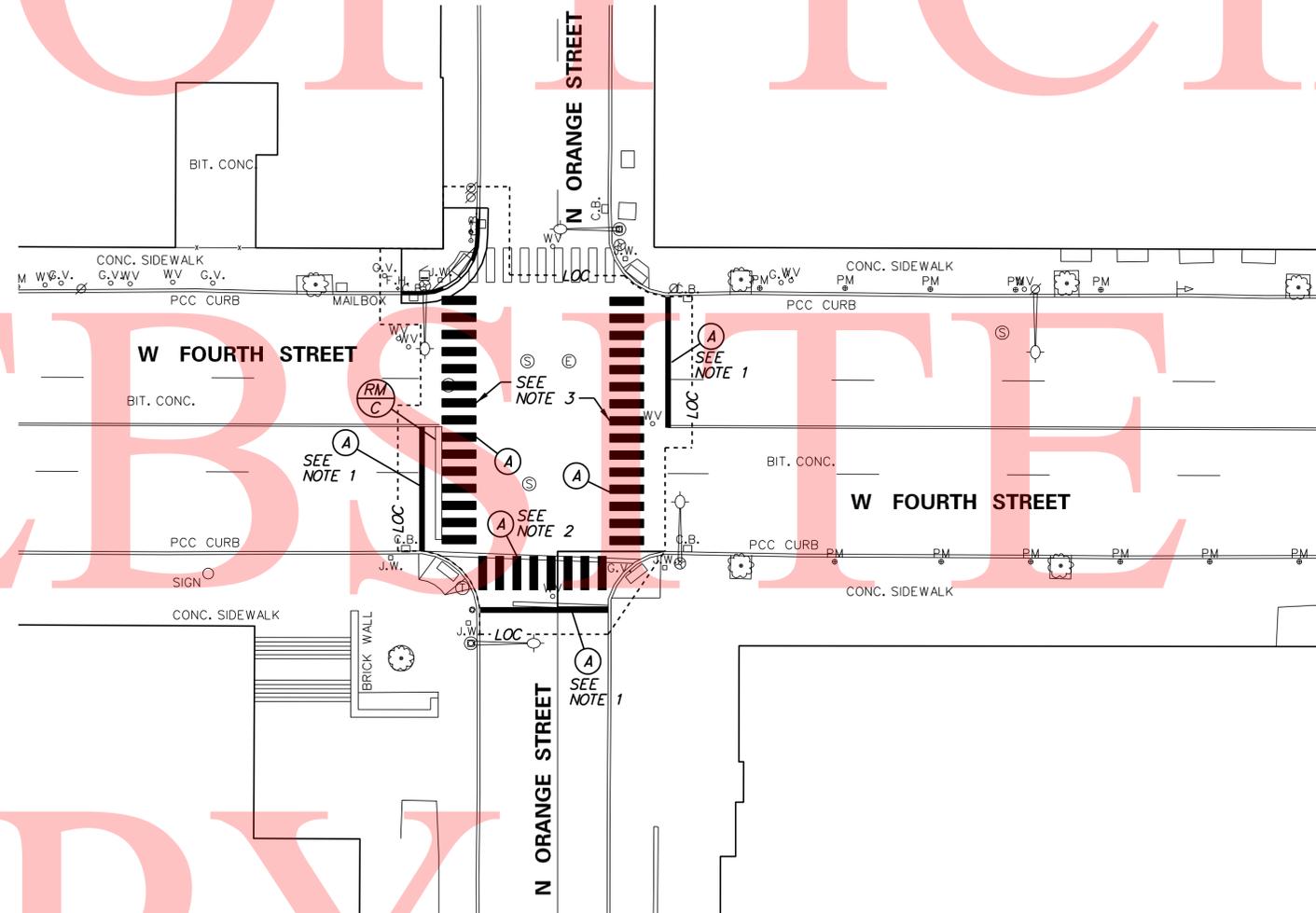
1. PROPOSED STOP LINE SHALL BE 16" WIDE AND PLACED AT A MINIMUM OF 4' BEHIND CROSSWALK.
2. CROSSWALK SYMBOLS SHALL BE 2' WIDE BY 8' LONG WITH 2' SPACING BETWEEN SYMBOLS.
3. CROSSWALK SYMBOLS SHALL BE RESTRIPE TO MATCH EXISTING 2' WIDE BY 8' LONG SYMBOLS.



UNOFFICIAL

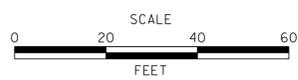
WEB SITE

COPY



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	728 SF

ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

**SIGNING AND
STRIPING PLAN**

SS-03
SHEET NO. 52
TOTAL SHTS. 78

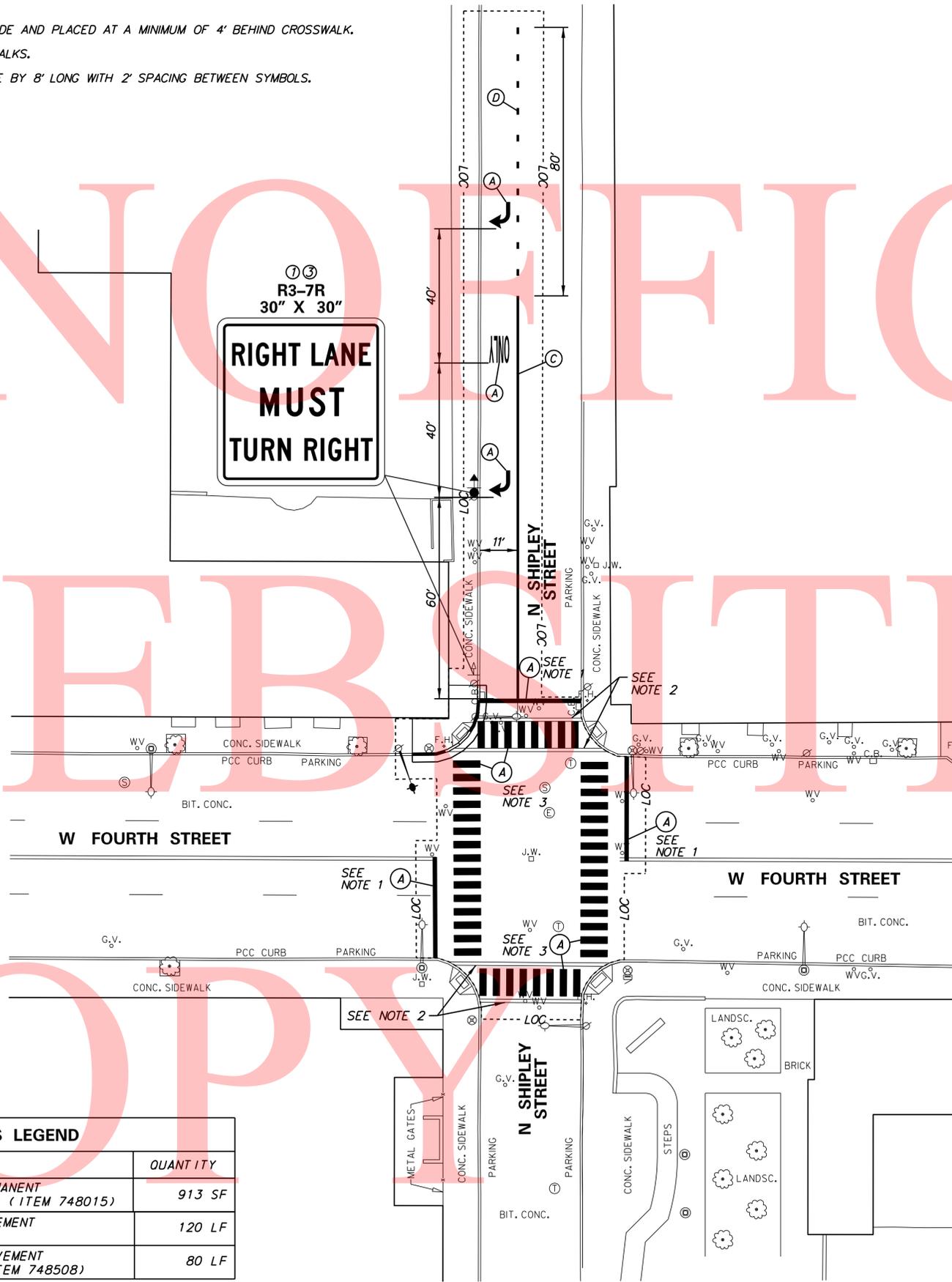
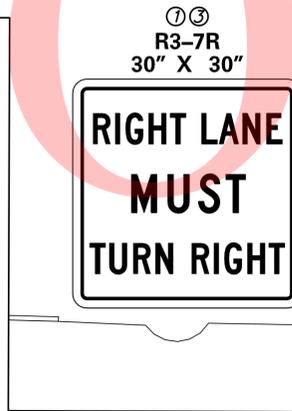
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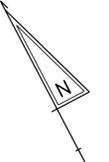
NOTES:

1. PROPOSED STOP LINE SHALL BE 16" WIDE AND PLACED AT A MINIMUM OF 4' BEHIND CROSSWALK.
2. REMOVE EXISTING TRANSVERSE CROSSWALKS.
3. CROSSWALK SYMBOLS SHALL BE 2' WIDE BY 8' LONG WITH 2' SPACING BETWEEN SYMBOLS.

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	913 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	120 LF
(D)	8" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748508)	80 LF



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ADDENDUMS / REVISIONS	

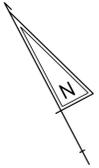


**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

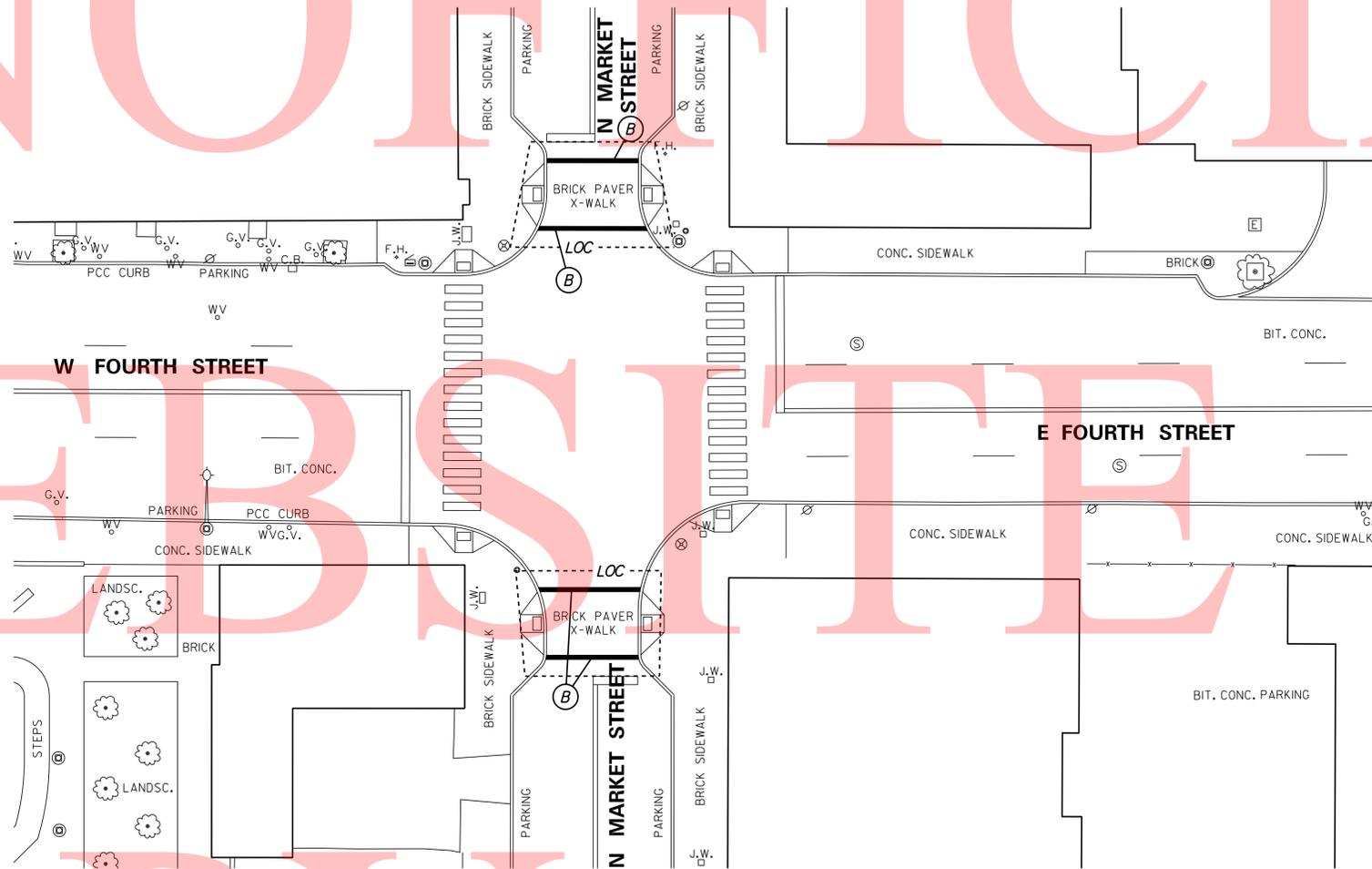
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

**SIGNING AND
STRIPING PLAN**

SS-04
SHEET NO. 53
TOTAL SHTS. 78



UNOFFICIAL



WEB SITE

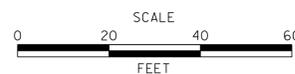
COPY

PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(B)	12" WHITE RETROFLECTIVE PREFORMED PATTERNED MARKINGS (ITEM 748513)	98 LF

ADDENDUMS / REVISIONS

NO.	DATE	DESCRIPTION



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

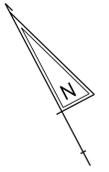
CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**SIGNING AND
STRIPING PLAN**

SS-05
SHEET NO.
54
TOTAL SHTS.
78

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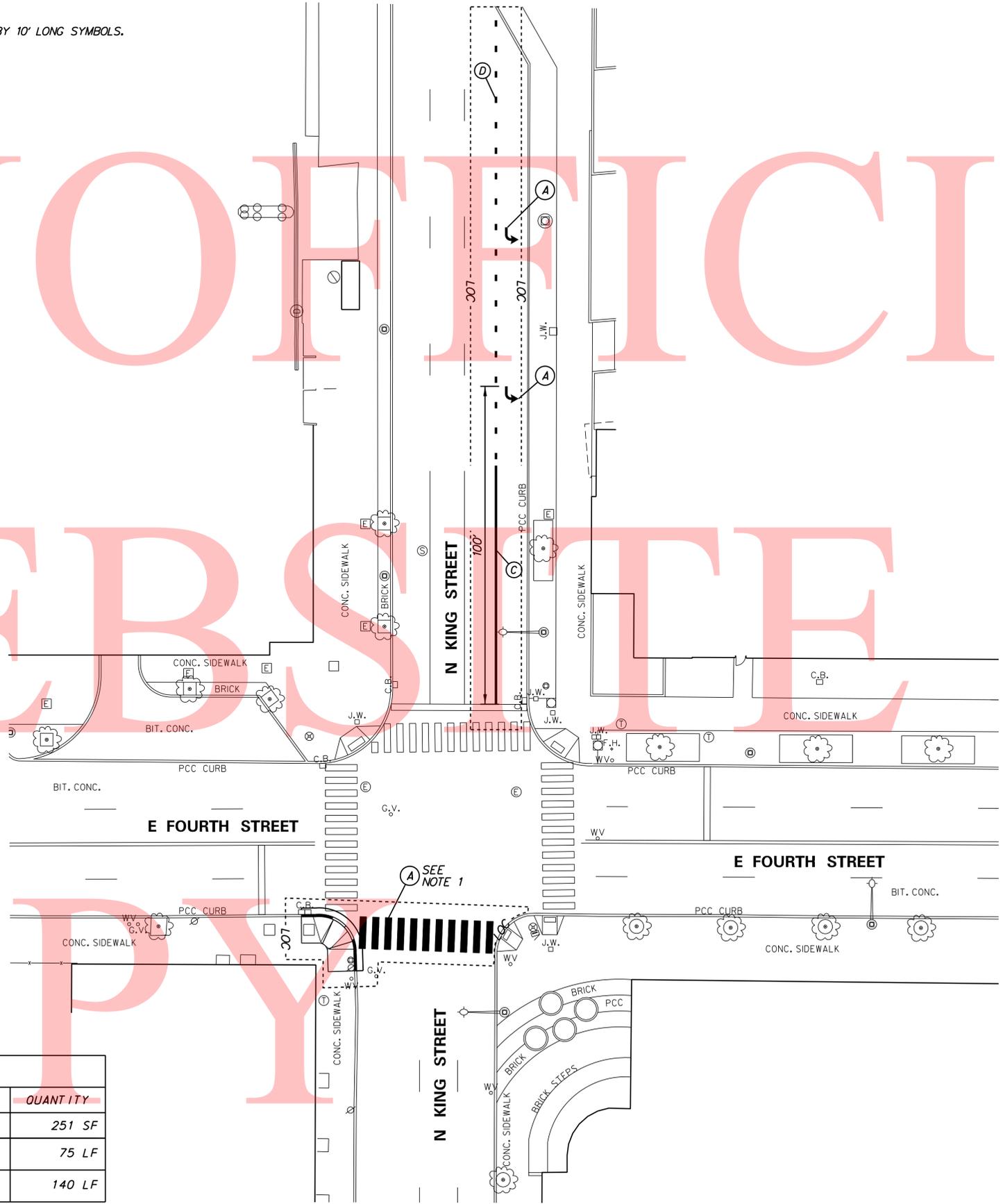
NOTES:
 1. CROSSWALK SYMBOLS SHALL BE RESTRIPEDED TO MATCH EXISTING 2' WIDE BY 10' LONG SYMBOLS.



UNOFFICIAL

WEBSITE

COPY

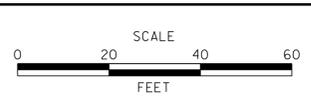


PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	251 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	75 LF
(D)	8" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748508)	140 LF

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ADDENDUMS / REVISIONS	

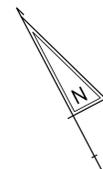


WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O.
	CHECKED BY: T.A.O.

