

GENERAL LOCATION OF CONTRACT

# THE STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION



CONSTRUCTION PLANS FOR:

## BAYARD SQUARE ENHANCEMENTS, CITY OF WILMINGTON

CONTRACT NUMBER: **T201020006**  
 FEDERAL AID PROJECT NUMBER: **ETEA-2010(10)**  
 COUNTY: **NEW CASTLE** M.R. #: **011**



PROJECT LOCATION  
NOT TO SCALE

U.S. CUSTOMARY  
UNITS

### DESIGN DESIGNATION

FUNCTIONAL CLASS: PRINCIPAL ARTERIAL	D.H.V. PROJECTED: N/A	YEAR: N/A
TYPE OF CONSTRUCTION: PEDESTRIAN IMPROVEMENTS	DESIGN SPEED: 25 M.P.H.	
A.A.D.T. CURRENT: 19,671	YEAR: 2005	TRUCKS: N/A %
A.A.D.T. PROJECTED: N/A	YEAR: N/A	DIRECTION OF DISTRIBUTION: N/A %

### INDEX OF SHEETS

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TOTAL SHEETS: 33

### APPROVED DESIGN EXCEPTIONS

DESIGN PARAMETER	REQUIRED	PROVIDED	DATE

### ADDENDA & REVISIONS

DESCRIPTION	NAME & DATE

### ASSOCIATED CONTRACTS

CONTRACT NO.	CONTRACT NAME
24-011-02	SR 2, SOUTH UNION STREET FROM RR BRIDGE TO SYCAMORE STREET, WILMINGTON

PREPARED BY  
THE CONSULTING FIRM OF  
**CENTURY ENGINEERING**  
CONSULTING ENGINEERS, SURVEYORS  
4134 N. DUPONT HWY.  
DOVER, DELAWARE 19901  
(302) 734-9188

**WILLIAM FRANKLYN COMPTON**  
REGISTERED PROFESSIONAL ENGINEER  
No. 12391  
DELAWARE

*William S. Compton* 4-30-12  
RECOMMENDED DATE

SPONSORED BY  
*Bayard Square Civic Association* 6/23/12  
BAYARD SQUARE CIVIC ASSOCIATION DATE

RECOMMENDED  
*Karl S.* 7/10/12  
CITY OF WILMINGTON DATE

RECOMMENDED  
*[Signature]* 8/14/12  
SQUAD MANAGER, CONSTRUCTION DATE

*[Signature]* 8/14/12  
GROUP ENGINEER, CONSTRUCTION DATE

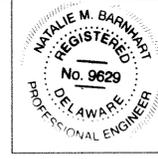
ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS (CONSTRUCTION) DATE

RECOMMENDED <i>Vincent W. Davis</i> STORMWATER ENGINEER DATE <u>25 SEP 2012</u>	<del>RECOMMENDED</del> SQUAD MANAGER, TRANSPORTATION SOLUTIONS (PROJECT DEVELOPMENT OR BRIDGE DESIGN) DATE _____	<del>RECOMMENDED</del> BRIDGE DESIGN ENGINEER DATE _____	RECOMMENDED <i>[Signature]</i> TRANSPORTATION ENHANCEMENTS PROGRAM MANAGER DATE <u>8-2-12</u>	<del>RECOMMENDED</del> ASSISTANT DIRECTOR, TRANSPORTATION SOLUTIONS DATE _____	APPROVED <i>Natalie Reinhart</i> CHIEF ENGINEER DATE <u>10/5/12</u>
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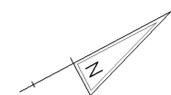
UNOFFICIAL  
WEBSITE  
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END CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 109 + 05.17

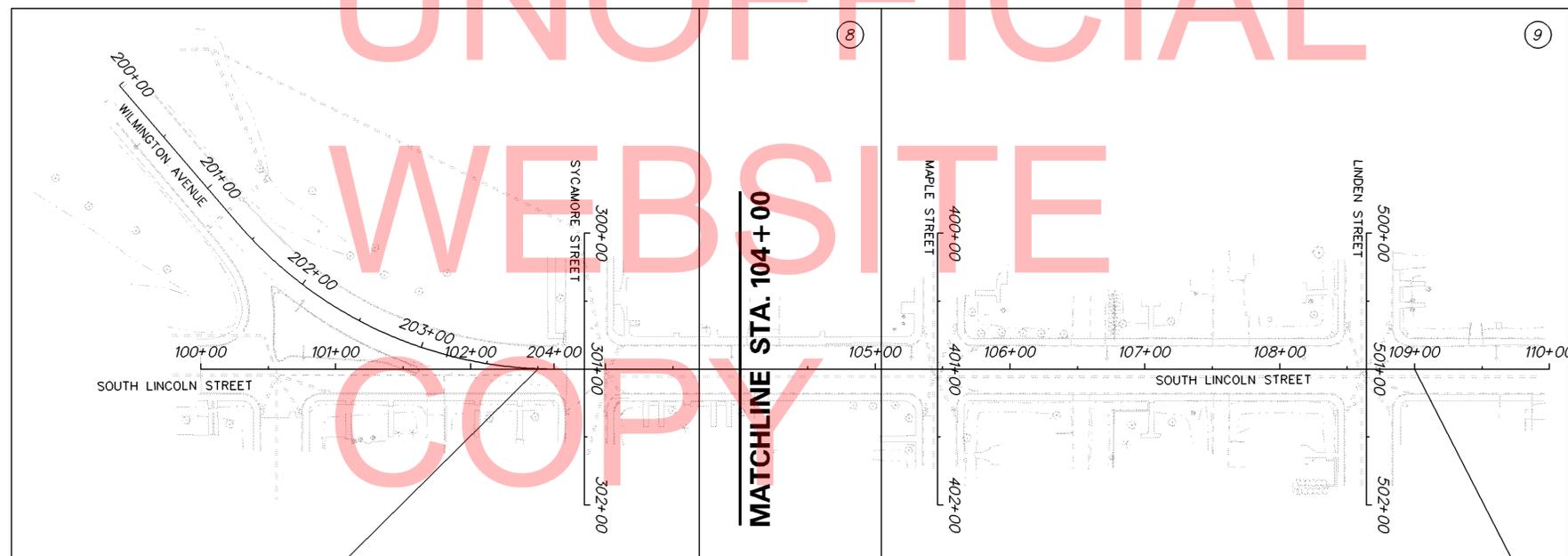
BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102 + 45.00



PLAN SHEET INDEX CROSS REFERENCE		
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SIGNING & STRIPING PLANS	27	28
RIGHT-OF-WAY PLANS	30	31



← TO ELSMERE



TO WILMINGTON →

**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102+45.00**

**END CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 109+05.17**

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ADDENDUMS / REVISIONS



**BAYARD SQUARE ENHANCEMENTS,  
CITY OF WILMINGTON**

CONTRACT	BRIDGE NO.	-
T201020006	DESIGNED BY:	AMW
COUNTY	CHECKED BY:	WFC
NEW CASTLE		

**PLAN SHEET INDEX**

SHEET NO.	2
TOTAL SHTS.	33

## EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE INLET
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

MANMADE ROADSIDE FEATURES	
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

RIGHT-OF-WAY SYMBOLS	
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE
	HISTORIC RIGHT-OF-WAY BASELINE
	EXISTING RIGHT-OF-WAY
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING DENIAL OF ACCESS
	EXISTING R/W & DENIAL OF ACCESS

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC METER
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER VENT OR CLEANOUT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE GUY WIRE ANCHOR
	UTILITY POLE
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD
	MANHOLE - UNDETERMINED OWNER

UTILITY COMPANY FACILITIES	
	ARTESIAN WATER (UNDERGROUND)
	DELMARVA POWER (ELECTRIC)
	DELMARVA POWER (GAS)
	CITY OF WILMINGTON (SANITARY SEWER)
	CITY OF WILMINGTON (WATER)
	COMCAST
	VERIZON
	CAVALIER TELEPHONE

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	BRICK PATTERNED SURFACE
	BUTT JOINT
	CONSTRUCTION BASELINE
	CONSTRUCTION SAFETY FENCE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CURB RAMP
	CLEAR ZONE
	DRAINAGE INLET
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPE 1
	GUARDRAIL, TYPE 2
	GUARDRAIL, TYPE 3
	GUARDRAIL END ANCHORAGE
	GUARDRAIL END TREATMENT, TYPE 1
	GUARDRAIL END TREATMENT, TYPE 2
	GUARDRAIL END TREATMENT, TYPE 3
	HORIZONTAL CLEARANCE
	IMPACT ATTENUATOR
	JUNCTION BOX - DRAINAGE
	LIMIT OF CONSTRUCTION
	MANHOLE
	PAVEMENT PATCH
	PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK @ 4"
	P.C.C. SIDEWALK @ 6"
	UNDERDRAIN
	UNDERDRAIN OUTLET

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE

## PROPOSED SYMBOLS

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	CONSTRUCTION SAFETY FENCE
	DRAINAGE INLET
	DO NOT DISTURB
	ENERGY DISSIPATOR
	FENCE
	FLARED END SECTION
	FILL WITH FLOWABLE FILL
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY OTHERS
	UNDERDRAIN / LENGTH
	UNDERDRAIN OUTLET PIPE

LANDSCAPING	
	LANDSCAPE PLANTINGS
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN

PROPOSED UTILITIES	
	PROPOSED DELMARVA POWER (GAS)

PAVEMENT SECTION(S)	
	BRICK PAVER SIDEWALK (HERRING BONE PATTERN)
	BRICK PAVER SIDEWALK (RUNNING BOND PATTERN)
	WARM MIX ASPHALT PATCHING
	ASPHALT MILL AND OVERLAY
	CONCRETE MILL AND HOTMIX OVERLAY

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GENERAL NOTES

- THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001, THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "MUTCD," DATED 2011, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
- THE CONTRACTOR SHALL GIVE TWO (2) WEEKS NOTICE TO THE PROPERTY OWNER WHEN ANY FIXTURE, SHRUB OR OTHER OBJECT MUST BE REMOVED FROM THE RIGHT OF WAY OR EASEMENT AREA. IF THE OWNER HAS NOT ATTEMPTED TO SALVAGE THIS PROPERTY, THE CONTRACTOR SHALL REMOVE IT WITHOUT OBLIGATION. COMPENSATION SHALL BE INCIDENTAL TO THE CONTRACT.
- THE ENDS OF ALL CURBS SHALL BE DEPRESSED FLUSH WITH THE PAVEMENT AT A RATIO OF TWELVE TO ONE (12:1) UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL PVC SLEEVES (4" INSIDE MINIMUM DIAMETER, 6" INSIDE MAXIMUM DIAMETER) IN PROPOSED CONCRETE AND BRICK SIDEWALKS, ISLANDS, AND MEDIANS FOR FUTURE TRAFFIC SIGN POSTS AS DIRECTED BY THE ENGINEER. THE LOWER END OF THE SLEEVE SHALL SIT ON THE TOP OF THE SUBBASE MATERIAL. THE COST SHALL BE INCIDENTAL TO THE CONTRACT.
- STAGING AREAS - PROPER EROSION AND SEDIMENT CONTROL MEASURES AS DETERMINED BY THE ENGINEER SHALL BE INSTALLED IN ALL STAGING AREAS. ALL AREAS USED BY THE CONTRACTOR FOR STAGING OPERATIONS SHALL BE FULLY RESTORED BY THE CONTRACTOR UPON COMPLETION OF THE CONTRACT. IF THE STAGING AREA IS PAVED, IT SHALL BE RESTORED TO ITS ORIGINAL CONDITION. IF THE AREA IS UNPAVED, IT SHALL BE RE-GRADED, TOPSOILED, SEEDED AND MULCHED IN ACCORDANCE WITH DELAWARE STANDARD SPECIFICATIONS 732, 734 AND 735, FOR TOPSOIL, SEED AND MULCH RESPECTIVELY, TO THE SATISFACTION OF THE ENGINEER. THE SEED SHALL ADHERE TO THE SPECIFICATIONS OF SECTION 734 FOR PERMANENT GRASS SEEDING - DRY GROUND. ALL COSTS ASSOCIATED WITH RESTORATION OF THE STAGING AREA SHALL BE AT THE CONTRACTOR'S EXPENSE. IF THE ENGINEER DETERMINES THAT A SATISFACTORY STAND OF GRASS DOES NOT EXIST AT THE TIME OF FINAL INSPECTION, ALL COSTS ASSOCIATED WITH REESTABLISHING A SATISFACTORY STAND OF GRASS SHALL BE AT THE CONTRACTOR'S EXPENSE. ALL STAGING AREAS SHALL BE SECTIONED OFF FROM THE TRAVELING PUBLIC USING TRAFFIC CONTROL DEVICES APPROVED BY THE ENGINEER, ALL COSTS SHALL BE PAID BY THE RESPECTIVE TRAFFIC CONTROL ITEM.
- SITE REVIEWER - AN EROSION CONTROL SITE REVIEWER SHALL BE A PERSON FROM THE CONTRACTOR'S STAFF ASSIGNED TO EROSION AND SEDIMENT CONTROL IMPLEMENTATION AND MAINTENANCE AND SHALL BE REQUIRED ON SPECIFIC PROJECTS. THE NAME AND DNREC CERTIFICATION NUMBER OF EACH SITE REVIEWER SO REQUIRED SHALL BE SUBMITTED TO THE DEPARTMENT. THE NAME OF THE DELAWARE REGISTERED PROFESSIONAL ENGINEER PROVIDING DIRECTION AND SUPERVISION OF THE SITE REVIEWER, AS REQUIRED IN SECTION 12.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS, SHALL ALSO BE SUBMITTED TO THE DEPARTMENT. THE SITE REVIEWER REQUIREMENTS IN EFFECT ON THIS PROJECT SHALL BE MARKED WITH AN "X" BELOW:

EROSION POTENTIAL FOR THIS PROJECT	SITE REVIEWER REQUIREMENT
( ) INSIGNIFICANT	NONE
( X ) MINOR	CONTRACTOR CERTIFICATION COURSE TRAINING ONLY, AS DEFINED IN SECTION 13 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
( ) MEDIUM	AT THE TIME OF BID OF THE CONTRACT, EITHER THE SUPERINTENDENT OR A SEPARATE INDIVIDUAL FROM THE CONTRACTOR'S STAFF SHALL BE A CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 12 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
( ) MAJOR	SUPERINTENDENT AND AN INDIVIDUAL FROM CONTRACTOR'S STAFF SHALL BE CCR. ONE INDIVIDUAL FROM THE CONTRACTOR'S STAFF MUST BE A CCR AT THE TIME OF BID OF THE CONTRACT. THE SUPERINTENDENT MUST BECOME A CCR WITHIN ONE YEAR AFTER THE AWARD OF CONTRACT.

7. ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR INCLUDE:

( )	NONE
( )	ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
( X )	RASTER FILES, IN .CAL AND .PDF FILE FORMAT, FOR ALL PLAN SHEETS.
( )	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
( )	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
( )	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

8. AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

( )	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
( X )	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR.
( )	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR.

9. THE DISTURBED AREA FOR THIS PROJECT IS 0.28 ACRES.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADHERING TO THE CONSTRUCTION SITE POLLUTION PREVENTION SPECIFICATIONS AS DETAILED IN SECTION 3.6 OF THE "DELAWARE EROSION AND SEDIMENT CONTROL HANDBOOK". ALL COSTS ASSOCIATED WITH ADHERING TO THE STANDARDS SHALL BE INCIDENTAL TO THE OVERALL CONTRACT COSTS.

11. THE EROSION AND SEDIMENT CONTROL PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE EROSION AND SEDIMENT CONTROL PLANS ARE VALID FOR A THREE YEAR PERIOD, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR SHALL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE EROSION AND SEDIMENT CONTROL PLAN APPROVAL. DELDOT WILL REVIEW THE CURRENT EROSION AND SEDIMENT CONTROL PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.

- EXISTING UTILITIES ARE SHOWN IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION. COMPLETENESS OR CORRECTNESS THEREOF IS NOT GUARANTEED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE UTILITY COMPANIES INVOLVED IN ORDER TO OBTAIN THE MOST ACCURATE INFORMATION AVAILABLE AS TO UTILITY LOCATION AND ELEVATION. NO CONSTRUCTION AROUND OR ADJACENT TO UTILITIES SHALL BEGIN WITHOUT NOTIFYING THEIR OWNERS AT LEAST TWO WORKING DAYS (48 HOURS) IN ADVANCE. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE EXISTING UTILITIES AND MAINTAIN UNINTERRUPTED SERVICE. ANY DAMAGE DONE TO THEM DUE TO HIS/HER NEGLIGENCE SHALL BE IMMEDIATELY AND COMPLETELY REPAIRED AT THE CONTRACTOR'S EXPENSE. TO LOCATE EXISTING UTILITIES IN THE FIELD PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL CONTACT MISS UTILITY OF DELMARVA THREE (3) CONSECUTIVE WORKING DAYS PRIOR TO EXCAVATION, AT 1-800-282-8555.
- THE CONTRACTOR WILL BE REQUIRED TO COORDINATE WORK WITH ANY CONSTRUCTION ACTIVITY BY UTILITY COMPANIES, OR ADJACENT CONSTRUCTION CONTRACTS. PROPOSED UTILITY RELOCATIONS ARE SHOWN ON THE PLANS. REFER TO UTILITY STATEMENT FOR DETAILED SCOPE AND TIMING OF UTILITY WORK.
- ALL PROPOSED UTILITY WORK SHOWN ON THE PLANS SHALL BE INSTALLED BY THE RESPECTIVE UTILITY COMPANY, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL SUPPLY MESSAGE BOARDS THAT ARE TO BE PLACED TEN (10) DAYS PRIOR TO ANY ROAD CONSTRUCTION AND THE FIRST FIVE (5) DAYS OF CONSTRUCTION. THE PROPER WORDING ON THE MESSAGE BOARD WILL NEED TO BE COORDINATED WITH THE DISTRICT SAFETY OFFICER.
- THE CONTRACTOR SHALL HAVE THE RIGHT-OF-WAY AND EASEMENT LINES STAKED OUT AND VERIFIED PRIOR TO PERFORMING ANY WORK. THE RIGHT OF WAY LINES MUST REMAIN STAKED FOR THE DURATION OF THE WORK.
- IF GPS IS UTILIZED FOR THIS PROJECT THE CONTRACTOR SHALL LOCALIZE ON THE SITE SPECIFIC CONTROL AS PROVIDED. ALL COSTS ARE INCIDENTAL TO ITEM 763501.

PROJECT NOTES

SECTION 100

- ANY DAMAGE DONE BY THE CONTRACTOR TO ITEMS NOTED TO BE RELOCATED OR RESET BY OTHERS OR BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE CONTRACTOR SHALL, AS PART OF HIS PROJECT SCHEDULE, SUBMIT TO THE ENGINEER AN ESTIMATE OF THE MONTHLY PAYMENTS EXPECTED TO BE RECEIVED ON THE CONTRACT. THIS WILL BE REFERENCED AS THE "MONTHLY PAYMENT CHART".

A CHART IN MICROSOFT EXCEL, MICROSOFT WORD, OR HAND WRITTEN FORMAT WILL BE ACCEPTABLE FOR THIS PURPOSE. THE CHART SHOULD INCLUDE, AS A MINIMUM, COLUMNS FOR THE MONTH, YEAR, AND ESTIMATED MONTHLY PAYMENTS. THE TOTAL OF ALL ESTIMATED MONTHLY PAYMENTS SHOULD EQUAL THE AWARDED CONTRACT TOTAL BID PRICE.

THE ENGINEER MAY REQUEST AN UPDATED "MONTHLY PAYMENT CHART" AT HIS DISCRETION, DEPENDING ON THE ACCURACY OF THE INITIAL ESTIMATES AND ACCORDING TO THE OVERALL NEEDS OF THE DEPARTMENT.

THE "MONTHLY PAYMENT CHART" WILL NOT BE CONSIDERED A BINDING DOCUMENT BY EITHER THE CONTRACTOR OR THE DEPARTMENT AND IS CONSIDERED SOLELY INFORMATIONAL.

ON PROJECTS REQUIRING CPM SCHEDULES, THE CONTRACTOR MAY, BUT IS NOT REQUIRED TO, "COST LOAD" THE CPM SCHEDULE IN ORDER TO GENERATE THE MONTHLY SPEND PAYMENT CHART.

COSTS TO PREPARE AND/OR UPDATE THE "MONTHLY PAYMENT CHART" ARE ADDRESSED AS FOLLOWS:

- ON CONTRACTS REQUIRING CPM SCHEDULES AND UPDATES, PREPARATION OF THE INITIAL CHART SHALL BE INCIDENTAL TO ITEM 763508. UPDATES SHALL BE INCIDENTAL TO ITEM 763509.
- ON CONTRACTS NOT REQUIRING CPM SCHEDULES, THE COST TO PREPARE AND UPDATE THE "MONTHLY PAYMENT CHART" SHALL BE INCLUDED IN ITEM 763000, INITIAL EXPENSE.

3. MOWING OF THE ROADSIDE WITHIN THE PROJECT LIMITS SHALL BE AT THE DISCRETION OF THE ENGINEER, AS IDENTIFIED IN THE STANDARD SPECIFICATIONS (SECTION 105.13). THE COST OF MOWING SHALL BE INCIDENTAL TO ALL CONTRACT ITEMS.

SECTION 200

- THE CONTRACTOR SHALL REMOVE AND RESET ALL MAILBOXES TO MAINTAIN MAIL SERVICE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL RELOCATE MAILBOXES AS REQUIRED BY THE PROPOSED GEOMETRICS AND AS DIRECTED BY THE ENGINEER. WHEN RELOCATING MAILBOXES IN CURBED SECTIONS, THE FACE OF THE MAILBOX SHALL BE FLUSH WITH THE BACK EDGE OF CURB. WHEN RELOCATING MAILBOXES IN OPEN SECTIONS, THE FACE OF THE MAILBOX SHALL SET BACK 8 INCHES FROM THE EDGE OF THE PAVED SHOULDER. THE BOTTOM OF THE MAILBOX SHALL BE SET 46 INCHES ABOVE THE ROADWAY SURFACE. MAILBOXES LOCATED AT DRIVEWAY ENTRANCES SHALL BE PLACED ON THE FAR SIDE OF THE DRIVEWAY IN THE DIRECTION OF TRAVEL. POSTS BEING RESET IN CONCRETE SIDEWALK SHALL BE PLACED IN AN APPROPRIATE SIZE PVC SLEEVE. COST FOR ALL WORK AND MATERIALS SHALL BE INCIDENTAL TO ITEM 201000 - CLEARING AND GRUBBING.
- IN AREAS WHERE TREES OR SHRUBS WILL BE OVERHANGING THE PROPOSED SIDEWALK, PRUNING MAY BE NECESSARY TO ACHIEVE A VERTICAL CLEAR SPACE OF 10 FEET ABOVE THE PROPOSED SIDEWALK ELEVATION. THE CONTRACTOR SHALL PRUNE EXISTING TREE AND SHRUB BRANCHES, WHICH OVERHANG THE SIDEWALK, IN ACCORDANCE WITH I.S.A. STANDARDS. THE CONTRACTOR SHALL NOTIFY DELDOT'S ROADSIDE ENVIRONMENTAL ADMINISTRATOR, EUGENE 'CHIP' ROSAN, JR. (302) 760-2185 AND/OR HIS DESIGNEE AND THE CITY OF WILMINGTON'S URBAN FOREST ADMINISTRATOR, AMANDA TOLINO, (302) 576-2582 OR ATOLINO@WILMINGTONDE.GOV, AT LEAST TWO (2) DAYS PRIOR TO THE PRUNING OPERATION. ALL COSTS ASSOCIATED WITH THE ABOVE WORK TO BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.
- THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXCAVATE TEST PITS ALONG THE PROPOSED CONSTRUCTION AT POINTS OF POSSIBLE UTILITY CONFLICTS, TO DETERMINE IF CONFLICTS EXIST. ANY CONFLICTS SHALL BE COORDINATED BY THE CONTRACTOR, WITH THE ENGINEER AND THE UTILITY COMPANY INVOLVED. THE ENGINEER SHALL ULTIMATELY DETERMINE THE SOLUTION TO THE UTILITY CONFLICT. TEST HOLES SHALL BE MEASURED AND PAID FOR IN ACCORDANCE WITH ITEM 208000, BUT ONLY TO THE ACTUAL DEPTH EXCAVATED.

SECTION 200

- ITEMS TO BE REMOVED UNDER ITEM 21000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS SHALL INCLUDE BUT NOT BE LIMITED TO THE FOLLOWING: FOUNDATIONS, GATES, AND STRUCTURES, WHICH ARE NOT DESIGNATED TO REMAIN WITHIN THE LIMITS OF CONSTRUCTION.
- THIS PROJECT IS COVERED UNDER A "NPDES" GENERAL PERMIT FOR CONSTRUCTION, UNDER THE GENERAL PERMIT, COMPLIANCE WITH DELDOT'S APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS WILL CONSTITUTE COMPLIANCE WITH THE NPDES INDUSTRIAL PERMITTING REQUIREMENTS FOR THIS CONSTRUCTION PROJECT. A COPY OF THE NPDES GENERAL PERMIT AND NOIS KEPT ON FILE IN EACH OF THE CONSTRUCTION OFFICES AND THE DEPARTMENT'S TEAM SUPPORT SECTION. A COPY OF THE GENERAL PERMIT OR THE NOIS CAN BE OBTAINED UPON REQUEST FROM EITHER THE DEPARTMENT'S STORMWATER ENGINEER OR THE APPROPRIATE CONSTRUCTION ENGINEER.
- REMOVAL OF EXISTING UNDERDRAIN IF ENCOUNTERED SHALL BE INCIDENTAL TO ITEM 202000. CONNECTING TO AN EXISTING UNDERDRAIN SYSTEM SHALL BE INCIDENTAL TO THE UNDERDRAIN ITEM BEING INSTALLED AS SHOWN ON THE PLANS IF REQUIRED.
- EXCEPT WHERE NOTED ON THE PLANS, THE CONTRACTOR SHALL CLEAR ALL TREES AND SHRUBS WITHIN THE LIMITS OF CONSTRUCTION. THE CONTRACTOR SHALL PROTECT TREES NOTED ON THE PLANS BY "DND" BY PLACING CONSTRUCTION SAFETY FENCE AT THE DRIPLINE OF THE TREES WITHIN THE LIMIT OF CONSTRUCTION AS SHOWN ON THE PLANS. THIS SHALL BE INCIDENTAL TO ITEM 201000.
- ALL TREE STUMPS AND ROOT MAT TO BE REMOVED SHALL BE ENTIRELY REMOVED REGARDLESS OF DEPTH. BURIAL OF ORGANIC MATTER IS NOT PERMITTED. CHIPPED MATERIAL MUST BE REMOVED PRIOR TO PLACING EMBANKMENT OR TOPSOIL.
- ROADSIDE AMENITIES WITHIN THE LIMITS OF CONSTRUCTION OR EASEMENT AREAS THAT ARE TO REMAIN IN PLACE AND ARE NOTED BY "DND" SHALL BE PROTECTED BY THE CONTRACTOR WITH SAFETY FENCE OR OTHER MEASURES AS DIRECTED BY THE ENGINEER. ALL COSTS SHALL BE INCIDENTAL TO ITEM 201000. ANY DAMAGE TO THESE TO THESE AMENITIES WHICH IS DONE BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.
- ALL EXISTING PIPES TO BE REMOVED SHALL BE HANDLED IN ACCORDANCE WITH SECTION 202.04.
- EXISTING PROPERTY MARKERS AND MONUMENTS THAT WILL BE DISTURBED DURING CONSTRUCTION SHALL BE REMOVED WITH REASONABLE CARE AND STORED PRIOR TO CLEARING AND GRUBBING. AFTER CONSTRUCTION, THE CONTRACTOR SHALL RESET THE PROPERTY MARKERS IN ACCORDANCE WITH SECTION 107.29 AT THE INTERSECTION OF THE RIGHT-OF-WAY AND PROPERTY LINE. WHERE MARKERS ARE TO BE RESET IN PROPOSED P.C.C. CONCRETE SIDEWALK, A PK NAIL SHALL BE SET IN THE CONCRETE TO IDENTIFY THE PROPERTY CORNER, IF EXISTING RIGHT-OF-WAY MARKER IS NOT RE-USABLE AS A RESULT OF THE METHOD THE CONTRACTOR USED TO REMOVE OR RESET IT, THE CONTRACTOR SHALL REPLACE IT WITH A COMPARABLE MARKER. ANY DISTURBED MARKERS SHALL BE RESET BY A REGISTERED DELAWARE PROFESSIONAL LAND SURVEYOR AT THE CONTRACTOR'S EXPENSE.
- TYPE A BORROW SHALL BE USED TO BACKFILL ALL DRY UNDERCUT AREAS AS DIRECTED BY THE ENGINEER.

