

THE STATE OF DELAWARE



DEPARTMENT OF TRANSPORTATION
OFFICE OF TRANSPORTATION SOLUTIONS
PLANS AND SPECIFICATIONS FOR

CONTRACT NUMBER - T201206105

CANAL MAINTENANCE

FEDERAL AID NUMBER – ESTP-2014(32)

PAGE 1 OF 33

CONTRACT TITLE- PAVEMENT & REHABILITATION – NORTH V, 2012

PLANS PREPARED BY: George LeCates
DESIGN TECHNICIAN

DATE RECOMMENDED: 6-13-14

DATE RECOMMENDED: 6-13-14

Paul H. Pelin
GROUP ENGINEER, CONSTRUCTION – NORTH II

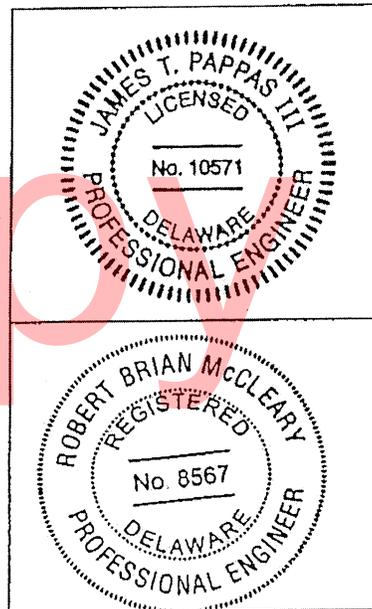
James A. Ferry
ASSIST. DIRECTOR, CONSTRUCTION - NORTH