SIGNING AND STRIPING PLAN

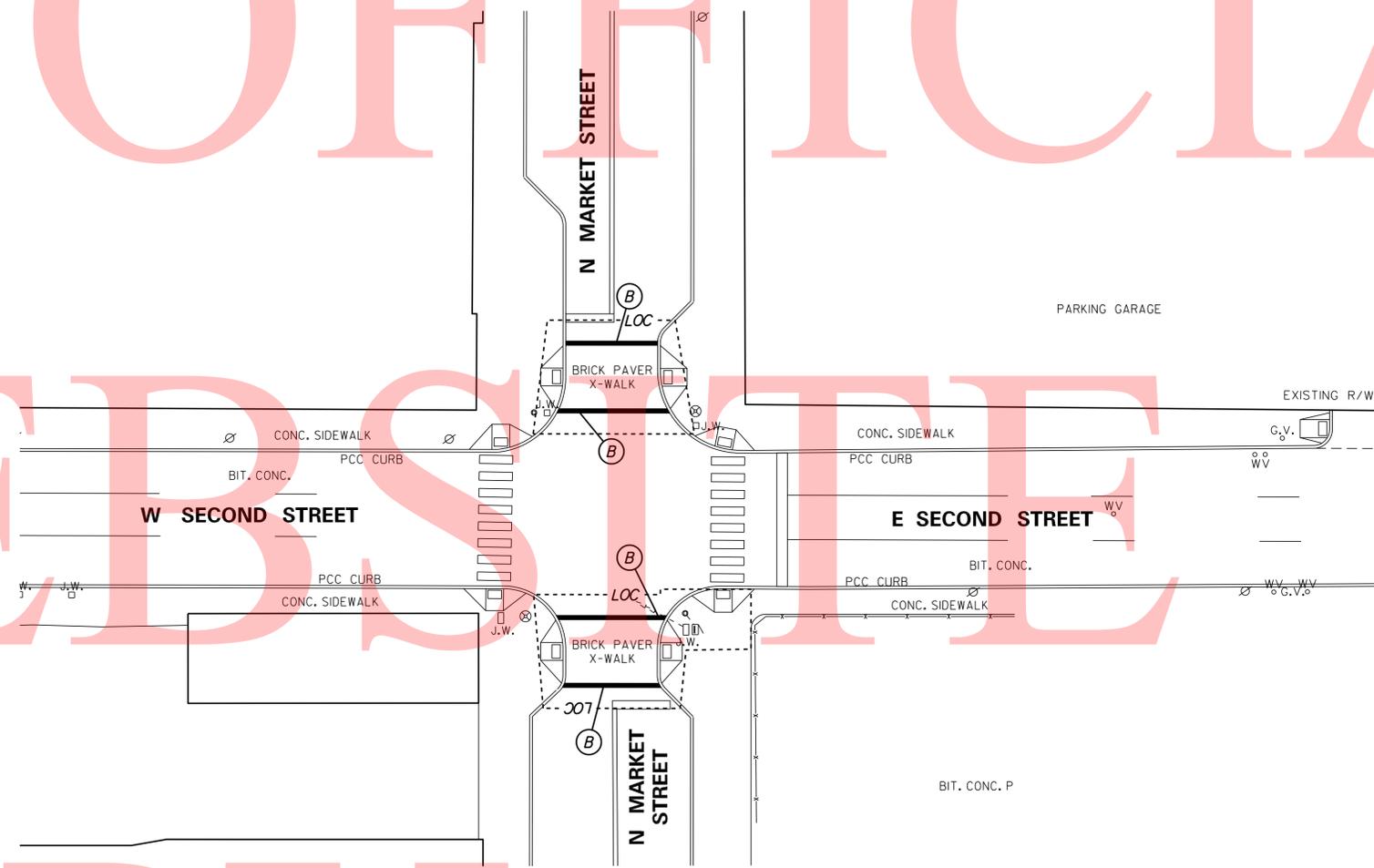
SS-06
SHEET NO. 55
TOTAL SHTS. 78



UNOFFICIAL

WEBSTER

COPY



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(B)	12" WHITE RETROFLECTIVE PREFORMED PATTERNED MARKINGS (ITEM 748513)	99 LF

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**SIGNING AND
STRIPING PLAN**

SS-07
SHEET NO.
56
TOTAL SHTS.
78

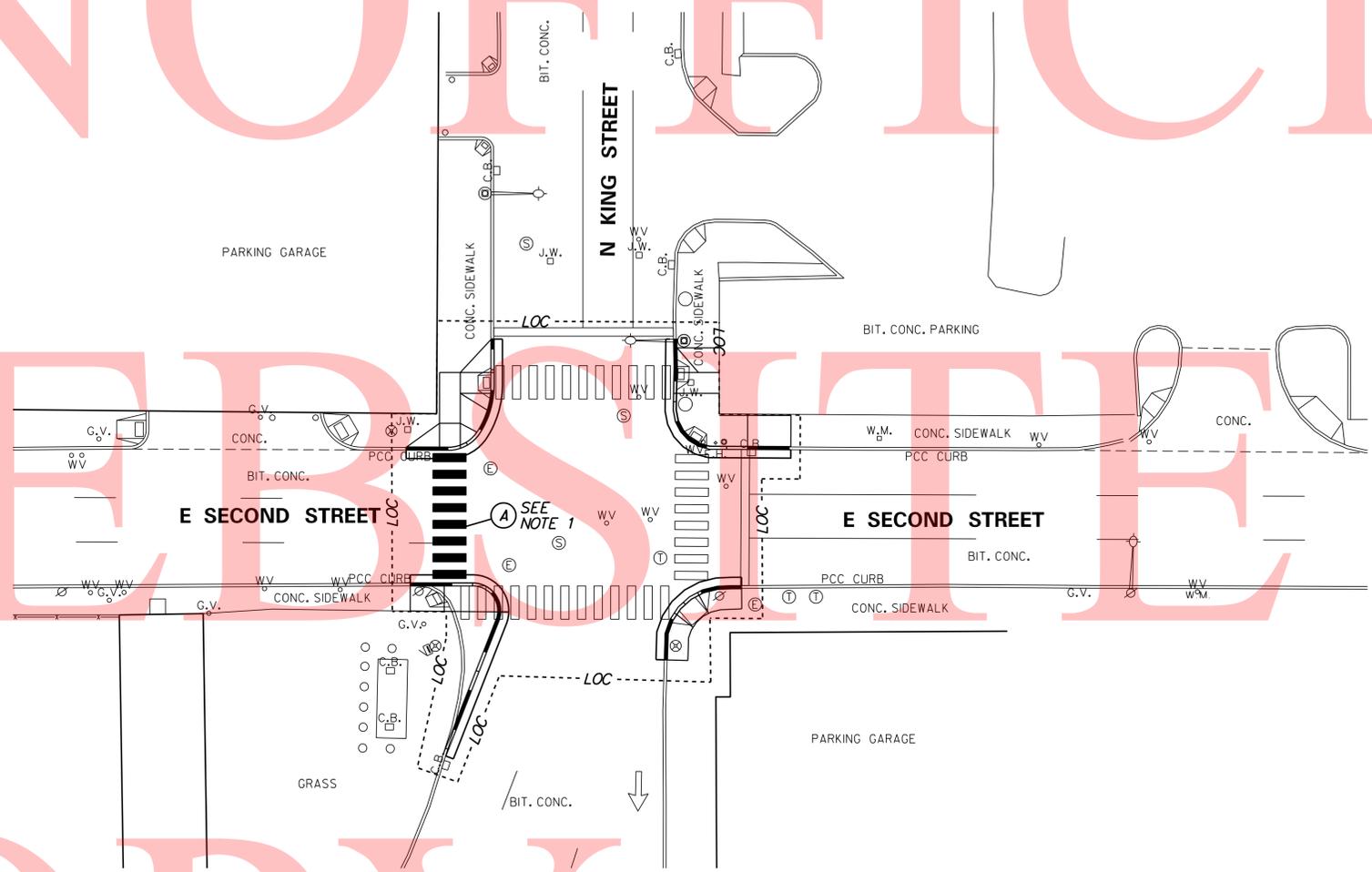
NOTES:
 1. CROSSWALK SYMBOLS SHALL BE 2' WIDE BY 8' LONG WITH 2' SPACING BETWEEN SYMBOLS.



UNOFFICIAL

WEBSTER

COPY



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	128 SF

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ADDENDUMS / REVISIONS	



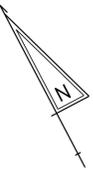
**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

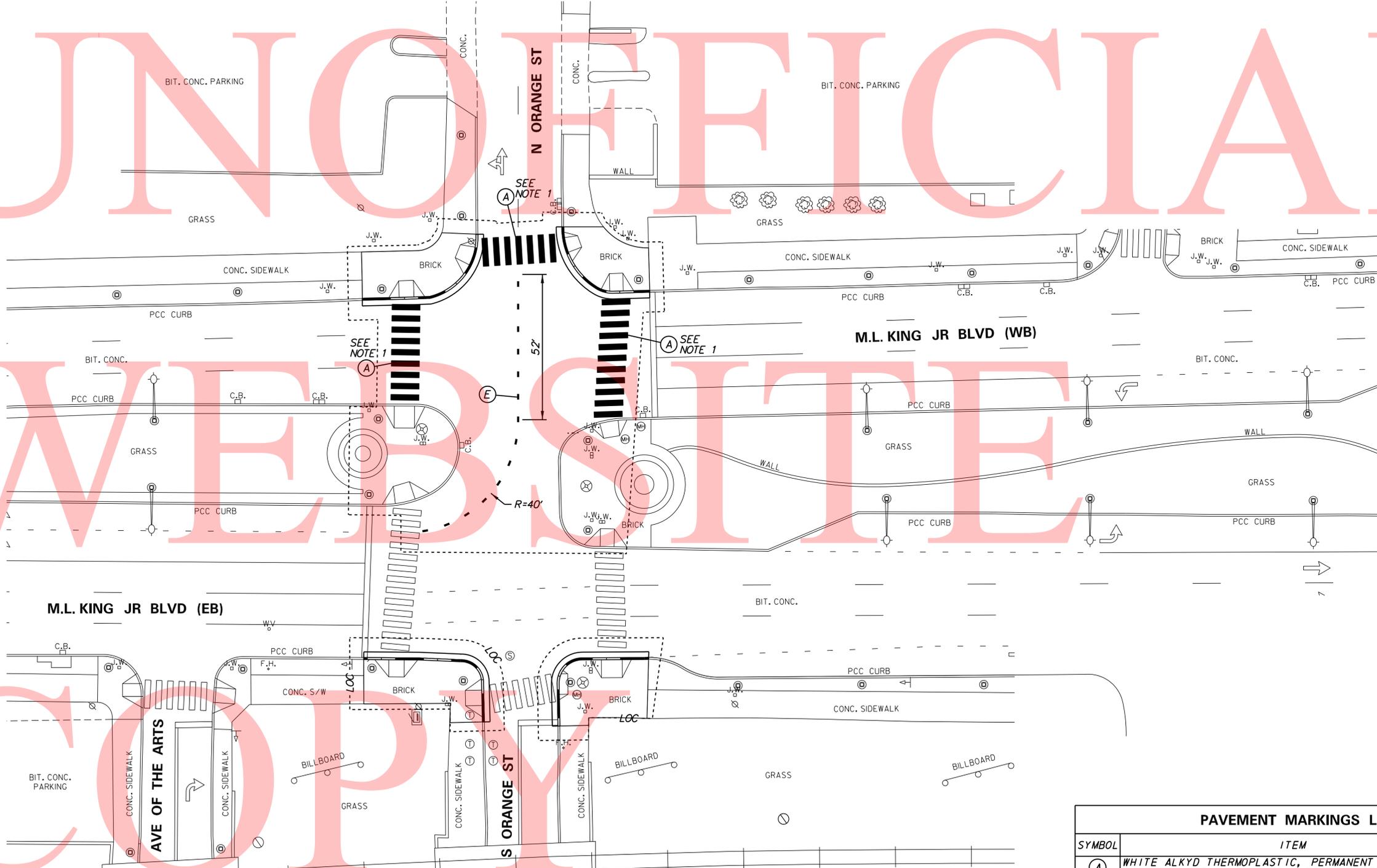
**SIGNING AND
 STRIPING PLAN**

SS-08
SHEET NO.
57
TOTAL SHTS.
78

NOTES:
 1. CROSSWALK SYMBOLS SHALL BE RESTRIPE TO MATCH EXISTING 2' WIDE BY 10' LONG SYMBOLS.



UNOFFICIAL



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	542 SF
(E)	4" DASHED WHITE ALKYD-THERMOPLASTIC PERMANENT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748014)	109 LF

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 2/22/2011 9:05:24 AM



ADDENDUMS / REVISIONS	



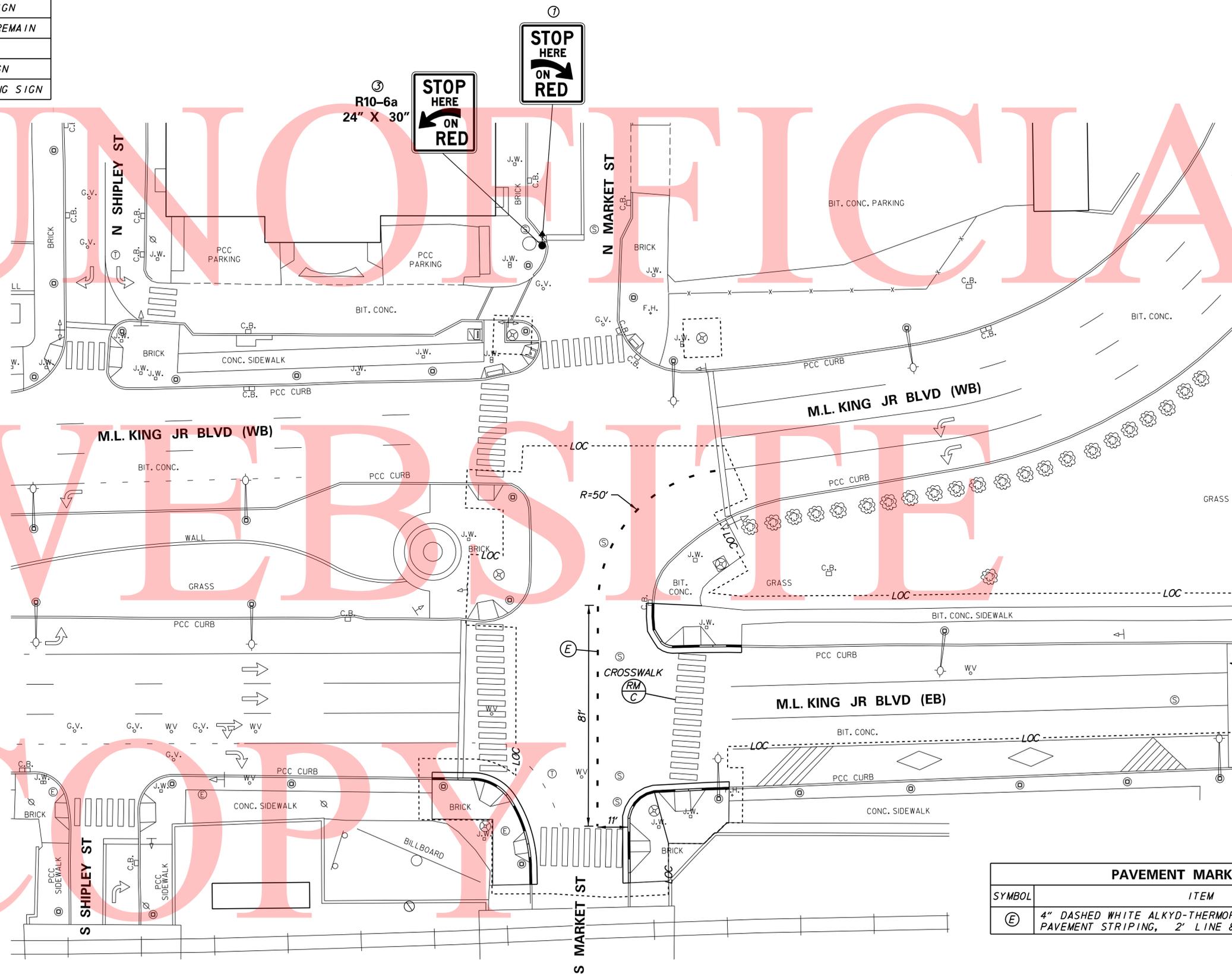
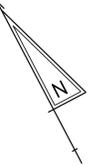
**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**SIGNING AND
 STRIPING PLAN**

SS-09
SHEET NO.
58
TOTAL SHTS.
78

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN



③
R10-6a
24" X 30"



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(E)	4" DASHED WHITE ALKYD-THERMOPLASTIC PERMANENT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748014)	153 LF

SEE SHEET 60 FOR EXTENT OF LOC

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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

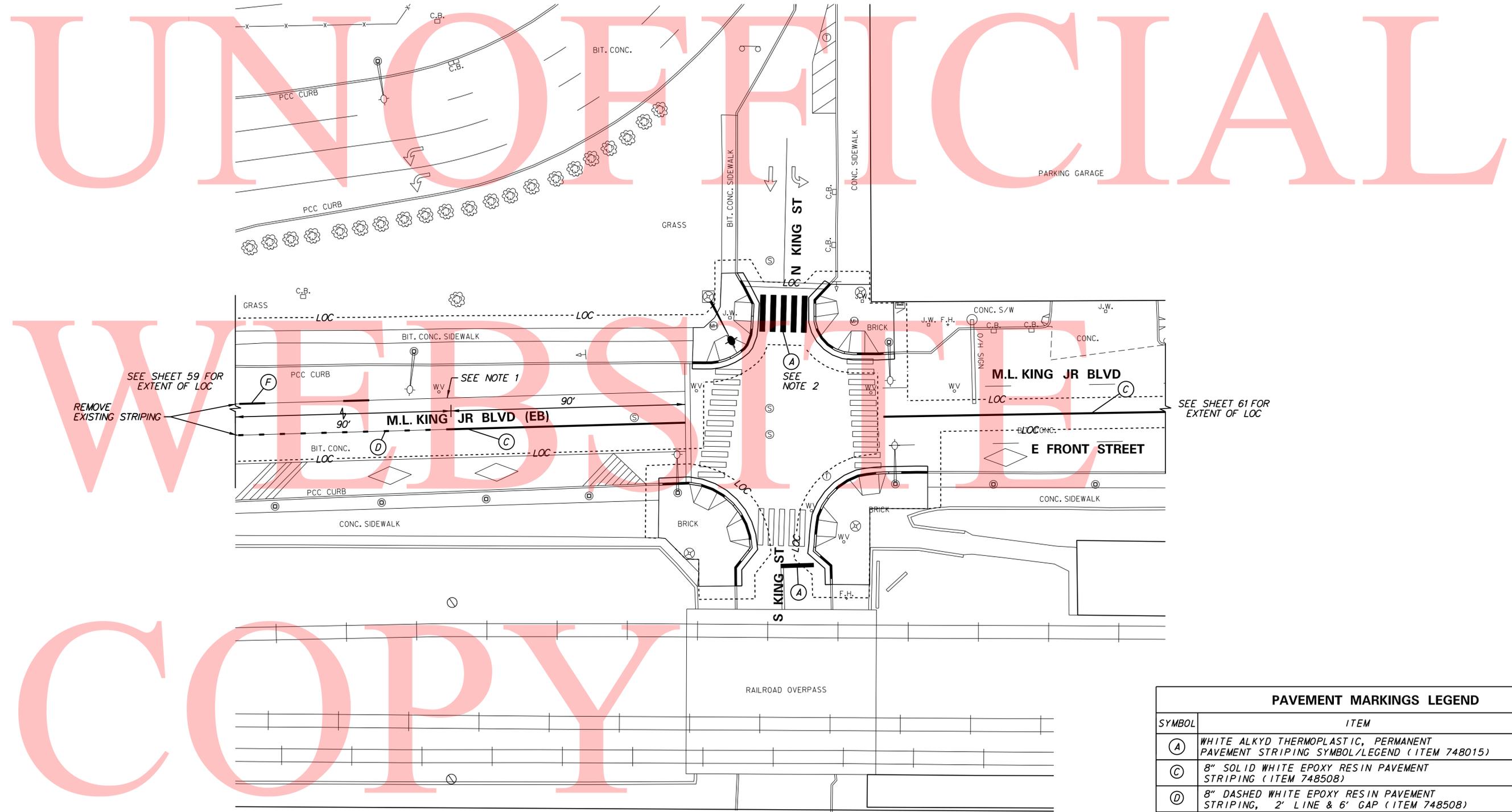
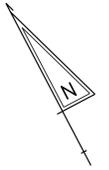
CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

**SIGNING AND
STRIPING PLAN**

SS-10
SHEET NO. 59
TOTAL SHTS. 78

NOTES:

1. EXISTING SOLID WHITE STRIPING TO REMAIN FROM MID-BLOCK TO STOP LINE.
2. CROSSWALK SYMBOLS SHALL BE RESTRIPEDED TO MATCH EXISTING 2' WIDE BY 14' LONG SYMBOLS.