SECTION 300

16. A. THE CONTRACTOR MAY ELECT TO USE THE FOLLOWING MATERIALS FOR ITEM 302007 (GRADED AGGREGATE BASE COURSE, TYPE B) PROVIDED THEY MEET THE REQUIREMENTS OF ITEM 302007. DELDOT'S M&R SECTION SHALL HAVE THE FINAL APPROVAL ON USING THE MATERIAL:
- a. CRUSHED STONE (PER STANDARD SPEC. 821)
  - b. CRUSHED CONCRETE (PER STANDARD SPEC. 821)
  - c. HOT MIX MILLINGS (PER SPECIAL PROVISION 302514)

THE CONTRACTOR WILL NOT BE ALLOWED TO MIX DIFFERENT MATERIALS (OR SIMILAR MATERIALS FROM ANY DIFFERENT SOURCES) TO MEET THE REQUIREMENTS OF 302007. ALL THE ABOVE LISTED MATERIALS ARE PERMITTED FOR USE ON THE JOB, PROVIDED THEY ARE SEPARATED INTO APPROVED AREAS. EACH AREA OF BASE COURSE MUST BE CONSTRUCTED USING MATERIALS FROM A SINGULAR SOURCE. FULL DEPTH, IN ORDER THAT PROPER TESTING MAY BE ACCOMPLISHED. THE CONTRACTOR AND DELDOT PROJECT RESIDENT SHALL AGREE ON THE LIMITS OF EACH SOURCE OF MATERIAL PRIOR TO PLACEMENT.

- B. THE QUANTITY USED FOR BASE COURSE OF EACH OF THE ABOVE LISTED MATERIALS WILL BE THE CONTRACTOR'S CHOICE, WITH THE TOTAL MEETING THE ADVERTISED QUANTITY OF ITEM 302007.
- C. THE CONTRACTOR MAY ALSO ELECT TO RECYCLE MILLINGS FOR THE USE IN HOT MIX AS PERMITTED BY THE STANDARD SPECIFICATIONS. THE OF THE QUANTITY OF MILLINGS USED FOR THIS PURPOSE, OR FOR BASE COURSE, LIES WITH THE CONTRACTOR.
- D. HOT MIX MILLINGS MAY BE GENERATED FROM THE FOLLOWING SOURCES:
  - a. MATERIAL MADE AVAILABLE WHEN MILLED ON THIS CONTRACT.
  - b. MATERIAL MILLED ON THIS CONTRACT AT THE CONTRACTOR'S CHOICE UNDER ITEM 302007
  - c. MILLED MATERIAL FURNISHED ON THE JOB FROM THE CONTRACTOR'S YARD OR OTHER OUTSIDE SOURCE. ALL MILLED MATERIALS SHALL BE REQUIRED TO MEET THE MATERIAL REQUIREMENTS OF ITEM 302514.
- E. PAYMENT CLARIFICATION:
  - a. SHOULD THE CONTRACTOR ELECT TO MILL PORTIONS OF HOT MIX SHOWN ON THE PLANS TO BE REMOVED UNDER ITEM 202000, THE COST OF MILLING THIS HOT MIX WILL BE INCIDENTAL TO ITEM 202000. THE MILLINGS GENERATED MAY BE RECYCLED INTO HOT MIX, UTILIZED FOR BASE COURSE, OR DISPOSED OF TO AN APPROVED SITE. HAULING COSTS FOR DISPOSAL AND/OR RECYCLING ARE INCIDENTAL TO ITEM 202000.
  - b. MILLINGS GENERATED MAY BE RECYCLED INTO HOT MIX, UTILIZED FOR BASE COURSE OR DISPOSED BY THE CONTRACTOR TO AN APPROVED SITE. NO SEPARATE PAYMENT WILL BE MADE FOR TRANSPORTING MILLINGS ON SITE OR TO AN APPROVED SITE.
  - c. SHOULD THE CONTRACTOR ELECT TO TEMPORARILY STOCKPILE MILLINGS ON THE JOB SITE FOR LATER USE, ALL COSTS FOR STOCKPILING AND SUBSEQUENT REHANDLING SHALL BE INCIDENTAL TO ITEM 202000.
  - d. MILLINGS USED FOR BASE COURSE SHALL BE PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF PROVISION 302514. NO SEPARATE PAYMENT WILL BE MADE TO FURNISH MILLINGS FROM AN OUTSIDE SOURCE OR TRANSPORT MILLINGS WITHIN THE PROJECT LIMITS. MILLINGS USED FOR BASE COURSE WILL BE PAID IN PLACE AT THE UNIT BID PRICE FOR ITEM 302007.
  - e. ALL COSTS TO UTILIZE THE MILLINGS IN HOT MIX WILL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE HOT MIX ITEM USING THE RECYCLED MATERIAL.
  - f. SPECIAL PROVISION 302514 IS PROVIDED TO SPECIFY THE MEANS OF LAY DOWN AND COMPACTION AS THE MATERIAL REQUIREMENTS FOR MILLINGS USED AS BASE COURSE. ALL COSTS TO BRING THE MILLINGS INTO COMPLIANCE WITH THE REQUIREMENTS OF 302514 AND THE QUANTITY OF MILLINGS USED FOR BASE COURSE WILL BE PAID FOR UNDER ITEM 302007.

SECTION 400

17. THE PAVEMENT SECTION FOR HOT-MIX RESIDENTIAL DRIVEWAYS, IF REQUIRED, SHALL BE 2" WARM MIX ASPHALT, TYPE C OVER 8" GRADED AGGREGATE BASE COURSE, UNLESS OTHERWISE IDENTIFIED ON THE PLANS.

SECTION 600

18. THE DEPARTMENT AND THE CONTRACTOR SHALL INSPECT ALL EXISTING PIPES AND DRAINAGE STRUCTURES TO BE USED IN THE FINAL DRAINAGE SYSTEM AND AGREE ON AND MAKE RECORD OF THE CONDITION PRIOR TO CONSTRUCTION. DAMAGES TO EXISTING PIPES AND DRAINAGE STRUCTURES DUE TO CONTRACTOR OPERATIONS SHALL BE REPAIRED OR REPLACED IN-KIND AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR IS REQUIRED TO VIDEO INSPECT NEW PIPE AND EXISTING PIPE RUNS (TO BE USED IN THE FINAL SYSTEM) TO CONFIRM CONDITION PRIOR TO ACCEPTANCE. PAYMENT FOR VIDEO INSPECTION OF PIPES WILL BE MADE UNDER ITEM 612529 - VIDEO INSPECTION.

SECTION 700

19. IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSITIONED IN 10 LINEAR FEET OR AS DIRECTED BY THE ENGINEER IN THE FIELD. PAYMENT FOR TRANSITIONING SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.
20. WHERE NEW SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, SAWCUT THE EXISTING SIDEWALK OR MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER AND SHALL BE PAID FOR IN ITEM 762002- SAWCUTTING, CONCRETE, FULL DEPTH.
21. STATION AND ELEVATION DATA GIVEN FOR DRAINAGE STRUCTURES ARE TO BE APPLIED ALONG THE CENTERLINE OF THE STRUCTURE WITH ITS INTERSECTION TO THE FLOWLINE FOR CURB INLETS, THE CENTER OF THE STRUCTURE FOR LAWN BASINS AND JUNCTION BOXES, AND THE CENTER OF THE LID FOR MANHOLES.
22. DRAINAGE INLET GRATES ADJACENT TO THE ROAD, WITHIN THE PROJECT LIMITS, WHICH ARE NOT TYPE 1 SHALL BE REPLACED. THE ACTUAL LOCATIONS, THE NEED FOR ANY GRATE MODIFICATIONS OR FOR NEW FRAMES SHALL BE DETERMINED BY THE ENGINEER. ALL REPLACED GRATES/FRAMES SHALL BE DELIVERED TO THE NEAREST DISTRICT MAINTENANCE YARD WITH THE COST OF DELIVERY INCIDENTAL TO ITEM 708500 - REPLACING CATCH BASIN GRATES. FINAL PAYMENT FOR REPLACED GRATES/FRAMES SHALL NOT BE MADE UNTIL RECEIPT OF DELIVERED MATERIALS IS PRODUCED, SIGNED BY A DELDOT MAINTENANCE YARD SUPERVISOR.
23. THE NEW CASTLE COUNTY DEPARTMENT OF PUBLIC WORKS SHALL SUPPLY AND THE STATE'S CONTRACTOR SHALL INSTALL NEW SELF SEALING MANHOLE FRAMES AND COVERS ON ALL COUNTY SEWER MANHOLES, THAT ARE NOT BEING RELOCATED, WITHIN THE PROJECT LIMITS IN ACCORDANCE WITH THE COUNTY'S STANDARD SPECIFICATIONS. THE EXISTING MANHOLE FRAMES AND COVERS THAT ARE REMOVED SHALL BECOME THE PROPERTY OF THE STATE'S CONTRACTOR. PAYMENT SHALL BE INCIDENTAL TO ITEM 710506 - ADJUST AND REPAIR EXISTING SANITARY SEWER MANHOLE.
24. ALL DISTURBED AREAS WITHIN THE LIMIT OF CONSTRUCTION, SHALL BE TOPSOILED (6" MINIMUM), SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. ALL COSTS ARE PAID UNDER THE RESPECTIVE ITEMS BEING INSTALLED.
25. THE CONTRACTOR SHALL INSTALL ALL PERMANENT SIGNAGE AND TEMPORARY SIGNAGE AS SHOWN ON THE CONTRACT DOCUMENTS. ALL COSTS COVERED UNDER THE APPROPRIATE ITEMS.
26. ALL PAVED AREAS (INCLUDING DRIVEWAYS) TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT.
27. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.
28. DURING CONSTRUCTION OF THE SIDEWALK, ALL BARE AREAS SHALL BE STABILIZED AT THE END OF EACH WORK DAY. BARE AREAS SHALL BE EITHER PERMANENTLY STABILIZED OR COVERED WITH GABC.
29. REPLACEMENT OF EXISTING AND/OR DAMAGED RESIDENTIAL ROOF DRAINAGE PIPES UNDER THE PROPOSED SIDEWALK SHALL BE INCIDENTAL TO ITEM 705504 BRICK SIDEWALK.

MISCELLANEOUS

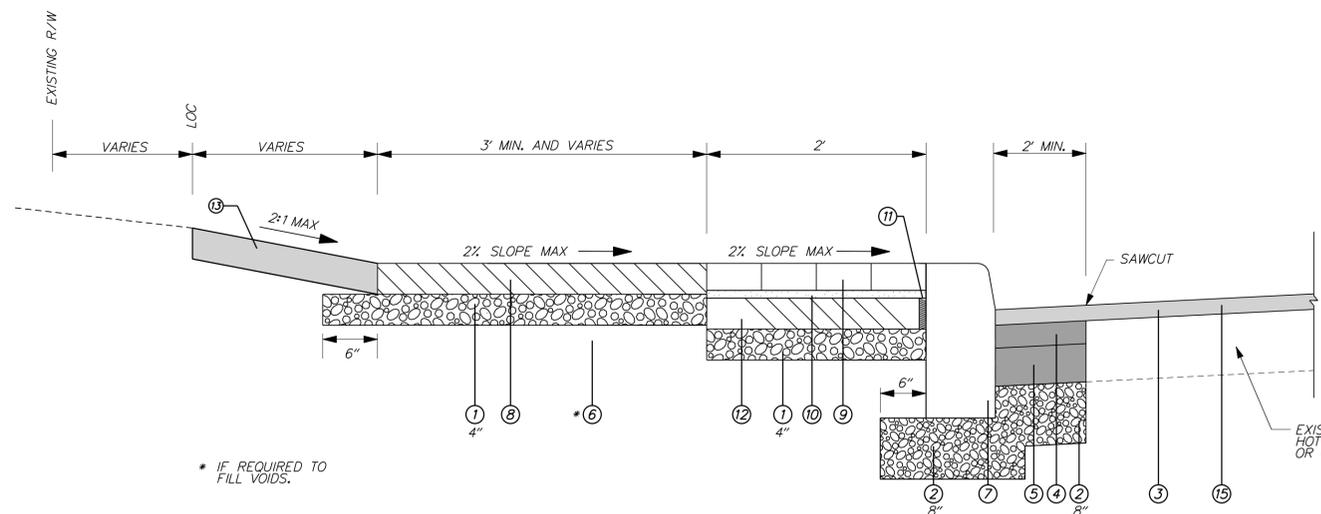
30. THERE ARE NO ENVIRONMENTAL PERMITS ASSOCIATED WITH THIS PROJECT. AS SUCH, AN ENVIRONMENTAL COMPLIANCE SHEET WAS NOT PREPARED.
31. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AN AREA SUITABLE FOR STOCKPILING BORROW, TOPSOIL, AND OTHER FILL MATERIAL REQUIRED FOR THE PROJECT IN ACCORDANCE WITH SECTIONS 110.07 AND 110.10 OF THE STANDARD SPECIFICATIONS. SUBJECT TO THE APPROVAL OF THE ENGINEER, STOCKPILE AREA MAY BE LOCATED OUTSIDE OF THE PROJECT LIMITS, IF NECESSARY. NO ADDITIONAL PAYMENT WILL OCCUR FOR OFFSITE STOCKPILING.
32. THE CONTRACTOR SHALL INSTALL ALL SIDEWALK TO CONFORM TO CURRENT ADA STANDARDS. ALL SIDEWALK CROSS SLOPES SHALL BE A MAXIMUM OF 2% ALONG THE PROPOSED CONSTRUCTION. ALL DRIVEWAY CROSSING AND CURB RAMPS SHALL CONFORM TO CURRENT DEPARTMENT AND ADA STANDARDS. ALL COSTS ARE INCIDENTAL TO THE ITEMS BEING INSTALLED.
33. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL RESIDENTS AND BUSINESSES DURING CONSTRUCTION ACTIVITIES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ALL COSTS INCIDENTAL TO THE CONTRACT.
34. THE CONTRACTOR SHALL MARK THEIR NAME IN THE CONCRETE, EITHER BY STAMPING OR BY INLAYING AN IMPROVED METAL PLATE. THE STAMPED LETTERS SHALL BE ONE INCH AND ONE-QUARTER INCH DEPTH. IF A METAL PLATE IS USED THE TOP OF THE PLATE SHALL BE FLUSH WITH THE TOP OF THE SIDEWALK. THE CONTRACTOR'S NAME SHALL BE PLACED AT THE ENDS OF THE FINISHED SIDEWALK AND MID-DISTANT BETWEEN THE ENDS IN ACCORDANCE WITH THE WILMINGTON CODE OF ORDINANCES. ALL COSTS ARE INCIDENTAL TO THE CONTRACT.
35. THE LOCATIONS OF ALL ABOVE GROUND ITEMS TO BE INSTALLED, SUCH AS LIGHTS, TREES, SIGNS, ETC. SHALL BE VERIFIED BY THE ENGINEER PRIOR TO INSTALLATION. ALL COSTS INCIDENTAL TO ITEM 763501.
36. PRIOR TO USE ON THIS CONTRACT, THE CONTRACTOR WILL BE REQUIRED TO SUBMIT A SAMPLE FOR EACH DIFFERENT PATTERN OF BRICK SIDEWALK AND BRICK PAVEMENT MARKINGS FOR COLOR AND DESIGN APPROVAL BY THE ENGINEER. ANY NECESSARY CHANGES TO THE COLOR WILL BE DIRECTED BY THE ENGINEER. ALL COSTS ARE INCIDENTAL TO THE CONTRACT.

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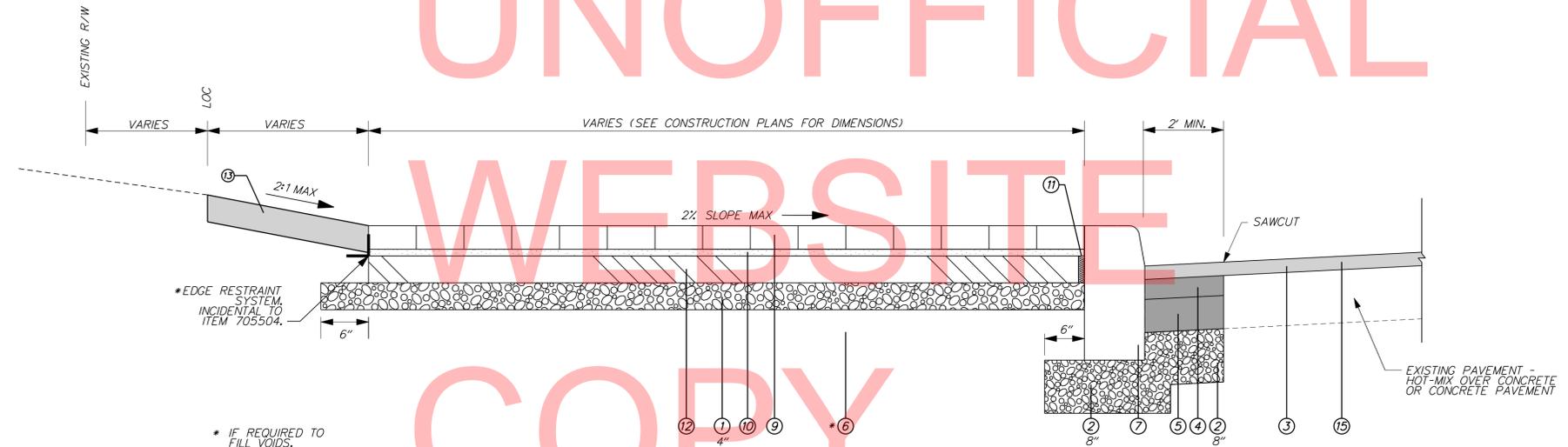
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 <b>DELAWARE DEPARTMENT OF TRANSPORTATION</b>	ADDENDUMS / REVISIONS			<b>BAYARD SQUARE ENHANCEMENTS, CITY OF WILMINGTON</b>	CONTRACT	BRIDGE NO.	-	<b>NOTES</b>	SHEET NO.	5
	T201020006				DESIGNED BY: AMW	TOTAL SHTS.	33			
	COUNTY				CHECKED BY: WFC					
	NEW CASTLE									

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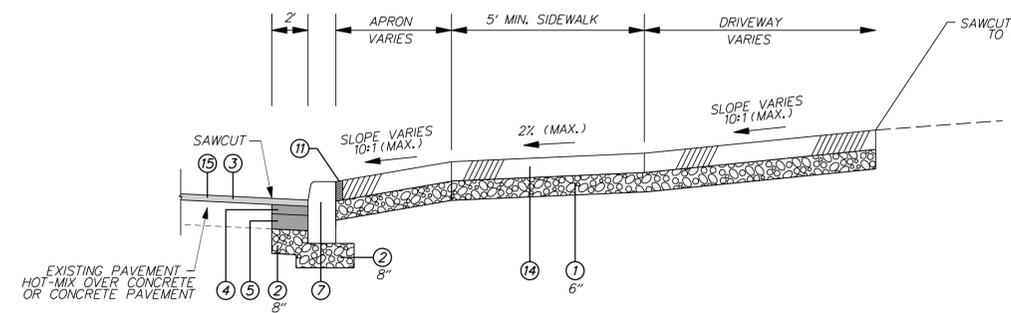


CONCRETE AND BRICK PAVER COMBINATION SIDEWALK SECTION



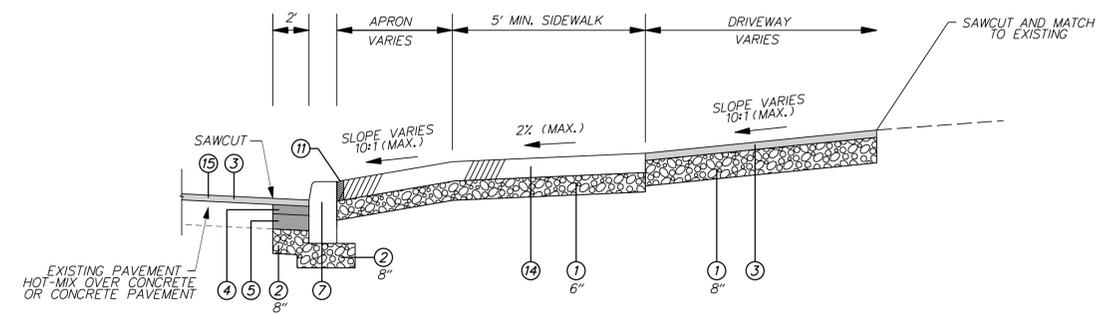
BUMP OUT TYPICAL SECTION @ INTERSECTIONS

- NOTES:
- EDGE RESTRAINT NOT REQUIRED WHERE BRICK SIDEWALK ABUTS ROADWAY, CURB OR OTHER STRUCTURE. CONTRACTOR SHALL INSTALL 1/2" EXPANSION JOINT.
  - WHEN PERFORMING FULL DEPTH CONCRETE SAWCUTTING, THE CUTTING ANY HOTMIX OVERLAY IS INCIDENTAL TO THE CONCRETE SAWCUTTING ITEM.



CONCRETE DRIVEWAY TIE-IN

\* SEE PLAN SHEETS FOR LIMIT OF CONCRETE TIE-IN.



ASPHALT DRIVEWAY TIE-IN

\* SEE PLAN SHEETS FOR LIMIT OF ASPHALT TIE-IN.

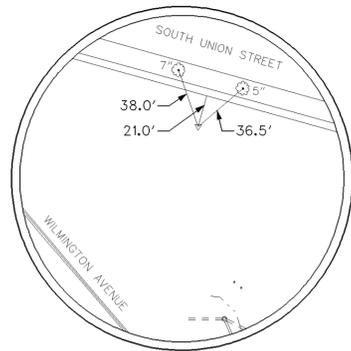
LEGEND

- ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
- ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B PATCHING
- ITEM 401827 - 2" WMA, SUPERPAVE, TYPE C, 160 GYR., PG 64-22 (NON-CARBONATE STONE)
- ITEM 401822 - 3" WMA, SUPERPAVE, TYPE B, 160 GYR., PG 64-22, PATCHING
- ITEM 401823 - 5" WMA, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYR., PG 64-22, PATCHING
- ITEM 209001 - BORROW, TYPE A
- ITEM 701014 - PCC CURB, TYPE 1-6
- ITEM 705001 - 4" PCC SIDEWALK
- ITEM 705504 - BRICK SIDEWALK WITH HAND TIGHT 1/8" JOINTS
- 1" DRY MORTAR MIX SETTING BED INCIDENTAL TO BRICK SIDEWALK ITEM 705504
- 1/2" EXPANSION JOINT, INCIDENTAL TO ITEMS BEING INSTALLED
- 4" PCC SIDEWALK INCIDENTAL TO BRICK SIDEWALK ITEM 705504
- ITEM 732002 - TOPSOIL, 6" DEPTH  
ITEM 734013 - PERMANENT GRASS SEEDING, DRY GROUND
- ITEM 705002 - 6" PCC SIDEWALK
- ITEM 760006 - PAVEMENT - MILLING, HOT-MIX, 2"

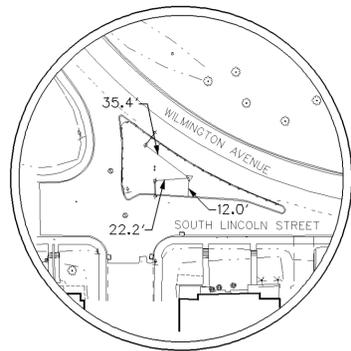
PR - POINT OF ROTATION  
PDGA - PROFILE DITCH GRADE APPLICATION  
PGA - PROFILE GRADE APPLICATION

MAXIMUM SINGLE LIFTS FOR THE FOLLOWING MATERIALS AFTER COMPACTION ARE:  
2" = WARM-MIX, TYPE C  
3" = WARM-MIX, TYPE B  
6" = WARM-MIX, BCBC  
8" = GABC

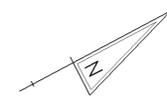
- NOTES:
- MAXIMUM ALGEBRAIC DIFFERENCE OF CROSS SLOPES SHALL NOT EXCEED 8%.
  - CONTRACTOR SHALL LOAD TEST ALL SUBGRADE SOIL PRIOR TO THE PLACEMENT OF THE SUBBASE COURSE. ALL UNSUITABLE MATERIAL FOUND SHALL BE EXCAVATED TO THE DEPTH OF STABLE SOIL AND BACKFILLED WITH APPROVED MATERIAL. THE CONTRACTOR SHALL COMPACT ALL MATERIALS TO MEET THE DELDOT STANDARD SPECIFICATIONS.
  - IF REQUIRED, BORROW, TYPE A SHALL BE PLACED IN SUCCESSIVE LAYERS NOT TO EXCEED 8" IN DEPTH, LOOSE MEASUREMENT, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  - SEE GRADES AND GEOMETRICS SHEETS FOR FURTHER DETAILS.