DATE RECOMMENDED: 6.12.14

James T. Pappas III
ASSIST. DIRECTOR, DESIGN

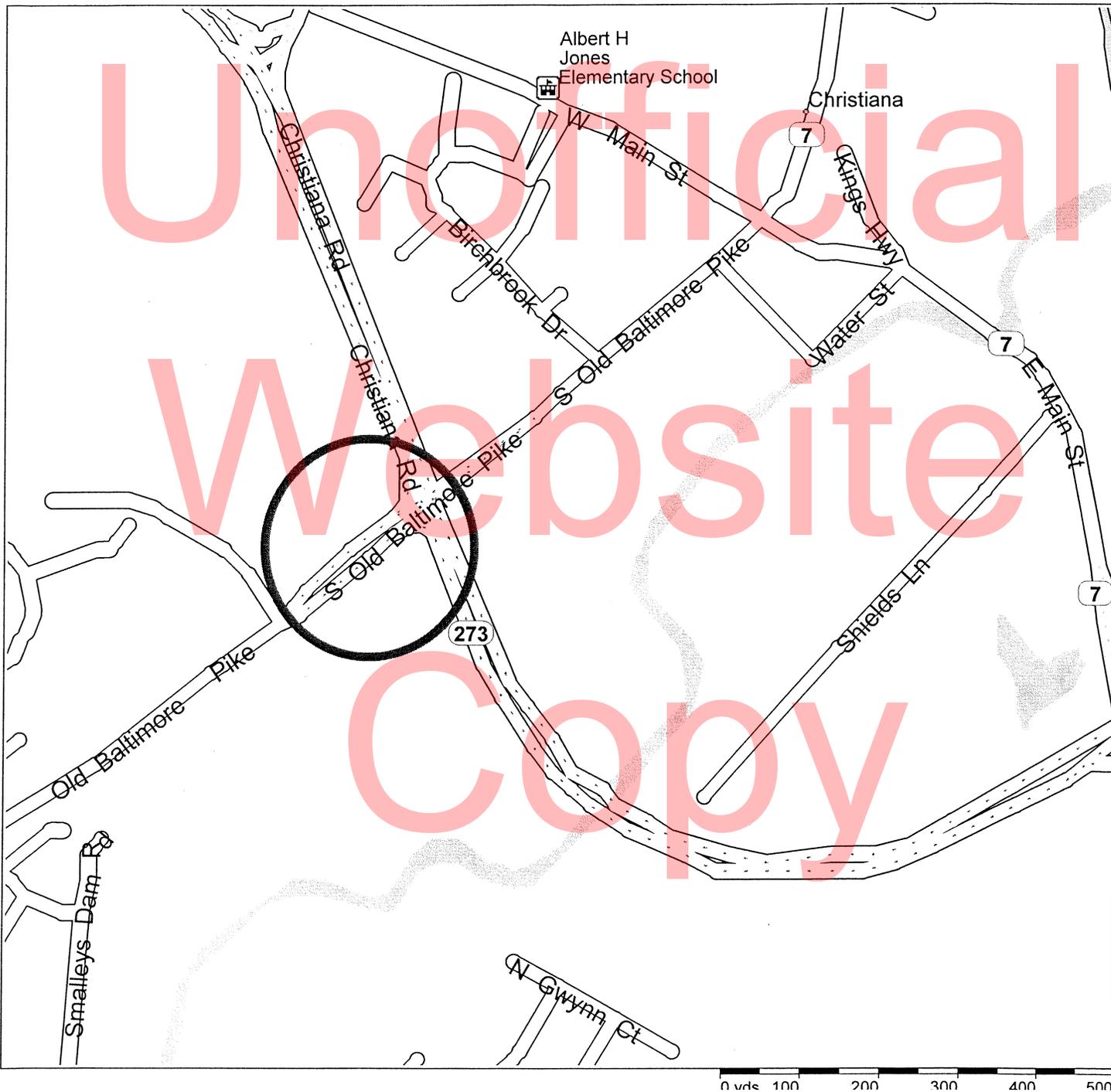
DATE RECOMMENDED: 6/12/14

Robert B. McCleary
CHIEF ENGINEER



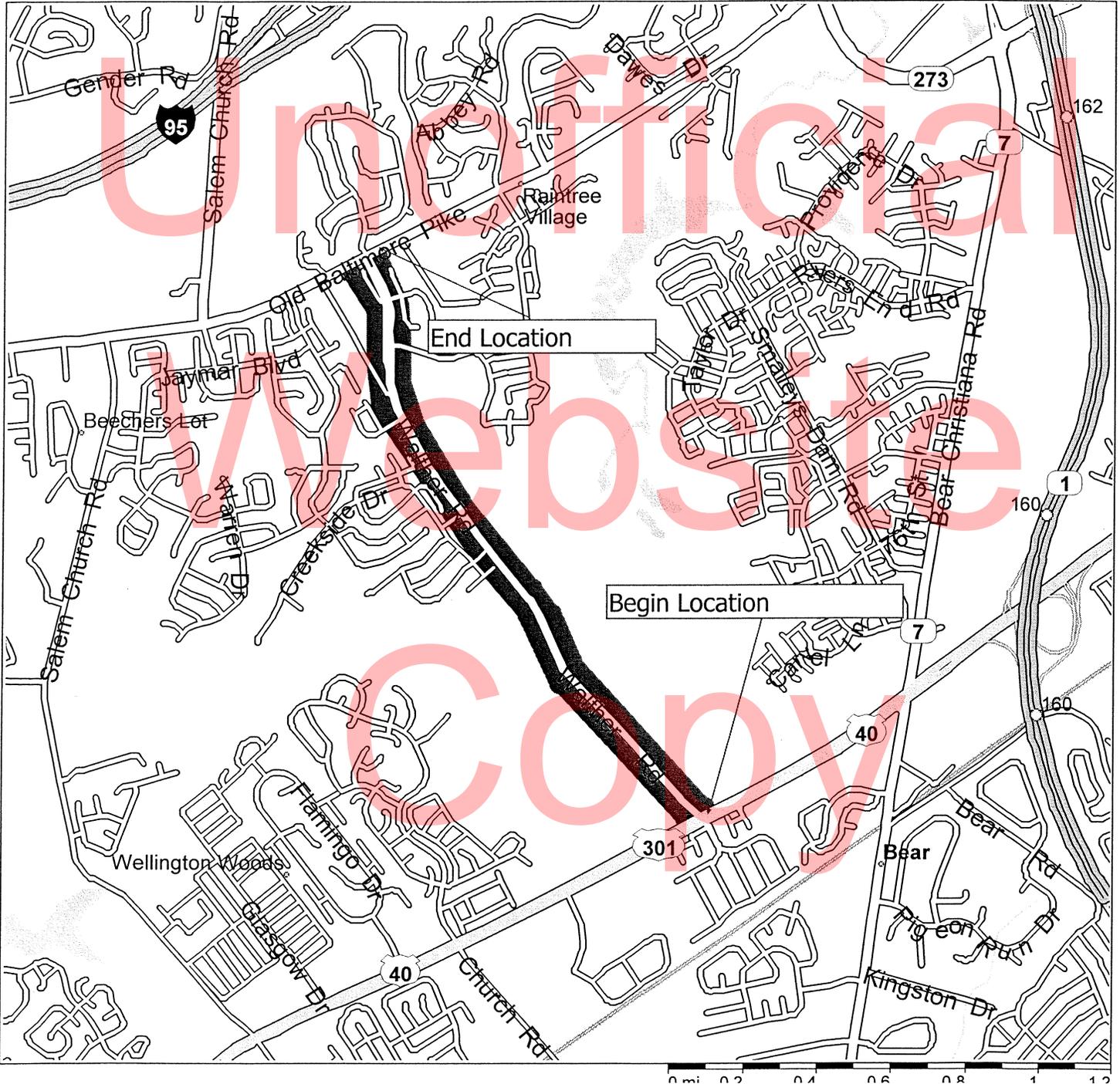