REMOVE EXISTING STRIPING
SEE SHEET 59 FOR EXTENT OF LOC

SEE SHEET 61 FOR EXTENT OF LOC

COPY

PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	159 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	199 LF
(D)	8" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748508)	90 LF
(E)	4" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748506)	90 LF

ADDENDUMS / REVISIONS

NO.	DATE	DESCRIPTION



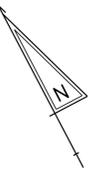
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

**SIGNING AND
STRIPING PLAN**

SS-11
SHEET NO. 60
TOTAL SHTS. 78

NOTES:
 1. CROSSWALK SYMBOLS SHALL BE 2' WIDE BY 8' LONG WITH 2' SPACING BETWEEN SYMBOLS.



UNOFFICIAL

WHEELS

COPY



SEE SHEET 60 FOR EXTENT OF LOC

26-043.10-000
 AMTRAK
 FORMERLY PHILADELPHIA, BALTIMORE
 & WASHINGTON R.R.C.O.
 D.R. N/A

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	220 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	203 LF
(G)	12" WHITE ALKYD-THERMOPLASTIC PERMANENT PAVEMENT STRIPING, (ITEM 748027)	30 LF

No. 21678-006, CAD SS12 - WILM.dgn
 7/22/2011 9:15:59 AM



ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O. CHECKED BY: T.A.O.

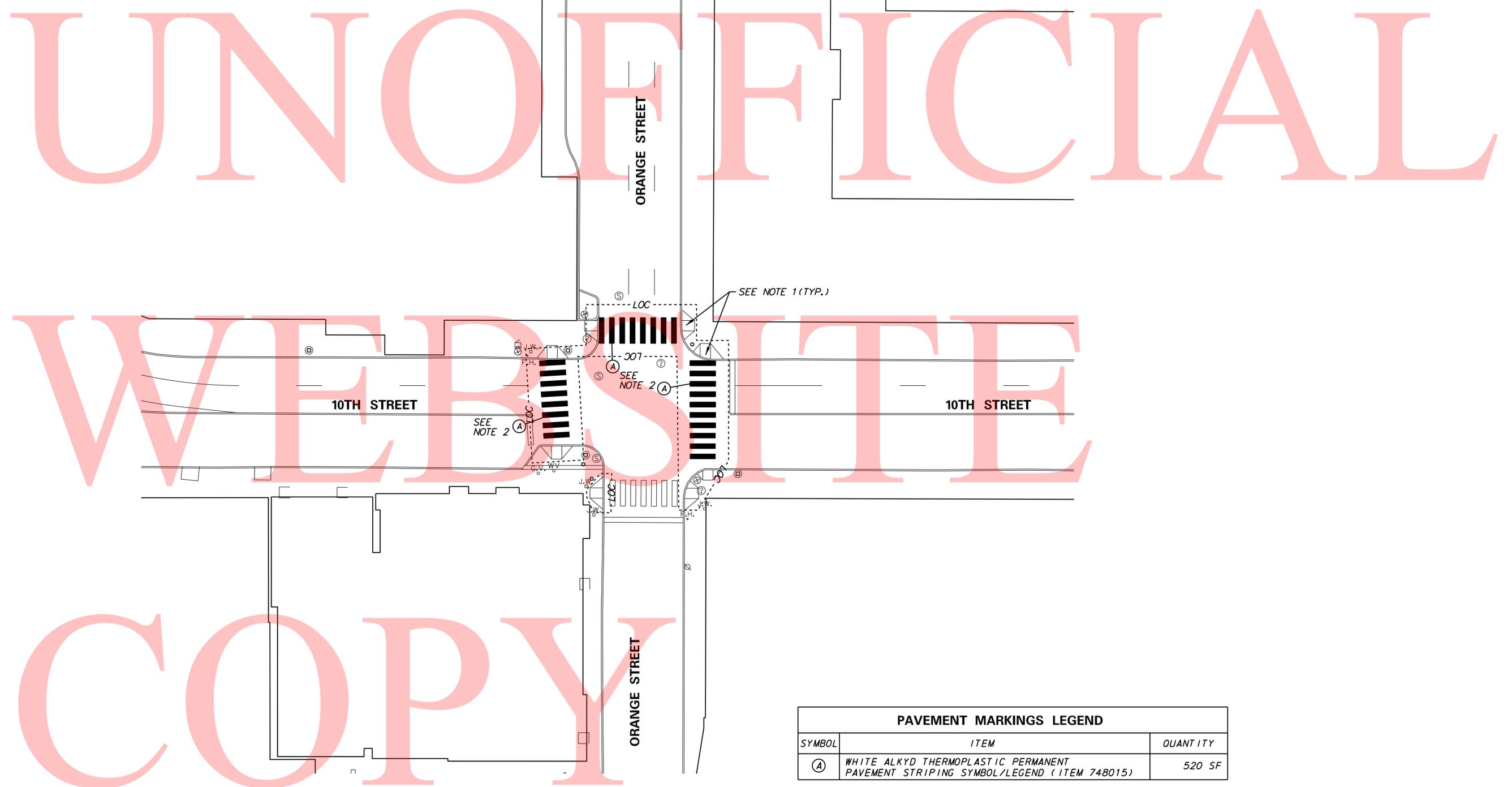
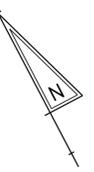
**SIGNING AND
 STRIPING PLAN**

SS-12
SHEET NO. 61
TOTAL SHTS. 78

NOTES:

1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON ALL OF THE EXISTING CURB RAMPS IN COMPLIANCE WITH ADA STANDARDS.

2. CROSSWALK SYMBOLS SHALL BE RESTRIPEDED TO MATCH EXISTING 2' WIDE BY 10' LONG SYMBOLS.



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	520 SF

No. 21678-006.15400.SS13 - WILM.dgn
 7/22/2011 9:52:21 AM



ADDENDUMS / REVISIONS	



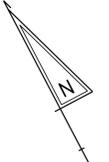
**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

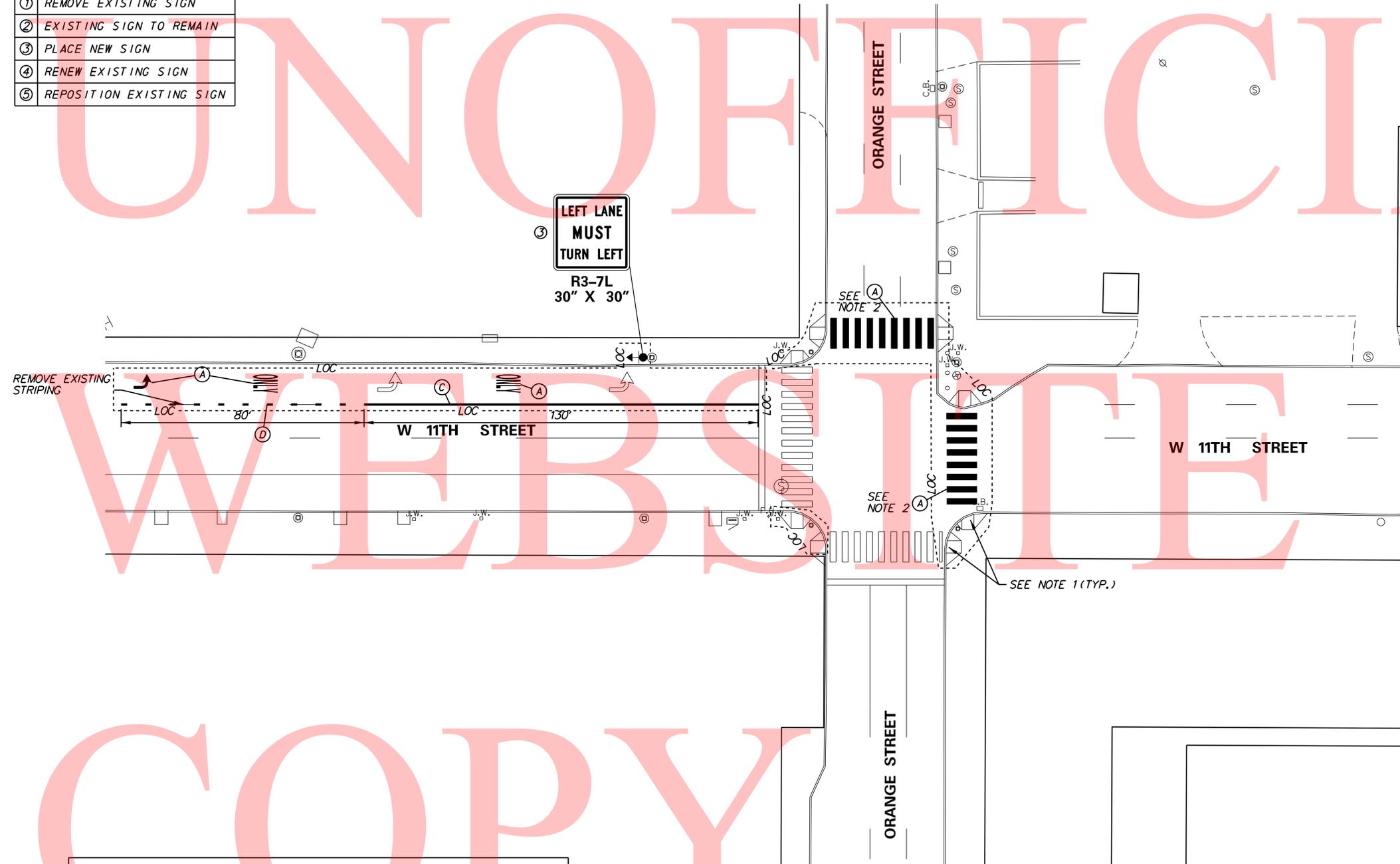
**SIGNING AND
STRIPING PLAN**

SS-13
SHEET NO.
62
TOTAL SHTS.
78

- NOTES:**
1. THE CONTRACTOR SHALL INSTALL DETECTABLE WARNING TRUNCATED DOMES ON ALL OF THE EXISTING CURB RAMP IN COMPLIANCE WITH ADA STANDARDS.
 2. CROSSWALK SYMBOLS SHALL BE RESTRIPE TO MATCH EXISTING 2' WIDE BY 10' LONG SYMBOLS.



SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	WHITE ALKYD THERMOPLASTIC, PERMANENT PAVEMENT STRIPING SYMBOL/LEGEND (ITEM 748015)	402 SF
(C)	8" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748508)	130 LF
(D)	8" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748508)	80 LF

ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**SIGNING AND
STRIPING PLAN**

SS-14
SHEET NO.
63
TOTAL SHTS.
78

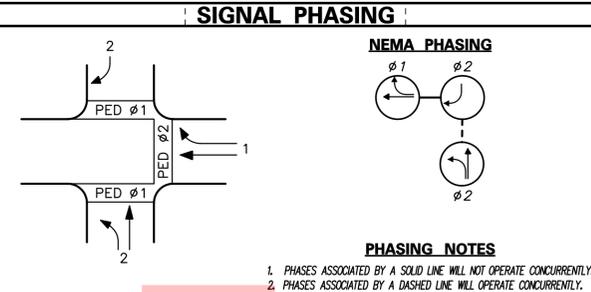
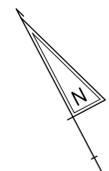
No. 21678-006, 10/00/SS14 - WILM.dgn
 2/22/2011 9:12:43 AM

CONDUIT RUN SCHEDULE					
CO#	* OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.5 IN	10 FT	-	(1) COMM, (6) 9/*14, (2) 4/*18
2*	1	2.5 IN	26 FT	-	(1) COMM, (6) 9/*14, (2) 4/*18
3*	1	2.5 IN	14 FT	-	(1) 9/*14
4*	1	4.0 IN	74 FT	-	(1) COMM, (2) 9/*14, (1) 4/*18
5*	1	2.5 IN	52 FT	-	(3) 9/*14, (1) 4/*18
6*	1	2.5 IN	9 FT	-	(2) 9/*14, (1) 4/*18
7*	1	2.5 IN	15 FT	-	(2) 9/*14, (1) 4/*18
8*	1	2.5 IN	84 FT	-	(1) 9/*14
9*	1	2.5 IN	17 FT	-	(1) 9/*14
10*	1	2.5 IN	5 FT	-	(1) 9/*14, (1) 4/*18
11*	1	4.0 IN	XX FT	-	(1) COMM

* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

NOTES:

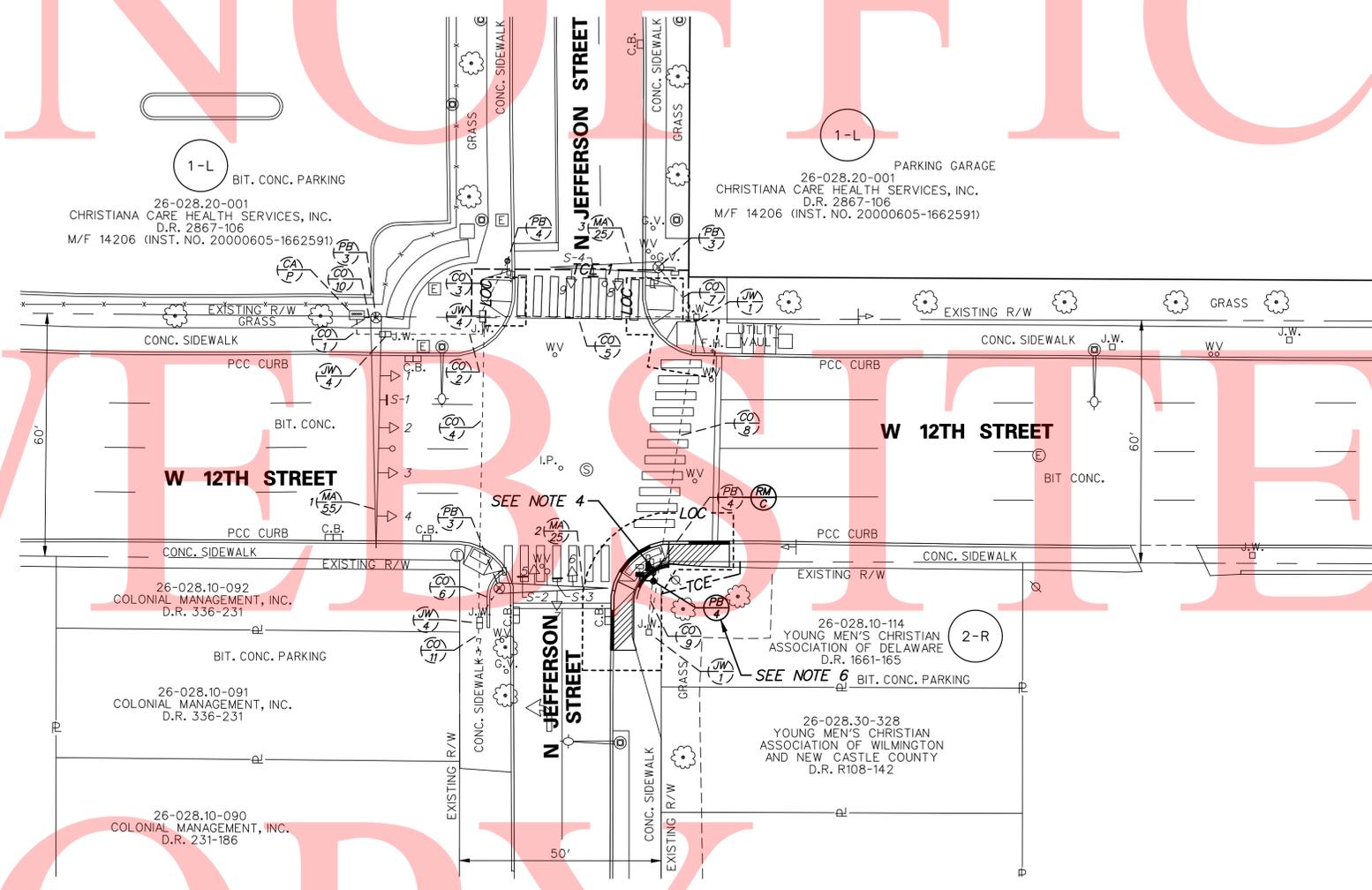
- DELDOT TRAFFIC SHALL INSTALL THE PROPOSED PEDESTRIAN COUNTDOWN SIGNAL HEADS ON PROPOSED POST TOP-MOUNT TWO-WAY ASSEMBLY ON THE PROPOSED ORNAMENTAL PEDESTRIAN PEDESTAL POLE. THE PROPOSED PEDESTRIAN SIGNALS SHALL BE WIRED WITH EXISTING (2) 9/*14 CABLES THROUGH EXISTING CONDUIT AND EXISTING JUNCTION WELL.
- THE PROJECT CONTRACTOR SHALL CONSTRUCT A NEW POLE BASE, TYPE 4 AND CONNECT TO EXISTING CONDUIT RUN #9. THE COST OF POSITIONING THE NEW POLE BASE IS INCIDENTAL TO ITEM 763501, CONSTRUCTION ENGINEERING.
- DELDOT TRAFFIC SHALL RELOCATE THE EXISTING R5-1 SIGN TO THE PROPOSED PEDESTRIAN PEDESTAL POLE IN ACCORDANCE WITH ITEM NO. 746897.