TRAVERSE POINT #1  
CAPPED REBAR



TRAVERSE POINT #2  
CAPPED REBAR



CURVE ①

PC ( 2001 )	201+33.54	634614.5152	611175.5992
PI ( )	202+75.63	634646.3608	611314.0758
CC ( )		634916.6292	611106.1217
PT ( 2002 )	204+00.00	634772.0529	611380.3437
Radius:	310.0000		
Delta:	49° 14' 58.3979"	Left	
Degree of Curvature (Arc):	18° 28' 57.0343"		
Length:	266.4657		
Tangent:	142.0913		
Chord:	258.3378		
Middle Ordinate:	28.1926		
External:	31.0131		
Tangent Direction:	N 77° 02' 55.7368" E		
Radial Direction:	S 12° 57' 04.2632" E		
Chord Direction:	N 52° 25' 26.5378" E		
Radial Direction:	S 62° 12' 02.6611" E		
Tangent Direction:	N 27° 47' 57.3389" E		

**HORIZONTAL / VERTICAL CONTROL DATA**

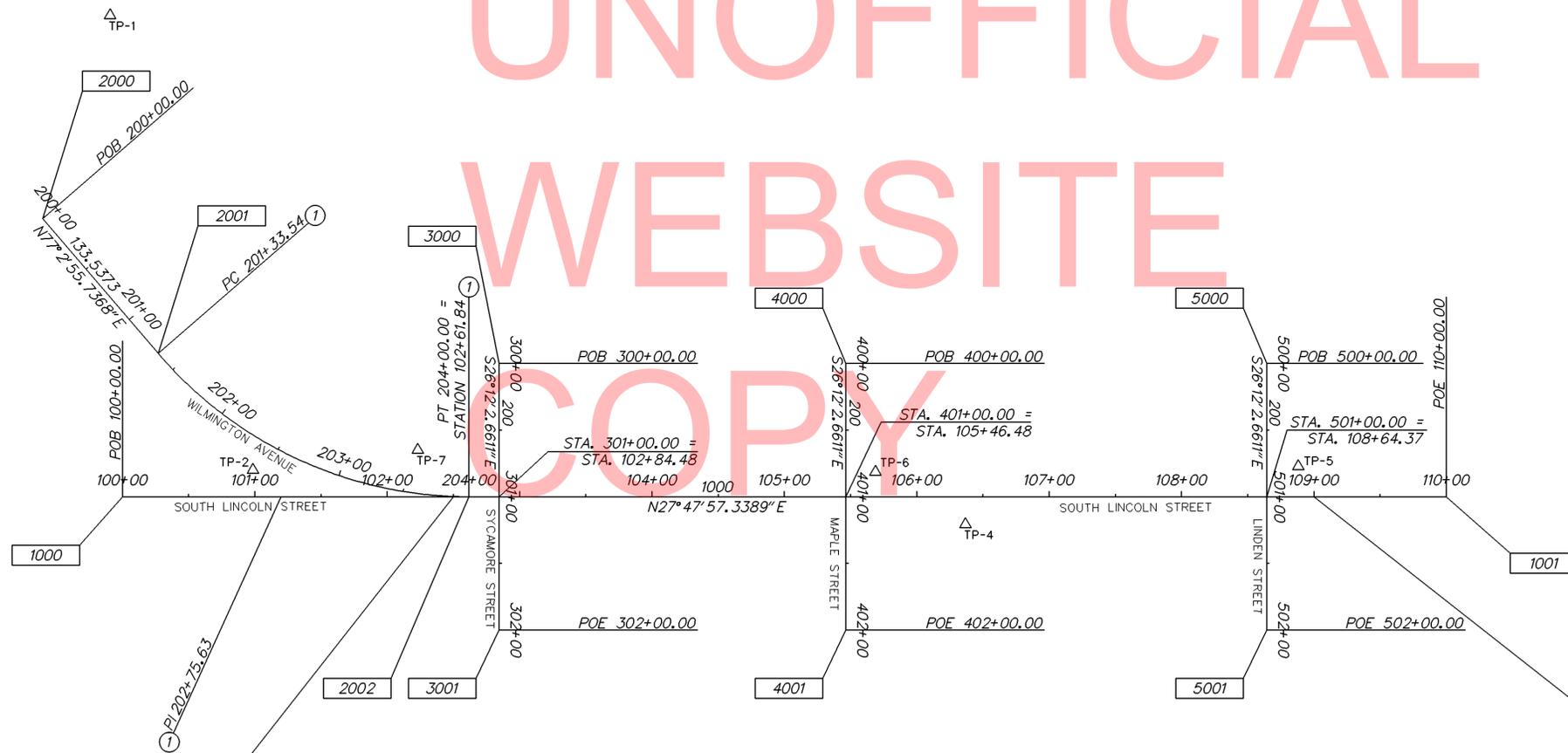
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
TP-1	---	---	634700.6967	610934.4141	113.41
TP-2	100+98.57	20.08	634636.9910	611286.4310	114.73
TP-4	106+36.82	20.77	635094.0670	611573.5942	127.29
TP-5	108+88.15	-22.74	635336.6842	611652.3212	131.11
TP-6	105+68.88	-18.60	635052.3279	611507.0863	126.14
TP-7	102+23.16	-35.17	634754.2355	611331.1918	119.41

**CONSTRUCTION ALIGNMENT CONTROL**

POINT	STATION	OFFSET	NORTHING	EASTING
1000	100+00.00	0.00	634540.4286	611258.2259
1001	110+00.00	0.00	635425.0156	611724.6011
2000	200+00.00	0.00	634584.5867	611045.4589
2001	201+33.54	0.00	634614.5152	611175.5992
2002	204+00.00	0.00	634772.0529	611380.3437
3000	300+00.00	0.00	634838.7148	611302.4423
3001	302+00.00	0.00	634745.4397	611479.3597
4000	400+00.00	0.00	635070.4761	611424.6324
4001	402+00.00	0.00	634977.2011	611601.5498
5000	500+00.00	0.00	635351.6770	611572.8881
5001	502+00.00	0.00	635258.4019	611749.8055

ALL ELEVATIONS ARE BASED ON NATIONAL GEODETIC VERTICAL DATUM (NAVD 1988) AND HORIZONTAL DATUM NORTH AMERICAN DATUM, NAD 83 (2007).

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**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102+45.00**

**EXISTING RW CENTERLINE AND  
PROPOSED BASELINE OF CONSTRUCTION**

**END CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 109+05.17**

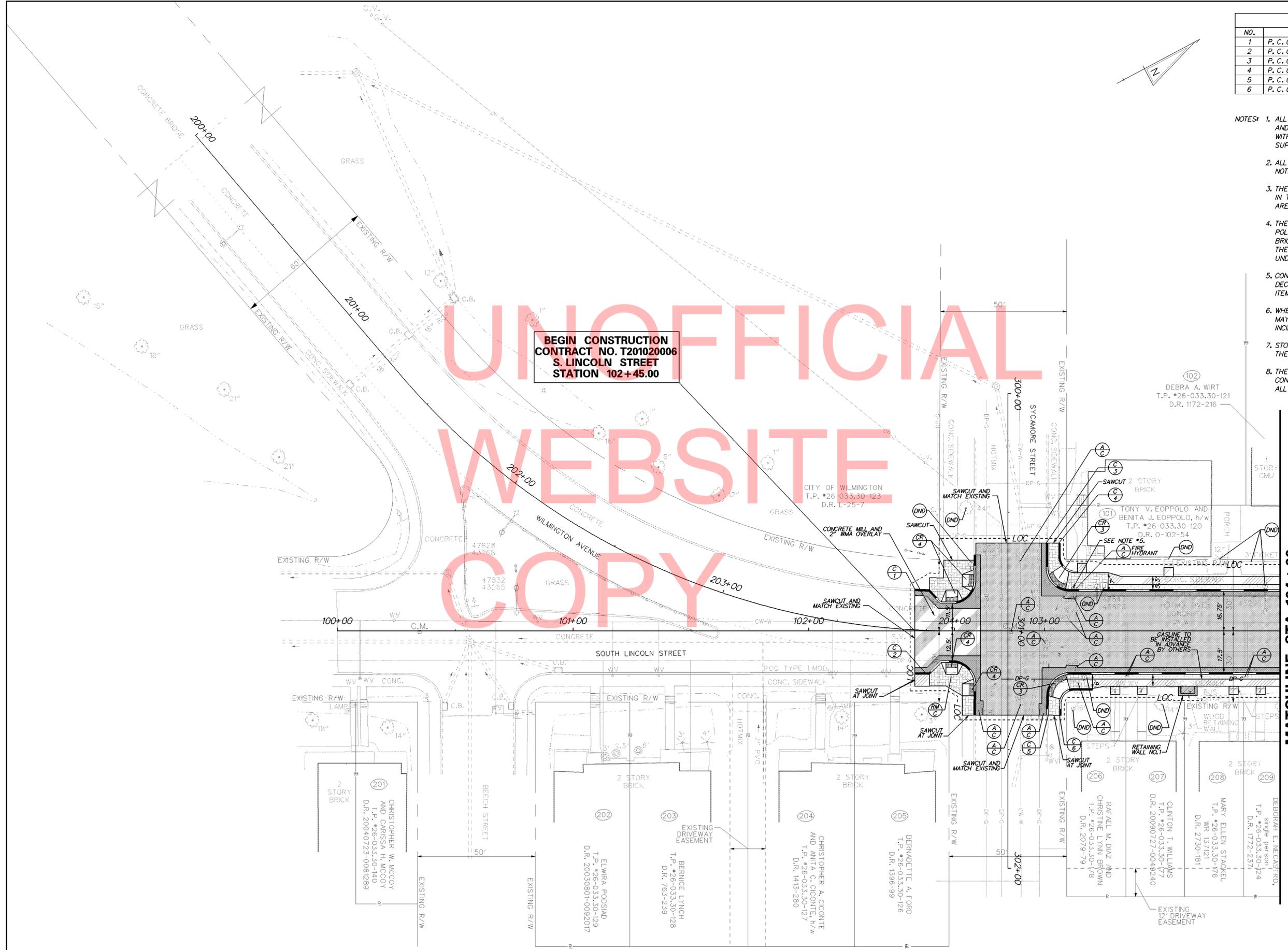
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CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P. C. C. CURB, TYPE 1-6	45.0'
2	P. C. C. CURB, TYPE 1-6	47.0'
3	P. C. C. CURB, TYPE 1-6	266.0'
4	P. C. C. CURB, TYPE 1-8	18.0'
5	P. C. C. CURB, TYPE 1-6	273.0'
6	P. C. C. CURB, TYPE 1-8	23.0'

- NOTES:
1. ALL EXISTING UTILITY POLES TO REMAIN. PROPOSED CURB AND DRAINAGE SHALL BE INSTALLED TO AVOID CONFLICT WITH UTILITY POLES. ALL COSTS TO INSTALL AND TEMPORARILY SUPPORT POLES ARE INCIDENTAL TO CONTRACT.
  2. ALL EXISTING UTILITIES ARE TO REMAIN UNLESS OTHERWISE NOTED IN THE PLANS.
  3. THE CONTRACTOR SHALL RECONSTRUCT STEPS AS REQUIRED IN THE FIELD AND AS IDENTIFIED ON THE PLANS. ALL COSTS ARE INCIDENTAL TO ITEM 705001.
  4. THE CONTRACTOR SHALL POUR A PCC COLLAR AROUND UTILITY POLES AND FIRE HYDRANTS LOCATED WITHIN THE PROPOSED BRICK SIDEWALK PRIOR TO PLACING BRICKS AS DIRECTED BY THE ENGINEER IN THE FIELD. ALL COSTS SHALL BE PAID UNDER ITEM #705504. SEE CONSTRUCTION DETAILS.
  5. CONTRACTOR SHALL REMOVE EXISTING BOLLARD AND INSTALL DECORATIVE BOLLARD. ALL COSTS SHALL BE PAID UNDER ITEM #720578 - BOLLARD, STEEL.
  6. WHEN SAWCUTTING THE CONCRETE PAVEMENT, THE CONTRACTOR MAY ENCOUNTER WIRE MESH. CUTTING THROUGH THE MESH IS INCIDENTAL TO ITEM #762002, SAW CUTTING, CONCRETE, FULL DEPTH.
  7. STORAGE OF MATERIALS AND DRIVING ON GRASS AREAS UNDER THE TREE DRIPLINE IS PROHIBITED.
  8. THE CONTRACTOR SHALL ADJUST EXISTING FIRE HYDRANT AS SHOWN. CONTRACTOR SHALL FOLLOW CITY OF WILMINGTON STANDARDS. ALL COSTS PAID UNDER ITEM #751000.

**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102+45.00**

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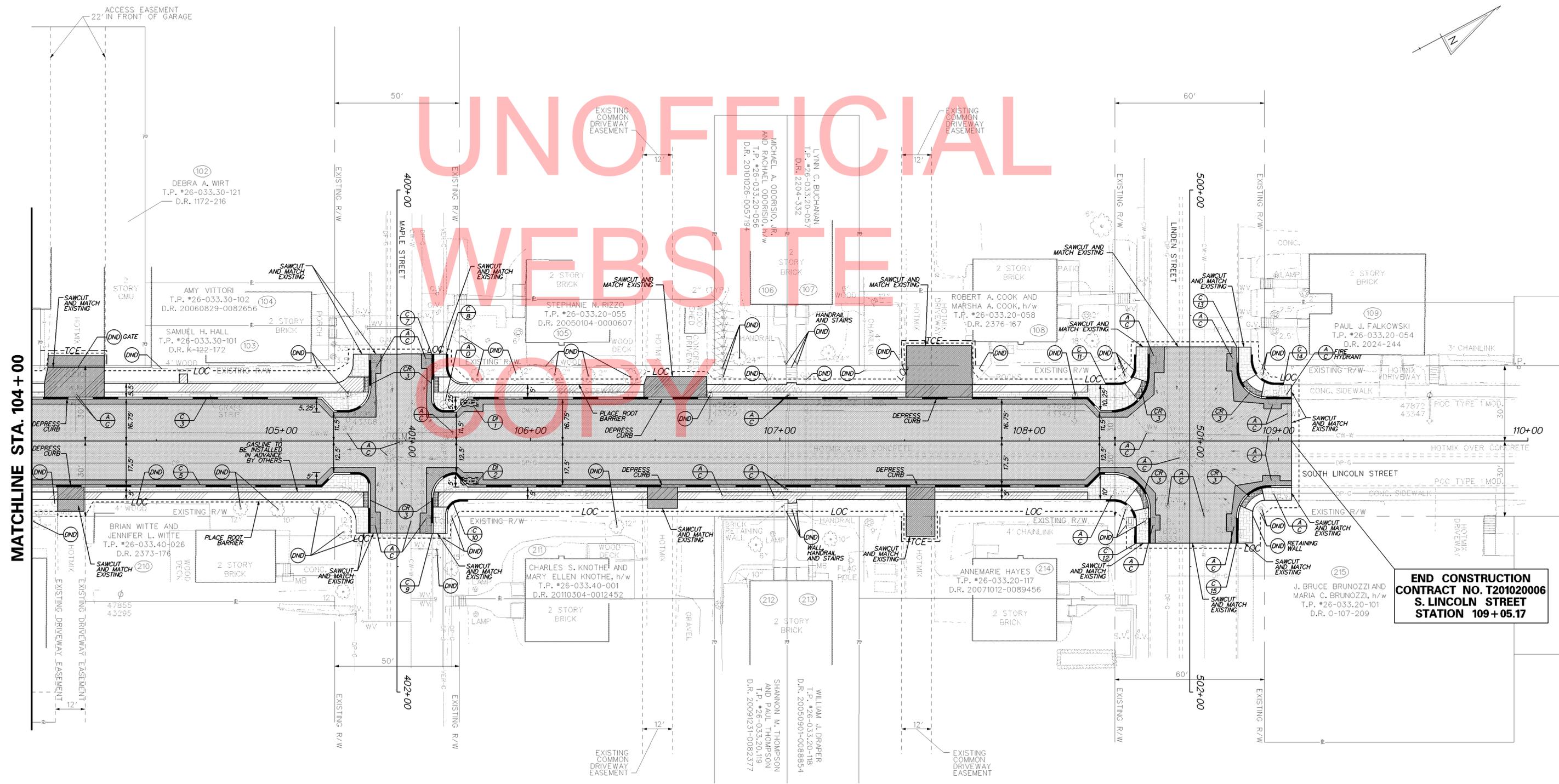
- NOTES:
1. ALL EXISTING UTILITY POLES TO REMAIN. PROPOSED CURB AND DRAINAGE SHALL BE INSTALLED TO AVOID CONFLICT WITH UTILITY POLES. ALL COSTS TO INSTALL AND TEMPORARILY SUPPORT POLES ARE INCIDENTAL TO CONTRACT.
  2. ALL EXISTING UTILITIES ARE TO REMAIN UNLESS OTHERWISE NOTED IN THE PLANS.
  3. THE CONTRACTOR SHALL RECONSTRUCT STEPS AS REQUIRED IN THE FIELD AND AS IDENTIFIED ON THE PLANS. ALL COSTS ARE INCIDENTAL TO ITEM 705001.
  4. THE CONTRACTOR SHALL POUR A PCC COLLAR AROUND UTILITY POLES AND FIRE HYDRANTS LOCATED WITHIN THE PROPOSED BRICK SIDEWALK PRIOR TO PLACING BRICKS AS DIRECTED BY THE ENGINEER IN THE FIELD. ALL COSTS SHALL BE PAID UNDER ITEM \*705504. SEE CONSTRUCTION DETAILS.
  5. THE CONTRACTOR SHALL INSTALL ROOT BARRIER AT STATIONS 104+74 TO 105+25 RIGHT AND 105+69 TO 106+89 LEFT. ALL COST INCIDENTAL TO ITEM \*705001.

6. CURB RUNS C-11, C-12, AND C-14 SHALL FOLLOW THE OUTSIDE SIDEWALK RADIUS AND TIE INTO CONCRETE POLE BASES OR CABINET BASES. ALL COSTS ARE INCIDENTAL TO ITEM \*701010 - PCC CURB TYPE 1-8.
7. WHEN SAWCUTTING THE CONCRETE PAVEMENT, THE CONTRACTOR MAY ENCOUNTER WIRE MESH. CUTTING THROUGH THE MESH IS INCIDENTAL TO ITEM \*762002, SAW CUTTING, CONCRETE, FULL DEPTH.
8. THE CONTRACTOR SHALL ADJUST EXISTING FIRE HYDRANT AS SHOWN. CONTRACTOR SHALL FOLLOW CITY OF WILMINGTON STANDARDS. ALL COSTS PAID UNDER ITEM \*751000.
9. THE BOTTOM LANDING AREA FOR STAIRS TO REMAIN SHALL BE CONSTRUCTED SO THAT IT IS IN ACCORDANCE WITH ADA GUIDELINES AND DRAIN PROPERLY.

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
7	P. C. C. CURB, TYPE 1-6	333.0'
8	P. C. C. CURB, TYPE 1-8	18.0'
9	P. C. C. CURB, TYPE 1-6	335.0'
10	P. C. C. CURB, TYPE 1-8	20.0'
11	P. C. C. CURB, TYPE 1-8	24.0'
12	P. C. C. CURB, TYPE 1-8	26.0'
13	P. C. C. CURB, TYPE 1-6	38.0'
14	P. C. C. CURB, TYPE 1-8	24.0'
15	P. C. C. CURB, TYPE 1-6	40.0'

DRAINAGE INLET SCHEDULE					
NO.	STATION	OFFSET	BOX SIZE	GRATE T.G. EL.	INV. EL.
*1	105+75.52	-16.75'	72" x 24"	1	126.08'
*2	105+75.52	17.50'	72" x 24"	1	126.13'

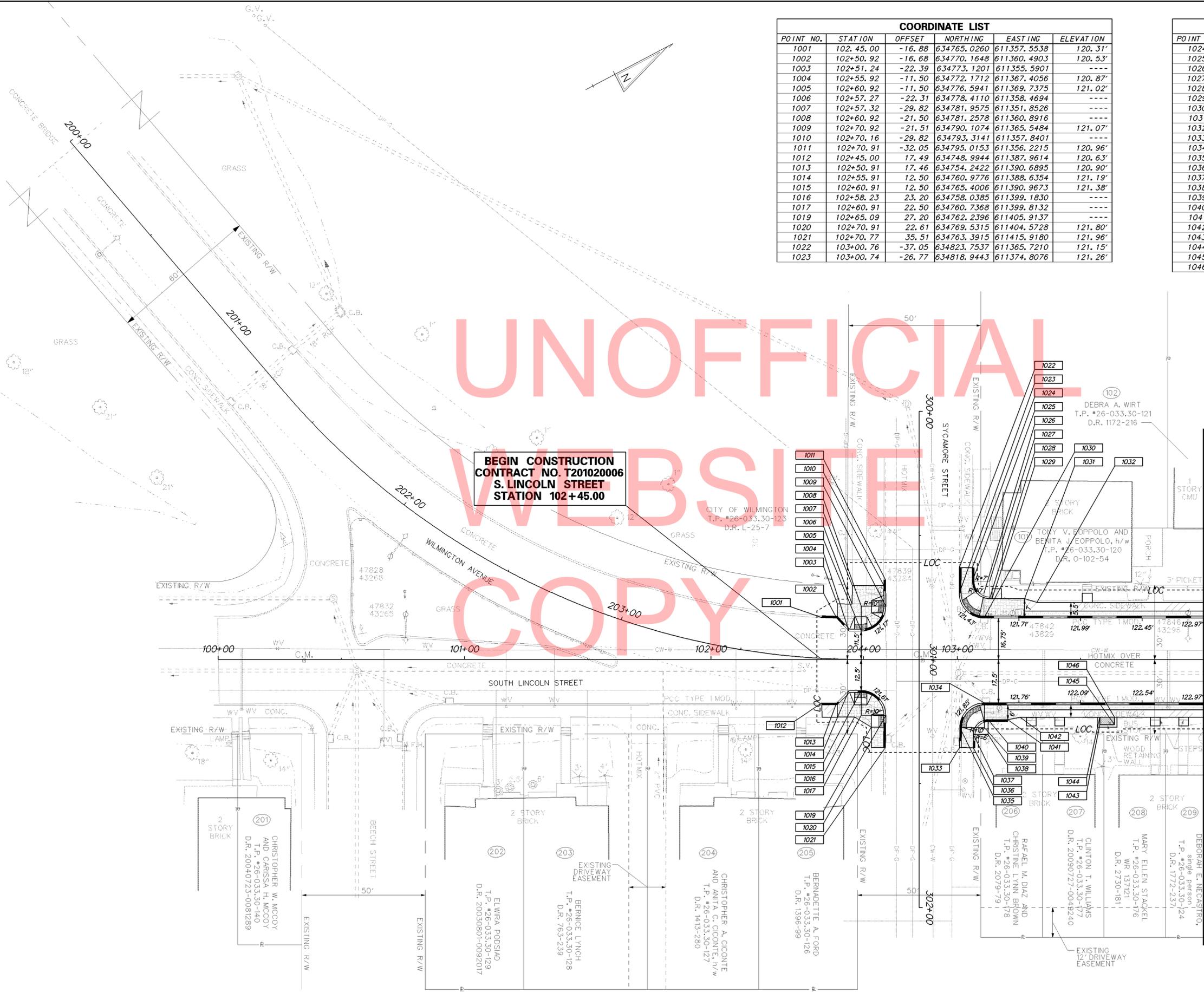
\* CONSTRUCT INLETS USING THE DOGHOUSE INLET BOX DETAIL IN THE DELAWARE DEPARTMENT OF TRANSPORTATION'S "STANDARD CONSTRUCTION DETAILS."



**END CONSTRUCTION**  
**CONTRACT NO. T201020006**  
**S. LINCOLN STREET**  
**STATION 109+05.17**

7/3/2012 G:\PROJECTS\085535.00 (Transportation Enhancement Program)\085535.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\cp02BS.dgn

6/28/2012 1:23:13 G:\PROJECTS\08535.00 (Transportation Enhancement Program)\08535.23 (Task 23, Bayard Square Improvements - Final Design)\CADD FILES\g01BS.dgn



COORDINATE LIST					
POINT NO.	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1001	102+45.00	-16.88	634765.0260	611357.5538	120.31'
1002	102+50.92	-16.68	634770.1648	611360.4903	120.53'
1003	102+51.24	-22.39	634773.1201	611355.5901	----
1004	102+55.92	-11.50	634772.1712	611367.4056	120.87'
1005	102+60.92	-11.50	634776.5941	611369.7375	121.02'
1006	102+57.27	-22.31	634778.4110	611358.4694	----
1007	102+57.32	-29.82	634781.9575	611351.8526	----
1008	102+60.92	-21.50	634781.2578	611360.8916	----
1009	102+70.92	-21.51	634790.1074	611365.5484	121.07'
1010	102+70.16	-29.82	634793.3141	611357.8401	----
1011	102+70.91	-32.05	634795.0153	611356.2215	120.96'
1012	102+45.00	17.49	634748.9944	611387.9614	120.63'
1013	102+50.91	17.46	634754.2422	611390.6895	120.90'
1014	102+55.91	12.50	634760.9776	611388.6354	121.19'
1015	102+60.91	12.50	634765.4006	611390.9673	121.38'
1016	102+58.23	23.20	634758.0385	611399.1830	----
1017	102+60.91	22.50	634760.7368	611399.8132	----
1019	102+65.09	27.20	634762.2396	611405.9137	----
1020	102+70.91	22.61	634769.5315	611404.5728	121.80'
1021	102+70.77	35.51	634763.3915	611415.9180	121.96'
1022	103+00.76	-37.05	634823.7537	611365.7210	121.15'
1023	103+00.74	-26.77	634818.9443	611374.8076	121.26'

COORDINATE LIST					
POINT NO.	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1024	103+06.08	-37.05	634828.4600	611368.1950	----
1025	103+06.09	-31.48	634825.8736	611373.1265	----
1026	103+10.74	-26.75	634827.7826	611379.4856	----
1027	103+13.09	-31.50	634832.0728	611376.3777	----
1028	103+13.09	-24.50	634828.8082	611382.5699	----
1029	103+14.06	-24.50	634829.6664	611383.0223	----
1030	103+10.74	-16.75	634823.1188	611388.3315	121.47'
1031	103+27.29	-24.50	634841.3722	611389.1939	----
1032	103+32.29	-23.00	634845.0955	611392.8527	----
1033	103+00.85	35.51	634789.9970	611429.9451	122.00'
1034	103+10.94	17.50	634807.3172	611418.7191	121.77'
1035	103+00.94	27.39	634793.8577	611422.8074	121.94'
1036	103+06.44	35.54	634794.9280	611432.5810	----
1037	103+06.47	30.22	634797.4381	611427.8853	----
1038	103+12.47	30.25	634802.7295	611430.7139	----
1039	103+10.94	27.50	634802.6534	611427.5650	----
1040	103+12.47	24.25	634805.5278	611425.4063	----
1041	103+20.43	24.25	634812.5671	611429.1177	----
1042	103+25.43	23.25	634817.4564	611430.5649	----
1043	103+57.79	26.25	634844.6845	611448.3116	----
1044	103+57.79	23.25	634846.0836	611445.6579	----
1045	103+63.79	26.25	634849.9920	611451.1099	----
1046	103+63.79	23.25	634851.3912	611448.4561	----

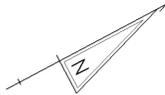
- NOTES:**
1. OFFSETS SHOWN IN THE GEOMETRIC TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE CONSTRUCTION & R/W BASELINE.
  2. UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB & GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
  3. RADIARE GIVEN TO THE EDGE OF PAVEMENT.
  4. UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
  5. THE CONTRACTOR SHALL STAKE ALL GRADES TO ENSURE POSITIVE DRAINAGE PRIOR TO CONSTRUCTION. ALL GRADES MUST BE APPROVED BY THE ENGINEER IN THE FIELD PRIOR TO CONSTRUCTION. ALL WORK REQUIRED FOR THE CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
  6. PROPOSED CURBS OR PAVEMENT THAT TIE INTO EXISTING PAVEMENT OR CURB SHALL MATCH THE EXISTING PAVEMENT OR CURB ELEVATIONS.
  7. SIDEWALK TO MATCH EXISTING GRADE OR AS DIRECTED IN THE FIELD BY THE ENGINEER. THE PROPOSED SIDEWALK SHALL NOT EXCEED 2% PER ADA REGULATIONS. THE SIDEWALK SHALL BE LAYED OUT FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION. ALL COSTS ARE INCIDENTAL TO THE PROJECT.