LOCATION MAP

Loc. #1 - Old Baltimore Pike from 500' South of Route 273 to Route 273



LOCATION MAP

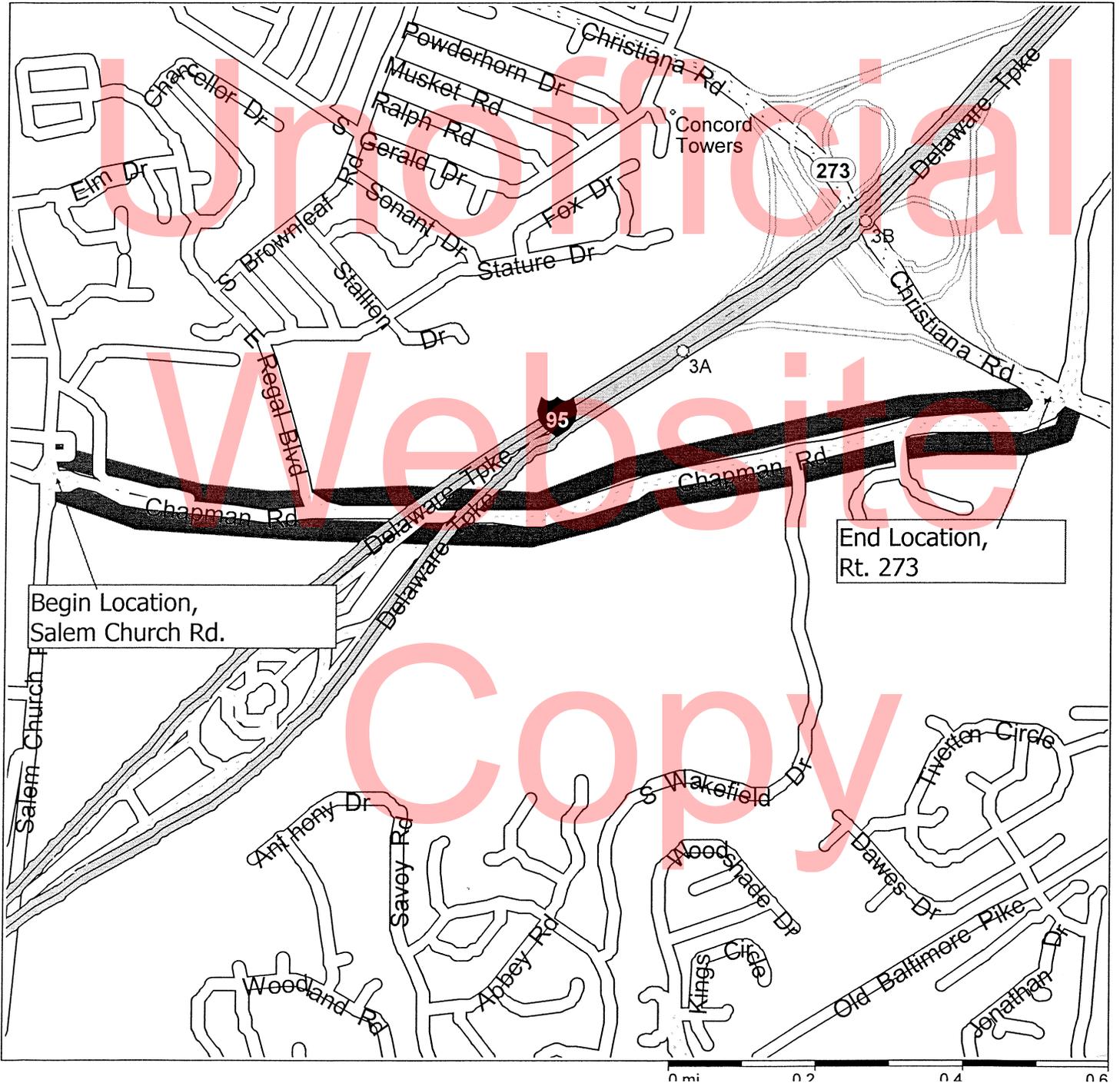
Loc. #2 - Walther Road from Route 40 to Old Baltimore Pike