LEGEND			
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY DELDOT TRAFFIC
	PROPOSED SIGNAL POLE BASE		ABANDON
	EXISTING SIGNAL POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTRIAN POLE BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTRIAN POLE BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING UTILITY POLE		PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	EXISTING JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	EXISTING SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED PEDESTRIAN PUSHBUTTON		EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING PEDESTRIAN PUSHBUTTON		PROPOSED SPAN WIRE
	PROPOSED VIDEO DETECTION		EXISTING SPAN WIRE
	EXISTING VIDEO DETECTION		RIGHT-OF-WAY OR PROPERTY LINE
	PROPOSED MICROWAVE DETECTION		PROPOSED SPAN INSULATOR
	EXISTING MICROWAVE DETECTION		EXISTING SPAN INSULATOR
	PROPOSED OVERHEAD SIGNING		SERVICE PEDESTAL
	EXISTING OVERHEAD SIGNING		
	PROPOSED OPTICOM RECEIVER		
	EXISTING OPTICOM RECEIVER		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)		

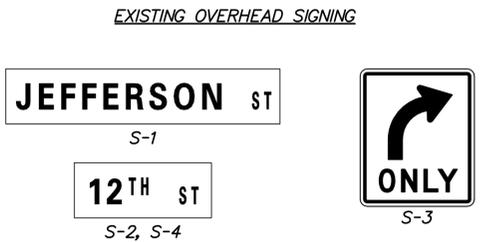
GENERAL SIGNAL NOTES

- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLE BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OVERHEAD SIGNS, PEDESTRIAN SIGNAL DISPLAYS, AND POLES.



MAST ARM SCHEDULE						
MA#	HEIGHT OF POLE	LENGTH OF ARM	* OF HEADS	* OPTICOM RECEIVERS	SE OF SIGNING	ARM MOUNT HEIGHT
1*	21'-6"	55 FT	4	1	5.0 SF	20'-0"
2*	21'-6"	25 FT	3	1	7.5 SF	20'-0"
3*	21'-6"	25 FT	2	1	2.5 SF	20'-0"

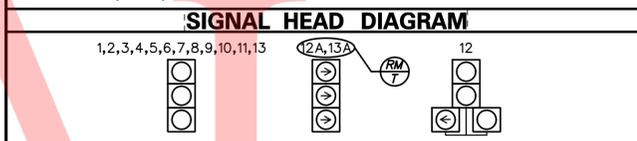
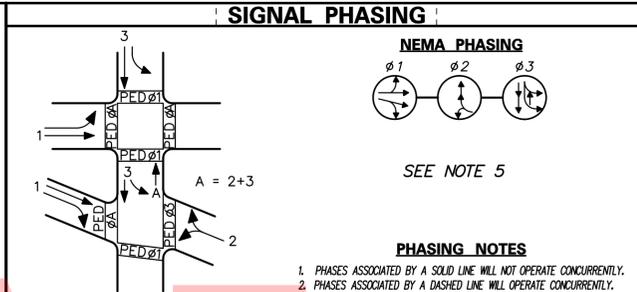
*DENOTES EXISTING MAST ARM



CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	1.0 IN	34 FT	-	(1) 2/*8 U.F. W/GROUND
2*	2	2.5 IN	34 FT	-	(2) COMM, (9) 9/*14, (2) 4/*18
3*	1	2.5 IN	4 FT	-	(1) 9/*14
4*	1	4.0 IN	48 FT	-	(1) COMM, (6) 9/*14, (2) 4/*18
5*	1	2.5 IN	75 FT	-	(1) 2/*8 U.F. W/GROUND, (2) 9/*14
6*	1	2.5 IN	15 FT	-	(1) 9/*14
7*	1	2.5 IN	13 FT	-	(1) 9/*14
8*	1	2.5 IN	64 FT	-	(4) 9/*14, (2) 4/*18
9*	1	2.5 IN	16 FT	-	(1) 9/*14
10*	1	2.5 IN	6 FT	-	(3) 9/*14, (2) 4/*18
11*	1	2.5 IN	10 FT	-	(1) 9/*14
12*	1	2.5 IN	15 FT	-	(1) 9/*14
13*	1	2.5 IN	16 FT	-	(1) 9/*14, (1) 4/*18
14*	1	4.0 IN	XX FT	-	(1) COMM
15*	1	1.0 IN	XX FT	-	(1) 2/*8 U.F. W/GROUND
16*	1	4.0 IN	XX FT	-	(1) COMM
17*	2	2.5 IN	24 FT	-	(1) 2/*8 U.F. W/GROUND, (1) COMM, (7) 9/*14, (3) 4/*18
18*	1	2.5 IN	10 FT	-	(1) 9/*14, (1) 4/*18
19*	1	2.5 IN	5 FT	-	(1) 9/*14
20*	1	2.5 IN	38 FT	-	(4) 9/*14, (2) 4/*18
21*	1	4.0 IN	55 FT	-	(1) 2/*8 U.F. W/GROUND, (1) COMM, (1) 9/*14
22*	1	2.5 IN	12 FT	-	(1) 9/*14
23*	1	2.5 IN	53 FT	-	(3) 9/*14, (2) 4/*18
24*	1	2.5 IN	8 FT	-	(3) 9/*14, (2) 4/*18
25*	1	2.5 IN	44 FT	-	(1) 2/*8 U.F. W/GROUND
26*	1	2.5 IN	20 FT	-	(1) 9/*14
27*	1	4.0 IN	XX FT	-	(1) COMM
28*	1	2.5 IN	XX FT	-	(1) 2/*8 U.F. W/GROUND

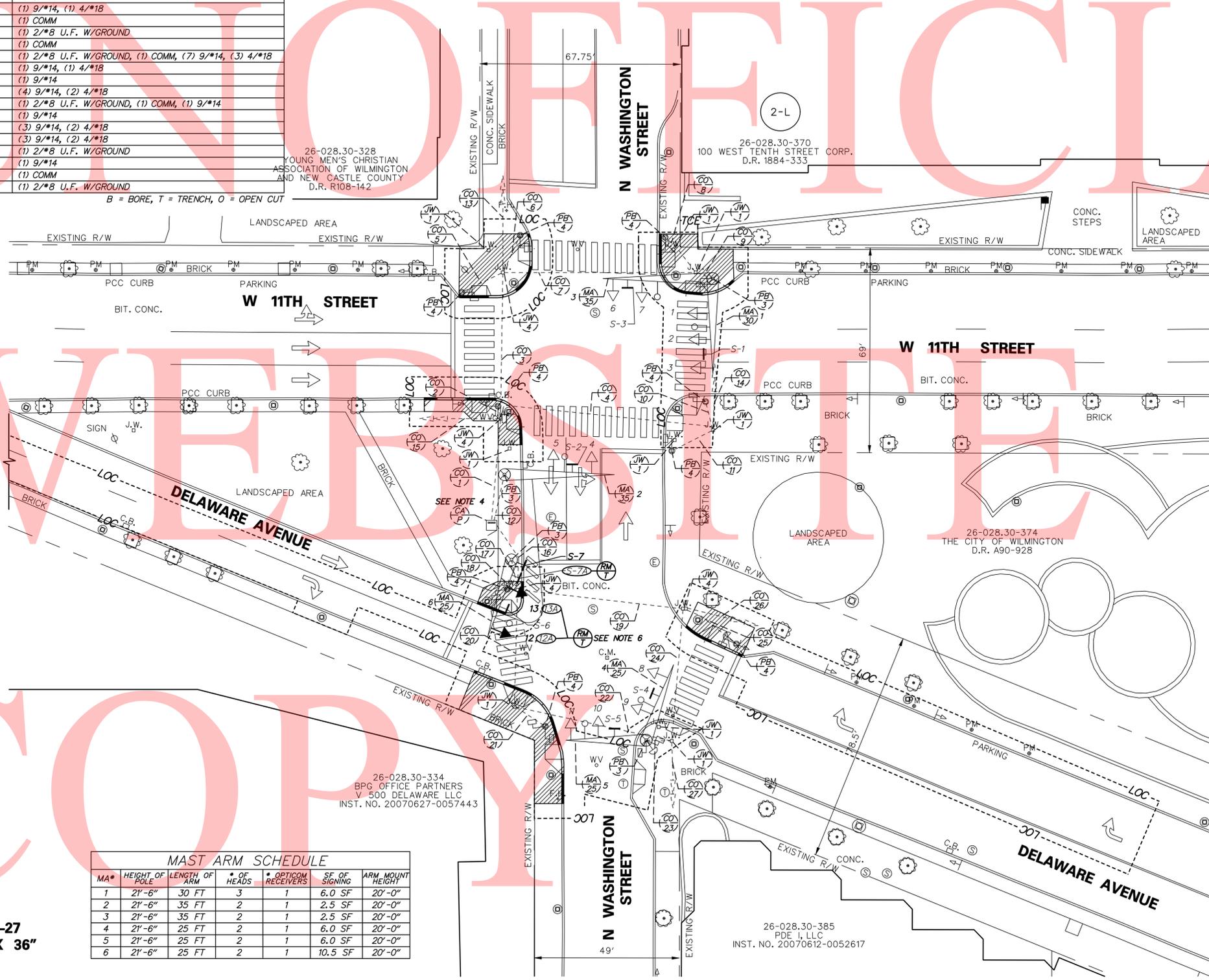
* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

NOTES:
 5. THE CITY OF WILMINGTON SHALL IMPLEMENT NEW SIGNAL PHASING AND ADJUST OFFSETS. THE SIGNAL SHALL OPERATE WITH SPLIT PHASING ON THE DELAWARE AVENUE APPROACHES TO N WASHINGTON STREET.
 6. DELDOT TRAFFIC SHALL REMOVE EXISTING SIGNAL HEADS 12A AND 13A AND EXISTING OVERHEAD SIGN S-7A FROM THE EXISTING MAST ARM. DELDOT TRAFFIC SHALL INSTALL PROPOSED SIGNAL HEADS 12 AND 13 AND PROPOSED OVERHEAD SIGN S-7 ON THE EXISTING MAST ARM, AS SHOWN.



LEGEND	
PROPOSED SIGNAL CABINET	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	REMOVE BY DELDOT TRAFFIC
PROPOSED SIGNAL POLE BASE	ABANDON
EXISTING SIGNAL POLE BASE	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTRIAN POLE BASE	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTRIAN POLE BASE	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED WOOD POLE	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING UTILITY POLE	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
PROPOSED JUNCTION WELL	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
EXISTING JUNCTION WELL	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
PROPOSED SIGNAL HEAD	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	PROPOSED PEDESTRIAN SIGNAL HEAD
PROPOSED PEDESTRIAN SIGNAL HEAD	EXISTING PEDESTRIAN SIGNAL HEAD
EXISTING PEDESTRIAN SIGNAL HEAD	PROPOSED PEDESTRIAN PUSHBUTTON
PROPOSED PEDESTRIAN PUSHBUTTON	EXISTING PEDESTRIAN PUSHBUTTON
EXISTING PEDESTRIAN PUSHBUTTON	PROPOSED VIDEO DETECTION
PROPOSED VIDEO DETECTION	EXISTING VIDEO DETECTION
EXISTING VIDEO DETECTION	PROPOSED MICROWAVE DETECTION
PROPOSED MICROWAVE DETECTION	EXISTING MICROWAVE DETECTION
EXISTING MICROWAVE DETECTION	OVERHEAD SIGNING
OVERHEAD SIGNING	PROPOSED OPTICOM RECEIVER
PROPOSED OPTICOM RECEIVER	EXISTING OPTICOM RECEIVER
EXISTING OPTICOM RECEIVER	PROPOSED MAST ARM
PROPOSED MAST ARM	EXISTING MAST ARM
EXISTING MAST ARM	PROPOSED LUMINAIRE
PROPOSED LUMINAIRE	EXISTING LUMINAIRE
EXISTING LUMINAIRE	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	EXISTING LOOP DETECTOR (TYPE 1 OR 2)

GENERAL SIGNAL NOTES
 1. ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 2. ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLE BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OVERHEAD SIGNS, PEDESTRIAN SIGNAL DISPLAYS, AND POLES.
 4. ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON



MA#	HEIGHT OF POLE	LENGTH OF ARM	# OF HEADS	# OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	30 FT	3	1	6.0 SF	20'-0"
2	21'-6"	35 FT	2	1	2.5 SF	20'-0"
3	21'-6"	35 FT	2	1	2.5 SF	20'-0"
4	21'-6"	25 FT	2	1	6.0 SF	20'-0"
5	21'-6"	25 FT	2	1	6.0 SF	20'-0"
6	21'-6"	25 FT	2	1	10.5 SF	20'-0"

WASHINGTON ST
S-1, S-4, S-6

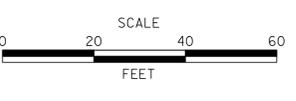
11TH ST
S-2, S-3

DELAWARE AVE
S-5



DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS



WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

CONTRACT 29-200-13
 COUNTY NEW CASTLE
 PERMIT NO. WILMINGTON
 DESIGNED BY: S.H.H. (WR&A)
 CHECKED BY: T.A.O. (WR&A)

SIGNAL PLAN 11TH STREET AND DELAWARE AVENUE @ WASHINGTON STREET

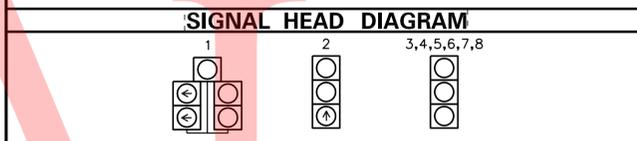
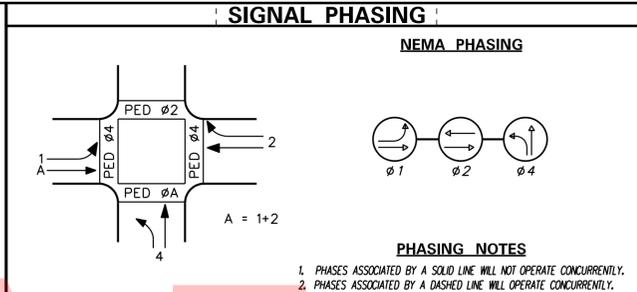
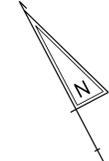
SHEET NO. 65
 TOTAL SHTS. 78

CONDUIT RUN SCHEDULE					
CO#	• OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE / WIRE
1*	1	2.0 IN	8 FT	-	EX. (4) 16/*14, EX. (1) 9/*14
2*	1	2.5 IN	11 FT	-	EX. (1) 16/*14, <REMOVE - EX. (1) 9/*14>
3*	1	2.5 IN	66 FT	-	EX. (2) 16/*14
4*	1	2.0 IN	3 FT	-	EX. (2) 4/*14
5*	1	2.0 IN	33 FT	-	EX. (2) 16/*14
6*	1	2.0 IN	61 FT	-	EX. (2) 16/*14
	1	1.5 IN	61 FT	-	EMPTY
7*	1	1.5 IN	3 FT	-	EX. (1) 16/*14
8*	1	2.0 IN	90 FT	-	EX. (1) 16/*14
9*	1	2.0 IN	10 FT	-	EX. (1) 16/*14, EX. (1) 2/*8 U.F. W/GROUND
10*	1	2.0 IN	XX FT	-	EX. (1) 2/*8 U.F. W/GROUND
11*	1	2.5 IN	2 FT	-	[NEW (1) 4/*14], EX. (5) 16/*14, EX. (1) 9/*14

* DENOTES EXISTING CONDUIT
B = BORE, T = TRENCH, O = OPEN CUT

NOTES:

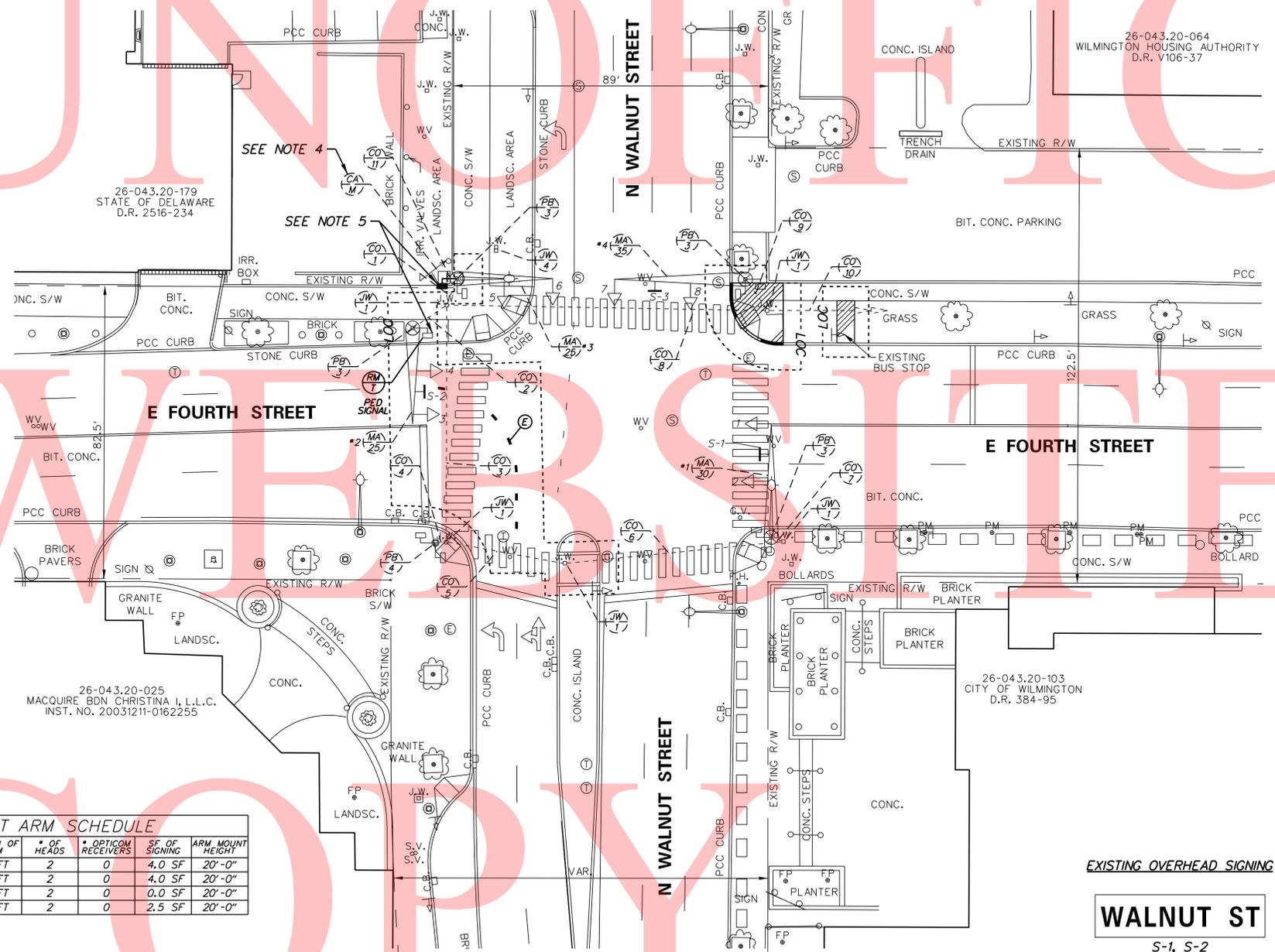
5. DELDOT TRAFFIC SHALL REMOVE THE EXISTING PEDESTRIAN SIGNAL HEAD FROM EXISTING TRAFFIC SIGNAL POLE, CLEAN THE EXPOSED HOLES WITH A WIRE BRUSH AND APPLY GALVANIZING COMPOUND TO FILL THE HOLE. DELDOT TRAFFIC SHALL THEN INSTALL THE REMOVED PEDESTRIAN SIGNAL HEAD ON EXISTING TRAFFIC SIGNAL POLE, AS SHOWN, WITH POLE-MOUNT RIGHT HAND CLAM SHELL.