**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102+45.00**

**MATCHLINE STA. 104+00**

**NOTES:**

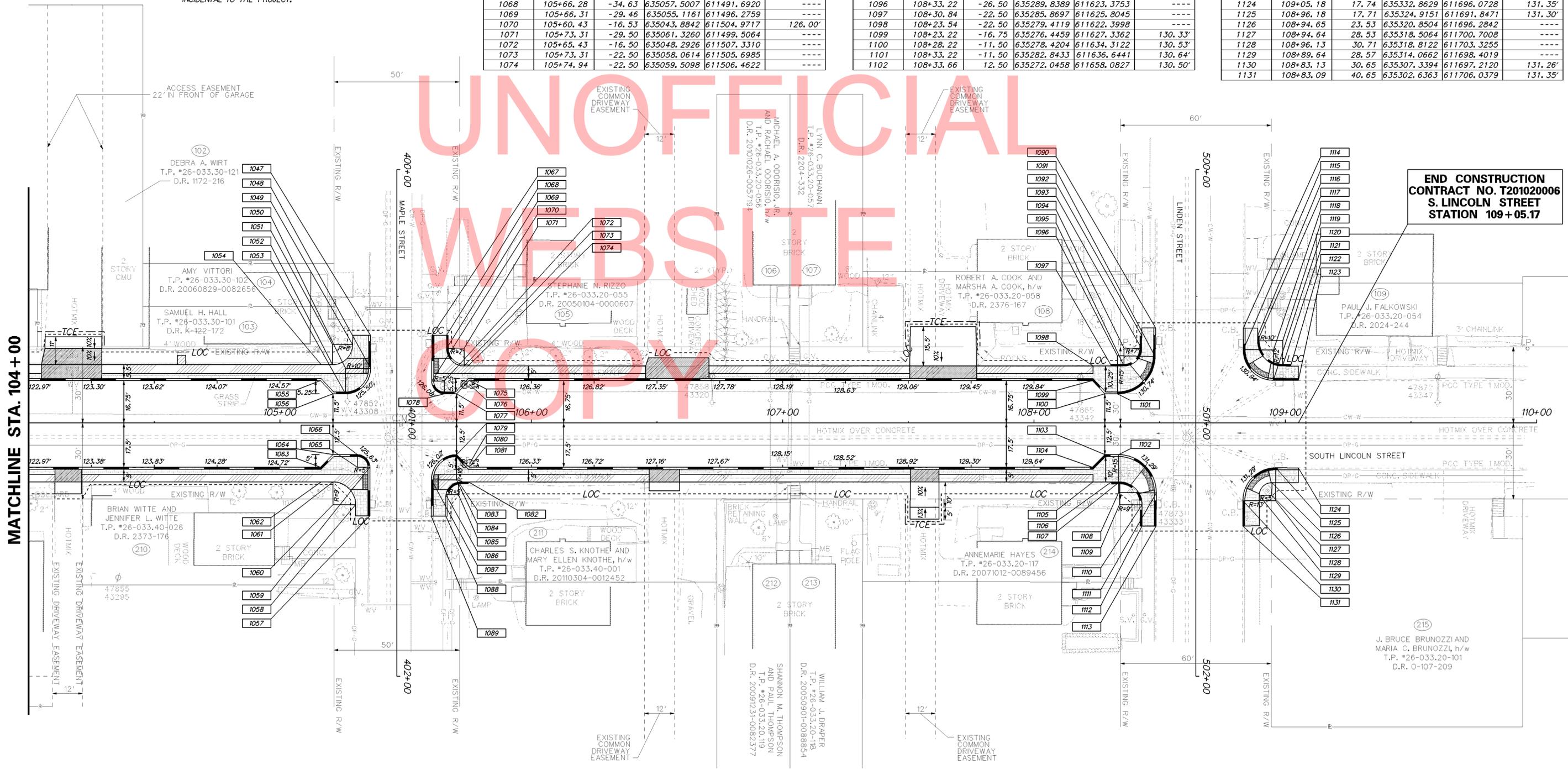
- OFFSETS SHOWN IN THE GEOMETRIC TABLES WITH A MINUS SIGN ARE TO THE LEFT OF THE CONSTRUCTION & R/W BASELINE.
- UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND CURB & GUTTER IS GIVEN TO THE EDGE OF PAVEMENT.
- RADIARE GIVEN TO THE EDGE OF PAVEMENT.
- UNLESS OTHERWISE NOTED, EDGE OF PAVEMENT ELEVATIONS AT CURB RETURNS ARE GIVEN AT 10 FOOT INTERVALS.
- THE CONTRACTOR SHALL STAKE ALL GRADES TO ENSURE POSITIVE DRAINAGE PRIOR TO CONSTRUCTION. ALL GRADES MUST BE APPROVED BY THE ENGINEER IN THE FIELD PRIOR TO CONSTRUCTION. ALL WORK REQUIRED FOR THE CALCULATING AND STAKING OF GRADES SHALL BE PAID FOR UNDER ITEM 763501 - CONSTRUCTION ENGINEERING.
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COORDINATE LIST					
POINT NO.	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1047	105+36.02	-34.61	635030.7230	611477.5953	125.51'
1048	105+36.07	-21.54	635024.6722	611489.1813	125.41'
1049	105+28.90	-30.89	635022.6953	611477.5713	----
1050	105+26.07	-21.50	635015.8082	611484.5521	----
1051	105+20.90	-31.00	635015.6720	611473.7408	----
1052	105+20.90	-23.00	635011.9410	611480.8175	----
1053	105+26.07	-11.50	635011.1444	611493.3980	125.40'
1054	105+16.39	-23.00	635007.9462	611478.7114	----
1055	105+16.07	-16.75	635004.7470	611484.0902	124.99'
1056	105+21.07	-11.50	635006.7215	611491.0661	125.29'
1057	105+35.78	36.71	634997.2475	611540.5722	125.68'
1058	105+30.10	32.30	634994.2860	611524.0202	----
1059	105+35.79	17.50	635006.2149	611523.5857	125.63'
1060	105+21.10	32.25	634986.3464	611529.7820	----
1061	105+21.10	23.25	634990.5438	611521.8207	----
1062	105+16.38	23.25	634986.3668	611519.6185	----
1063	105+16.07	17.50	634988.7737	611514.3873	125.12'
1064	105+21.07	12.50	634995.5285	611512.2962	125.37'
1065	105+30.79	17.50	635001.7932	611521.2514	----
1066	105+30.79	12.50	635004.1250	611516.8285	125.59'
1067	105+60.54	-34.66	635052.4407	611488.9840	125.90'
1068	105+66.28	-34.63	635057.5007	611491.6920	----
1069	105+66.31	-29.46	635055.1161	611496.2759	----
1070	105+60.43	-16.53	635043.8842	611504.9717	126.00'
1071	105+73.31	-29.50	635061.3260	611499.5045	----
1072	105+65.43	-16.50	635048.2926	611507.3310	----
1073	105+73.31	-22.50	635058.0614	611505.6985	----
1074	105+74.94	-22.50	635059.5098	611506.4622	----

COORDINATE LIST					
POINT NO.	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1075	105+75.27	-16.75	635075.1125	611511.6984	125.93'
1076	105+15.11	-15.25	635003.1972	611484.9688	126.02'
1077	105+70.27	-11.50	635050.2411	611514.0106	126.13'
1078	105+65.43	-11.50	635045.9607	611511.7539	126.13'
1079	105+70.27	12.50	635039.0481	611535.2407	126.14'
1080	105+75.27	17.50	635041.1391	611541.9955	126.12'
1081	105+77.77	15.75	635044.1667	611541.6135	126.05'
1082	105+74.95	23.25	635038.1826	611546.9370	----
1083	105+71.01	23.25	635034.6902	611545.0958	----
1084	105+70.27	22.50	635034.3843	611544.0866	----
1085	105+71.01	28.25	635032.3584	611549.5187	----
1086	105+66.01	28.24	635027.9395	611547.1791	----
1087	105+60.27	22.48	635025.5478	611539.4051	125.92'
1088	105+65.99	36.74	635023.9631	611554.6897	----
1089	105+60.24	36.73	635018.8776	611551.9955	126.16'
1090	108+48.43	-37.49	635308.4192	611620.7505	130.74'
1091	108+48.21	-26.21	635302.9722	611630.6221	130.82'
1092	108+42.90	-34.40	635302.0965	611620.9020	----
1093	108+42.84	-29.40	635299.7043	611625.2931	----
1094	108+35.84	-29.50	635293.5573	611621.9443	----
1095	108+35.84	-22.50	635290.2927	611628.1364	----
1096	108+33.22	-26.50	635289.8389	611623.3753	----
1097	108+30.84	-22.50	635285.8697	611625.8045	----
1098	108+23.54	-22.50	635279.4119	611622.3998	----
1099	108+23.22	-16.75	635276.4459	611627.3362	130.33'
1100	108+28.22	-11.50	635278.4204	611634.3122	130.53'
1101	108+33.22	-11.50	635282.8433	611636.6441	130.64'
1102	108+33.66	12.50	635272.0458	611658.0827	130.50'

COORDINATE LIST					
POINT NO.	STATION	OFFSET	NORTHING	EASTING	ELEVATION
1103	108+28.22	12.50	635267.2274	611655.5423	130.37'
1104	108+23.22	17.50	635260.4725	611627.6333	130.11'
1105	108+23.53	23.25	635258.0657	611662.8646	----
1106	108+30.84	23.25	635264.5331	611666.2744	----
1107	108+34.03	23.25	635267.3593	611667.7644	----
1108	108+33.66	27.50	635265.0502	611671.3515	----
1109	108+34.03	32.25	635263.1619	611675.7257	----
1110	108+43.03	32.30	635271.0983	611679.9699	----
1111	108+48.66	27.53	635278.3029	611678.3776	130.90'
1112	108+42.98	40.65	635267.1643	611687.3262	----
1113	108+48.63	40.65	635272.1618	611689.9610	131.17'
1114	108+83.30	-37.48	635339.2588	611637.0217	130.91'
1115	108+83.25	-28.92	635335.2284	611644.5702	130.82'
1116	108+88.96	-34.40	635342.8387	611642.3823	----
1117	108+88.97	-32.45	635341.9316	611644.1168	----
1118	108+95.25	-28.86	635345.8140	611650.2222	----
1119	108+98.97	-32.48	635350.7930	611648.7511	----
1120	108+95.18	-16.86	635340.1534	611660.8031	131.21'
1121	108+98.92	-22.48	635346.0898	611657.5761	----
1122	109+02.07	-22.47	635348.8643	611659.0547	----
1123	109+05.18	-16.80	635348.9709	611655.5203	131.31'
1124	109+05.18	17.74	635332.8629	611696.0728	131.35'
1125	108+96.18	17.71	635324.9151	611691.8471	131.30'
1126	108+94.65	23.53	635320.8504	611696.2842	----
1127	108+94.64	28.53	635318.5064	611700.7008	----
1128	108+96.13	30.71	635318.8122	611703.3255	----
1129	108+89.64	28.57	635314.0662	611698.4019	----
1130	108+83.13	30.65	635307.3394	611697.2120	131.26'
1131	108+83.09	40.65	635302.6363	611706.0379	131.35'

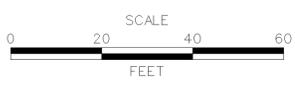


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WEBSITE  
COPY

END CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 109+05.17

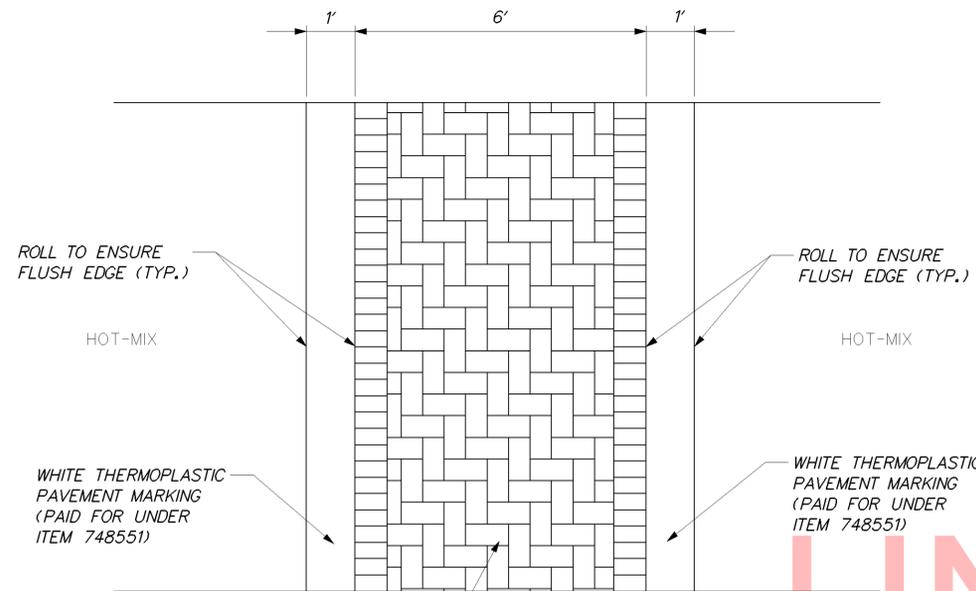
6/28/2012 10:05:35:00 (Transportation Enhancement Program)\085355.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\gg02BS.dgn

ADDENDUMS / REVISIONS



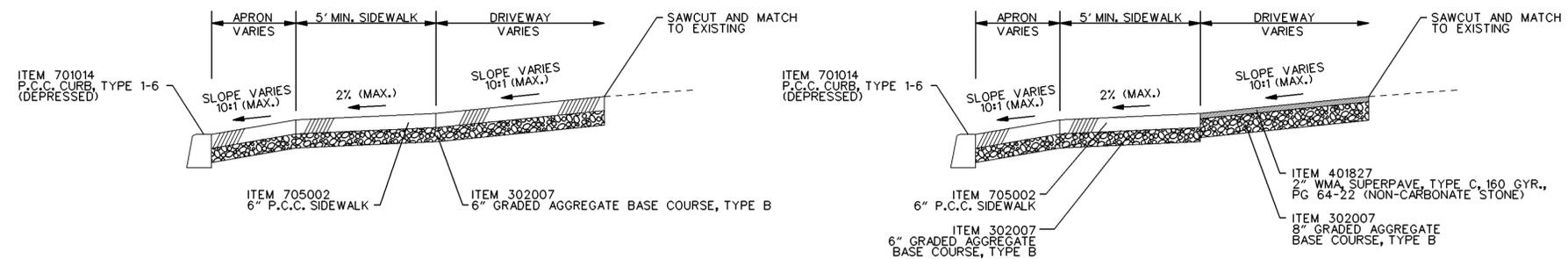
CONTRACT T201020006	BRIDGE NO. -
COUNTY NEW CASTLE	DESIGNED BY: AMW
	CHECKED BY: WFC

<b>GRADES AND GEOMETRICS</b>	SHEET NO. 11
	TOTAL SHTS. 33



CROSSWALK APPLICATION  
 -HERRING BONE PATTERN WITH SOLDIER COURSE  
 -BRICK RED COLOR WITH SAND COLORED GROUT LINES

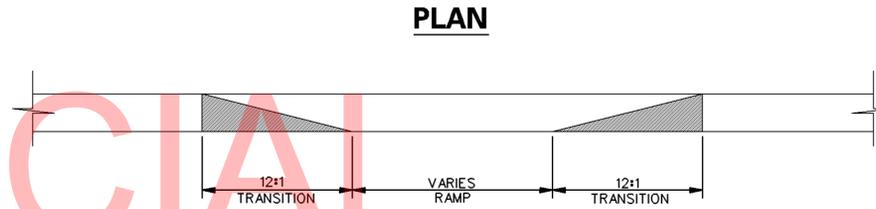
**ITEM 748551 – PREFORMED THERMOPLASTIC DECORATIVE PAVEMENT MARKING SYSTEM- AT CROSSWALK**  
 N.T.S.



**CONCRETE TIE-IN**

**ASPHALT TIE-IN**

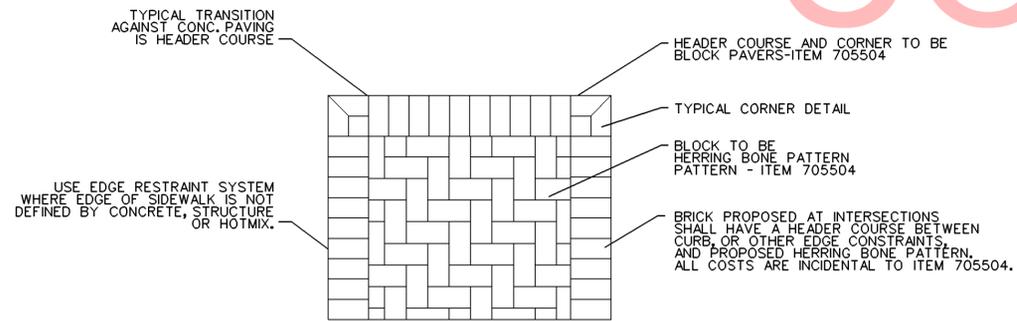
\* SEE PLAN SHEETS FOR LIMIT OF ASPHALT TIE-IN.



**CONCRETE DRIVEWAY APRON SECTION**

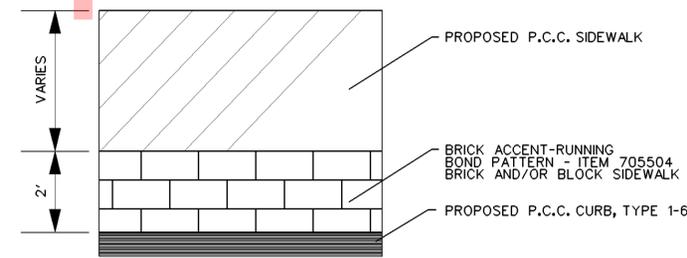
NOTE:  
 ALL ENTRANCES SHALL BE CONSTRUCTED PER DELDOT STANDARDS OR AS DIRECTED IN FIELD BY ENGINEER. ALL COSTS INCIDENTAL TO ITEMS BEING INSTALLED.

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WEBSITE  
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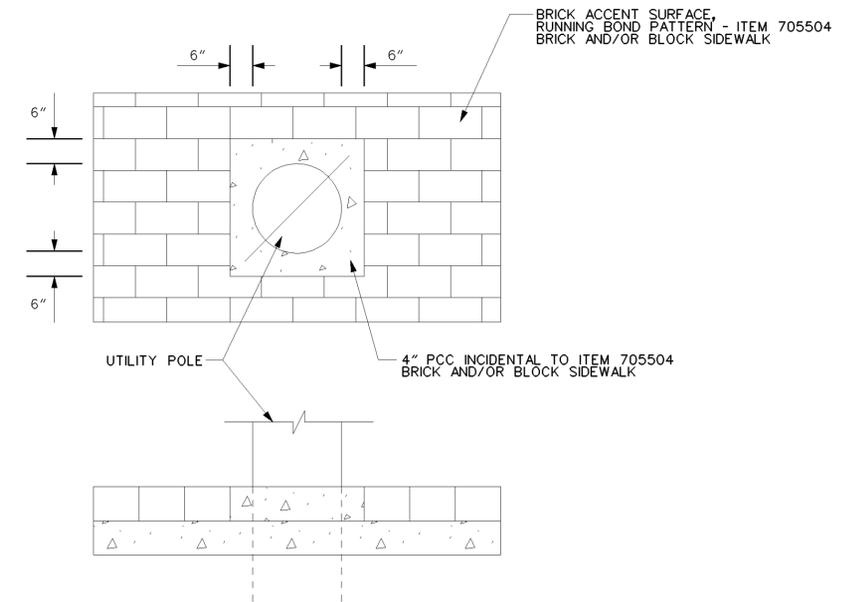


**BRICK SIDEWALK PATTERN AT INTERSECTIONS**  
 N.T.S.

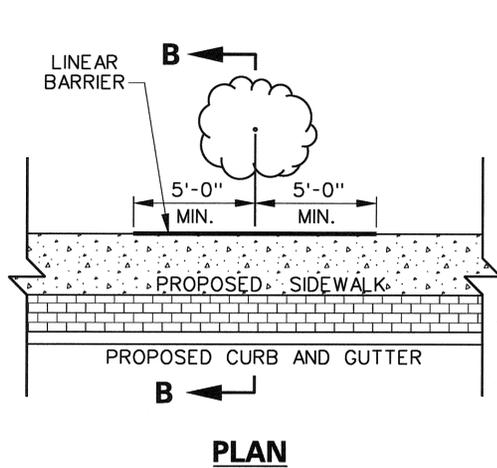
NOTE:  
 EDGE RESTRAINT NOT REQUIRED WHERE BRICK SIDEWALK ABUTS ROADWAY, CURB OR OTHER STRUCTURE, TO BE PAID UNDER ITEM 705504 - BRICK AND/OR BLOCK SIDEWALK.



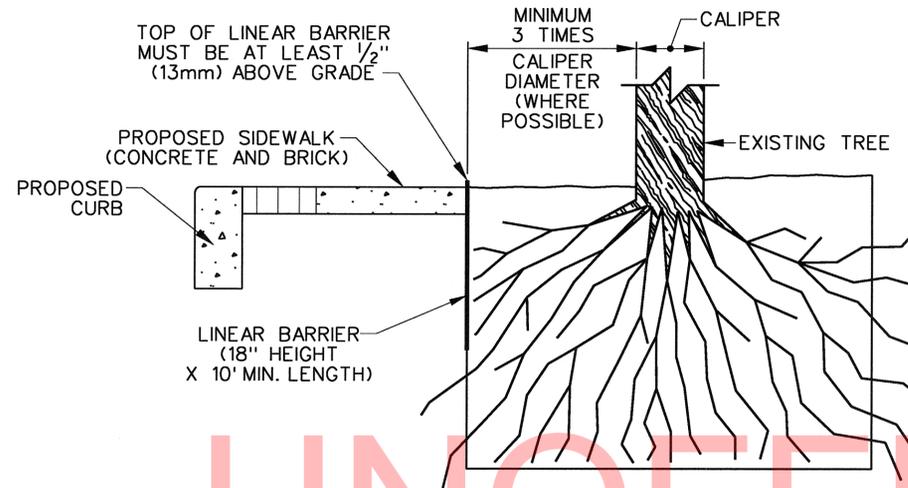
**BRICK SIDEWALK PATTERN COMBINED WITH P.C.C. SIDEWALK**  
 N.T.S.



**PCC COLLAR**  
 N.T.S.



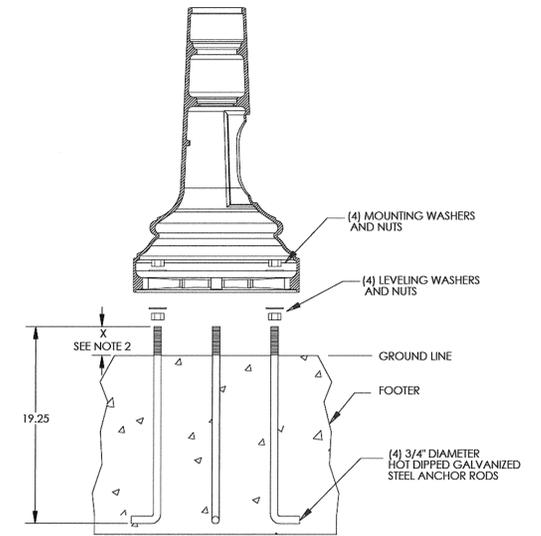
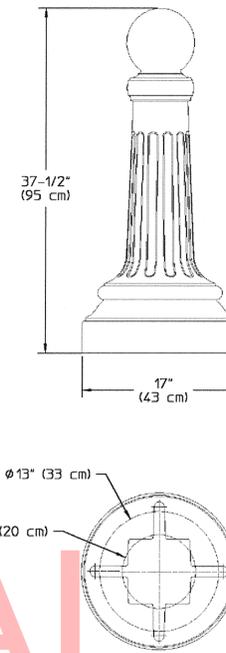
PLAN



SECTION B-B

**LINEAR ROOT BARRIER PLACEMENT DETAIL**

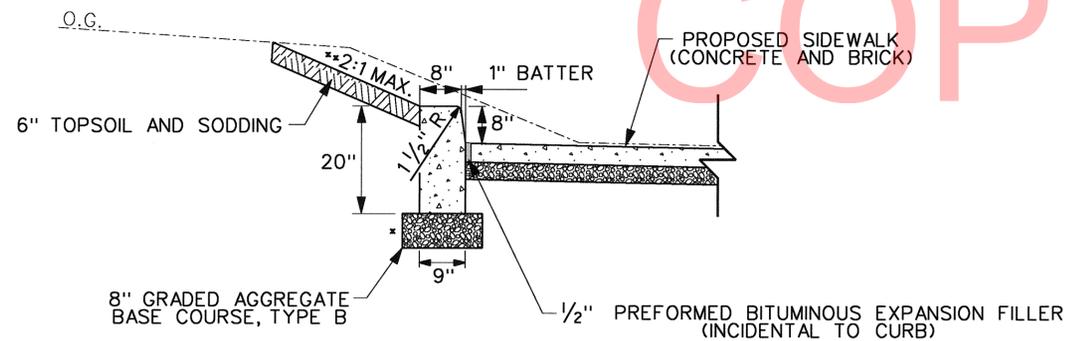
NOTE: PLACE LINEAR BARRIER AS SHOWN ON THE PLANS. ALL COSTS PAID INCIDENTAL TO ITEM #705001.