CHAPMAN ROAD, RED LION ROAD, NEWTOWN ROAD, WALTHER ROAD

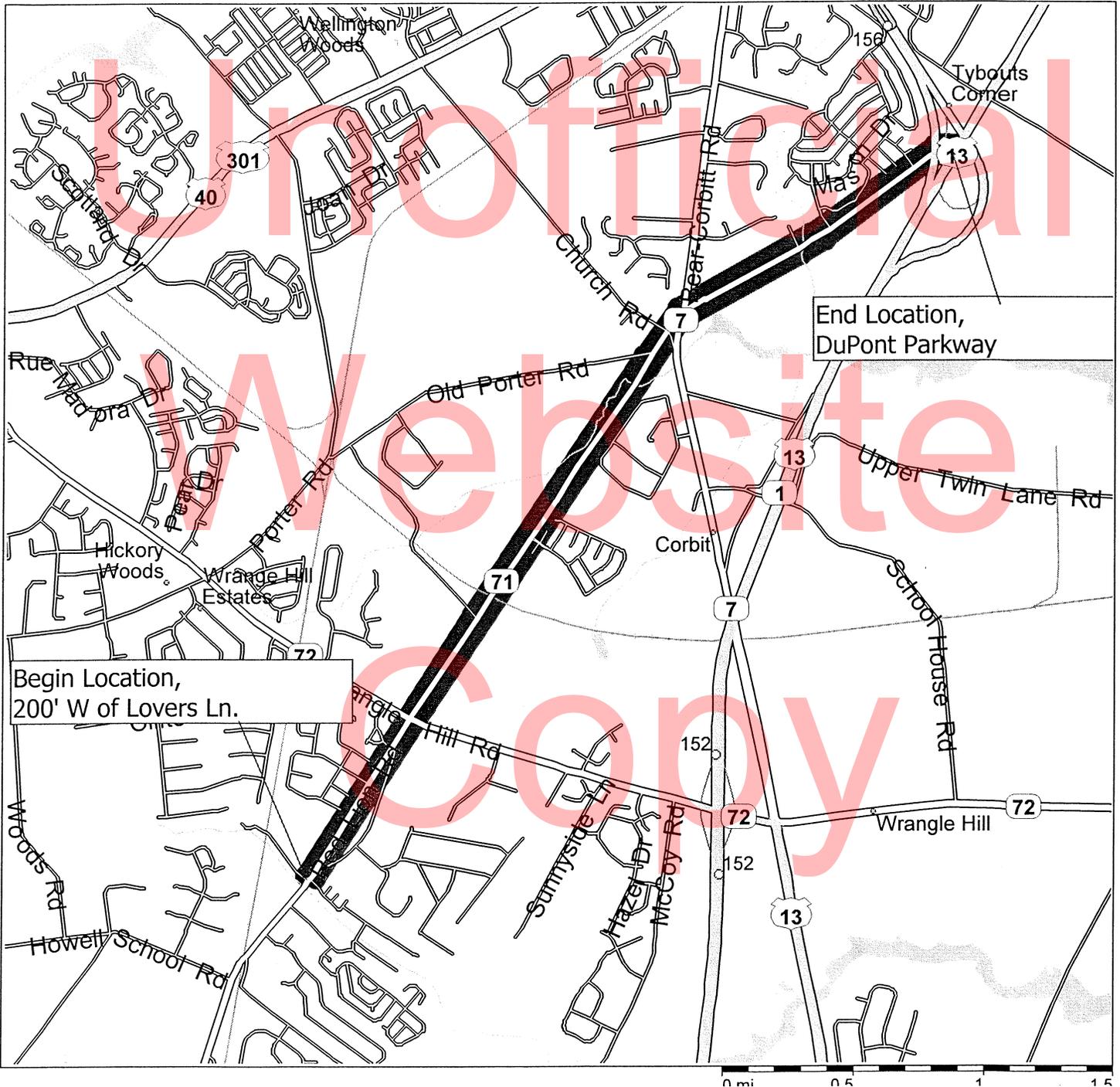
LOCATION MAP

Loc. #3 - Chapman Rd. from Salem Church Rd. to Rt. 273



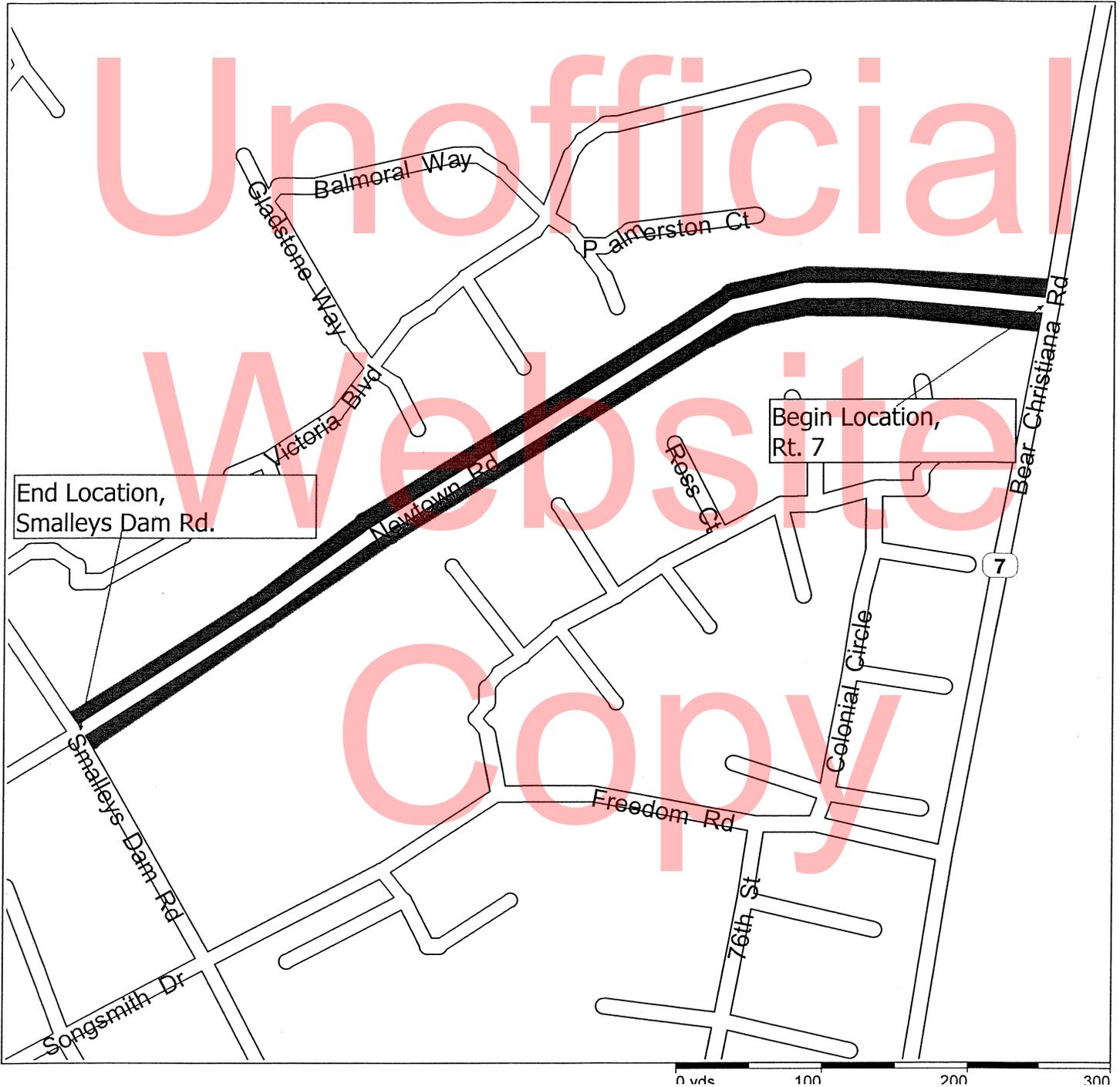
LOCATION MAP

Loc. #4 - Red Lion Rd. from 200' W of Lovers Ln. to DuPont Parkway



LOCATION MAP

Loc. #5 - Newtown Rd. from Rt. 7 to Smalley's Dam Rd.