LEGEND

	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY DELDOT TRAFFIC
	PROPOSED SIGNAL POLE BASE		ABANDON
	EXISTING SIGNAL POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTRIAN POLE BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTRIAN POLE BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING UTILITY POLE		PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
	EXISTING JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
	EXISTING SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED PEDESTRIAN PUSHBUTTON		EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING PEDESTRIAN PUSHBUTTON		PROPOSED SPAN WIRE
	PROPOSED VIDEO DETECTION		EXISTING SPAN WIRE
	EXISTING VIDEO DETECTION		RIGHT-OF-WAY OR PROPERTY LINE
	PROPOSED MICROWAVE DETECTION		PROPOSED SPAN INSULATOR
	EXISTING MICROWAVE DETECTION		EXISTING SPAN INSULATOR
	PROPOSED OVERHEAD SIGNING		SERVICE PEDESTAL
	EXISTING OVERHEAD SIGNING		
	PROPOSED OPTICOM RECEIVER		
	EXISTING OPTICOM RECEIVER		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)		

- GENERAL SIGNAL NOTES**
- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLE BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OVERHEAD SIGNS, PEDESTRIAN SIGNAL DISPLAYS, AND POLES.
 - ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON



MAST ARM SCHEDULE

MA#	HEIGHT OF POLE	LENGTH OF ARM	• OF HEADS	• OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	30 FT	2	0	4.0 SF	20'-0"
2	21'-6"	25 FT	2	0	4.0 SF	20'-0"
3	21'-6"	25 FT	2	0	0.0 SF	20'-0"
4	21'-6"	35 FT	2	0	2.5 SF	20'-0"

PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(E)	4" DASHED WHITE ALKYD-THERMOPLASTIC PERMANENT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748014)	50 LF

*PROPOSED PAVEMENT MARKINGS TO BE INSTALLED BY THE CONTRACTOR, NOT DELDOT TRAFFIC

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	3	2.5 IN	3 FT	-	EX. (2) 2/*8 U.F. W/GROUND, EX. (5) 9/*14, EX. (2) 4/*18, EX. (2) COMM, [NEW (1) 9/*14]
2*	1	2.5 IN	3 FT	-	EX. (1) 9/*14
3*	1	1.5 IN	XX FT	-	EX. (1) 2/*8 U.F. W/GROUND
4*	1	1.5 IN	XX FT	-	EX. (1) COMM
5*	1	2.5 IN	51 FT	-	EX. (2) 9/*14
6*	1	2.5 IN	3 FT	-	EX. (2) 9/*14
7*	1	2.5 IN	38 FT	-	EX. (2) 9/*14, EX. (2) 4/*18
8*	1	2.5 IN	50 FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (1) 9/*14, EX. (1) COMM
9*	1	1.5 IN	XX FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (1) COMM
10*	1	2.5 IN	55 FT	-	EX. (1) 9/*14
11*	1	2.5 IN	3 FT	-	EX. (1) 9/*14
12*	1	2.5 IN	70 FT	-	EX. (1) COMM
13	1	2.5 IN	16 FT	0	[NEW (1) 9/*14]

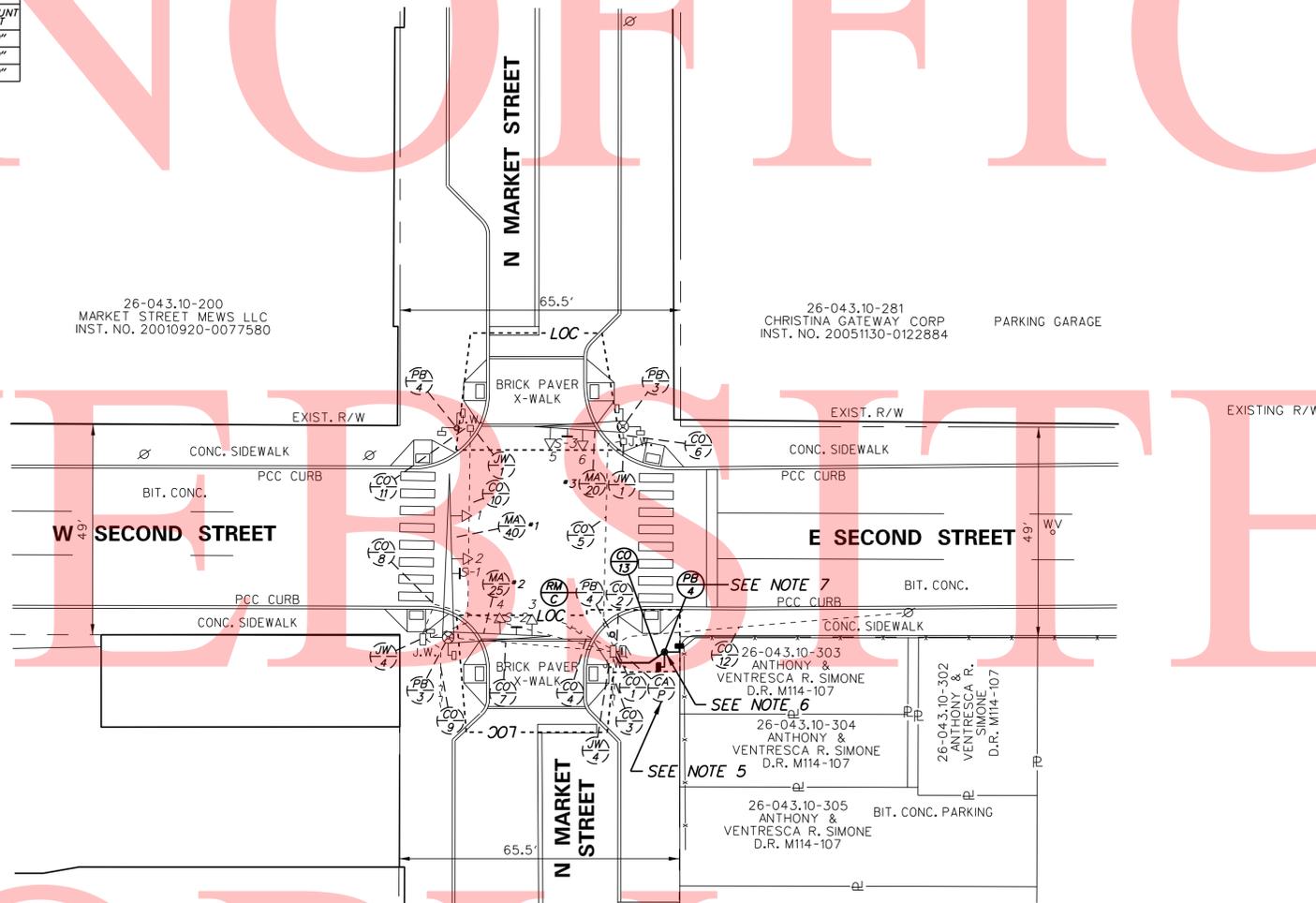
* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

MAST ARM SCHEDULE						
MA#	HEIGHT OF POLE	LENGTH OF ARM	# OF HEADS	# OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	40 FT	2	0	3.0 SF	20'-0"
2	21'-6"	25 FT	2	0	2.5 SF	20'-0"
3	21'-6"	20 FT	2	0	2.5 SF	20'-0"

EXISTING OVERHEAD SIGNING

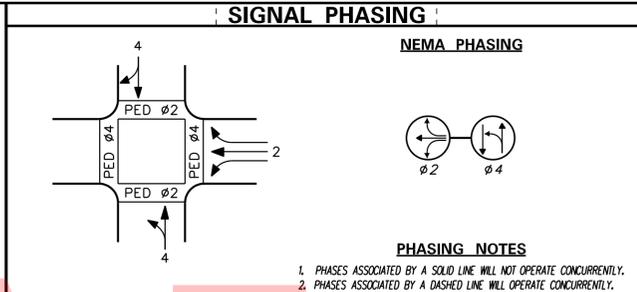
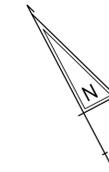
MARKET ST
S-1

2ND ST
S-2, S-3

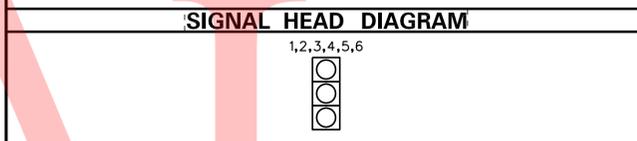


NOTES:

- DELDOT TRAFFIC SHALL INSTALL NEW ORNAMENTAL PEDESTRIAN SIGNAL POLE AND LED COUNTDOWN PEDESTRIAN SIGNALS.
- THE PROJECT CONTRACTOR SHALL CONSTRUCT A NEW POLE BASE, TYPE 4. THE COST OF POSITIONING THE NEW POLE BASE IS INCIDENTAL TO ITEM 763501, CONSTRUCTION ENGINEERING.



- PHASING NOTES
- PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.
 - PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.



LEGEND			
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY DELDOT TRAFFIC
	PROPOSED SIGNAL POLE BASE		ABANDON
	EXISTING SIGNAL POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTRIAN POLE BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTRIAN POLE BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING UTILITY POLE		PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	EXISTING JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED VIDEO DETECTION
	PROPOSED PEDESTRIAN PUSHBUTTON		EXISTING VIDEO DETECTION
	EXISTING PEDESTRIAN PUSHBUTTON		PROPOSED MICROWAVE DETECTION
	PROPOSED VIDEO DETECTION		EXISTING MICROWAVE DETECTION
	EXISTING VIDEO DETECTION		PROPOSED SPAN WIRE
	PROPOSED MICROWAVE DETECTION		EXISTING SPAN WIRE
	EXISTING MICROWAVE DETECTION		RIGHT-OF-WAY OR PROPERTY LINE
	OVERHEAD SIGNING		PROPOSED SPAN INSULATOR
	PROPOSED OPTICOM RECEIVER		EXISTING SPAN INSULATOR
	EXISTING OPTICOM RECEIVER		SERVICE PEDESTAL
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)		

GENERAL SIGNAL NOTES

- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
- ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
- ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MSS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLE BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OVERHEAD SIGNS, PEDESTRIAN SIGNAL DISPLAYS, AND POLES.
- ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON.

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.0 IN	3 FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (2) 16/*14, EX. (2) 9/14, EX. (2) 4/*14, EX. (2) 4/*18 [NEW (5) 9/*14]
2*	1	2.5 IN	4 FT	-	[NEW (2) 9/*14], EX. (1) 9/*14
3*	1	2.5 IN	68 FT	-	EX. (1) 9/*14
4*	1	1.5 IN	16 FT	-	EX. (1) 9/*14
5	1	2.5 IN	10 FT	T	[NEW (2) 9/*14]

* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

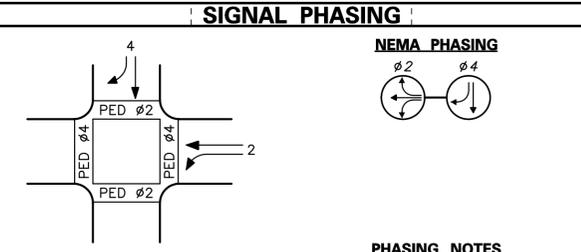
OVERHEAD SCHEDULE

OH#	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE
1*	12 FT	EX. (1) 2/*8 U.F. W/GROUND
2*	50 FT	[NEW (2) 9/*14], EX. (1) 16/*14, EX. (1) 9/*14, EX. (1) 4/*18
3*	55 FT	[NEW (1) 9/*14], EX. (1) 16/*14, EX. (1) 9/*14, EX. (1) 4/*18
4*	14 FT	[NEW (1) 9/*14], EX. (1) 9/*14

*DENOTES EXISTING OVERHEAD RUN

NOTES:

- DELDOT TRAFFIC SHALL INSTALL THE PROPOSED PEDESTRIAN COUNTDOWN SIGNAL DISPLAY WITH POLE-MOUNT CLAM SHELL ASSEMBLY ON THE EXISTING SIGNAL POLE. THE PROPOSED PEDESTRIAN SIGNAL SHALL BE WIRED WITH (1) 9/*14 CABLE.
- DELDOT TRAFFIC SHALL INSTALL THE PROPOSED PEDESTRIAN COUNTDOWN SIGNAL HEADS ON PROPOSED POST TOP-MOUNT TWO-WAY ASSEMBLY ON THE PROPOSED PEDESTRIAN PEDESTAL POLE. THE PROPOSED PEDESTRIAN SIGNAL SHALL BE WIRED WITH (2) 9/*14 CABLES THROUGH PROPOSED CONDUIT TO THE EXISTING JUNCTION WELL.
- DELDOT TRAFFIC SHALL INSTALL THE PROPOSED PEDESTRIAN COUNTDOWN SIGNAL DISPLAY WITH POLE-MOUNT CLAM SHELL ASSEMBLY ON THE EXISTING UTILITY POLE. THE PROPOSED PEDESTRIAN SIGNAL SHALL BE WIRED WITH (1) 9/*14 CABLE.



SIGNAL HEAD DIAGRAM

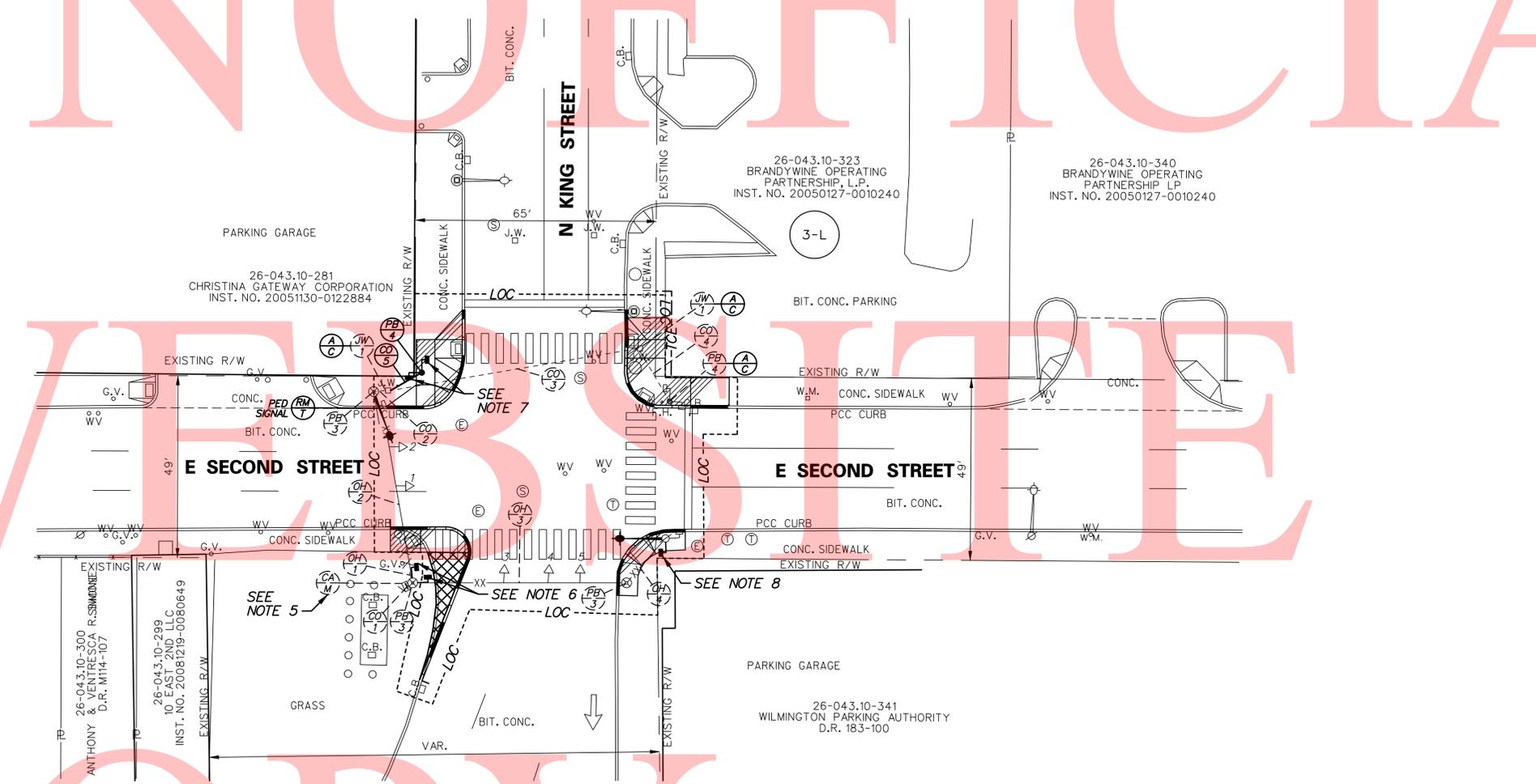


LEGEND

■	PROPOSED SIGNAL CABINET	⊗	REMOVE BY CONTRACTOR
□	EXISTING SIGNAL CABINET	⊗	REMOVE BY DELDOT TRAFFIC
○	PROPOSED SIGNAL POLE BASE	⊗	ADJUST BY CONTRACTOR
⊗	EXISTING SIGNAL POLE BASE	⊗	ABANDON
⊗	PROPOSED PEDESTRIAN POLE BASE	⊗	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
⊗	EXISTING PEDESTRIAN POLE BASE	⊗	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
⊗	PROPOSED WOOD POLE	⊗	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊗	EXISTING UTILITY POLE	⊗	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊗	PROPOSED JUNCTION WELL	⊗	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
J.W.	EXISTING JUNCTION WELL	⊗	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
→	PROPOSED SIGNAL HEAD	⊗	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
→	EXISTING SIGNAL HEAD	⊗	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
→	PROPOSED PEDESTRIAN SIGNAL HEAD	⊗	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
→	EXISTING PEDESTRIAN SIGNAL HEAD	⊗	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
→	PROPOSED PEDESTRIAN PUSHBUTTON	⊗	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
→	EXISTING PEDESTRIAN PUSHBUTTON	⊗	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
→	PROPOSED VIDEO DETECTION	—	PROPOSED SPAN WIRE
→	EXISTING VIDEO DETECTION	—XX—	EXISTING SPAN WIRE
→	PROPOSED MICROWAVE DETECTION	---	RIGHT-OF-WAY OR PROPERTY LINE
→	EXISTING MICROWAVE DETECTION	◆	PROPOSED SPAN INSULATOR
→	OVERHEAD SIGNING	◇	EXISTING SPAN INSULATOR
→	PROPOSED OPTICOM RECEIVER	□	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
→	EXISTING OPTICOM RECEIVER	□	EXISTING LOOP DETECTOR (TYPE 1 OR 2)
→	PROPOSED MAST ARM	□	SERVICE PEDESTAL
→	EXISTING MAST ARM		
→	PROPOSED LUMINAIRE		
→	EXISTING LUMINAIRE		