**ITEM 720578 – BOLLARD, STEEL  
N.T.S.**

NOTES:

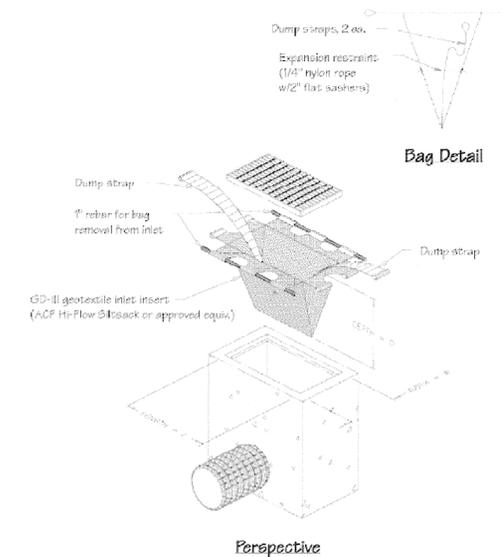
1. THE CONTRACTOR SHALL SUBMIT, FOR APPROVAL, CONSTRUCTION DETAILS AND DESIGN SPECIFICATIONS BY THE PRODUCT MANUFACTURER.
2. UTILIZE LEVELING NUTS TO MOUNT THE POST BASE. DIMENSION "X" SHALL EQUAL 3 1/4".
3. ALLOW FOR WATER DRAINAGE WHEN GROUTING GAP BETWEEN BASE AND FOUNDATION.
4. ALL COSTS INCIDENTAL TO ITEM 720578.



**SIDEWALK WITH OUTSIDE CURB  
NOT TO SCALE**

NOTES:

1. THE ADDITIONAL UP-RIGHT CURB PLACED OUTSIDE OF THE SIDEWALK WILL BE PAID UNDER ITEM #701010 - P.C.C. CURB TYPE 1-B.
- \* COST OF 8" DENSE GRADED AGGREGATE UNDER OUTSIDE CURB, FROM 6" IN FRONT OF CURB FACE TO 6" BEHIND BACK OF CURB WILL BE PAID UNDER ITEM #302007.
- \*\*GRADE SHALL MATCH EXISTING SLOPE OR 2:1 MAX.



Notes:

1. This practice shall only be used in situations in which Inlet Protection - Type 1 cannot be used due to site constraints. These include, but are not limited to partially completed parking areas, streets, roads, etc.
2. It may be necessary to transition from Type 1 to Type 2 Inlet Protection as construction proceeds.
3. For areas where there is a concern for oil run-off or spills, Insert shall meet one of the above specifications with an oil-absorbent pillow or shall be made completely from an oil-absorbent material with a woven pillow.

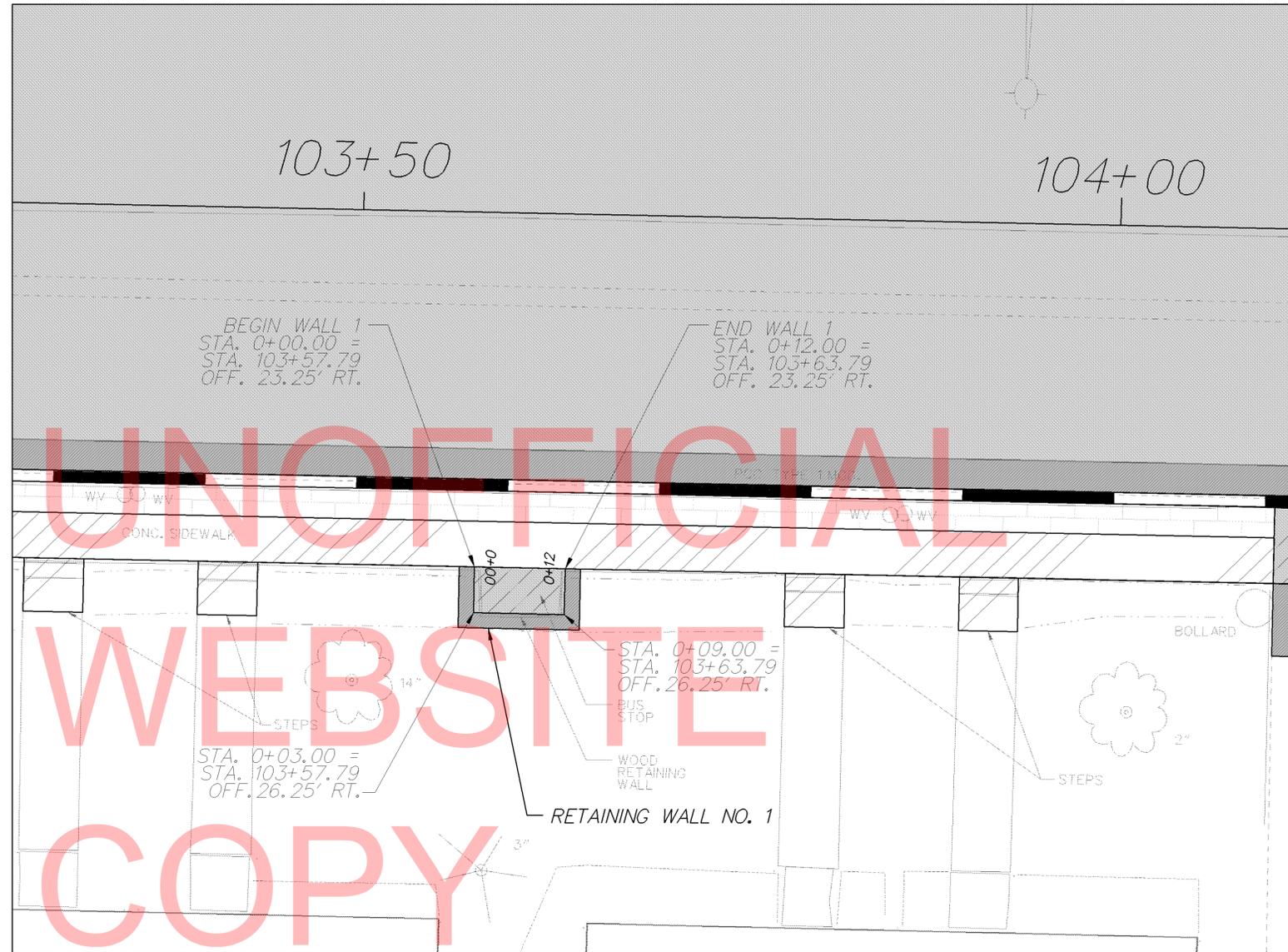
Materials:

The geotextile inlet insert shall meet or exceed the specifications of Type GD-III geotextile in accordance with Appendix A-3 of the Delaware Erosion & Sediment Control Handbook.

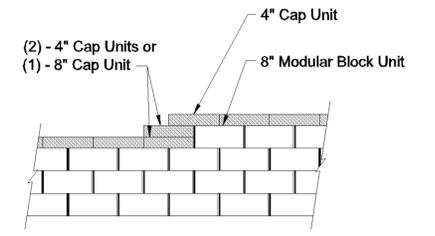
**INLET PROTECTION  
N.T.S.**

**GENERAL NOTES:**

- THE DETAILS PROVIDED PER THIS PLAN SHEET ARE FOR REFERENCE ONLY AND ARE NOT ENGINEERED DETAILS. THE DETAILS ARE SIMPLY MEANT TO ASSIST THE BIDDER IN DETERMINING A REASONABLE COST FOR CONSTRUCTION OF THE PROPOSED MODULAR RETAINING WALL.
- THE CONTRACTOR SHALL CONTACT/EMPLOY A MODULAR RETAINING WALL MANUFACTURER FOR THE PROPOSED RETAINING WALL TO BE DESIGNED BY THE PRODUCT MANUFACTURER'S ENGINEER. CONTRACTOR SHALL SUBMIT, FOR APPROVAL, CONSTRUCTION DRAWINGS AND DESIGN CALCULATIONS FOR THE RETAINING WALL SYSTEM PREPARED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF DELAWARE.
- ACCEPTABLE MODULAR BLOCK RETAINING WALLS ARE:
  - "KEystone RETAINING WALL SYSTEMS", AS MANUFACTURED BY ANCHOR CONCRETE PRODUCTS, INC., P.O. BOX 601, BRIDGEPORT, NJ 08014 (TELEPHONE 609-241-1544).
  - "VERSA-LOK RETAINING WALL SYSTEMS", AS MANUFACTURED BY BLINKLEY & OBER, INC., P.O. BOX 7, EAST PETERSBURG, PA 17520 (TELEPHONE 717-569-0441).
  - "ALLAN BLOCK RETAINING WALL SYSTEMS" STYLE AB THREE AS MANUFACTURED BY FIZZANO BROS., INC., CHESTER PIKE, CRUM LYNNE, PA 19022 (TELEPHONE 610-833-1100).
  - APPROVED EQUAL.
- ANY GEOGRID REINFORCEMENT, UNDERDRAINS/DRAINAGE SYSTEM THAT MAY BE DEEMED NECESSARY BY THE MODULAR RETAINING WALL SYSTEM DESIGN ENGINEER SHALL BE INCIDENTAL TO THE PROJECT AND AT NO ADDITIONAL COST TO THE OWNER.
- SHOP DRAWINGS SHALL INCLUDE THE FOLLOWING INFORMATION: TYPE OF WALL, LOCATION, LENGTH, TOP ELEVATION(S), PROPOSED BOTTOM OF FOOTING OR LEVELING PAD ELEVATION(S), MODULAR BLOCKS, WALL DRAINAGE UNIT, AND CROSS SECTIONS INCLUDING DRAINAGE ZONE, BACKFILL MATERIAL TYPE AND LIMITS, AND QUANTITIES. DETAILS FOR ALL DRAIN OUTLETS, INCLUDING SOLID FLEXIBLE PIPING EXTENDING THRU THE MODULAR BLOCK WALL SHALL BE PROVIDED IN THE SHOP DRAWINGS. CONTRACTOR SHALL VERIFY ALL FIELD DIMENSIONS PRIOR TO SUBMITTAL OF SHOP DRAWINGS.
- PERFORM REQUIRED DESIGN OF THE RETAINING WALL IN ACCORDANCE WITH THE 2002 AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES WITH CURRENT INTERIMS.
- WALLS SHALL BE CONSTRUCTED FROM LOW END TO HIGH END.
- WALLS SHALL BE STEPPED AS NECESSARY TO MAINTAIN A 2" MINIMUM VERTICAL DISTANCE MEASURED FROM THE FINAL FILL TO TOP OF CAP UNIT.
- WALLS SHALL EXTEND A MINIMUM OF ONE BLOCK UNIT BENEATH THE COMPACTED FILL ALONG THE FRONT FACE.
- ALL EXPOSED MODULAR BLOCK SURFACES SHALL HAVE A SPLIT-FACED TEXTURE.
- LEVELING PAD, SIZE, AND MATERIAL, SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.
- WALLS SHALL HAVE A NEAR VERTICAL FACE WITH MINIMUM SETBACKS AS RECOMMENDED BY THE MANUFACTURER.
- THE CONTRACTOR SHALL INSTALL PER MANUFACTURER'S RECOMMENDATIONS.
- ALL COSTS FOR PROVIDING ALL MATERIALS AS NOTED AND DETAILED INCLUDING SHOP DRAWINGS SHALL BE INCIDENTAL TO ITEM 602615 - MODULAR BLOCK RETAINING WALL.

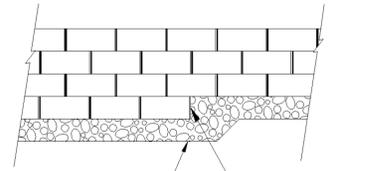


**PLAN VIEW - RETAINING WALL NO. 1**  
SCALE: 1" = 5'

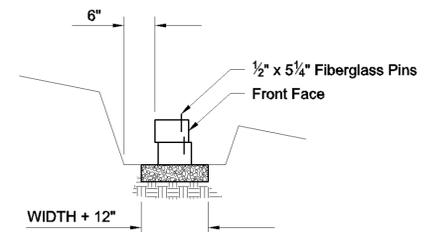


Note:  
1. Secure all cap units with Manufacturer's Recommendations.

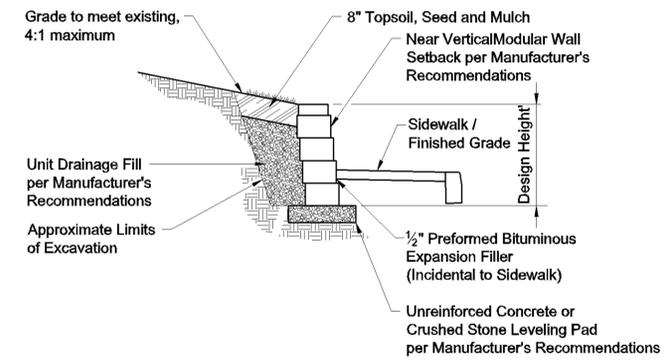
**TOP OF WALL STEPS**  
N.T.S.



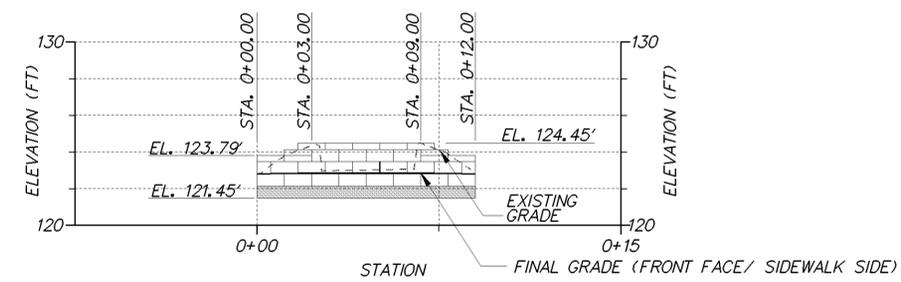
**Elevation**  
8" Crushed Rock or Unreinforced Concrete (2,000 psi minimum) Leveling Pad  
N.T.S.



**Section**  
**LEVELING PAD DETAILS**  
N.T.S.



**TYPICAL GRAVITY WALL**  
(RETAINING WALL 1)  
N.T.S.

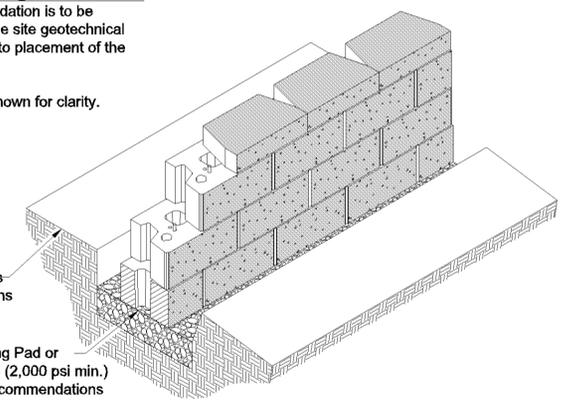


**ELEVATION VIEW - RETAINING WALL NO. 1**  
SCALE: 1" = 5'

**Base Leveling Pad Notes:**

- The base foundation is to be approved by the site geotechnical engineer prior to placement of the leveling pad.
- Geogrid not shown for clarity.

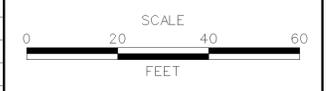
Excavation Limits as shown on Plans  
Crushed Stone Leveling Pad or Unreinforced Concrete (2,000 psi min.) per Manufacturer's Recommendations



**TYPICAL WALL ISOMETRIC SECTION VIEW**  
N.T.S.

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ADDENDUMS / REVISIONS	



CONTRACT	BRIDGE NO.	-
T201020006	DESIGNED BY:	AMW
COUNTY	CHECKED BY:	WFC
NEW CASTLE		

<b>RETAINING WALL DETAILS</b>	
SHEET NO.	14
TOTAL SHTS.	33

TRAFFIC CONTROL NOTES

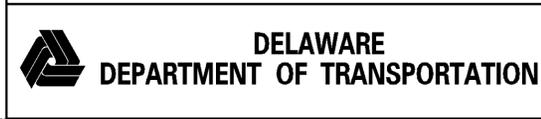
1. ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.
2. THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF EXISTING PAVEMENT WITHIN THE PROJECT LIMITS FOR THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER OR DESIGNEE. ALL REPAIRS ARE AT THE CONTRACTOR'S EXPENSE AS DIRECTED BY THE ENGINEER.
3. CONTRACTOR SHALL ONLY WORK ON ONE SIDE OF THE ROAD AT A TIME OR AS DIRECTED IN FIELD BY THE ENGINEER.
4. CONTRACTOR SHALL FOLLOW PROPOSED PEDESTRIAN DETOUR PLAN AS SHOWN ON THE PLANS, UNLESS OTHERWISE DIRECTED BY ENGINEER IN THE FIELD. PEDESTRIAN DETOUR SHALL FOLLOW GUIDELINES IN DEMUTC.D.
5. CONTRACTOR SHALL NOT WORK ON MORE THAN ONE BLOCK AT A TIME, EXCEPT FOR FINAL MILL AND OVERLAY. ALL PROPOSED WORK WITHIN ONE CITY BLOCK SHALL BE COMPLETED PRIOR TO STARTING ANY WORK ON ANOTHER CITY BLOCK.
6. UNLESS OTHERWISE APPROVED BY THE ENGINEER, THE ROADWAY SHALL NOT BE MILLED UNTIL ALL NEW CURBS HAVE BEEN INSTALLED, AND BASE HOT-MIX IS PLACED IN THE PATCH AREAS. MILLING AND FINAL OVERLAY MAY THEN PROCEED FOR THE ENTIRE LENGTH OF THE JOB.

GENERAL PROJECT NOTES FOR SEDIMENT AND STORMWATER MANAGEMENT

- 1) DUST CONTROL  
DUST IS TO BE CONTROLLED EXCLUSIVELY THROUGH THE USE OF WATER. COSTS ASSOCIATED WITH THE FURNISHING AND APPLICATION OF WATER FOR DUST CONTROL SHALL BE INCIDENTAL TO ALL THE CONTRACT ITEMS.
- 2) EXISTING INLETS AND PIPES  
ALL EXISTING INLETS AND PIPES WITHIN THE PROJECT LIMITS TO REMAIN SHALL BE CLEANED OUT. THE COST OF CLEANING OUT THE EXISTING PIPES TO REMAIN IS INCIDENTAL TO ITEM #201000.
- 3) REVIEW AND OR APPROVAL OF THE SEDIMENT AND STORMWATER MANAGEMENT PLAN SHALL NOT RELIEVE THE CONTRACTOR FROM HIS OR HER RESPONSIBILITIES FOR COMPLIANCE WITH THE THE REQUIREMENTS OF THE SEDIMENT AND STORMWATER REGULATIONS, NOR SHALL IT RELIEVE THE CONTRACTOR FROM THE ERRORS OR OMISSIONS IN THE APPROVED PLAN.
- 4) SOIL DISTURBANCE  
FOLLOWING SOIL DISTURBANCE OR REDISTURBANCE, PERMANENT OR TEMPORARY STABILIZATION SHALL BE COMPLETED WITHIN 14 CALENDAR DAYS.
- 5) THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE AND REPAIR OF ALL EROSION AND SEDIMENT CONTROLS, AND STORMWATER MANAGEMENT PRACTICES DURING THE PROJECT.
- 6) EROSION CONTROL MEASURES SHALL BE INSTALLED AS SHOWN ON THE PLANS OR AS DIRECTED BY ENGINEER IN THE FIELD.
- 7) STAGING AREAS. PROPER EROSION AND SEDIMENT CONTROL MEASURES AS DETERMINED BY THE ENGINEER SHALL BE INSTALLED IN ALL STAGING AREAS, AT COMPLETION OF THE PROJECT. STAGING AREAS THAT ARE PAVED SHALL BE RESTORED TO ITS ORIGINAL CONDITION. IF UNPAVED, STAGING AREAS SHALL BE REGRADED TO THE SATISFACTION OF THE ENGINEER, TOPSOILED SEEDED, AND MULCHED IN ACCORDANCE WITH DELDOT STANDARD SPECIFICATIONS SECTIONS 732, 734, AND 735 FOR TOPSOIL, SEED AND MULCH RESPECTIVELY. SEED SHALL ADHERE TO THE SPECIFICATIONS FOR PERMANENT GRASS SEEDING - DRY GROUND. ALL COSTS ASSOCIATED TO THE ABOVE ITEMS SHALL BE AT THE CONTRACTOR'S EXPENSE. IF THE ENGINEER DETERMINES THAT A SATISFACTORY STAND OF GRASS DOES NOT EXIST AT THE TIME OF FINAL INSPECTION. ALL COSTS ASSOCIATED WITH RE-ESTABLISHING A SATISFACTORY STAND OF GRASS SHALL ALSO BE AT THE CONTRACTOR'S EXPENSE.
- 8) WITHIN THE LIMITS OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL VEGETATION WITHIN THE DELDOT RIGHT OF WAY TO A MAXIMUM OF 6".
- 9) SOIL STOCKPILES (IF REQUIRED.) STOCKPILES OF SOIL SHALL BE STABILIZED AND SEEDED PER DELDOT REQUIREMENTS.
- 10) THE CONTRACTOR SHALL PLACE SILT FENCE (IF REQUIRED) AS DIRECTED IN THE FIELD BY THE ENGINEER. ALL COSTS ARE INCLUDED UNDER ITEM 251000.

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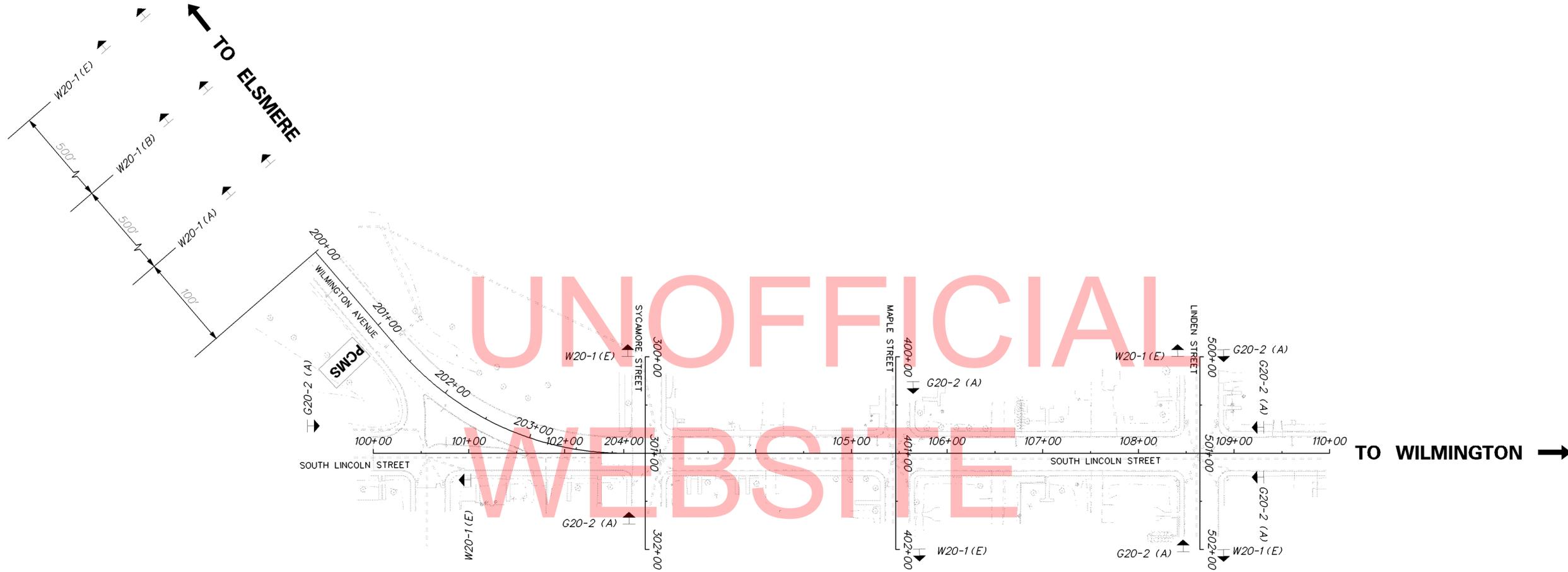
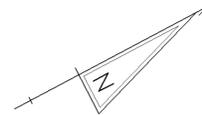
ADDENDUMS / REVISIONS

**BAYARD SQUARE ENHANCEMENTS,  
CITY OF WILMINGTON**

CONTRACT	BRIDGE NO.	-
T201020006	DESIGNED BY:	AMW
COUNTY	CHECKED BY:	WFC
NEW CASTLE		

**CONSTRUCTION PHASING,  
M.O.T. AND EROSION  
CONTROL NOTES**

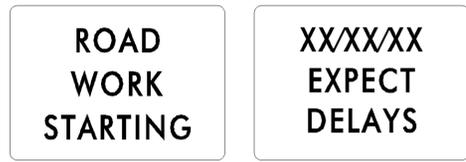
SHEET NO.	15
TOTAL SHTS.	33



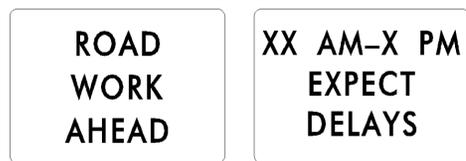
**ADVANCED WARNING SIGN LAYOUT**

\* CMS BOARDS SHALL DISPLAY THE FOLLOWING MESSAGES 10 DAYS PRIOR TO CONSTRUCTION, THROUGH 5 DAYS AFTER THE BEGINNING OF CONSTRUCTION:

10 DAYS PRIOR TO CONSTRUCTION:



5 DAYS AFTER CONSTRUCTION BEGUN:



\* NOTE: THE CONTRACTOR SHALL LAYOUT SIGNS FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION. THE COVERING OF ANY CONFLICTING SIGNS SHALL BE INCIDENTAL TO ITEM# 743024. ALL SIGNS PAID UNDER ITEM # 743024.

CONSTRUCTION ZONE WARNING SIGNS

\* DISTANCE LEGEND

LETTER	DISTANCE
A	500 FT
B	1000 FT
C	1500 FT
D	1/2 MILE AHEAD
E	BOTH SHOULDERS CLOSED
F	



G20-2  
48" X 24"



W20-1  
48" X 48"

\* NOTE: ALL PERMANENT WARNING SIGNS PAID UNDER ITEM# 743024.