GENERAL CONTRACT PLAN NOTES

REVISED 9/2013

GENERAL

1. CONSTRUCTION OF THIS CONTRACT SHALL CONFORM TO DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED AUGUST 2001 AS AMENDED BY THE SUPPLEMENTAL SPECIFICATIONS, THE SPECIAL PROVISIONS, THE MOST RECENT STANDARD CONSTRUCTION DETAILS, DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THESE PLANS.
2. THE PURPOSE OF THIS CONTRACT IS TO MAKE GENERAL IMPROVEMENTS TO ROADS AND/OR STREETS WITHIN THE DISTRICT HEREIN SPECIFIED. DEPENDING ON FUND AVAILABILITY, THE DEPARTMENT RESERVES THE RIGHT TO ADD OR DELETE LOCATIONS AND/OR QUANTITIES FOR THIS CONTRACT. SUCH LOCATION OR QUANTITY ADDITIONS OR DELETIONS SHALL NOT BE CAUSE FOR AN INCREASE OR DECREASE IN ANY CONTRACT UNIT BID PRICES. NO ITEM PRICES ARE TO BE RENEGOTIATED DUE TO EITHER AN INCREASE OR DECREASE IN QUANTITY USAGE RESULTING FROM SAID ADDITIONS OR DELETIONS.
3. THE CONTRACTOR SHALL PROVIDE ALL RESIDENTS AND BUSINESSES WHO LIVE ADJACENT TO THE WORK ZONE A MINIMUM 48 HR. PRIOR WRITTEN NOTICE FOR THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES, CONTRACTOR NAME & ADDRESS, AND DELDOT CONTACT NUMBERS. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND/OR PROPERTY OWNER IN ADVANCE OF THE CLOSURE.
4. THE CONTRACTOR SHALL SUBMIT THE REQUIRED COPIES OF A DETAILED PROGRESS SCHEDULE (PSC) AS OUTLINED IN STANDARD SPECIFICATION 108.04 PRIOR TO OR AT THE PRECONSTRUCTION MEETING. DETAILS SHALL INCLUDE A DESCRIPTION OF EACH WORK ACTIVITY, THE PLANNED DAYS OF WORK, MULTIPLE CREWS OR SHIFTS, AND SCHEDULED WORKING HOURS. DURING THE CONTRACT, THE CONTRACTOR SHALL SUBMIT TWO-WEEK (OR AS REQUIRED) "LOOK AHEAD" SCHEDULES TO THE AREA ENGINEER/ MANAGER NO LATER THAN NOON OF EACH THURSDAY. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

GENERAL CONTRACT PLAN NOTES

MAINTENANCE OF TRAFFIC

5. THE CONTRACTOR SHALL BE REQUIRED TO SHOW PROOF THAT HE HAS SUFFICIENT APPROVED STRIPING MATERIALS ON HAND TO ENSURE STRIPING IS COMPLETED PRIOR TO FULL OPENING ROADWAY TO TRAFFIC.

6. AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY STRIPING SHALL BE UTILIZED AT LOCATIONS THAT REQUIRE PERMANENT STRIPING. TEMPORARY PAVEMENT STRIPING MUST MATCH PERMANENT PAVEMENT STRIPING IN ALL REGARDS. TEMPORARY PAVEMENT MARKINGS SHALL BE PAID AT THE APPLICABLE CONTRACT UNIT PRICE. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE TEMPORARY MARKINGS IN GOOD CONDITION SUCH THAT THE PAVEMENT IS PROPERLY DELINEATED AT ALL TIMES. ANY REFRESHING OF THE TEMPORARY MARKINGS WILL BE AT THE CONTRACTOR'S EXPENSE.

7. ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.

8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO AND ALONG PEDESTRIAN FACILITIES AT ALL TIMES DURING CONSTRUCTION. EARTH, STONE AND GRAVEL SURFACES ARE NOT ACCEPTABLE FOR PROVIDING PEDESTRIAN ACCESS. WHERE PEDESTRIAN ROUTES ARE CLOSED OR BLOCKED, ALTERNATE PEDESTRIAN ACCESS SHALL BE PROVIDED USING TA-28 AND/OR TA-29 OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES OR TO THE SATISFACTION OF THE ENGINEER. PEDESTRIAN DETOUR ROUTES OR ALTERNATE PEDESTRIAN FACILITIES SHALL CONSIST OF ADA COMPLIANT FACILITIES TO THE LEVEL OF WHICH EXISTS ON THE PEDESTRIAN ROUTE. THE COST FOR ANY PROVISIONS SHALL BE INCIDENTAL TO ITEM 763XXX - MAINTENANCE OF TRAFFIC.

PEDESTRIAN MAINTENANCE OF TRAFFIC: THIS WORK SHALL CONSIST OF PROVIDING AND MAINTAINING AN ACCESSIBLE PEDESTRIAN ROUTE THROUGHOUT THE PROJECT'S LIMITS IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130.

THE CONTRACTOR SHALL BE REQUIRED TO REVIEW EACH CURB RAMP LOCATION AND SUBMIT THE APPROPRIATE MAINTENANCE OF TRAFFIC DETAIL AND DEVICES TO THE ENGINEER FOR EACH LOCATION AT LEAST 2 WEEKS BEFORE CONSTRUCTION FOR

GENERAL CONTRACT PLAN NOTES

REVIEW, COMMENT, AND APPROVAL BY THE DISTRICT TRAFFIC SAFETY OFFICER

THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:

- ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPLICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.
- MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES AT ALL TIMES.
- PROVIDE PEDESTRIAN ACCESS THROUGH OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE CURRENT CONDITIONS.
- WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.
- SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7 FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.

A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTIRE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS OR VERTICAL ELEVATION CHANGES GREATER THAN ¼ IN. IN GRADE OR TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIER TO WHEELCHAIR USE.

9. AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR SHALL BE REQUIRED FOR ALL LOCATIONS SPECIFIED IN THE CONTRACT PLANS. A COPY OF THE CERTIFICATION CARD FOR THE ATSSA TRAFFIC CONTROL SUPERVISOR SHALL BE SUBMITTED AT THE PRECONSTRUCTION MEETING. THE PAPER CERTIFICATE IS NOT ACCEPTABLE.