GENERAL SIGNAL NOTES

- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
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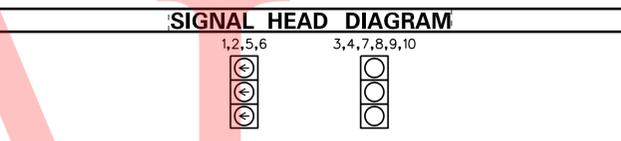
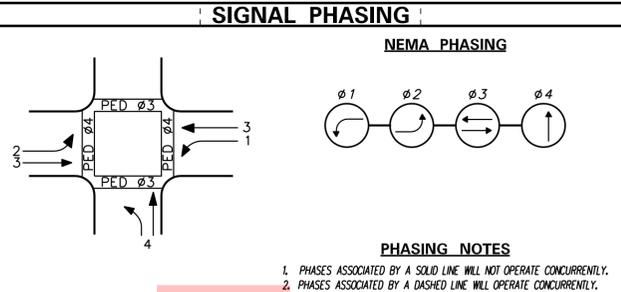
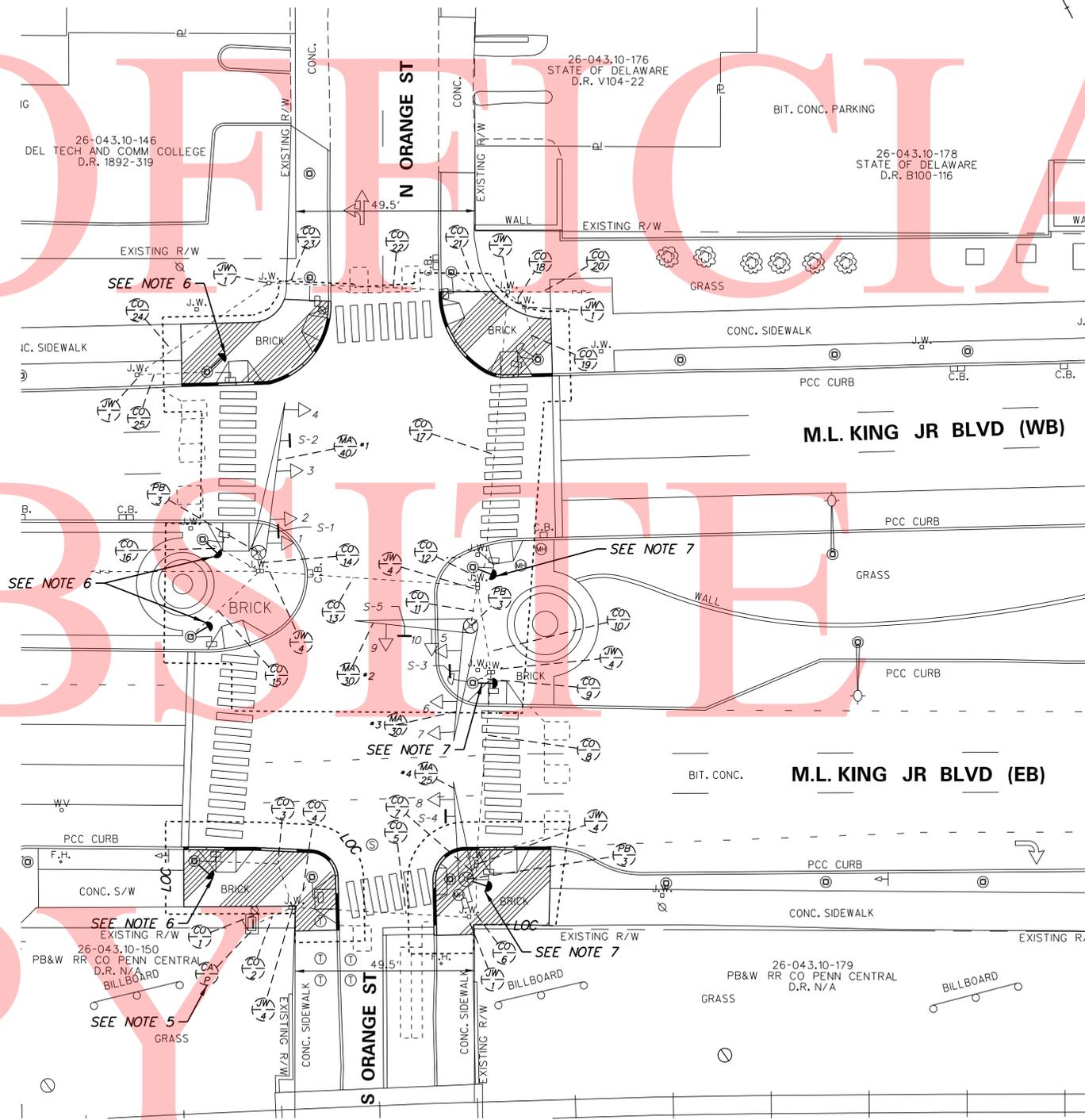
CONDUIT RUN SCHEDULE						
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE	
1*	1	2.0 IN	5 FT	-	EX. (1) 2/*8 U.F. W/GROUND	
2*	3	2.5 IN	10 FT	-	EX. (8) 9/*14, EX. (6) 4/*14, EX. (7) 4/*18, EX. (1) COMM [NEW (4) 9/*14], <REMOVE - EX. (4) 4/*14>	
3*	1	1.5 IN	35 FT	-	[NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
4*	1	1.5 IN	10 FT	-	EX. (1) 4/*14	
5*	1	4.0 IN	46 FT	-	EX. (8) 9/*14, EX. (5) 4/*14, EX. (6) 4/*18, EX. (1) COMM [NEW (3) 9/*14], <REMOVE - EX. (3) 4/*14>	
6*	1	4.0 IN	13 FT	-	EX. (8) 9/*14, EX. (5) 4/*14, EX. (5) 4/*18, EX. (1) COMM [NEW (3) 9/*14], <REMOVE - EX. (3) 4/*14>	
	1	4.0 IN	13 FT	-	EX. (1) FIBER OPTIC, SINGLE-MODE, 144 CT., EX. (1) FIBER OPTIC, SINGLE-MODE, 48 CT.	
7*	1	2.5 IN	5 FT	-	EX. (2) 9/*14, EX. (1) 4/*14	
8*	1	4.0 IN	50 FT	-	EX. (6) 9/*14, EX. (6) 4/*14, EX. (4) 4/*18, EX. (1) COMM [NEW (3) 9/*14], <REMOVE - EX. (3) 4/*14>	
	1	4.0 IN	50 FT	-	EX. (1) FIBER OPTIC, SINGLE-MODE, 144 CT., EX. (1) FIBER OPTIC, SINGLE-MODE, 48 CT.	
9*	1	1.5 IN	6 FT	-	EX. (3) 4/*14	
10*	1	4.0 IN	21 FT	-	EX. (6) 9/*14, EX. (5) 4/*14, EX. (3) 4/*18, EX. (1) COMM [NEW (3) 9/*14], <REMOVE - EX. (3) 4/*14>	
	1	4.0 IN	21 FT	-	EX. (1) FIBER OPTIC, SINGLE-MODE, 144 CT., EX. (1) FIBER OPTIC, SINGLE-MODE, 48 CT.	
11*	1	2.5 IN	9 FT	-	EX. (3) 9/*14	
12*	1	1.5 IN	4 FT	-	EX. (3) 4/*14	
13*	1	4.0 IN	57 FT	-	EX. (2) 9/*14, EX. (1) COMM [NEW (2) 9/*14], <REMOVE - EX. (2) 4/*14>	
	1	4.0 IN	57 FT	-	EX. (1) FIBER OPTIC, SINGLE-MODE, 144 CT., EX. (1) FIBER OPTIC, SINGLE-MODE, 48 CT., EX. (1) 4/*18	
14*	1	2.5 IN	5 FT	-	EX. (2) 9/*14	
15*	1	1.5 IN	25 FT	-	[NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
16*	1	2.5 IN	17 FT	-	[NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
17*	1	4.0 IN	80 FT	-	EX. (4) 4/*14, EX. (2) 4/*18, EX. (1) COMM [NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
	1	4.0 IN	80 FT	-	EMPTY	
18*	1	1.0 IN	5 FT	-	EX. (2) 4/*14	
19*	1	2.5 IN	14 FT	-	EX. (2) 4/*14, EX. (6) LIGHTING	
20*	2	4.0 IN	XX FT	-	EX. (1) COMM	
21*	1	4.0 IN	19 FT	-	EX. (1) 4/*14	
22*	2	4.0 IN	64 FT	-	EX. (1) 4/*14, EX. (2) 4/*18 [NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
23*	1	1.5 IN	11 FT	-	EX. (1) 4/*14	
24*	2	4.0 IN	43 FT	-	EX. (1) 4/*18 [NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	
25*	1	1.5 IN	19 FT	-	[NEW (1) 9/*14], <REMOVE - EX. (1) 4/*14>	

* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

MAST ARM SCHEDULE						
MA#	HEIGHT OF POLE	LENGTH OF ARM	# OF HEADS	# OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	40 FT	4	0	8.0 SF	20'-0"
2	21'-6"	30 FT	2	0	8.0 SF	20'-0"
3	21'-6"	30 FT	3	0	6.0 SF	20'-0"
4	21'-6"	25 FT	1	0	3.0 SF	20'-0"



- NOTES:
- DELDOT TRAFFIC SHALL INSTALL PEDESTRIAN PUSHBUTTONS ON EXISTING POLE.
 - DELDOT TRAFFIC SHALL REPLACE EXISTING PUSHBUTTONS USING EXISTING WIRING.
 - DELDOT TRAFFIC SHALL INSTALL R10-3e SIGNS IMMEDIATELY ADJACENT TO ALL PUSHBUTTONS IN ACCORDANCE WITH ITEM NO. 746897.



LEGEND	
PROPOSED SIGNAL CABINET	REMOVE BY CONTRACTOR
EXISTING SIGNAL CABINET	REMOVE BY DELDOT TRAFFIC
PROPOSED SIGNAL POLE BASE	ABANDON
EXISTING SIGNAL POLE BASE	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTRIAN POLE BASE	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTRIAN POLE BASE	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED WOOD POLE	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING UTILITY POLE	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED JUNCTION WELL	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
EXISTING JUNCTION WELL	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED SIGNAL HEAD	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING SIGNAL HEAD	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED PEDESTRIAN SIGNAL HEAD	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING PEDESTRIAN SIGNAL HEAD	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
PROPOSED PEDESTRIAN PUSHBUTTON	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
EXISTING PEDESTRIAN PUSHBUTTON	PROPOSED SPAN WIRE
PROPOSED VIDEO DETECTION	EXISTING SPAN WIRE
EXISTING VIDEO DETECTION	RIGHT-OF-WAY OR PROPERTY LINE
PROPOSED MICROWAVE DETECTION	PROPOSED SPAN INSULATOR
EXISTING MICROWAVE DETECTION	EXISTING SPAN INSULATOR
OVERHEAD SIGNING	SERVICE PEDESTAL
PROPOSED OPTICOM RECEIVER	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
EXISTING OPTICOM RECEIVER	EXISTING LOOP DETECTOR (TYPE 1 OR 2)
PROPOSED MAST ARM	
EXISTING MAST ARM	
PROPOSED LUMINAIRE	
EXISTING LUMINAIRE	

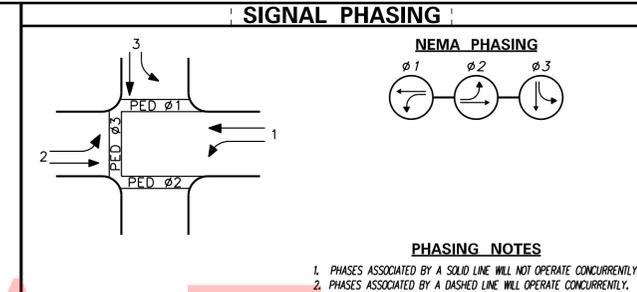
- GENERAL SIGNAL NOTES
- EXISTING LOOP DETECTORS - TO REMAIN: SYSTEM - 6' x 6' - EASTBOUND AND WESTBOUND M.L.KING JR. BLVD AND NORTHBOUND AND SOUTHBOUND ORANGE STREET RECEIVING LANES TYPE #2 - 6' x 25' - NORTHBOUND ORANGE STREET THROUGH MOVEMENT
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL UNDERGROUND SIGNAL EQUIPMENT - E.G., JUNCTION WELLS, POLE BASES, AND CONDUIT. DELDOT TRAFFIC SHALL BE RESPONSIBLE FOR THE INSTALLATION AND REMOVAL OF ALL ELECTRICAL CABLES AND ABOVE GROUND SIGNAL EQUIPMENT - E.G., HEADS, OVERHEAD SIGNS, PEDESTRIAN SIGNAL DISPLAYS, AND POLES.
 - ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON.

- NOTES:**
 5. DELDOT TRAFFIC SHALL REMOVE THE EXISTING PEDESTRIAN SIGNAL HEAD FROM THE EXISTING SIGNAL POLE, AS SHOWN.
 6. DELDOT TRAFFIC SHALL REMOVE ALL PEDESTRIAN PUSHBUTTONS.

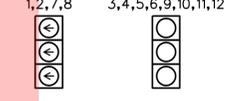
OVERHEAD SCHEDULE

OH#	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE
1*	59 FT	EX. (1) 9/*14
2*	100 FT	EX. (1) COMM
3*	90 FT	EX. (1) 16/*14, EX. (1) 9/*14, EX. (1) 4/*18
4*	79 FT	EX. (2) 16/*14, EX. (2) 4/*18
5*	85 FT	EX. (2) 16/*14, EX. (3) 4/*18, EX. (1) COMM
6*	66 FT	EX. (1) 9/*14

*DENOTES EXISTING OVERHEAD RUN



SIGNAL HEAD DIAGRAM

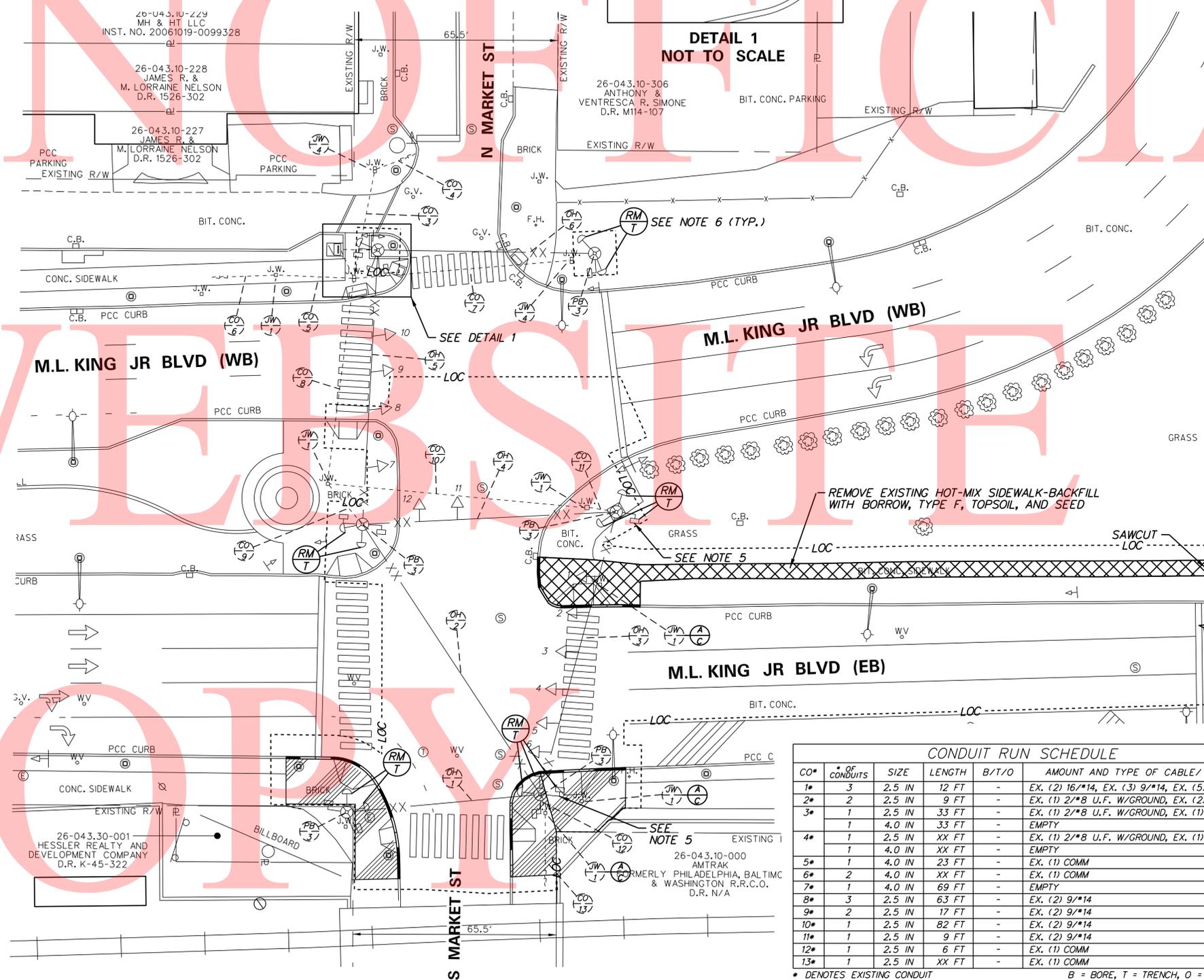
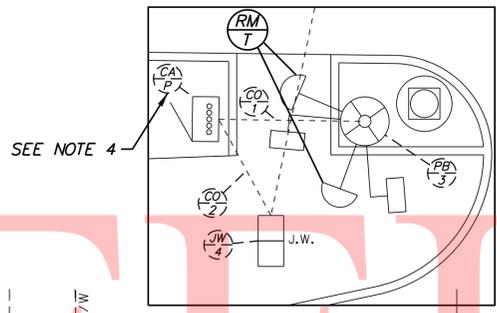


LEGEND

- PROPOSED SIGNAL CABINET
- EXISTING SIGNAL CABINET
- PROPOSED SIGNAL POLE BASE
- EXISTING SIGNAL POLE BASE
- PROPOSED PEDESTRIAN POLE BASE
- EXISTING PEDESTRIAN POLE BASE
- PROPOSED WOOD POLE
- EXISTING UTILITY POLE
- PROPOSED JUNCTION WELL
- EXISTING JUNCTION WELL
- PROPOSED SIGNAL HEAD
- EXISTING SIGNAL HEAD
- PROPOSED PEDESTRIAN SIGNAL HEAD
- EXISTING PEDESTRIAN SIGNAL HEAD
- PROPOSED PEDESTRIAN PUSHBUTTON
- EXISTING PEDESTRIAN PUSHBUTTON
- PROPOSED VIDEO DETECTION
- EXISTING VIDEO DETECTION
- PROPOSED MICROWAVE DETECTION
- EXISTING MICROWAVE DETECTION
- OVERHEAD SIGNING
- PROPOSED OPTICOM RECEIVER
- EXISTING OPTICOM RECEIVER
- PROPOSED MAST ARM
- EXISTING MAST ARM
- PROPOSED LUMINAIRE
- EXISTING LUMINAIRE
- PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
- EXISTING LOOP DETECTOR (TYPE 1 OR 2)
- REMOVE BY CONTRACTOR
- REMOVE BY DELDOT TRAFFIC
- ADJUST BY CONTRACTOR
- ABANDON
- PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
- EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
- PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
- EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
- PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
- EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
- PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
- EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
- PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
- EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
- PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
- EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
- PROPOSED SPAN WIRE
- EXISTING SPAN WIRE
- RIGHT-OF-WAY OR PROPERTY LINE
- PROPOSED SPAN INSULATOR
- EXISTING SPAN INSULATOR
- SERVICE PEDESTAL

GENERAL SIGNAL NOTES

- ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
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- ALL NEW SIGNAL CONTROLLERS SUPPLIED AS PART OF THIS CONTRACT SHALL BE INSTALLED BY THE CITY OF WILMINGTON.