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ADDENDUMS / REVISIONS	



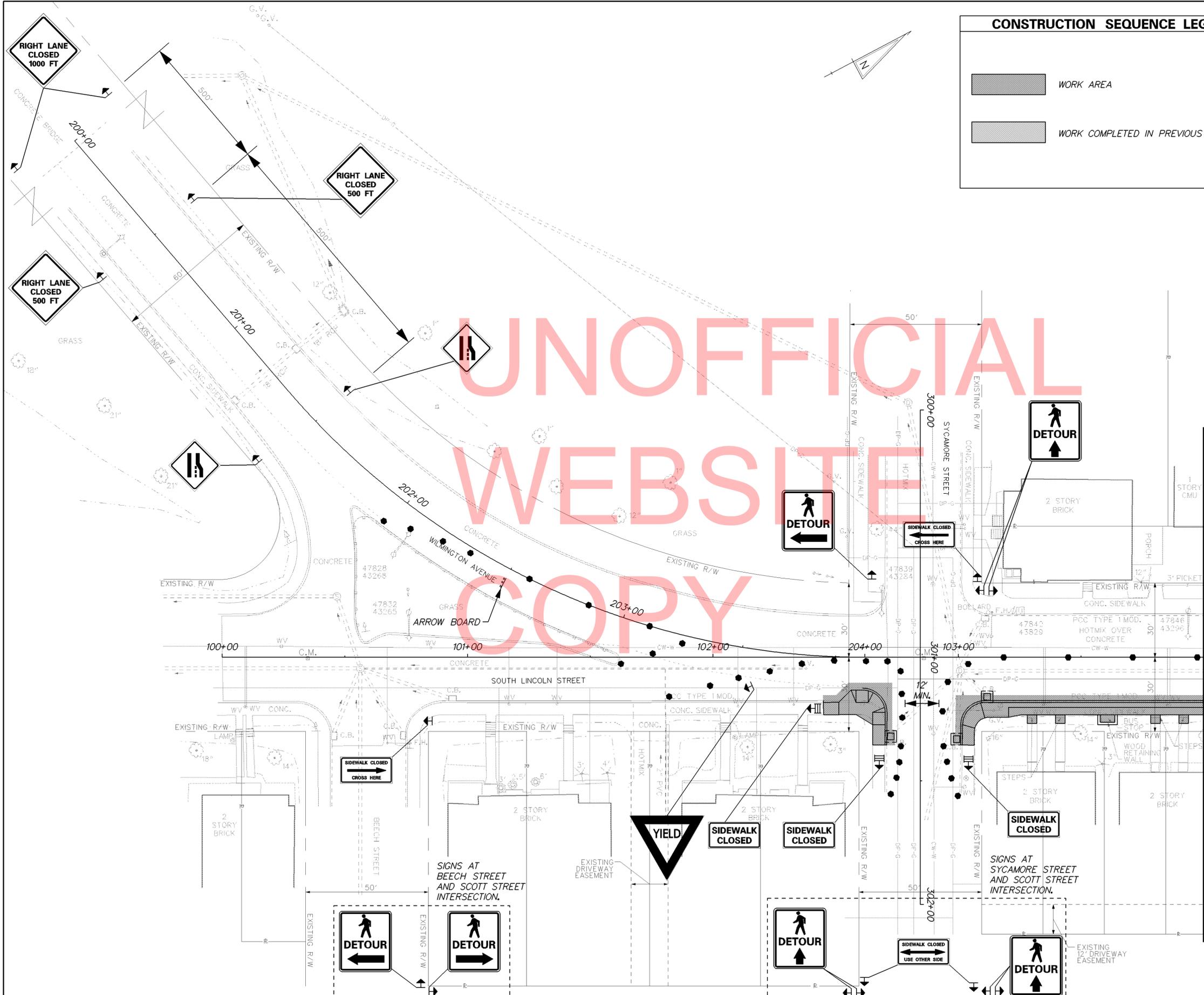
**BAYARD SQUARE ENHANCEMENTS,  
CITY OF WILMINGTON**

CONTRACT T201020006	BRIDGE NO. -
COUNTY NEW CASTLE	DESIGNED BY: AMW
	CHECKED BY: WFC

**CONSTRUCTION PHASING,  
M.O.T. AND EROSION  
CONTROL PLANS**

SHEET NO.	16
TOTAL SHTS.	33

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CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM

**SEQUENCE OF CONSTRUCTION**  
 THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

- PHASE 1**
- PHASE 1 - SYCAMORE STREET TO MAPLE STREET (EAST SIDE)
1. INSTALL ADVANCE WARNING SIGNS PER ADVANCE WARNING SIGN LAYOUT SHEET.
  2. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
  3. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
  4. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE RIGHT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.
  5. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
  6. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
  7. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
  8. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

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**SEQUENCE OF CONSTRUCTION**

THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

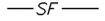
**PHASE 1**

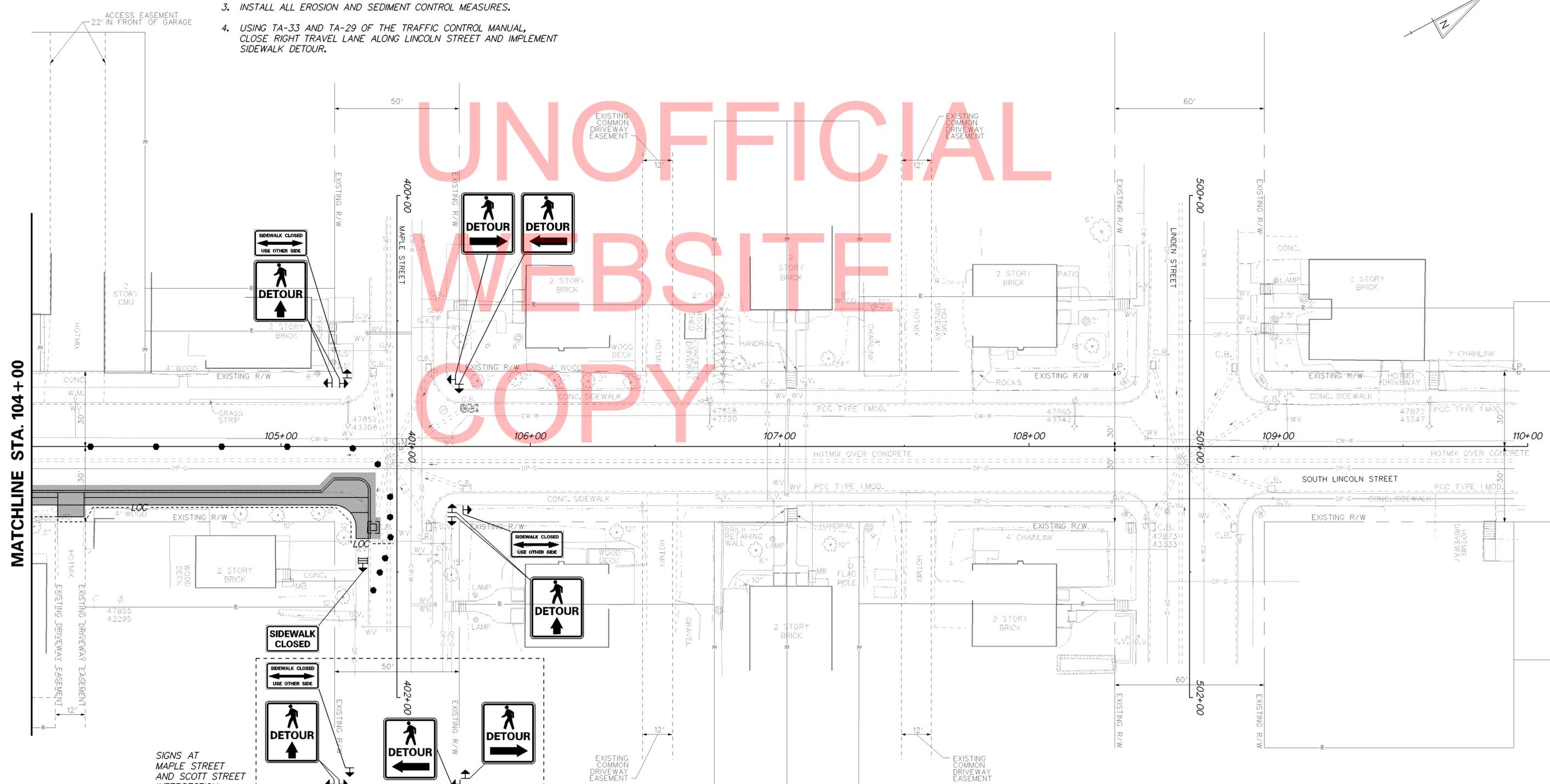
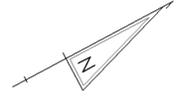
PHASE 1 - SYCAMORE STREET TO MAPLE STREET (EAST SIDE)

1. INSTALL ADVANCE WARNING SIGNS PER ADVANCE WARNING SIGN LAYOUT SHEET.
2. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
3. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
4. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE RIGHT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.

5. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
6. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
7. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
8. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

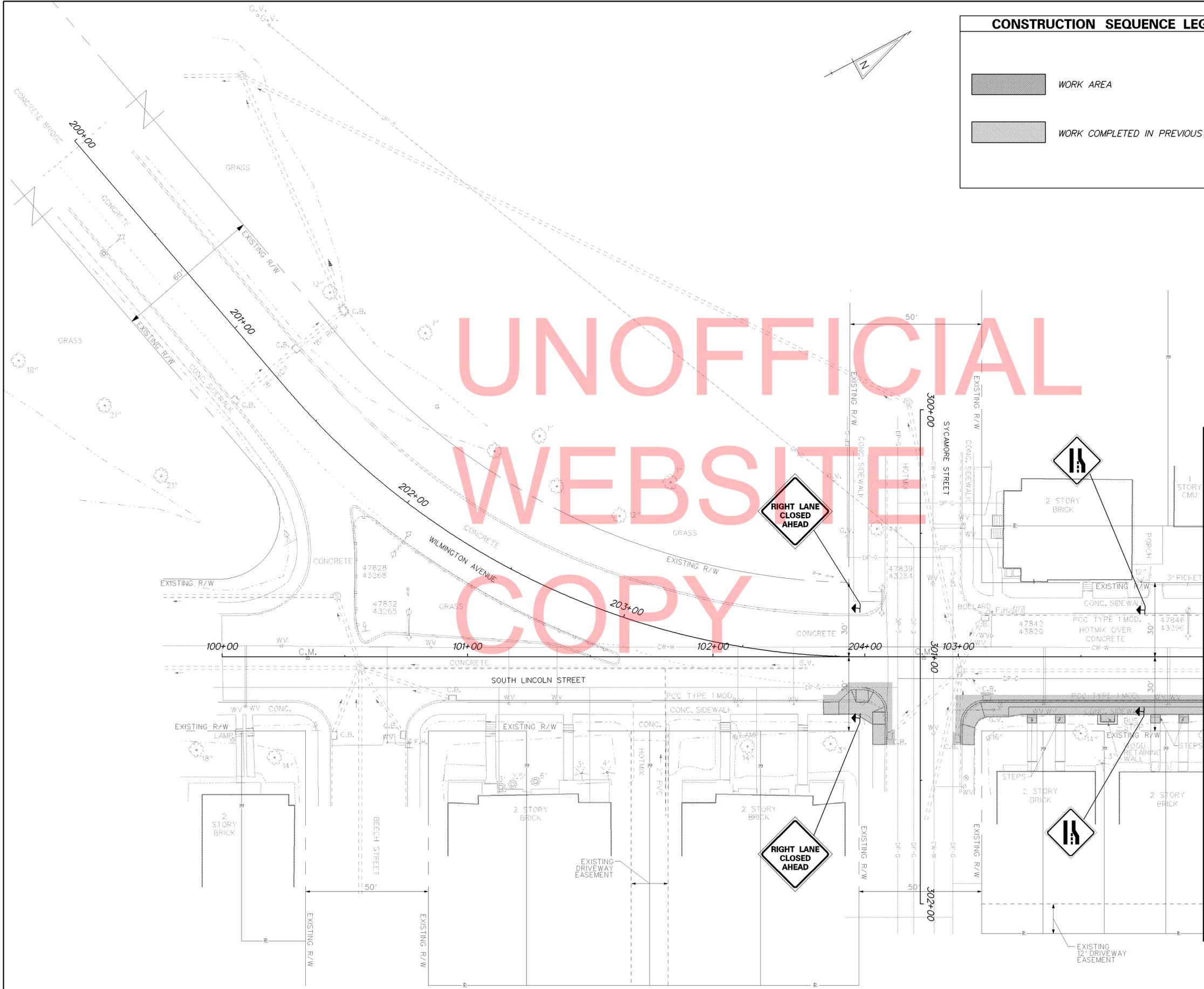
CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM



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RIGHT LANE  
CLOSED  
AHEAD

CONSTRUCTION SEQUENCE LEGEND	
	WORK AREA
	WORK COMPLETED IN PREVIOUS PHASE

LEGEND	
	SILT FENCE
	STABILIZED CONSTRUCTION ENTRANCE
	INLET SEDIMENT CONTROL
	STONE CHECK DAM

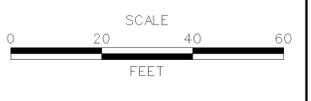
**SEQUENCE OF CONSTRUCTION**  
 THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

- PHASE 2**  
 PHASE 2 - MAPLE STREET TO LINDEN STREET (EAST SIDE)
1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
  2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
  3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE RIGHT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.
  4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
  5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
  6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
  7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

MATCHLINE STA. 104+00

ADDENDUMS / REVISIONS



CONTRACT T201020006	BRIDGE NO. -
COUNTY NEW CASTLE	DESIGNED BY: AMW CHECKED BY: WFC

<b>CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLANS</b>	SHEET NO. 19 TOTAL SHTS. 33
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**SEQUENCE OF CONSTRUCTION**

THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

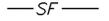
**PHASE 2**

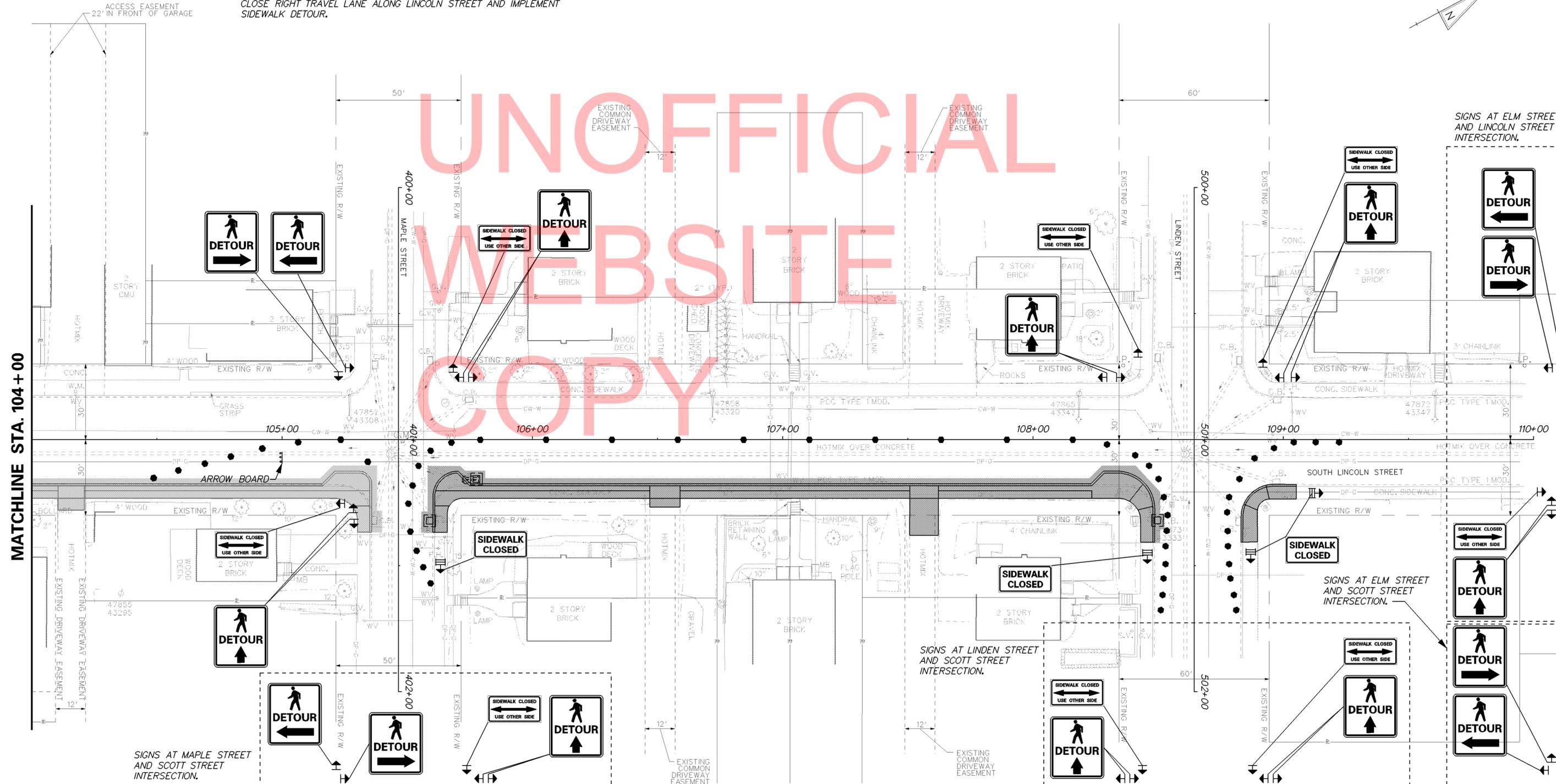
PHASE 2 - MAPLE STREET TO LINDEN STREET (EAST SIDE)

1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE RIGHT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.

4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

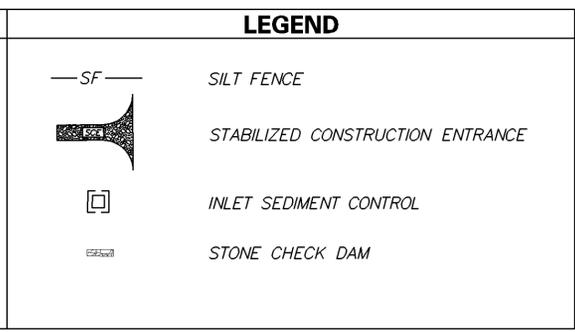
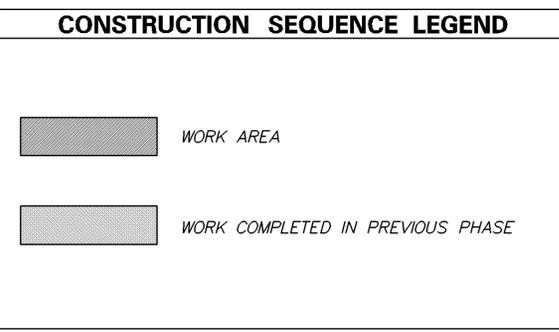
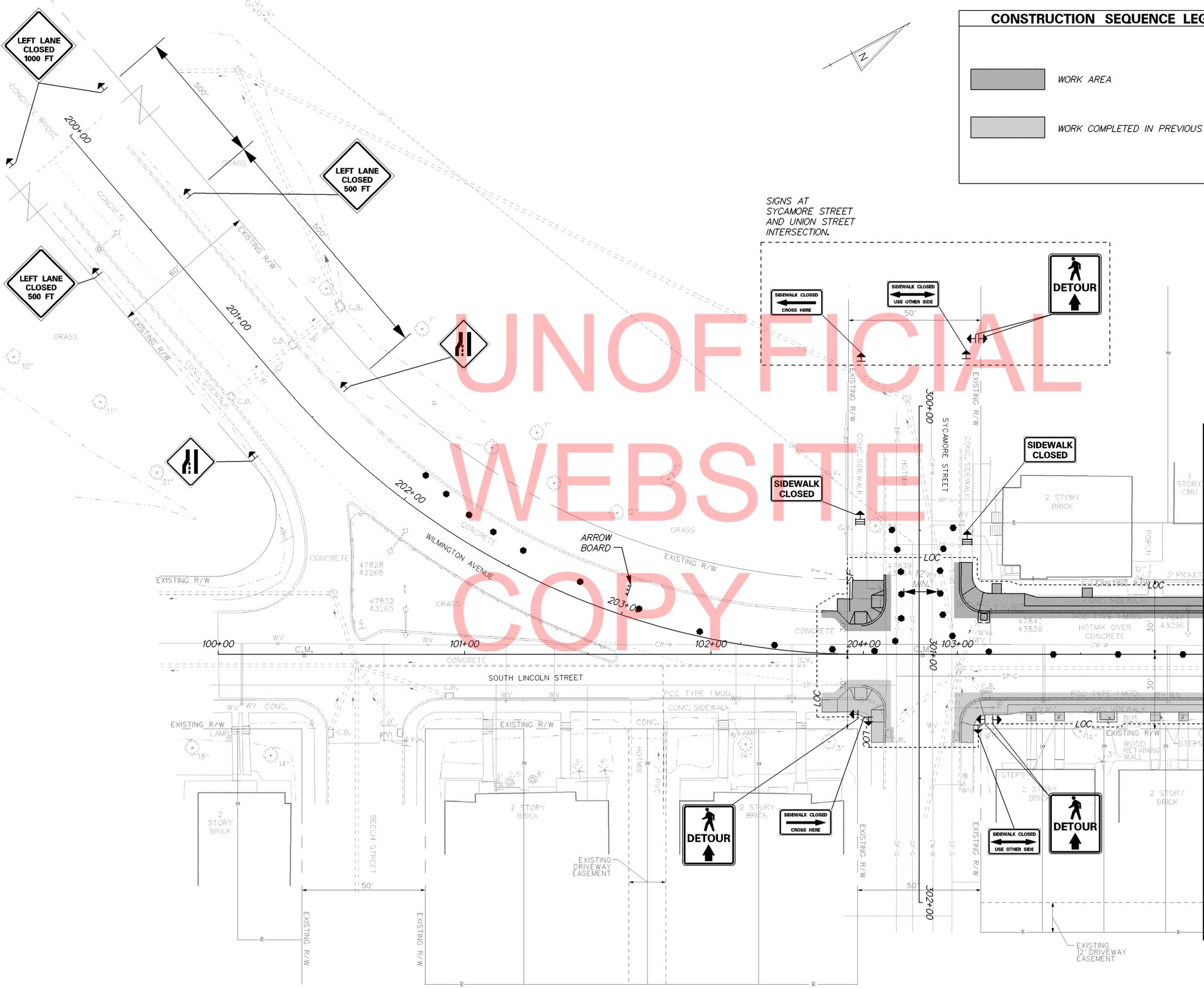
CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM



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**SEQUENCE OF CONSTRUCTION**  
 THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

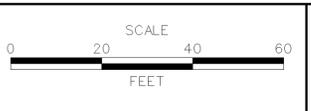
- PHASE 3**
- PHASE 3 - SYCAMORE STREET TO MAPLE STREET (WEST SIDE)
1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
  2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
  3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE LEFT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.
  4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
  5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
  6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
  7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

SIGNS AT SYCAMORE STREET AND UNION STREET INTERSECTION.

MATCHLINE STA. 104+00

ADDENDUMS / REVISIONS



CONTRACT	T201020006
COUNTY	NEW CASTLE
BRIDGE NO.	-
DESIGNED BY:	AMW
CHECKED BY:	WFC

**SEQUENCE OF CONSTRUCTION**

THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

**PHASE 3**

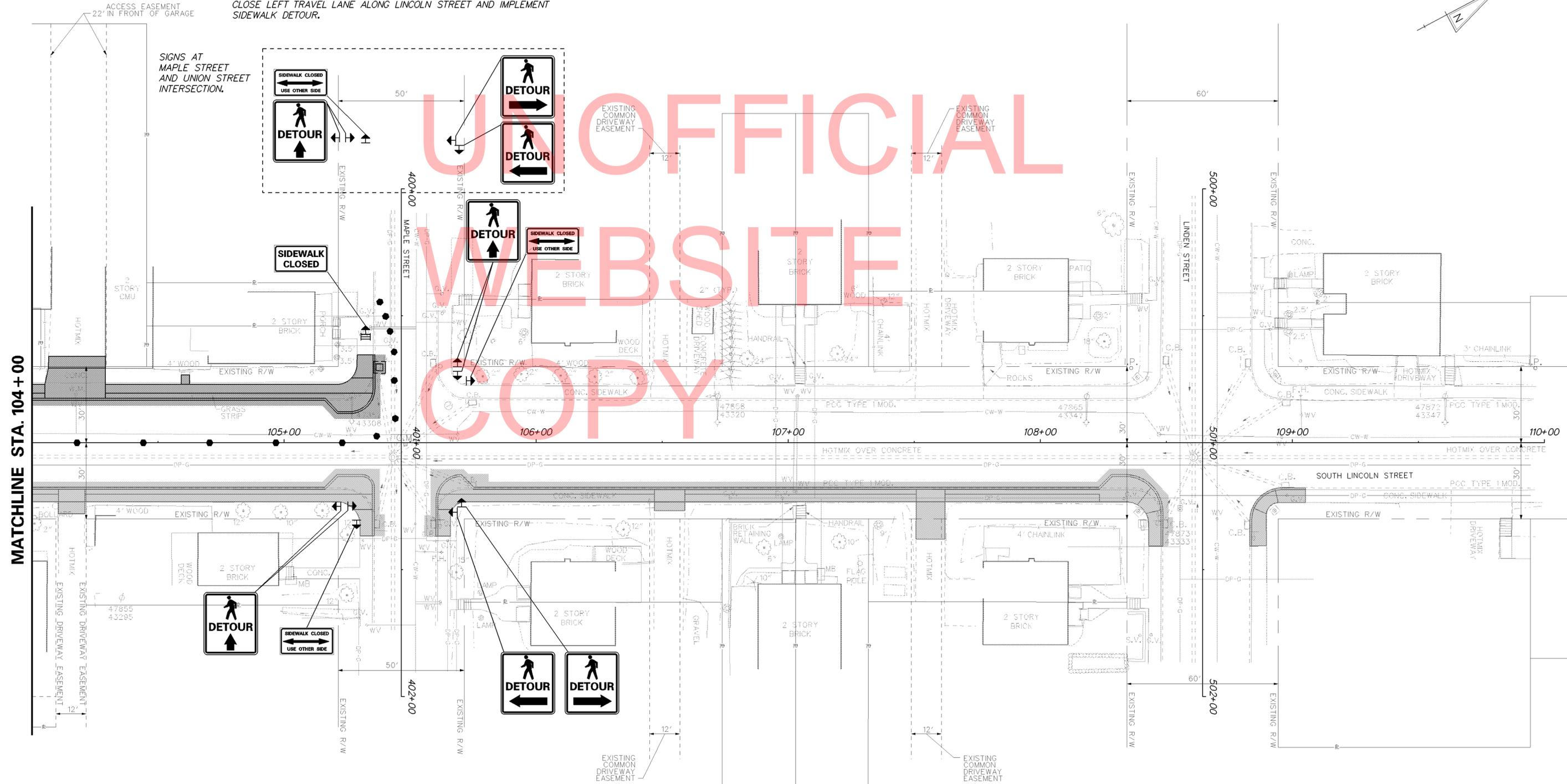
PHASE 3 - SYCAMORE STREET TO MAPLE STREET (WEST SIDE)

1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE LEFT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.