PAVEMENT & REHABILITATION, NORTH V, 2012

GENERAL CONTRACT PLAN NOTES

PAVEMENT MARKINGS

10. AT THE PRECONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT DETAILED DRAWINGS (INCLUDING BUT NOT LIMITED TO EXISTING STRIPING LENGTHS, LANE AND SHOULDER WIDTHS, TURN LANE LENGTHS, LOCATIONS OF STOP BARS, TURN ARROWS, CROSSWALKS AND RAILROAD CROSSINGS) THAT DEPICT THE EXISTING PAVEMENT MARKINGS FOR EACH PROJECT LOCATION. THESE DRAWINGS WILL BE REVIEWED BY THE DEPARTMENT'S TRAFFIC SECTION TO DETERMINE IF ANY CHANGES TO THE FINAL PAVEMENT MARKINGS ARE REQUIRED. FINAL PAVEMENT MARKINGS SHALL CONFORM TO ALL EXISTING PATTERNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, WHITE EDGE LINES SHALL WRAP AROUND THE RADIUS OF ALL SIDE STREETS AND MAJOR COMMERCIAL ENTRANCES TO A TANGENT POINT. YELLOW CENTERLINES SHALL BE CONTINUOUS AROUND MEDIAN ISLANDS. ALL DOUBLE YELLOW CENTERLINES SHALL BE PLACED IN A 5-6-5 CONFIGURATION (2 EACH, 5" YELLOW STRIPES WITH A 6" GAP BETWEEN). CONTRACT.

11. ANY ERRONEOUS MARKING WILL NOT BE PAID FOR AND SHALL BE CORRECTED IMMEDIATELY AT THE CONTRACTORS EXPENSE. ERRONEOUS MARKINGS OR SHADOWS THAT EXCEED ONE (1) INCH IN WIDTH SHALL BE REMOVED BY EITHER SAND OR WATER BLASTING AS DIRECTED BY THE ENGINEER. NO OTHER REMOVAL METHODS WILL BE ALLOWED. A FLAT BLACK PAINT OR DRIVEWAY SEALER SHALL BE APPLIED IN THE AREA OF THE REMOVED MARKING TO MASK THE REPAIR. ANY DAMAGE TO THE PAVEMENT CAUSED BY REMOVAL OF ERRONEOUS MARKINGS SHALL BE REPAIRED / REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTORS EXPENSE.

12. STOP LINES "BARS" SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.16 OF THE 2011 DELAWARE MUTCD.

13. ALL CROSSWALKS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.18 OF THE 2011 DELAWARE MUTCD.

TRAFFIC SIGNAL NOTES

14. TRAFFIC DETECTION LOOPS SHALL BE PLACED IN THE FINAL WEARING SURFACE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT & REHABILITATION, NORTH V, 2012

GENERAL CONTRACT PLAN NOTES

15. TRAFFIC DETECTION LOOPS THAT ARE CURRENTLY 5'x7' SHALL BE REPLACED WITH LOOPS THAT ARE 6'x6' AT THE SAME LOCATION. STOP BAR DETECTION LOOPS SHALL BE THE SAME SIZE AS CURRENTLY EXISTING, AND SHOULD BE PLACED STARTING TWO FEET BEHIND THE STOP BAR, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF SEPARATE SIGNAL PLANS HAVE BEEN DEVELOPED, THE PLANS SUPERSEDE THIS NOTE.

PROJECT

16. UNLESS SPECIFIED, OVERLAY WIDTHS SHALL MATCH EXISTING WIDTHS.

17. THE COST OF CLIPPING BACK THE EDGES OF THE ROADWAY, SHOULDERS, AND THE FIRST 3' OF UNPAVED DRIVEWAY ENTRANCES, PICKING UP AND DISPOSING OF WASTE AND EXCESS MATERIAL, AND CLEANING THE EXISTING PAVEMENT PRIOR TO OVERLAY SHALL BE INCIDENTAL TO SECTION 401(). PREPARING UNPAVED DRIVEWAY ENTRANCES BEYOND THE FIRST 3' SHALL BE PAID UNDER THE APPROPRIATE CONTRACT ITEMS: 202000 AND/OR 30200X.

18. BUTT JOINTS SHALL BE PLACED AT ALL INTERSECTING HOT MIX ROADS AND ANY OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. BUTT JOINTS AT DRAINAGE STRUCTURES NOT TO BE RESURFACED SHALL BE AS PER THE INCLUDED PLAN DETAIL OR LOCATION NOTES. BUTT JOINTS CUT PRIOR TO THE DAY OF THE PAVEMENT OVERLAY SHALL BE RAMPED WITH HOT MIX TRM. IN MILLING AREAS, THERE WILL BE NO SEPARATE PAYMENT FOR BUTT JOINTS. AFTER PAVEMENT MILLING, ALL TRANSVERSE VERTICAL DIFFERENCES, RAISED EDGES OF MANHOLES, CATCH BASINS, WATER VALVE BOXES, ETC...SHALL BE RAMPED WITH HOT-MIX TRM AT A 20:1 OR FLATTER SLOPE PRIOR TO OPENING THE ROADWAY TO TRAFFIC. PAVEMENT MILLINGS WILL NOT BE ALLOWED FOR USE AS RAMPING MATERIAL.

19. TAPER MILLING IS INTENDED FOR MILLING IN THE AREA OF FIXED STRUCTURES (I.E. CURBS GUARDRAIL, ETC.) THE MILLED DEPTH AT THE STRUCTURE SHALL BE THE DEPTH OF THE PROPOSED OVERLAY AND 0" AT A DISTANCE OF +/- 6½' FROM THE STRUCTURE. THE COMPUTED DEPTH FOR PAYMENT PURPOSES IS THE AVERAGE OF THE TWO DEPTHS.