CONDUIT RUN SCHEDULE

CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	3	2.5 IN	12 FT	-	EX. (2) 16/*14, EX. (3) 9/*14, EX. (5) 4/*18
2*	2	2.5 IN	9 FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (2) COMM
3*	1	2.5 IN	33 FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (1) COMM
	1	4.0 IN	33 FT	-	EMPTY
4*	1	2.5 IN	XX FT	-	EX. (1) 2/*8 U.F. W/GROUND, EX. (1) COMM
	1	4.0 IN	XX FT	-	EMPTY
5*	1	4.0 IN	23 FT	-	EX. (1) COMM
6*	2	4.0 IN	XX FT	-	EX. (1) COMM
7*	1	4.0 IN	69 FT	-	EMPTY
8*	3	2.5 IN	63 FT	-	EX. (2) 9/*14
9*	2	2.5 IN	17 FT	-	EX. (2) 9/*14
10*	1	2.5 IN	82 FT	-	EX. (2) 9/*14
11*	1	2.5 IN	9 FT	-	EX. (2) 9/*14
12*	1	2.5 IN	6 FT	-	EX. (1) COMM
13*	1	2.5 IN	XX FT	-	EX. (1) COMM

* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.5 IN	15 FT	X	[NEW (3) 9/*14], EX. (1) 2/*8 U.F. W/GROUND, EX. (2) 16/*14
2*	1	2.5 IN	3 FT	X	[NEW (2) 9/*14], EX. (1) 2/*8 U.F. W/GROUND, EX. (2) 16/*14
3*	2	2.5 IN	50 FT	X	[NEW (1) 9/*14], [NEW (1) 1/*6 AWG], [NEW (1) 1/*6 GROUND]
4*	1	2.5 IN	11 FT	X	[NEW (1) 9/*14], [NEW (1) 1/*6 AWG], [NEW (1) 1/*6 GROUND]

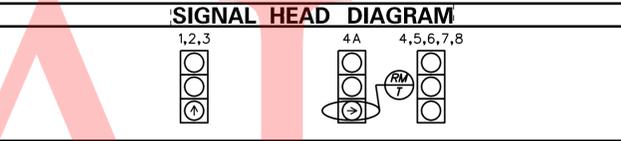
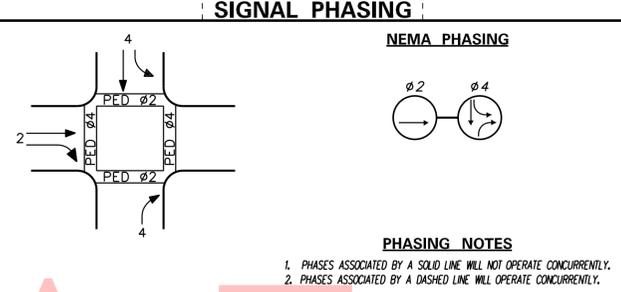
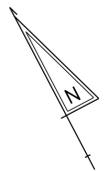
* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

OVERHEAD SCHEDULE		
OH#	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE
1*	61 FT	[NEW (1) 9/*14], EX. (1) 9/*14
2*	86 FT	[NEW (1) 9/*14], EX. (1) 2/*8 U.F. W/GROUND, EX. (1) 16/*14
3*	55 FT	EX. (1) 2/*8 U.F. W/GROUND, EX. (1) 16/*14
4*	XX FT	EX. (1) 2/*8 U.F. W/GROUND

* DENOTES EXISTING OVERHEAD RUN

NOTES:

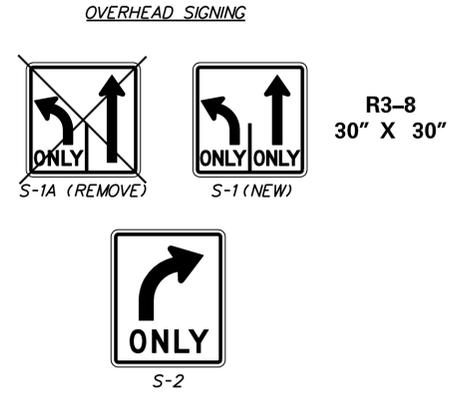
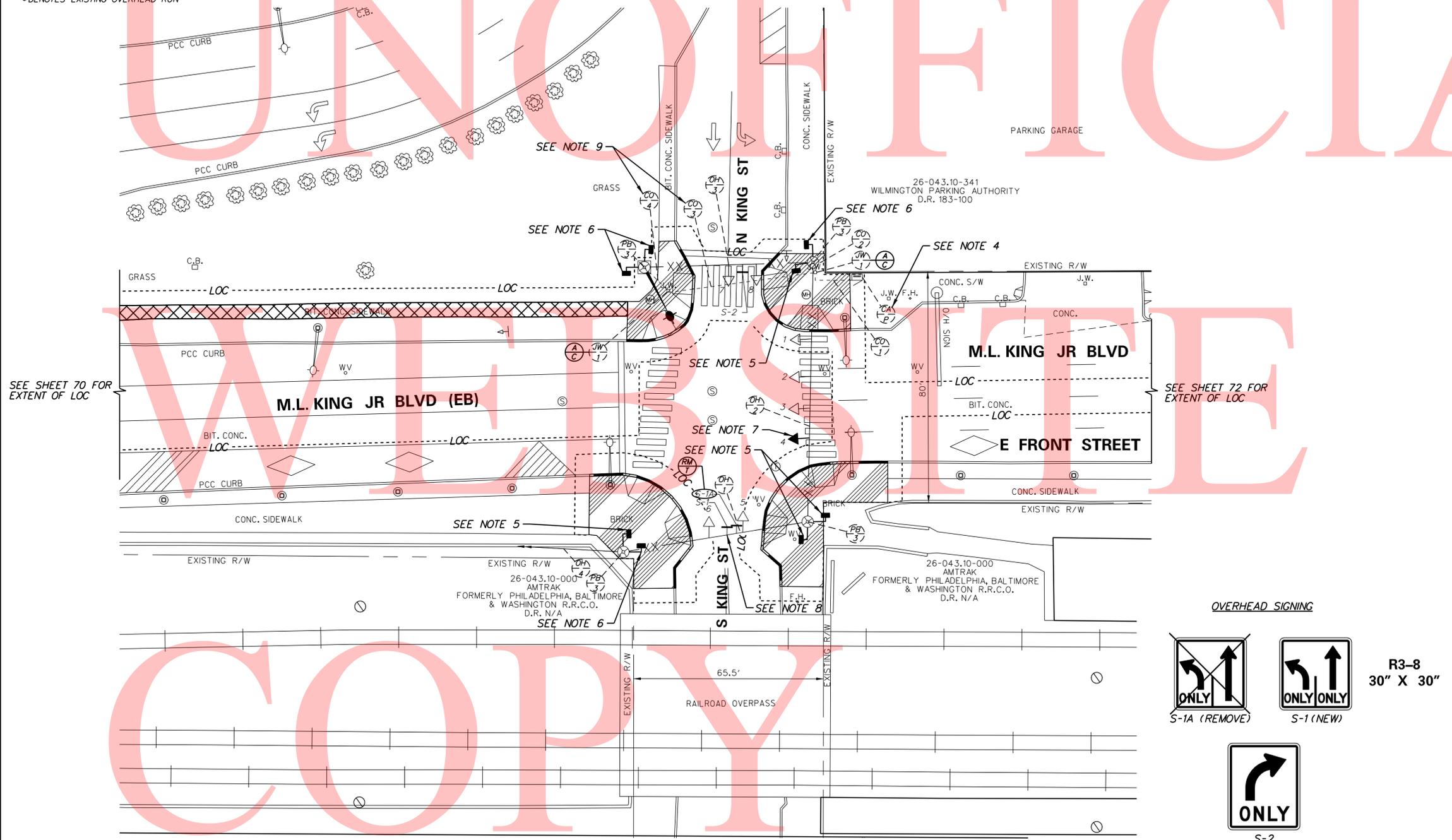
- DELDOT TRAFFIC SHALL UPGRADE ALL EXISTING PEDESTRIAN SIGNAL HEADS WITH L.E.D. COUNTDOWN MODULES. THE PROPOSED L.E.D COUNTDOWN MODULES SHALL USE EXISTING WIRING.
- DELDOT TRAFFIC SHALL INSTALL THE PROPOSED PEDESTRIAN SIGNAL DISPLAY ON THE EXISTING SIGNAL POLE WITH POLE-MOUNT CLAM SHELL ASSEMBLY. THE PROPOSED PEDESTRIAN SIGNAL SHALL BE WIRED WITH (1) 9/*14 CABLE.
- DELDOT TRAFFIC SHALL REMOVE EXISTING GREEN RIGHT-TURN ARROW INDICATION ON SIGNAL HEAD *4 AND INSTALL NEW GREEN BALL INDICATION.
- DELDOT TRAFFIC SHALL REMOVE EXISTING OVERHEAD SIGN S-1A FROM THE EXISTING SPAN WIRE AND INSTALL PROPOSED OVERHEAD SIGN S-1, AS SHOWN.
- THE PROJECT CONTRACTOR SHALL INSTALL THE UNDERGROUND SPLICE AND PROPOSED ELECTRIC CABLE THROUGH CONDUIT RUN *3 AND *4 TO THE TOP OF THE EXISTING SIGNAL POLE FOR THE PROPOSED LIGHT FIXTURE.



LEGEND			
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY DELDOT TRAFFIC
	PROPOSED SIGNAL POLE BASE		ABANDON
	EXISTING SIGNAL POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED PEDESTRIAN POLE BASE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTRIAN POLE BASE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED WOOD POLE		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING UTILITY POLE		PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED JUNCTION WELL		EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	EXISTING JUNCTION WELL		PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED PEDESTRIAN PUSHBUTTON		EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING PEDESTRIAN PUSHBUTTON		PROPOSED SPAN WIRE
	PROPOSED VIDEO DETECTION		EXISTING SPAN WIRE
	EXISTING VIDEO DETECTION		RIGHT-OF-WAY OR PROPERTY LINE
	PROPOSED MICROWAVE DETECTION		PROPOSED SPAN INSULATOR
	EXISTING MICROWAVE DETECTION		EXISTING SPAN INSULATOR
	OVERHEAD SIGNING		SERVICE PEDESTAL
	PROPOSED OPTICOM RECEIVER		
	EXISTING OPTICOM RECEIVER		
	PROPOSED MAST ARM		
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)		

GENERAL SIGNAL NOTES

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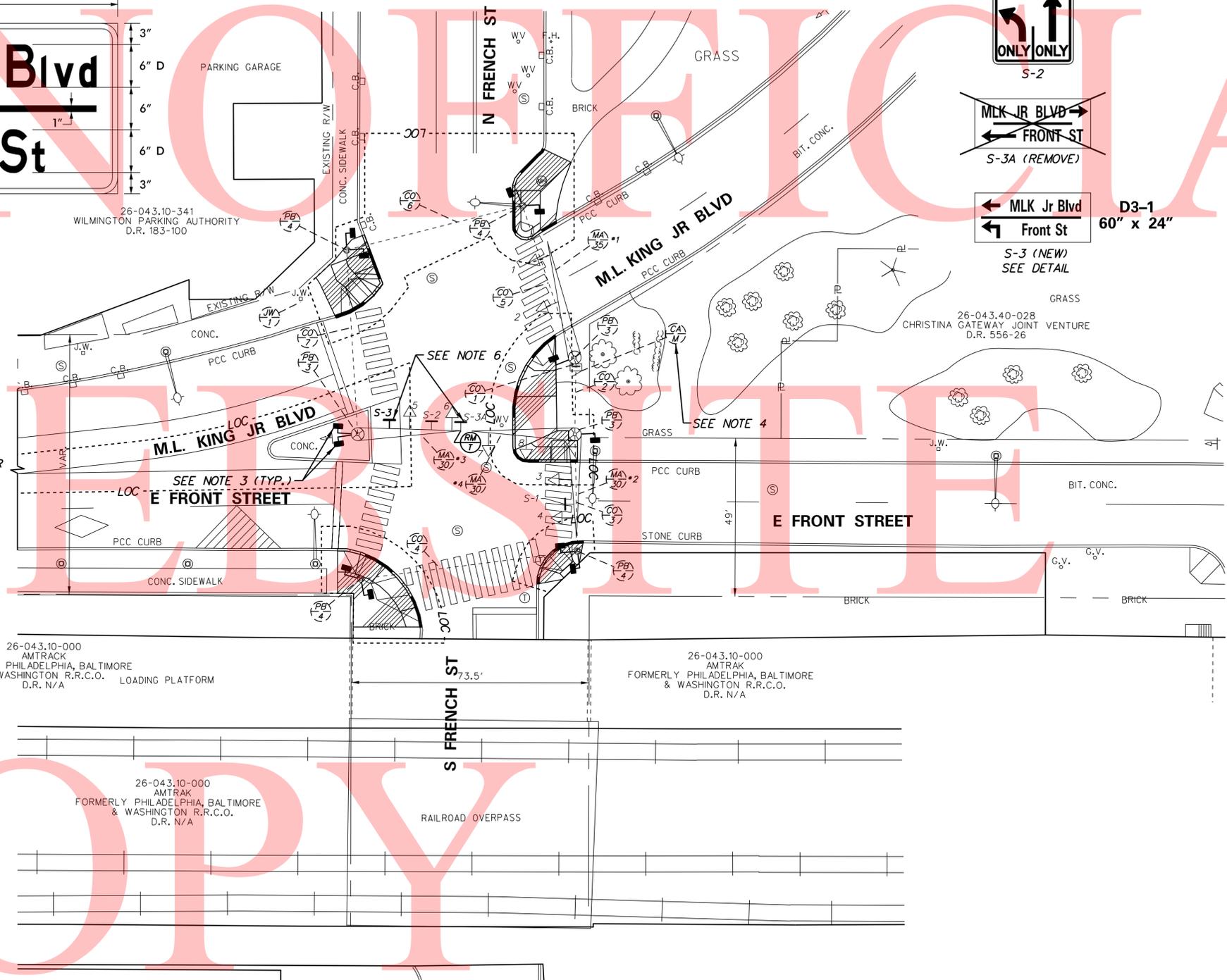
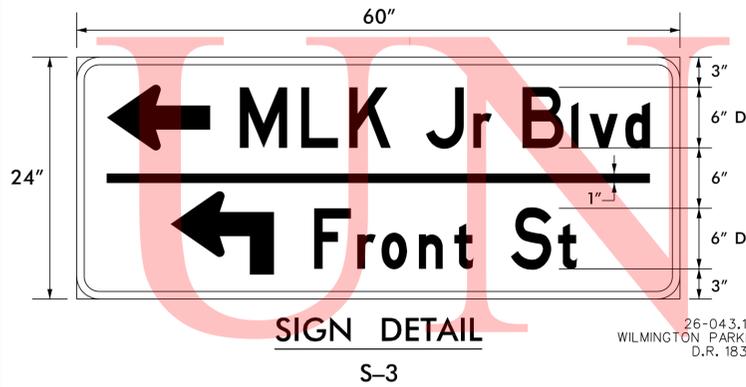


CONDUIT RUN SCHEDULE					
CO#	# OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	2.0 IN	3 FT	-	EX. (1) 2/*8 U.F. W/GROUND EX. (3) 16/*14, EX. (4) 9/*14
2*	1	2.5 IN	22 FT	-	EX. (1) 16/*14, EX. (2) 9/*14
3*	1	2.5 IN	36 FT	-	EX. (2) 9/*14
4*	1	2.5 IN	68 FT	-	EX. (1) 9/*14
5*	1	2.5 IN	52 FT	-	EX. (1) 16/*14, EX. (2) 9/*14
6*	1	2.5 IN	56 FT	-	EX. (1) 16/*14, EX. (1) 9/*14
7*	1	2.5 IN	57 FT	-	EX. (1) 16/*14

* DENOTES EXISTING CONDUIT B = BORE, T = TRENCH, O = OPEN CUT

NOTES:

- DELDOT TRAFFIC SHALL UPGRADE ALL EXISTING PEDESTRIAN SIGNAL HEADS WITH L.E.D. COUNTDOWN MODULES. THE PROPOSED L.E.D. COUNTDOWN MODULES SHALL BE SPLICED TO EXISTING CABLES.
- DELDOT TRAFFIC SHALL REMOVE THE EXISTING OVERHEAD SIGN (S-3A) AND PLACE THE NEW SIGN (S-3) AT THE PROPOSED LOCATION ALONG THE EXISTING MAST ARM.