4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

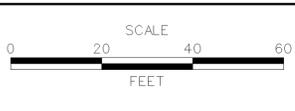
CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM



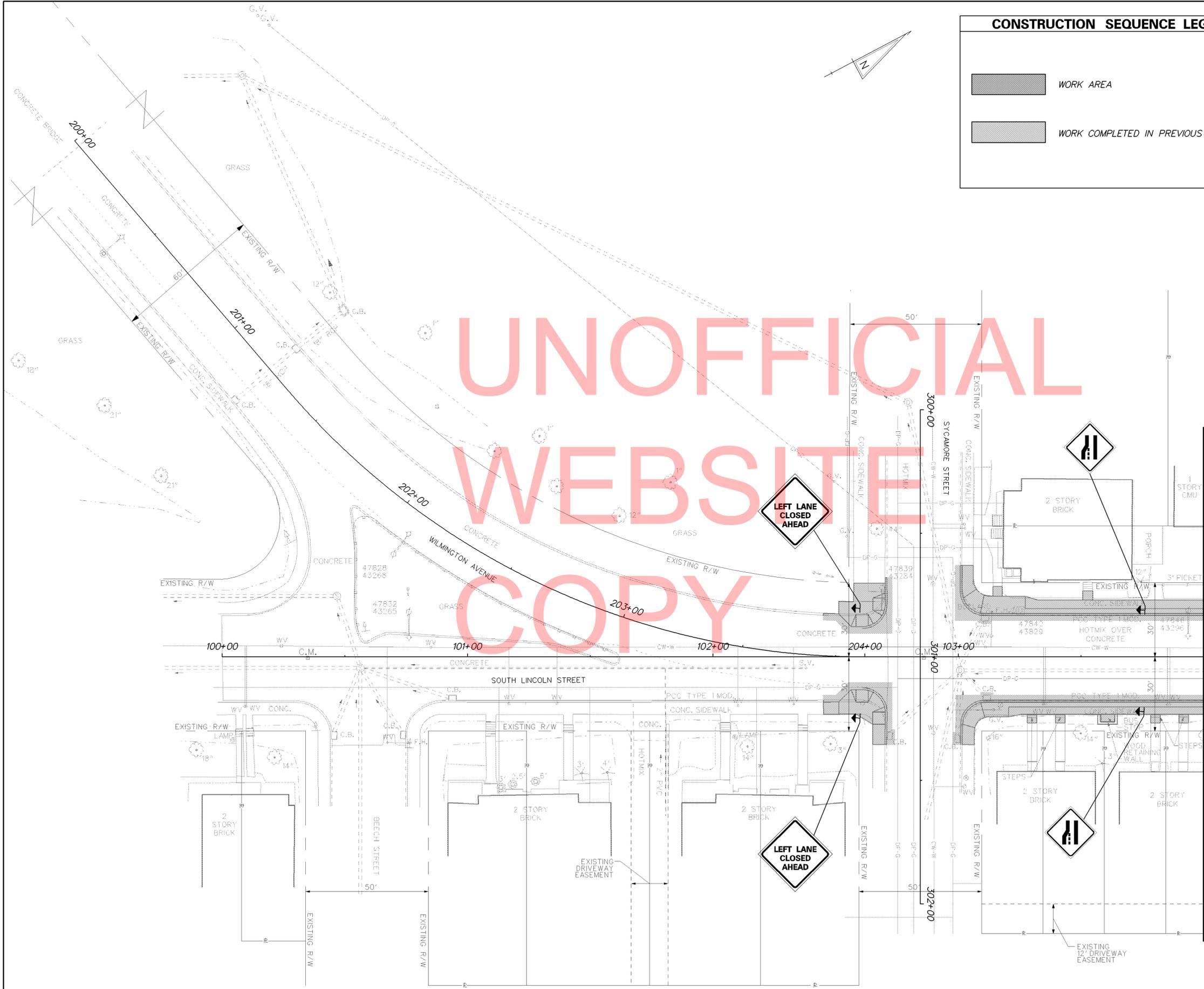
6/28/2012 11:42:14 G:\PROJECTS\08535.00 (Transportation Enhancement Program)\08535.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\cs2ES.dgn

ADDENDUMS / REVISIONS



CONTRACT T201020006	BRIDGE NO. -
COUNTY NEW CASTLE	DESIGNED BY: AMW
	CHECKED BY: WFC

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CONSTRUCTION SEQUENCE LEGEND	
	WORK AREA
	WORK COMPLETED IN PREVIOUS PHASE

LEGEND	
	SILT FENCE
	STABILIZED CONSTRUCTION ENTRANCE
	INLET SEDIMENT CONTROL
	STONE CHECK DAM

**SEQUENCE OF CONSTRUCTION**  
 THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

- PHASE 4**  
 PHASE 4 - MAPLE STREET TO LINDEN STREET (WEST SIDE)
1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
  2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
  3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE LEFT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.
  4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
  5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
  6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
  7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

LEFT LANE CLOSED AHEAD

LEFT LANE CLOSED AHEAD

MATCHLINE STA. 104+00

ADDENDUMS / REVISIONS



CONTRACT	T201020006
COUNTY	NEW CASTLE
BRIDGE NO.	-
DESIGNED BY:	AMW
CHECKED BY:	WFC

SHEET NO.	23
TOTAL SHTS.	33

**SEQUENCE OF CONSTRUCTION**

THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

**PHASE 4**

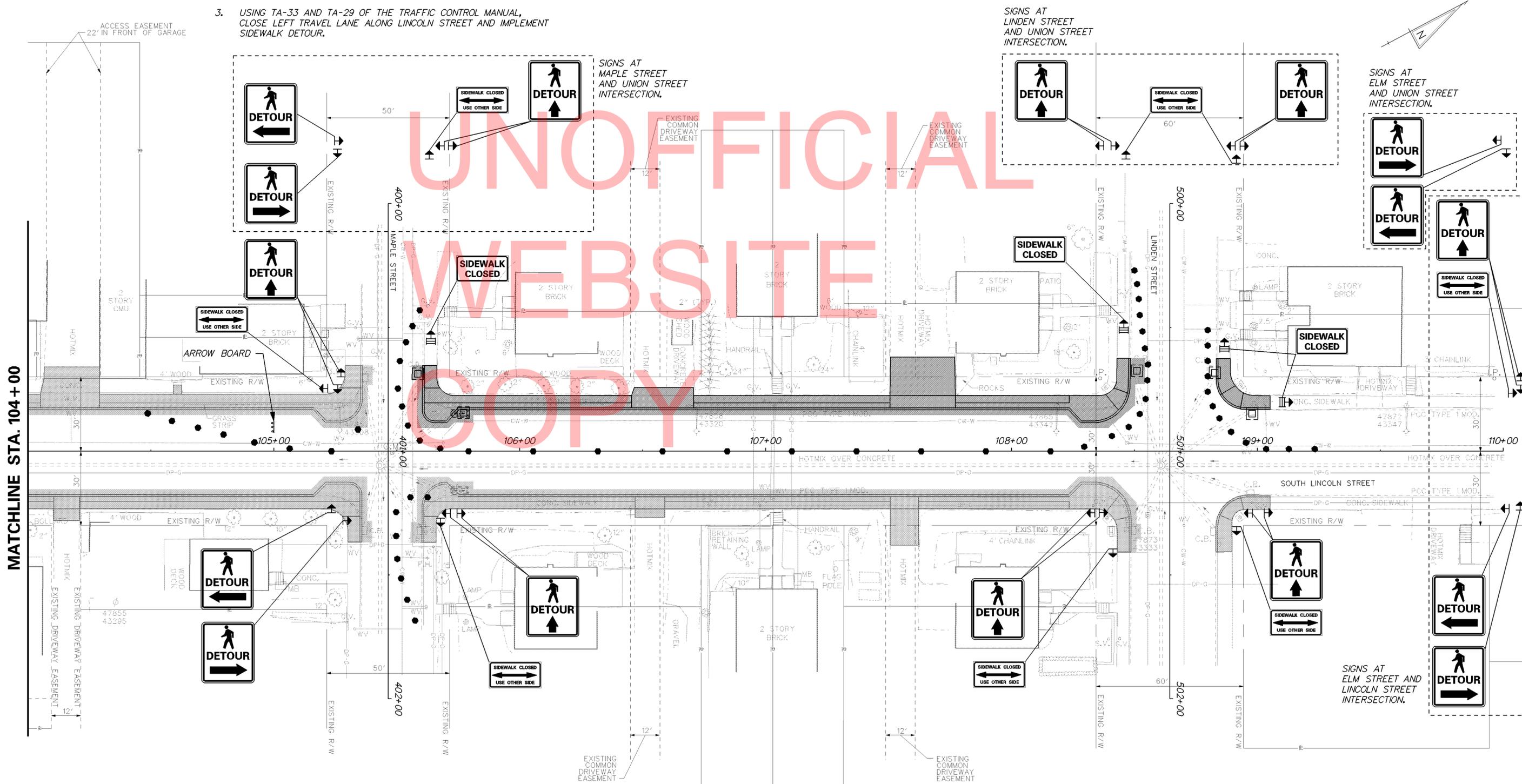
PHASE 4 - MAPLE STREET TO LINDEN STREET (WEST SIDE)

1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
3. USING TA-33 AND TA-29 OF THE TRAFFIC CONTROL MANUAL, CLOSE LEFT TRAVEL LANE ALONG LINCOLN STREET AND IMPLEMENT SIDEWALK DETOUR.

4. REMOVE EXISTING SIDEWALK AND CURB WITHIN THE CONSTRUCTION LIMITS OF THE PHASE.
5. CONSTRUCT ALL DRAINAGE IMPROVEMENTS WITHIN THIS STAGE OF CONSTRUCTION.
6. CONSTRUCT PROPOSED CURB, SIDEWALK, AND DRIVEWAY APRONS WITHIN THIS PHASE OF CONSTRUCTION.
7. TOPSOIL, SEED AND PLACE RETENTION BLANKET ON ALL DISTURBED AREAS WITHIN THIS PHASE OF CONSTRUCTION.

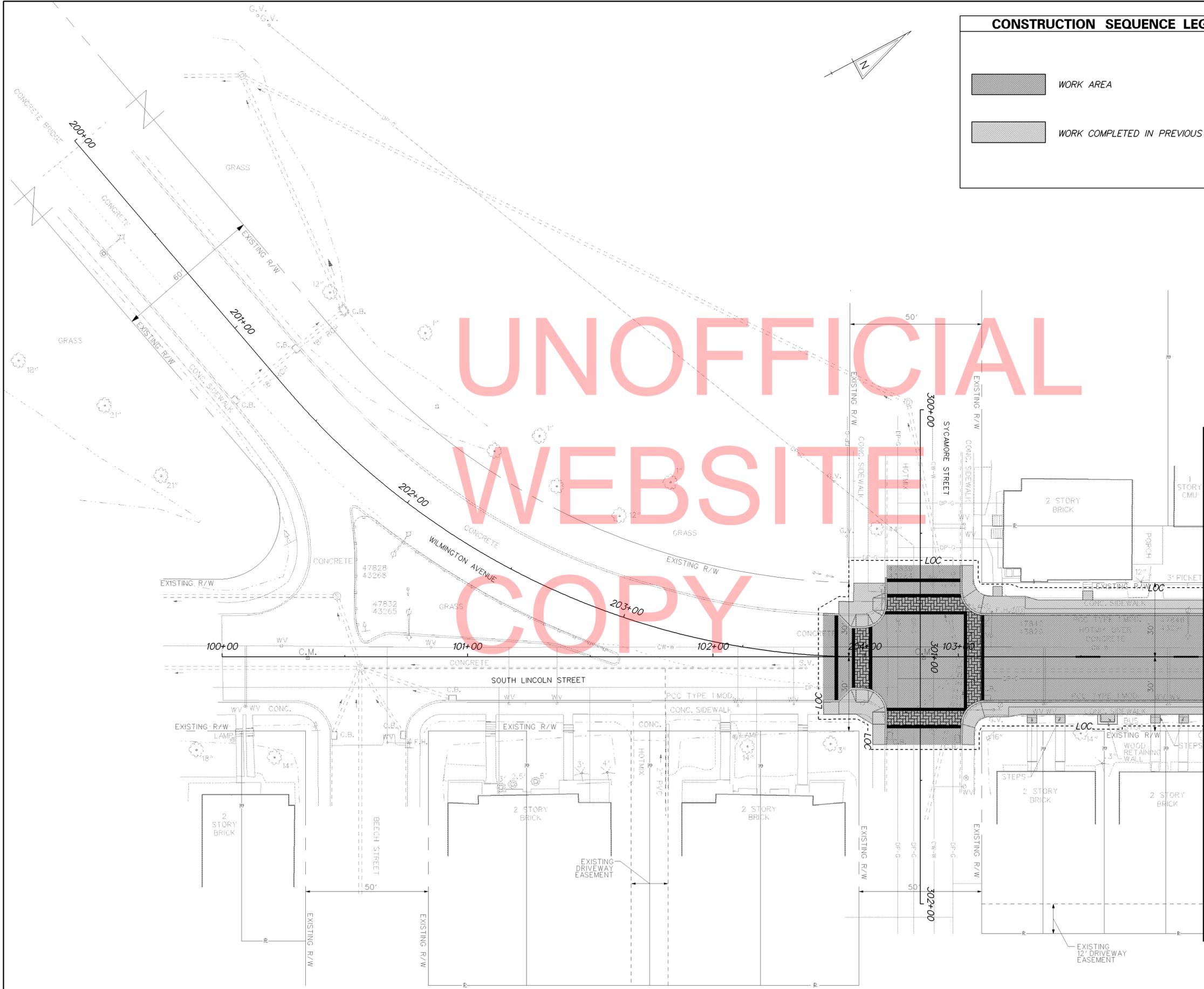
CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM



6/28/2012 11:09:38 G:\PROJECTS\08535.00 (Transportation Enhancement Program)\08535.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\cs42BS.dgn

6/28/2012 11:08:47 G:\PROJECTS\08535.00 (Transportation Enhancement Program)\08535.23 (Task 23, Bayard Square Improvements - Final Design)\CADD FILES\cs51BS.dgn



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WEBSITE  
COPY

CONSTRUCTION SEQUENCE LEGEND	LEGEND
WORK AREA	SILT FENCE
WORK COMPLETED IN PREVIOUS PHASE	STABILIZED CONSTRUCTION ENTRANCE
	INLET SEDIMENT CONTROL
	STONE CHECK DAM

**SEQUENCE OF CONSTRUCTION**  
 THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

- PHASE 5**  
 PHASE 5 - SYCAMORE STREET TO LINDEN STREET
1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
  2. UTILIZE TA-29 OF THE TRAFFIC CONTROL MANUAL IN ORDER TO MILL EXISTING ASPHALT AND CONCRETE AND PAVE 2" OF WMA, SUPERPAVE, TYPE C.
  3. LAYOUT AND PLACE FINAL CONFIGURATION OF PERMANENT PAVEMENT MARKINGS ON LINCOLN STREET.
  4. ALL DISTURBED AREAS SHALL BE GRADED TO DRAIN PROPERLY. THE CONTRACTOR SHALL PLACE TOPSOIL, PERMANENT SEEDING AND MULCH BLANKET ON ALL AREAS IN FINAL CONDITIONS OR DIRECTED BY THE ENGINEER.
  5. THE CONTRACTOR SHALL COORDINATE THE PLACEMENT OF THE FINAL SIGNAGE WITH STATE FORCES, WHERE APPLICABLE. THE CONTRACTOR SHALL REMOVE ANY TEMPORARY SIGNAGE.
  6. THE CONTRACTOR SHALL REMOVE ALL EROSION AND SEDIMENT CONTROL SYSTEMS STILL REMAINING IN THIS PHASE PER THE APPROVAL OF THE SEDIMENT CONTROL INSPECTOR.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

MATCHLINE STA. 104+00

**SEQUENCE OF CONSTRUCTION**

THE CONTRACTOR SHALL COMPLETE ALL WORK IN THE FOLLOWING SEQUENCE. ALL WORK SHALL BE COMPLETED IN A PHASE/STAGE BEFORE STARTING WORK IN A SUBSEQUENT PHASE/STAGE UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DISTURBED AREAS IN A STAGE SHALL BE STABILIZED AS DIRECTED BY THE ENGINEER BEFORE BEGINNING WORK IN THE NEXT PHASE/STAGE.

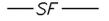
**PHASE 5**

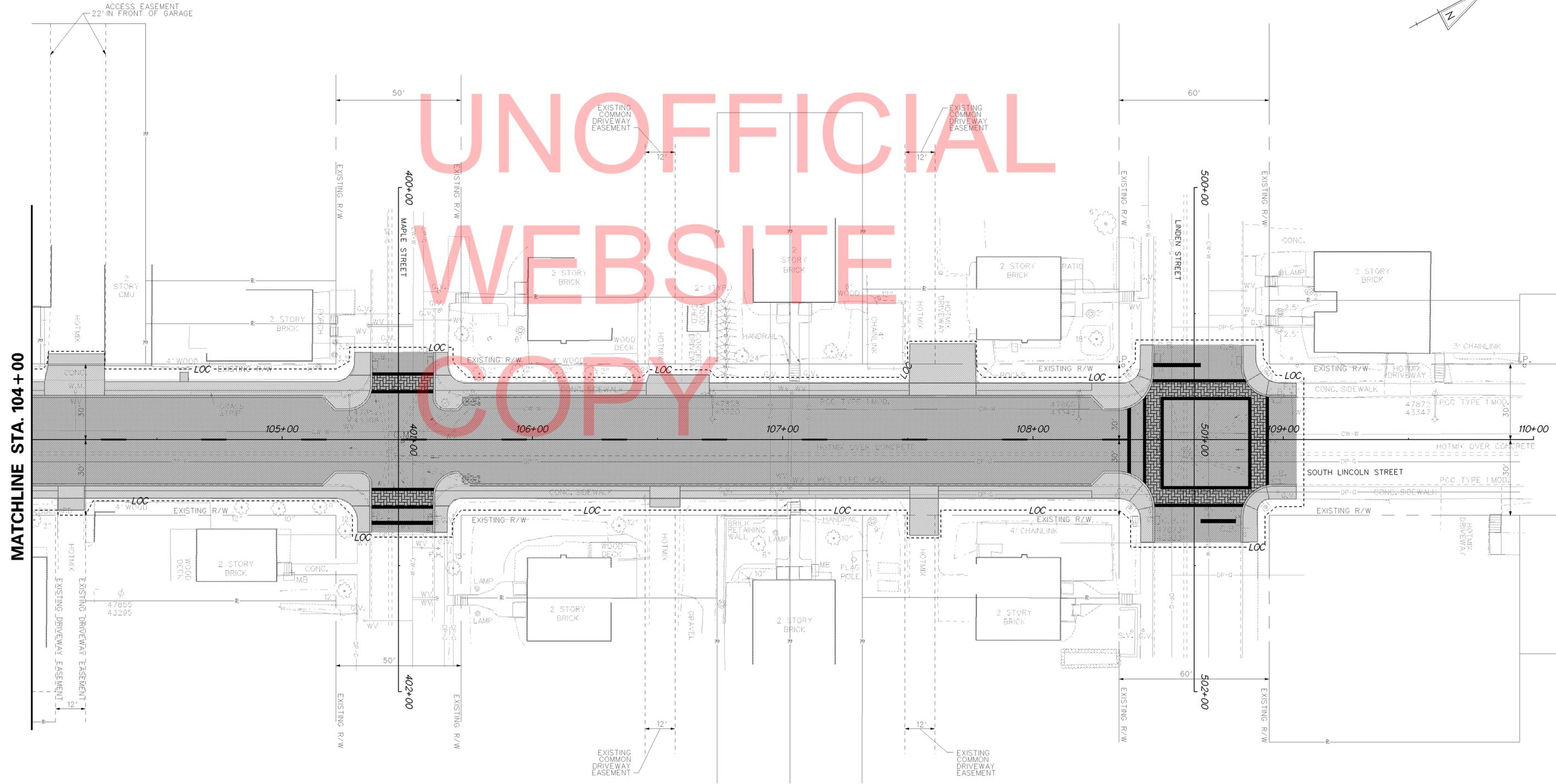
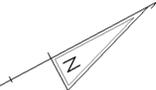
PHASE 5 - SYCAMORE STREET TO LINDEN STREET

1. PLACE PORTABLE CHANGEABLE MESSAGE SIGN TEN (10) DAYS IN ADVANCE OF CONSTRUCTION.
2. UTILIZE TA-29 OF THE TRAFFIC CONTROL MANUAL IN ORDER TO MILL EXISTING ASPHALT AND CONCRETE AND PAVE 2" OF WMA, SUPERPAVE, TYPE C.

3. LAYOUT AND PLACE FINAL CONFIGURATION OF PERMANENT PAVEMENT MARKINGS ON LINCOLN STREET.
4. ALL DISTURBED AREAS SHALL BE GRADED TO DRAIN PROPERLY. THE CONTRACTOR SHALL PLACE TOPSOIL, PERMANENT SEEDING AND MULCH BLANKET ON ALL AREAS IN FINAL CONDITIONS OR DIRECTED BY THE ENGINEER.
5. THE CONTRACTOR SHALL COORDINATE THE PLACEMENT OF THE FINAL SIGNAGE WITH STATE FORCES, WHERE APPLICABLE. THE CONTRACTOR SHALL REMOVE ANY TEMPORARY SIGNAGE.
6. THE CONTRACTOR SHALL REMOVE ALL EROSION AND SEDIMENT CONTROL SYSTEMS STILL REMAINING IN THIS PHASE PER THE APPROVAL OF THE SEDIMENT CONTROL INSPECTOR.

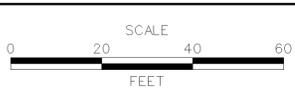
CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ENTRANCES DURING THIS PHASE OF CONSTRUCTION. COSTS ASSOCIATED WITH MAINTAINING ENTRANCES ARE INCIDENTAL TO THE CONTRACT.

CONSTRUCTION SEQUENCE LEGEND		LEGEND	
	WORK AREA		SILT FENCE
	WORK COMPLETED IN PREVIOUS PHASE		STABILIZED CONSTRUCTION ENTRANCE
			INLET SEDIMENT CONTROL
			STONE CHECK DAM



6/28/2012 11:08:00 (Transportation Enhancement Program)\08535.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\cs52BS.dgn

ADDENDUMS / REVISIONS



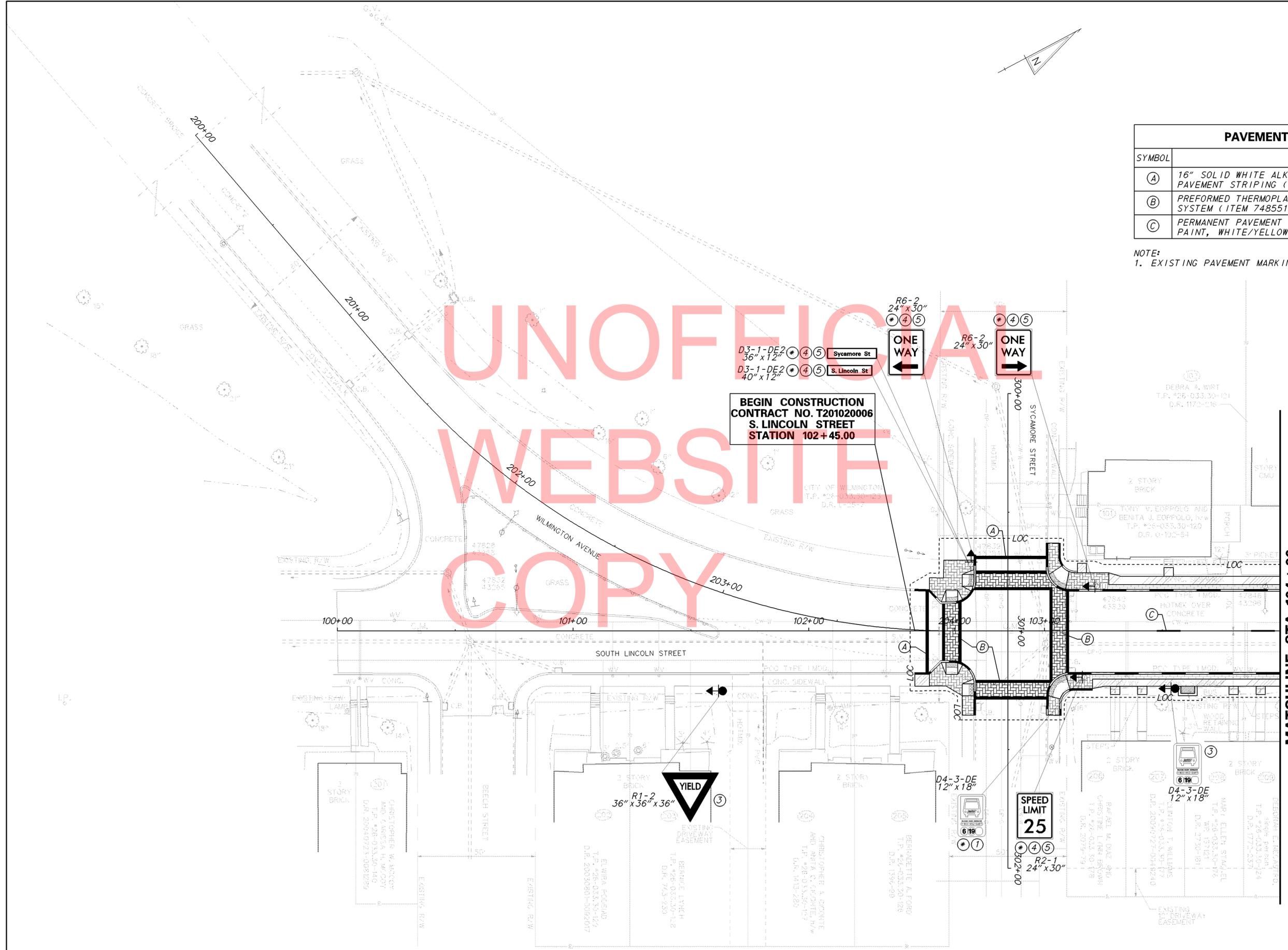
CONTRACT T201020006	BRIDGE NO. -
COUNTY NEW CASTLE	DESIGNED BY: AMW
	CHECKED BY: WFC

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

⊛ SIGN MOUNTED ON UTILITY POLE.

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	86 SF
(B)	PREFORMED THERMOPLASTIC DECORATIVE PAVEMENT SYSTEM (ITEM 748551)	987 SF
(C)	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5" (ITEM 748548)	20 LF

NOTE:  
1. EXISTING PAVEMENT MARKINGS TO REMAIN UNLESS OTHERWISE NOTED.