PAVEMENT & REHABILITATION, NORTH V, 2012

GENERAL CONTRACT PLAN NOTES

20. ALL MILLED MATERIAL SHALL REMAIN THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE STATED.

21. EXCAVATION UP TO 1' IN FRONT OF CURB WILL BE INCIDENTAL TO ITEM 701XXX. HOT MIX PLACED IN FRONT OF CURB WILL BE PAID UNDER ITEM 401XXX. USE OF CONCRETE AS FILL IN FRONT OF CURB WILL NO LONGER BE ACCEPTED UNLESS DIRECTED BY THE ENGINEER. AT EACH LOCATION, BACKFILLING SHALL BE PERFORMED IMMEDIATELY UPON REMOVAL OF FORMS FOR CURB OR SIDEWALK. TOPSOIL, SEEDING, SEALING OF VALLEY GUTTERS, REMOVING OF FORMS, AND PARGING OF CATCH BASINS, SHALL BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS OF COMPLETION OF THE ITEM OF WORK. ITEM 732XXX, TOPSOIL, SHALL BE USED AS BACKFILL MATERIAL BEHIND CURB AND AROUND SIDEWALK OR/AS DIRECTED BY THE ENGINEER. TOPSOIL SHALL BE SCREENED/SIFTED SO AS NO MATERIAL GREATER THAN .5" IS PRESENT IN MATERIAL USED FOR BACKFILL. EXCAVATED MATERIAL, NOT NEEDED ON THE PROJECT, SHALL BE REMOVED FROM THE CONTRACT AT THE CONTRACTORS EXPENSE. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

22. ALL ADJUSTMENTS SHALL BE MADE NO MORE THAN SEVEN (7) CALENDAR DAYS PRIOR TO THE PLACEMENT OF THE FINAL COURSE OF HOT MIX UNLESS OTHERWISE APPROVED BY THE ENGINEER. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

23. ALL PAVING, INCLUDING TURN LANES, SHOULDERS AND INTERSECTIONS, IS TO BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS FROM THE TIME THE AREA WAS MILLED. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

24. THE CONTRACTOR SHALL TAKE CARE IN REMOVING PAVEMENT AROUND UTILITIES, BUTT JOINTS, CURBS, ETC. SO THAT EXISTING PAVEMENT BEYOND THE SPECIFIED DEPTHS IS NOT DAMAGED. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS MAY RESULT IN PLACING LEVELING COURSES AT THE CONTRACTOR'S EXPENSE. THE REMOVAL AND CLEAN UP OF THE HOT MIX RESIDUE WEDGE REMAINING AFTER MILLING OPERATIONS SHALL BE INCIDENTAL TO THE MILLING ITEM. THE REMOVAL OF EXISTING RAISED PAVEMENT MARKERS (RPM'S) SHALL BE INCIDENTAL TO THE APPLICABLE MILLING AND/OR RECLAMATION ITEMS.

25. ALL PORTLAND CEMENT CONCRETE MUST BE RECEIVED FROM AN APPROVED

GENERAL CONTRACT PLAN NOTES

PRODUCTION PLANT UNLESS OTHERWISE APPROVED BY THE ENGINEER. ONLY CALIBRATED VOLUMETRIC CONCRETE TRUCKS WILL BE PERMITTED TO PROVIDE ON-SITE MIXTURES. THIS PERTAINS TO BOTH WORK OCCURRING DURING NORMAL WORKING HOURS AND NIGHTTIME RESTRICTED WORK.

26. DRAINAGE INLETS, WHICH ARE 4' GREATER IN DEPTH AND ARE SLATED FOR REPAIR WILL HAVE STEPS INSTALLED IN ACCORDANCE WITH SECTION 708.07 OF THE STANDARD SPECIFICATIONS. PAYMENT SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR ITEM 710001.

27. THE PORTLAND CEMENT CONCRETE USED WITHIN ITEM 710002 - ADJUST AND REPAIR EXISTING MAN HOLES AND ITEM 750000 - ADJUST WATER VALVE BOX SHALL CONFORM TO THE REQUIREMENTS OF CLASS A CONCRETE AS STATED WITHIN SECTION 503.02 OF THE DEPARTMENT'S STANDARD SPECIFICATIONS.

28. WHEN JACKHAMMERING OR THE USE OF A BREAKER IS NECESSARY ON CONTRACTS THAT OCCUR DURING THE NIGHTTIME HOURS THE JACKHAMMERING AND BREAKING MUST CEASE AT 12:00AM.

29. CURB RAMP AND/OR SIDEWALK CONSTRUCTION (NOT INCLUDING MEDIAN ISLANDS) SHALL BE COMPLETED DURING A SINGLE OPERATION. THE RAMP/SIDEWALK SHALL BE REMOVED AND PLACED BACK THE SAME NIGHT/DAY AND OPEN TO PEDESTRIAN TRAFFIC THE FOLLOWING DAY. THE USE OF A MONOLITHIC POUR SHALL BE PERMITTED WITH A BOND BREAKER TO BE PLACED AT THE BACK OF THE CURB AND EDGE OF SIDEWALK. IF THE CURB RAMP/SIDEWALK CANNOT BE COMPLETED IN A SINGLE OPERATION THE EXCAVATED AREA MUST BE BACKFILLED TO GRADE WITH COMPACTED MILLING MATERIAL ONLY. THIS MATERIAL WILL BE INCIDENTAL TO THE CURB RAMP ITEM. THE DRESSING UP OF THE AREA BEING PAID AS A CURB RAMP WITH TOPSOIL AND SEED WILL BE INCIDENTAL TO THE CURB RAMP ITEMS.