OVERHEAD SIGNING

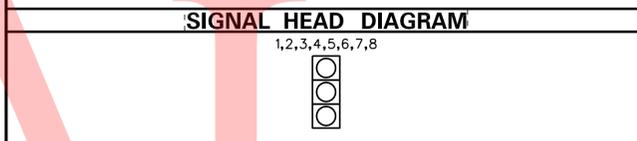
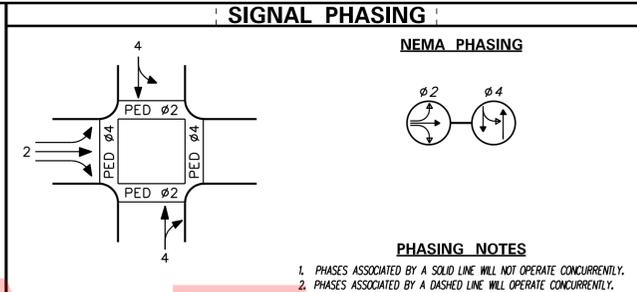
FRENCH ST
S-1



MLK Jr Blvd
Front St
D3-1
60" x 24"
S-3 (NEW)
SEE DETAIL

WILMINGTON RENAISSANCE PEDESTRIAN SAFETY IMPROVEMENTS

MAST ARM SCHEDULE						
MA#	HEIGHT OF POLE	LENGTH OF ARM	# OF HEADS	# OPTICOM RECEIVERS	SF OF SIGNING	ARM MOUNT HEIGHT
1	21'-6"	35 FT	2	0	0.0 SF	20'-0"
2	21'-6"	30 FT	2	0	3.0 SF	20'-0"
3	21'-6"	30 FT	2	0	8.0 SF	20'-0"
4	21'-6"	30 FT	2	0	0.0 SF	20'-0"

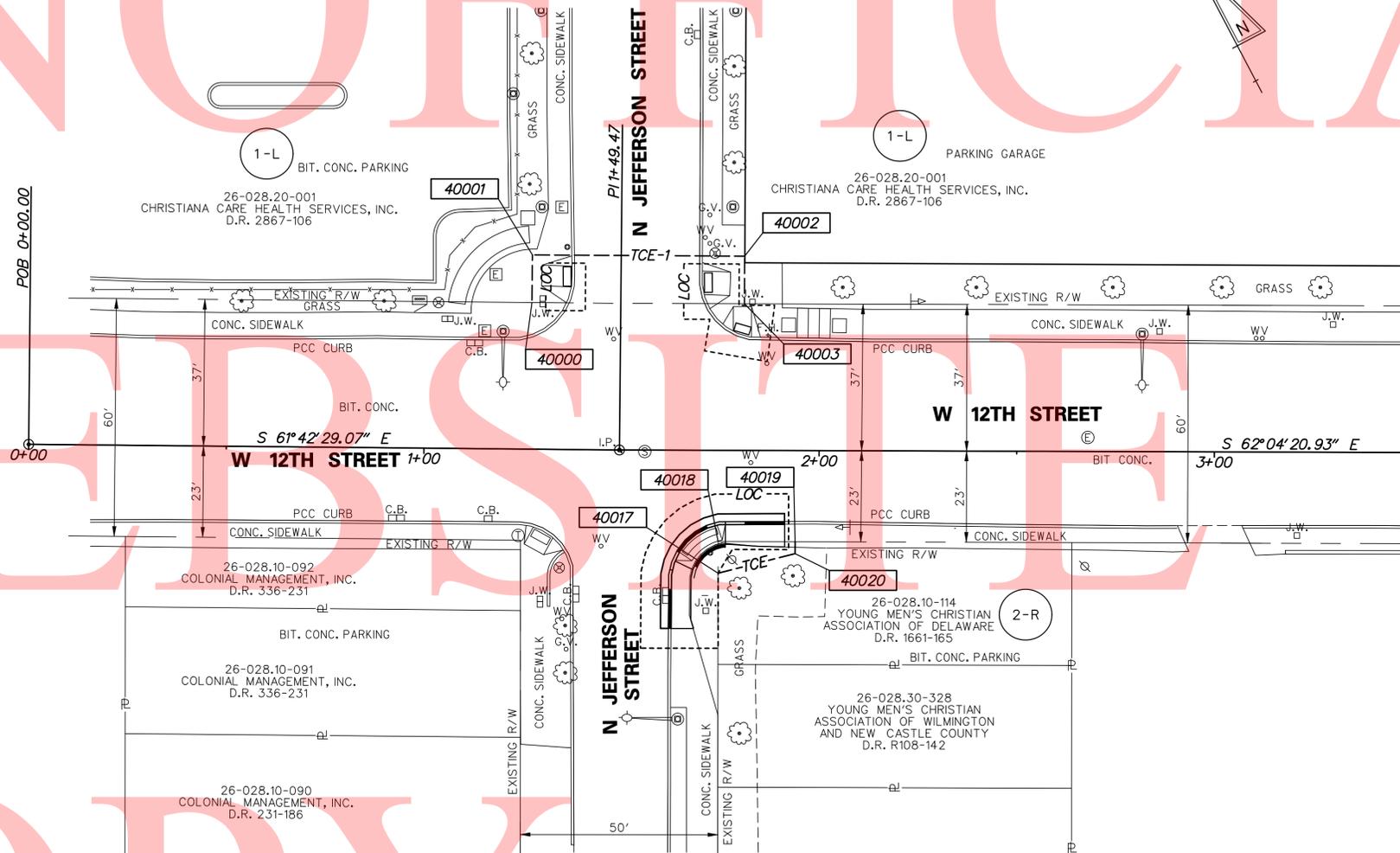


LEGEND		
	PROPOSED SIGNAL CABINET	REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET	REMOVE BY DELDOT TRAFFIC
	PROPOSED SIGNAL POLE BASE	ABANDON
	EXISTING SIGNAL POLE BASE	ABANDON
	PROPOSED PEDESTRIAN POLE BASE	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING PEDESTRIAN POLE BASE	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED WOOD POLE	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING UTILITY POLE	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED JUNCTION WELL	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED SIGNAL HEAD	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	EXISTING SIGNAL HEAD	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED PEDESTRIAN SIGNAL HEAD	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING PEDESTRIAN SIGNAL HEAD	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED PEDESTRIAN PUSHBUTTON	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
	EXISTING PEDESTRIAN PUSHBUTTON	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
	PROPOSED VIDEO DETECTION	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING VIDEO DETECTION	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED MICROWAVE DETECTION	PROPOSED SPAN WIRE
	EXISTING MICROWAVE DETECTION	EXISTING SPAN WIRE
	OVERHEAD SIGNING	EXISTING SPAN WIRE
	PROPOSED OPTICOM RECEIVER	RIGHT-OF-WAY OR PROPERTY LINE
	EXISTING OPTICOM RECEIVER	PROPOSED SPAN INSULATOR
	PROPOSED MAST ARM	EXISTING SPAN INSULATOR
	EXISTING MAST ARM	SERVICE PEDESTAL
	PROPOSED LUMINAIRE	
	EXISTING LUMINAIRE	
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)	

- GENERAL SIGNAL NOTES**
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UNOFFICIAL

WEB SITE

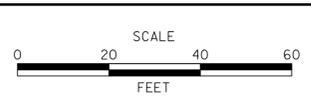


PREPARED BY
THE CONSULTING FIRM OF
WR&A
WHITMAN, REQUARDT & ASSOCIATES, LLP
ENGINEERS - ARCHITECTS - PLANNERS EST. 1915

SEAL

RECOMMENDED	DATE
RECOMMENDED	
TEAM SUPPORT SQUAD MANAGER	DATE 20
TEAM SUPPORT ENGINEER	DATE 20
ASSISTANT DIRECTOR, ENGINEERING SUPPORT	DATE 20
"AS-ACQUIRED" PLANS	
I Certify that all proposed Right-of-Way has been acquired in the name of THE STATE OF DELAWARE and that these plans accurately depict the nature and extent of THE REAL ESTATE SECTION acquisition under this project.	
CHIEF, REAL ESTATE	DATE 20

ADDENDUMS / REVISIONS



**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**RIGHT OF WAY PLAN
SHEET 1 OF 4**

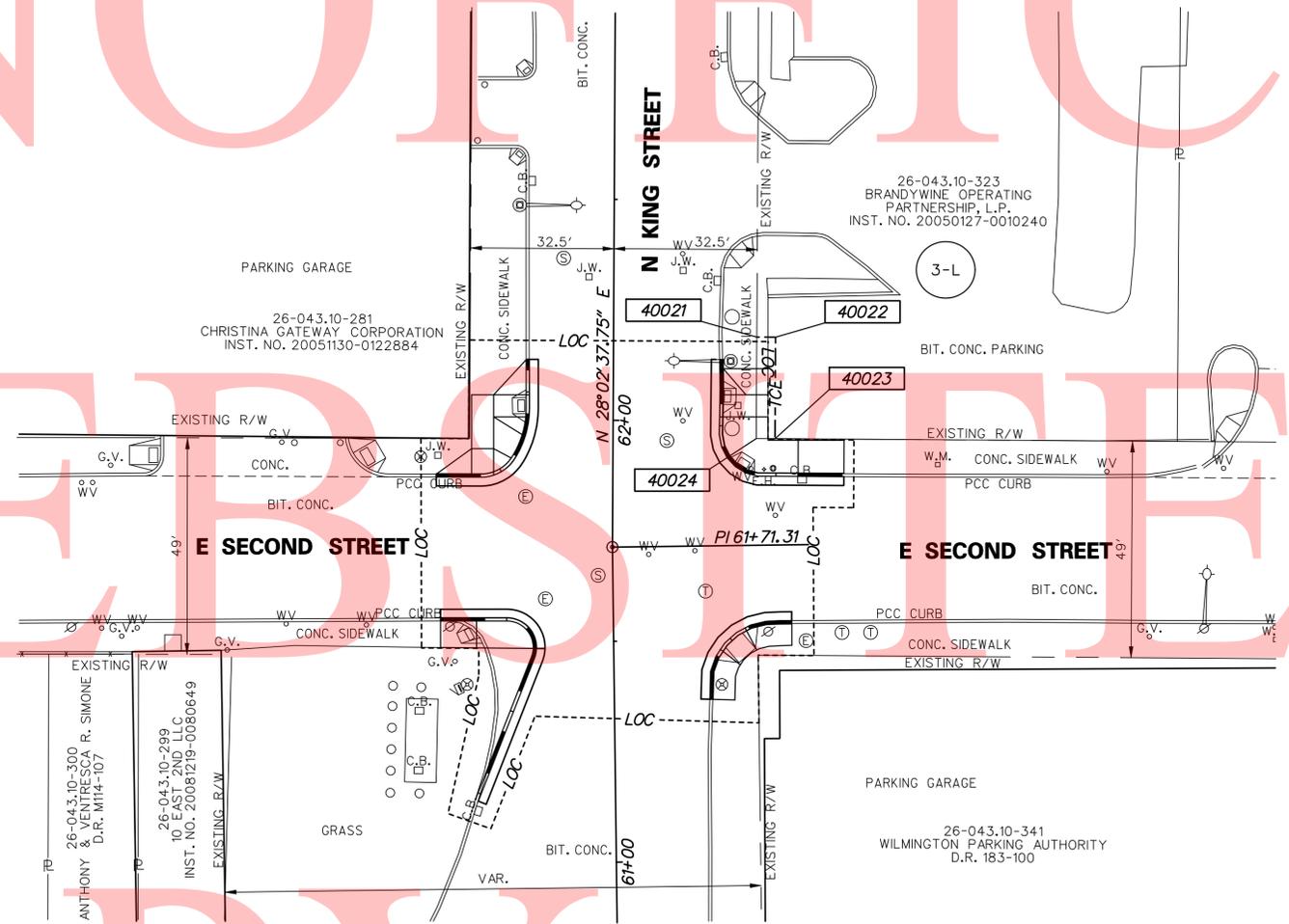
RW-01
SHEET NO.
73
TOTAL SHTS.
78

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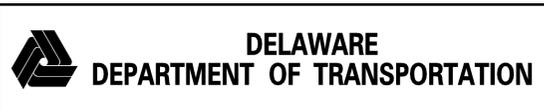
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WEBSITE

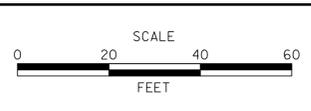
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ADDENDUMS / REVISIONS	



**WILMINGTON RENAISSANCE
 PEDESTRIAN SAFETY
 IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

**RIGHT OF WAY PLAN
 SHEET 4 OF 4**

RW-04
SHEET NO.
76
TOTAL SHTS.
78

ASSESSMENT NUMBER	OWNERSHIP OF RECORD	TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)							
26-028.20-001	(1-L) CHRISTIANA CARE HEALTH SERVICES, INC.	TCE-1	2867-106/ M/F 14206	10.980							
ALIGNMENT NUMBER & DESCRIPTION: 12 - TWELFTH STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
40000	12	1+27.00	-37.00	637222.7645	618201.6031						
40001	12	1+27.00	-49.00	637233.3310	618207.2907	N 28°17'30.93" E	12.0000				
40002	12	1+81.00	-49.00	637208.0607	618254.6597	S 61°55'15.58" E	53.6881				
40003	12	1+81.00	-37.00	637197.4582	618249.0394	S 27°55'39.07" W	12.0000				
40000	12	1+27.00	-37.00	637222.7645	618201.6031	N 61°55'15.42" W	53.7644				

FIGURE RW_1L.TCE-1 AREA = 644.7117 SQ. FT. (0.0148 ACRES)

ASSESSMENT NUMBER	OWNERSHIP OF RECORD	TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)							
26-028.20-001	(1-L) CHRISTIANA CARE HEALTH SERVICES, INC.	TCE-2	2867-106/ M/F 14206	10.980							
ALIGNMENT NUMBER & DESCRIPTION: 12 - TWELFTH STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
40004	12	4+31.00	-37.00	637080.3696	618469.9246	N 27°55'39.07" E	9.9300				
40005	12	4+31.00	-46.93	637089.1432	618474.5754	S 61°59'28.08" E	21.1300				
40006	12	4+52.13	-46.90	637079.2203	618493.2306	N 27°46'39.12" E	19.1001				
40007	12	4+52.08	-66.00	637096.1194	618502.1319	S 62°04'20.93" E	4.4000				
40008	12	4+56.48	-66.00	637094.0586	618506.0195	S 27°55'39.07" W	15.7202				
40009	12	4+56.48	-50.28	637080.1692	618498.6569	S 79°25'08.99" W	21.3286				
40014	12	4+39.79	-37.00	637076.2528	618477.6910	N 62°04'20.93" W	8.7900				
40004	12	4+31.00	-37.00	637080.3696	618469.9246						

FIGURE RW_1L.TCE-2 AREA = 225.3115 SQ. FT. (0.0052 ACRES)

ASSESSMENT NUMBER	OWNERSHIP OF RECORD	TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)							
26-028.30-370	(2-L) 100 WEST TENTH STREET CORP	TCE	1884-333	0.924							
ALIGNMENT NUMBER & DESCRIPTION: 11 - ELEVENTH STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
40010	11	20+22.23	-24.50	636715.7680	618382.3395	N 27°57'38.59" E	9.0000				
40011	11	20+22.23	-33.50	636723.7174	618386.5593	S 62°02'21.41" E	5.9300				
40012	11	20+28.16	-33.50	636720.9371	618391.7971	S 27°57'38.59" W	9.0000				
40013	11	20+28.16	-24.50	636712.9876	618387.5773	N 62°02'21.41" W	5.9300				
40010	11	20+22.23	-24.50	636715.7680	618382.3395						

FIGURE RW_2L.TCE AREA = 53.3700 SQ. FT. (0.0012 ACRES)

ASSESSMENT NUMBER	OWNERSHIP OF RECORD	TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)							
26-043.10-323	(3-L) BRANDYWINE OPERATING PARTNERSHIP LP	TCE	20050127-0010240	1.020							
ALIGNMENT NUMBER & DESCRIPTION: KING - KING STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
40021	KING	62+19.00	32.50	633211.0378	618283.0312	S 61°57'22.25" E	4.2500				
40022	KING	62+19.00	36.75	633209.0397	618286.7822	S 28°02'37.75" W	22.9740				
40023	KING	61+96.03	36.75	633188.7631	618275.9811	N 61°54'08.27" W	4.2500				
40024	KING	61+96.03	32.50	633190.7647	618272.2319	N 28°02'37.75" E	22.9700				
40021	KING	62+19.00	32.50	633211.0378	618283.0312						

FIGURE RW_3L.TCE AREA = 97.6310 SQ. FT. (0.0022 ACRES)

ASSESSMENT NUMBER	OWNERSHIP OF RECORD	TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)							
26-028.10-114	(2-R) YOUNG MEN'S CHRISTIAN ASSOCIATION OF DELAWARE	TCE	1661-165	0.064							
ALIGNMENT NUMBER & DESCRIPTION: 12 - TWELFTH STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
40017	12	1+74.54	31.00	637140.4025	618211.4845	N 27°46'38.38" E	8.0000				
40018	12	1+74.52	23.00	637147.4807	618215.2128	S 62°04'20.93" E	19.4800				
40019	12	1+94.00	23.00	637138.3571	618232.4242	S 27°55'39.07" W	3.0000				
40020	12	1+94.00	26.00	637135.7065	618231.0191	N 76°28'58.42" W	20.0911				
40017	12	1+74.54	31.00	637140.4025	618211.4845						

FIGURE RW_2R.TCE AREA = 107.1085 SQ. FT. (0.0025 ACRES)

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LEGEND	
FEE	AREA OF ACQUISITION
RW	AREA OCCUPIED BY EXISTING RW
PE	PERMANENT EASEMENT
TCE	TEMPORARY CONSTRUCTION EASEMENT
*	OFFSET IS LEFT OF BASELINE
**	CURVE TURNS TO THE LEFT



ADDENDUMS / REVISIONS

NOT TO SCALE

**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT	BRIDGE NO.	N/A
T200920013	DESIGNED BY:	J.J.O.
COUNTY	CHECKED BY:	T.A.O.
NEW CASTLE		

RIGHT-OF-WAY DATA SHEET	
SHEET NO.	77
TOTAL SHTS.	78

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COUNTY ASSESSMENT PARCEL NUMBER	PLAN SHEET NUMBER	OWNERSHIP OF RECORD	TITLE SOURCE	PROPERTY AREA BEFORE ACQUISITION (ACRE) D=DEED C=CALCULATED A=ASSESSMENT	ACQUISITION CODE FEE, R/W, P/E, TCE	AREA TO BE ACQUIRED				PROPERTY AREA REMAINING (SQ. FEET /ACRES)	DEED RECORD OF ACQUISITION	REMARKS
						ACQUISITION (SQ. FEET /ACRES)	AREA OCCUPIED BY EXISTING RIGHT OF WAY (SQ. FEET /ACRES)	EASEMENT				
								PERMANENT (SQ. FEET /ACRES)	TEMPORARY (SQ. FEET /ACRES)			
26-028.20-001	73 (RW-01) 74 (RW-02)	(1-L) CHRISTIANA CARE HEALTH SERVICES, INC.	2867-106/ M/F 14206	A - 10.98	TCE-1 TCE-2			644.7117 / 0.015 225.3115 / 0.005	478288.80 / 10.98			
26-028.30-370	75 (RW-03)	(2-L) 100 WEST TENTH STREET CORP	1884-333	D - 0.92	TCE			53.37 / 0.001	40246.00 / 0.92			
26-043.10-323	76 (RW-04)	(3-L) BRANDYWINE OPERATING PARTNERSHIP LP	20050127-0010240	A - 1.02	TCE			97.631 / 0.002	44431.20 / 1.02			
26-028.10-114	73 (RW-01)	(2-R) YOUNG MEN'S CHRISTIAN ASSOCIATION OF DELAWARE	1661-165	C - 0.06	TCE			107.1085 / 0.002	2774.50 / 0.06			

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ADDENDUMS / REVISIONS	

NOT TO SCALE

**WILMINGTON RENAISSANCE
PEDESTRIAN SAFETY
IMPROVEMENTS**

CONTRACT T200920013	BRIDGE NO. 	N/A
COUNTY NEW CASTLE	DESIGNED BY: J.J.O.	
	CHECKED BY: T.A.O.	

RIGHT-OF-WAY TABULATION SHEET	ACQUISITION CODES FEE - ACQUISITION R/W - AREA OCCUPIED BY EXISTING R/W	P/E - PERMANENT EASEMENT TCE - TEMPORARY EASEMENT
	SHEET NO. 78	TOTAL SHTS. 78