**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102+45.00**

**MATCHLINE STA. 104+00**

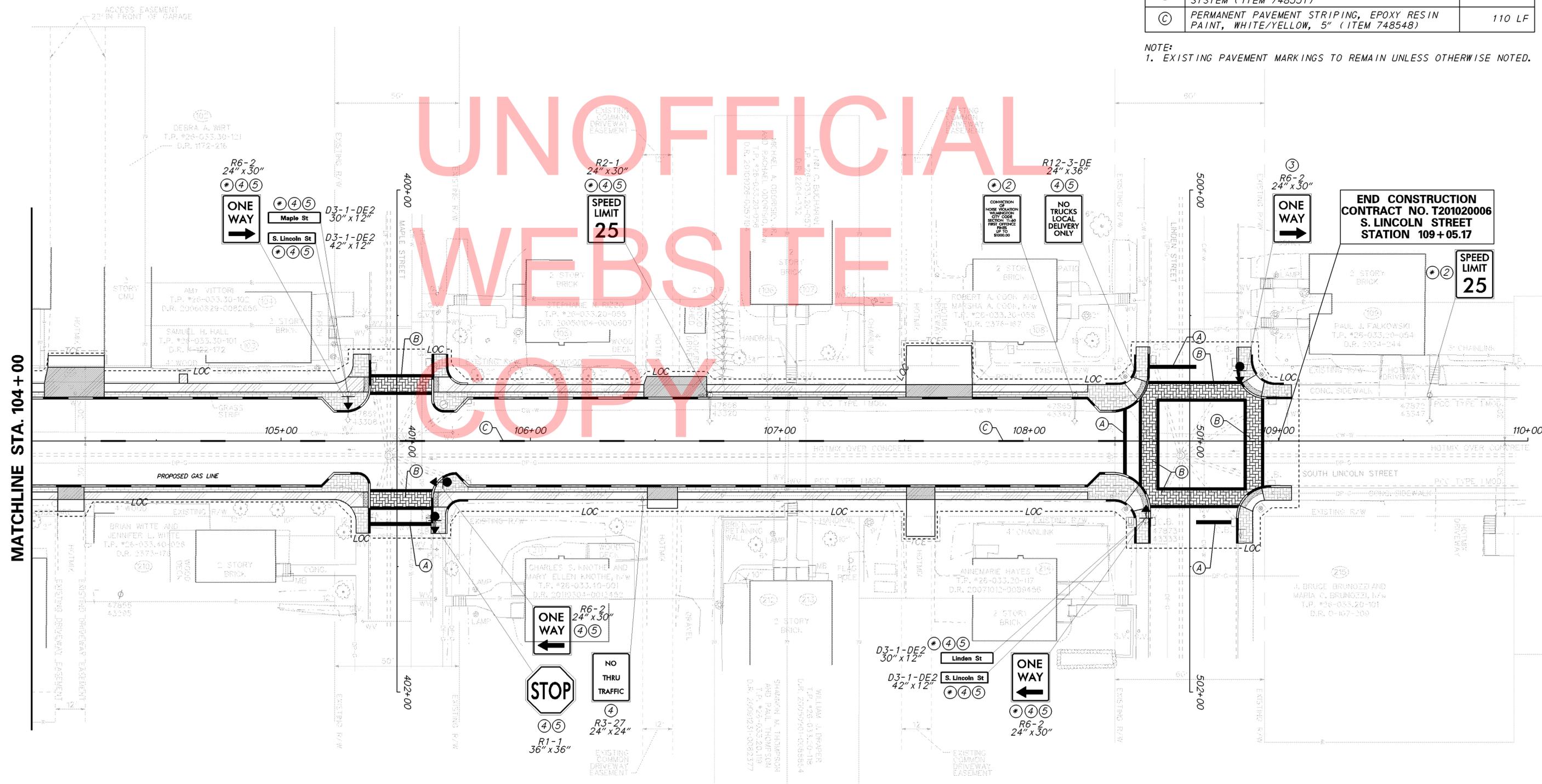
6/27/2012 10:53:00 (Transportation Enhancement Program)\08535.23 (Task 23, Bayard Square Improvements - Final Design)\CADD FILES\ss01BS.dgn

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

⊛ SIGN MOUNTED ON UTILITY POLE.

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	123 SF
(B)	PREFORMED THERMOPLASTIC DECORATIVE PAVEMENT SYSTEM (ITEM 748551)	1615 SF
(C)	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5" (ITEM 748548)	110 LF

NOTE:  
1. EXISTING PAVEMENT MARKINGS TO REMAIN UNLESS OTHERWISE NOTED.



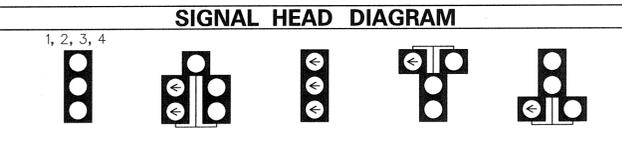
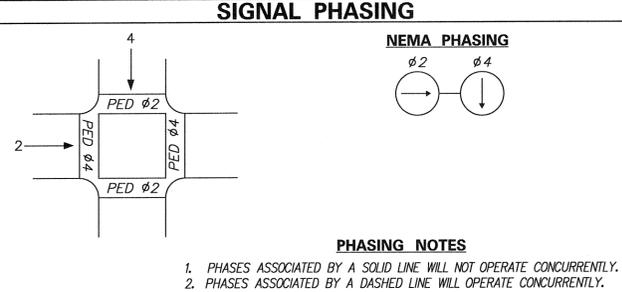
6/27/2012 10:55:00 (Transportation Enhancement Program)\08535.23 (Task 23; Bayard Square Improvements - Final Design)\CADD FILES\ss02BS.dgn

CONDUIT RUN SCHEDULE						
CO#	# OF CONDUITS	SIZE	TYPE	LENGTH	B/T/O/ P/A	AMOUNT AND TYPE OF CABLE/WIRE
X1	1	1.5"	GALV	35'	-	EXISTING (1)*8/2 U.F.W./GRND
X2	1	2.5"	GALV	25'	-	EXISTING (1)*14/16 (1)COMM.CABLE] NEW (4)*14/9
X3	1	1.5"	GALV	15'	-	REMOVE (1)*14/9 NEW (1)*14/9
XOH4	-	-	-	55'	-	EXISTING (1)*14/16 NEW (2)*14/9
OH5	-	-	-	40'	A	NEW (1)*14/9
6	1	1.5"	GALV	35'	T/P	NEW (1)*14/9
7	1	1.5"	GALV	25'	T/P	NEW (1)*14/9
8	1	2.5"	GALV	40'	T/P	NEW (2)*14/9
9	1	2.5"	PVC	5'	T	NEW (1)*14/9
10	1	4"	HDPE	35'	B	NEW (1)*14/9
11	1	2.5"	PVC	5'	T	NEW (1)*14/9

**LEGEND**  
 X- DENOTES EXISTING  
 SM- SINGLE MODE FIBER OPTICS  
 MM- MULTI MODE FIBER OPTICS  
 B- BORE  
 T- TRENCH  
 O- OPEN CUT  
 P- ATTACH TO POLE  
 A- AERIAL

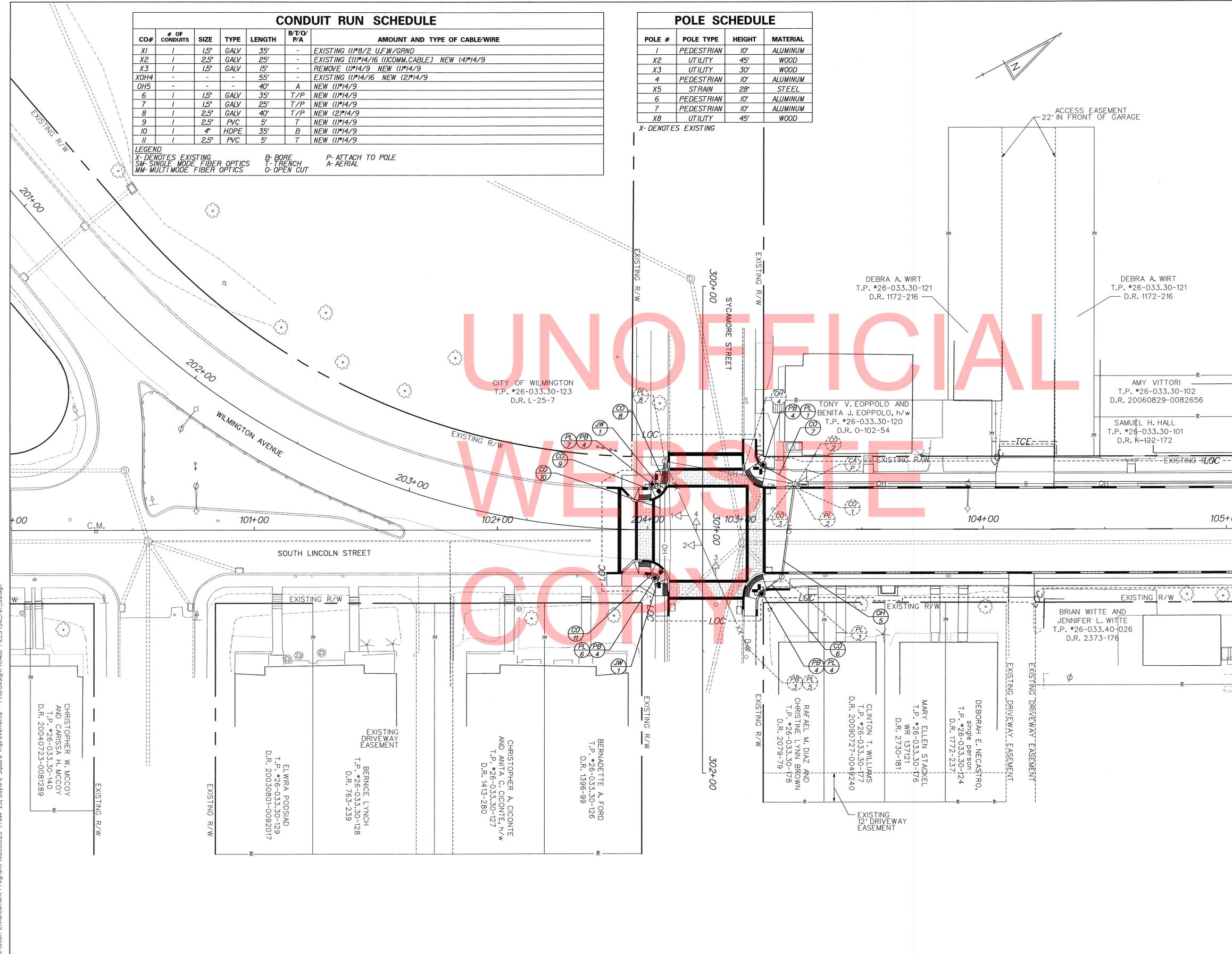
POLE SCHEDULE			
POLE #	POLE TYPE	HEIGHT	MATERIAL
1	PEDESTRIAN	10'	ALUMINUM
X2	UTILITY	45'	WOOD
X3	UTILITY	30'	WOOD
4	PEDESTRIAN	10'	ALUMINUM
X5	STRAN	28'	STEEL
6	PEDESTRIAN	10'	ALUMINUM
7	PEDESTRIAN	10'	ALUMINUM
X8	UTILITY	45'	WOOD

X- DENOTES EXISTING



LEGEND			
	PROPOSED SIGNAL CABINET		REMOVE BY CONTRACTOR
	EXISTING SIGNAL CABINET		REMOVE BY OTHERS
	PROPOSED SIGNAL POLE BASE		ABANDON
	EXISTING SIGNAL POLE BASE		PROPOSED POLE IDENTIFIER (# OF POLE)
	PROPOSED PEDESTRIAN POLE BASE		EXISTING POLE IDENTIFIER (# OF POLE)
	EXISTING PEDESTRIAN POLE BASE		PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	PROPOSED WOOD POLE		EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING UTILITY POLE		PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	PROPOSED JUNCTION WELL		EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL		PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	PROPOSED SIGNAL HEAD		EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
	EXISTING SIGNAL HEAD		PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	PROPOSED PEDESTRIAN SIGNAL HEAD		EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
	EXISTING PEDESTRIAN SIGNAL HEAD		PROPOSED MAST ARM IDENTIFIER (SEE MAST ARM SCHEDULE)
	PROPOSED PEDESTRIAN PUSHBUTTON		EXISTING MAST ARM IDENTIFIER (SEE MAST ARM SCHEDULE)
	EXISTING PEDESTRIAN PUSHBUTTON		PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
	PROPOSED VIDEO DETECTION		EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
	EXISTING VIDEO DETECTION		PROPOSED SPAN WIRE
	PROPOSED MICROWAVE DETECTION		EXISTING SPAN WIRE
	EXISTING MICROWAVE DETECTION		RIGHT-OF-WAY
	PROPOSED OPTICOM RECEIVER		PROPOSED SPAN INSULATOR
	EXISTING OPTICOM RECEIVER		EXISTING SPAN INSULATOR
	PROPOSED MAST ARM		SERVICE PEDESTAL
	EXISTING MAST ARM		
	PROPOSED LUMINAIRE		
	EXISTING LUMINAIRE		
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)		
	EXISTING LOOP DETECTOR (TYPE 1 OR 2)		

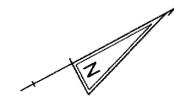
- GENERAL SIGNAL NOTES**
- ALL SIGNAL EQUIPMENT REMOVED FROM THIS PROJECT IS TO BE RETURNED TO CITY OF WILMINGTON OR DELDOT TRAFFIC-DOVER, DELAWARE AS DIRECTED.
  - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS ARE TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
  - PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAT 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
  - ALL GALVANIZED COPPER (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET, BOLTED AND COMPRESSION FITTINGS ARE NOT ACCEPTABLE.
  - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY ENTITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT IMMEDIATELY BEFORE CONSTRUCTION.



RECOMMENDED [Signature] DATE: 6/29/12      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      APPROVED TRAFFIC ENGINEER \_\_\_\_\_ DATE: \_\_\_\_\_      APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER \_\_\_\_\_ DATE: \_\_\_\_\_

<p><b>DELAWARE DEPARTMENT OF TRANSPORTATION</b></p>	ADDENDUM / REVISIONS		SCALE 0 20 40 60 FEET	<b>BAYARD SQUARE ENHANCEMENTS, CITY OF WILMINGTON</b>	CONTRACT T201020006	PERMIT NO.	<b>SIGNAL PLAN S. LINCOLN STREET @ SYCAMORE STREET</b>	SHEET NO. 29
					COUNTY NEW CASTLE	DESIGNED BY: MSK		TOTAL SHTS. 33
						CHECKED BY: BAM		

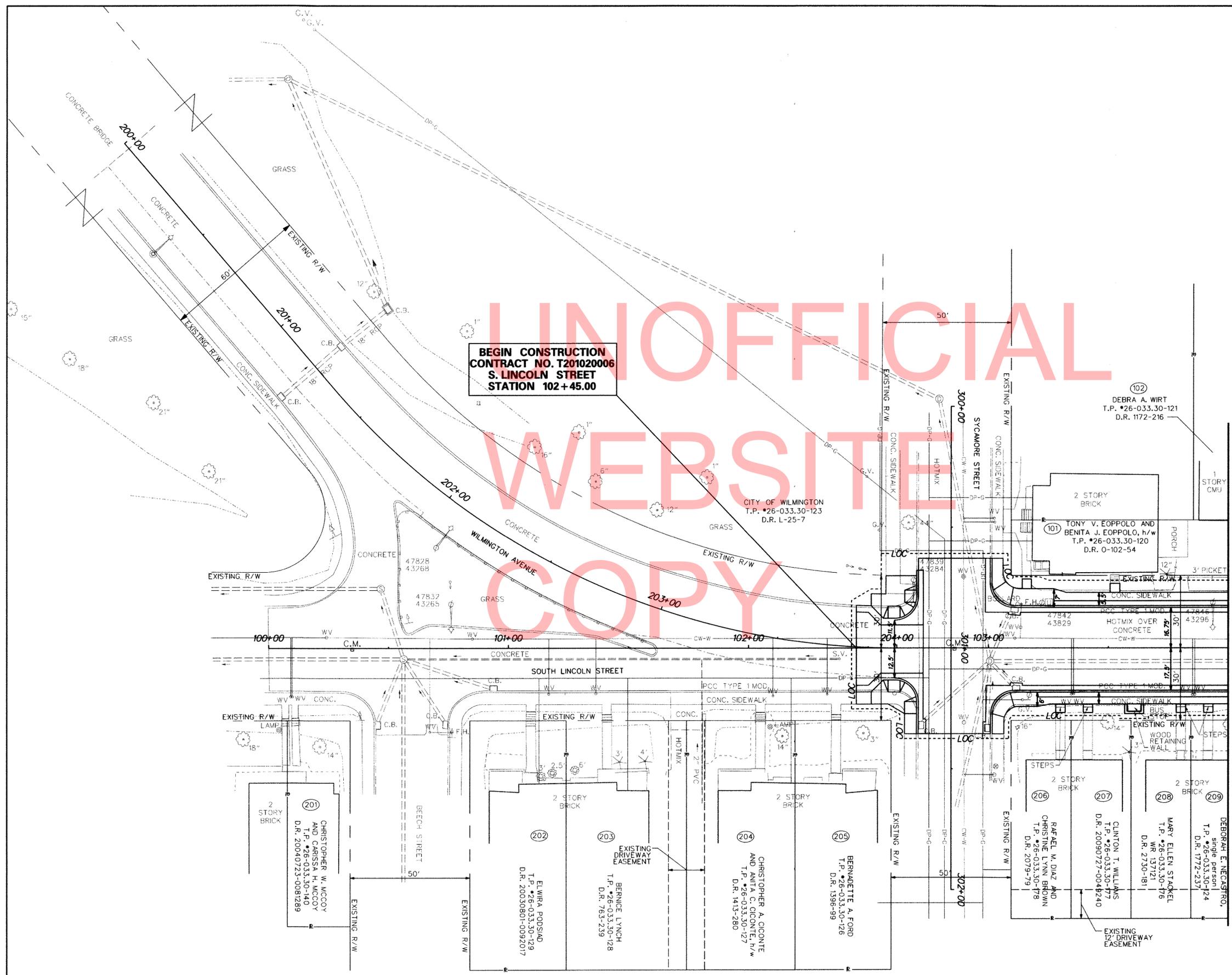
6/29/2012 9:53:00 AM C:\PROJETS\0853523 (Transportation Enhancement Program)\0853523 (Task 23, Bayard Square Improvements - Final Design)\CADD FILES\SG\_ref\_08.dgn



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**BEGIN CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 102 + 45.00**

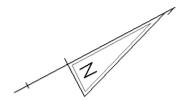
**MATCHLINE STA. 104 + 00**



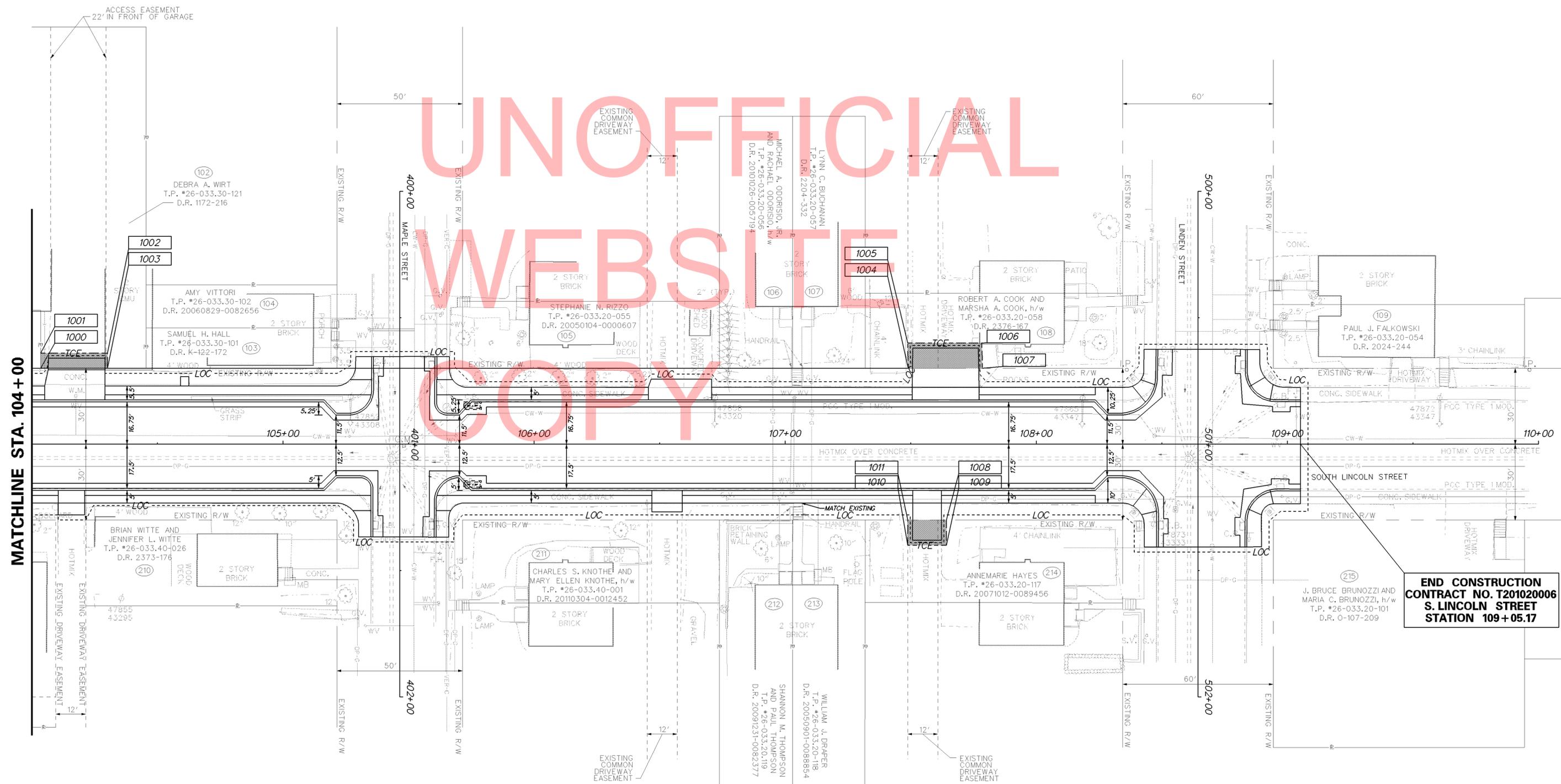
PREPARED BY <b>CENTURY ENGINEERING</b> <small>CONSULTING ENGINEERS, SURVEYORS                  4134 N. DUPONT HWY.                  DOVER, DELAWARE 19901                  (302) 734-9188</small>		
RECOMMENDED <i>[Signature]</i>	4-30-2012 DATE	
SPONSORED BY BAYARD SQUARE CIVIC ASSOCIATION <i>[Signature]</i>		
RECOMMENDED CITY OF WILMINGTON <i>[Signature]</i>		7/10/2012 DATE
RECOMMENDED - AS TO PROCESS - TEAM SUPPORT SQUAD MANAGER <i>[Signature]</i>		
TEAM SUPPORT ENGINEER <i>[Signature]</i>		9/21/2012 DATE
ASSISTANT DIRECTOR ENGINEERING SUPPORT <i>[Signature]</i>		9/24/2012 DATE

<p><b>DELAWARE DEPARTMENT OF TRANSPORTATION</b></p>	ADDENDUMS / REVISIONS	SCALE 0 20 40 60 FEET	<b>BAYARD SQUARE ENHANCEMENTS, CITY OF WILMINGTON</b>	CONTRACT T201020006	BRIDGE NO. -	SHEET NO. 30 TOTAL SHTS. 33	
				COUNTY NEW CASTLE	DESIGNED BY: AMW		<b>RIGHT OF WAY</b>
				CHECKED BY: WFC			

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MATCHLINE STA. 104 + 00

END CONSTRUCTION  
CONTRACT NO. T201020006  
S. LINCOLN STREET  
STATION 109 + 05.17

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ADDENDUMS / REVISIONS	



BAYARD SQUARE ENHANCEMENTS,  
CITY OF WILMINGTON

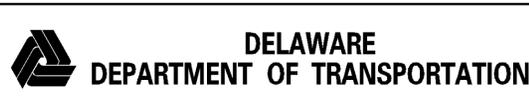
CONTRACT	BRIDGE NO.	-
T201020006	DESIGNED BY:	AMW
COUNTY	CHECKED BY:	WFC
NEW CASTLE		

RIGHT OF WAY	SHEET NO.	31
	TOTAL SHTS.	33

ASSESSMENT NUMBER	OWNERSHIP OF RECORD				TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)				
26-033.30-121	(102) DEBRA A. WIRT				TCE	1172-216	0.179				
<b>ALIGNMENT NUMBER &amp; DESCRIPTION:</b> 1000 - SOUTH LINCOLN STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
1000	1000	104+03.48	-30.00	634911.3342	611419.8620	N 62°12'02.66" W	6.0000				
1001	1000	104+03.48	-36.00	634914.1324	611414.5545	N 27°47'57.34" E	26.6187				
1002	1000	104+30.10	-36.00	634937.6790	611426.9688	S 62°16'54.85" E	6.0000				
1003	1000	104+30.11	-30.00	634934.8882	611432.2802	S 27°47'57.34" W	26.6272				
1000	1000	104+03.48	-30.00	634911.3342	611419.8620						
FIGURE 30102 AREA = 159.7377 SQ. FT. (0.0037 ACRES)											
ASSESSMENT NUMBER	OWNERSHIP OF RECORD				TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)				
26-033.20-058	(108) ROBERT A. COOK AND MARSHA A. COOK, H/W				TCE	2376-167	0.060				
<b>ALIGNMENT NUMBER &amp; DESCRIPTION:</b> 1000 - SOUTH LINCOLN STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
1004	1000	107+49.01	-30.00	635216.9803	611581.0059	N 63°38'01.79" W	10.0031				
1005	1000	107+48.76	-40.00	635221.4227	611572.0433	N 27°47'57.34" E	29.5548				
1006	1000	107+78.31	-40.00	635247.5665	611585.8269	S 63°28'45.13" E	10.0025				
1007	1000	107+78.53	-30.00	635243.1002	611594.7769	S 27°47'57.34" W	29.5278				
1004	1000	107+49.01	-30.00	635216.9803	611581.0059						
FIGURE 30108 AREA = 295.4127 SQ. FT. (0.0068 ACRES)											
ASSESSMENT NUMBER	OWNERSHIP OF RECORD				TYPE OF ACQUISITION	TITLE SOURCE	PARCEL AREA (ACRES)				
26-033.20-117	(214) ANNEMARIE HAYES				TCE	20071012-0089456	0.080				
<b>ALIGNMENT NUMBER &amp; DESCRIPTION:</b> 1000 - SOUTH LINCOLN STREET											
PT. NO.	ALIGN. NO.	STATION	OFFSET *	NORTH	EAST	BEARING	DISTANCE	CHORD BEARING	CHORD LENGTH	ARC LENGTH	RADIUS **
1011	1000	107+48.77	30.00	635188.7907	611633.9719	N 27°47'57.34" E	15.4687				
1008	1000	107+64.24	30.00	635202.4741	611641.1861	S 62°12'02.66" E	10.0000				
1009	1000	107+64.24	40.00	635197.8104	611650.0320	S 27°47'57.34" W	15.4687				
1010	1000	107+48.77	40.00	635184.1269	611642.8178	N 62°12'02.66" W	10.0000				
1011	1000	107+48.77	30.00	635188.7907	611633.9719						
FIGURE 30214 AREA = 154.6870 SQ. FT. (0.0036 ACRES)											

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LEGEND	
FEE	AREA OF ACQUISITION
RW	AREA OCCUPIED BY EXISTING RW
PE	PERMANENT EASEMENT
TCE	TEMPORARY CONSTRUCTION EASEMENT
*	OFFSET IS LEFT OF BASELINE
**	CURVE TURNS TO THE LEFT



ADDENDUMS / REVISIONS

**BAYARD SQUARE ENHANCEMENTS,  
CITY OF WILMINGTON**

CONTRACT	BRIDGE NO.	-
T201020006	DESIGNED BY:	WRC
COUNTY	CHECKED BY:	WFC
NEW CASTLE		

RIGHT-OF-WAY DATA SHEET	
SHEET NO.	32
TOTAL SHTS.	33

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