30. ANY MILLING MATERIAL REQUIRED TO MAKE AREAS ADA COMPLIANT IF NOT COMPLETED IN THE SAME SHIFT AT CURB, SIDEWALK, CURB RAMPS, ISLANDS, ETC. WILL BE INCIDENTAL TO THE PAY ITEM ASSOCIATED WITH THE WORK. MILLING MATERIAL IS THE ONLY APPROVED MATERIAL FOR THIS PRACTICE.

31. WHEN CONTRACT INCLUDES ITEMS 503001 AND 503002 PATCHES WILL BE PRESUMED TO BE STANDARD PATCH SIZES AND LOCATIONS WILL BE DETERMINED AT THE CONSTRUCTION PRE-WALK. THE USE OF STEEL PLATES WILL BE PROHIBITED. ROADWAY MUST BE RETURNED TO UNRESTRICTED USE AT THE END OF EACH WORK DAY. ALL PCC PAVEMENT WILL BE PRESUMED TO BE 9".

DELAWARE DEPARTMENT OF TRANSPORTATION

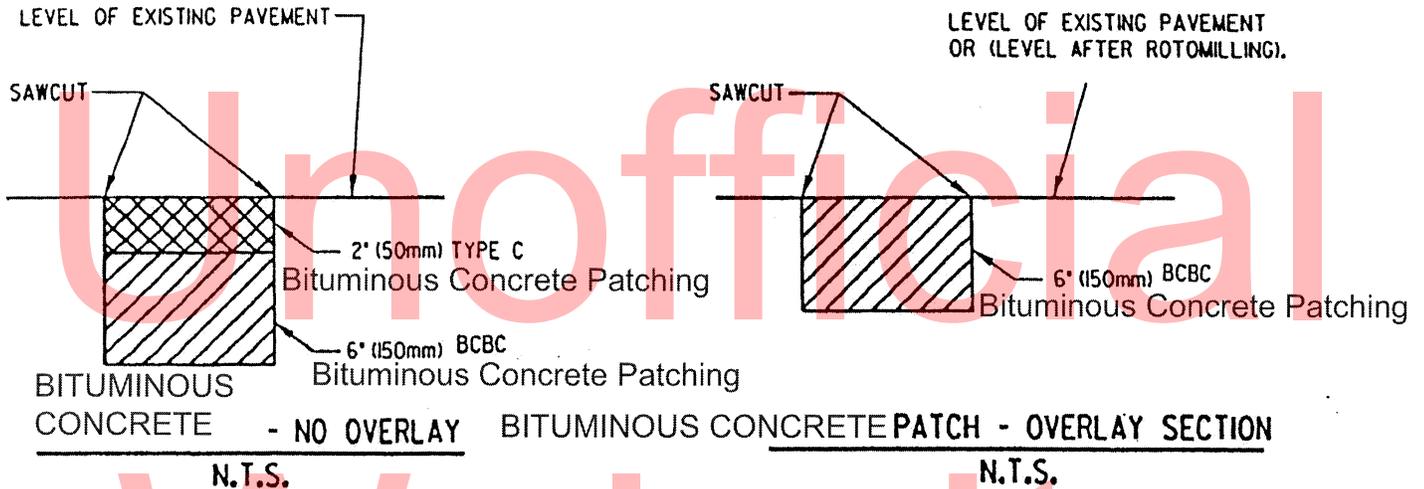
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CANAL

CHAPMAN ROAD, RED LION ROAD, NEWTOWN ROAD, WALTHER R

DETAILS SHEET



•UNDERCUT EXCAVATION PATCHING TO BE UTILIZED BELOW 8" (200mm) WHEN DIRECTED BY THE ENGINEER.

•UNDERCUT EXCAVATION PATCHING TO BE UTILIZED BELOW 6" (150mm) WHEN DIRECTED BY THE ENGINEER.

NOTE: PREFERRED EXCAVATION METHOD TO BE BY ROTOMILLING. SAWCUTTING EDGES SHALL BE AS DIRECTED BY ENGINEER AND DEPENDENT ON THE CONDITION LEFT BY THE ROTOMILLING OPERATION. BACKFILL FOR UNDERCUT EXCAVATION SHALL BE SUPERPAVE BCBC PATCH (SEPERATE LIFT).

MINIMUM WIDTH OF PATCHING 6' (1.8M).

THIS DETAIL SHALL BE USED FOR ALL HOT MIX PATCHES UNLESS OTHERWISE SHOWN ON THE PLANS.

ALL HOT MIX TO BE PLACED USING A PAVER.

WHEN PCC PATCHING IS REQUIRED ON A COMPOSITE PAVEMENT(HOT-MIX OVER CONCRETE), THE CONTRACTOR MAY ELECT TO ELIMINATE ANY GRADE DIFFERENCE BETWEEN THE TOP OF THE CONCRETE PATCH AND THE TOP OF THE EXISTING HOT-MIX PAVEMENT BY EITHER PLACING HOT-MIX TRM MATERIAL(PAYMENT UNDER ITEM 402000) OR PLACING CONCRETE FLUSH WITH EXISTING HOT-MIX PAVEMENT(ADDITIONAL CONCRETE PAID AS ITEM 503503). IF CONCRETE IS PLACED FLUSH WITH EXISTING HOT-MIX, THE ROTOMILLING OF THE CONCRETE WILL BE PAID FOR AS ROTOMILLING HOT-MIX.

P.C.C. PATCHING NOTES

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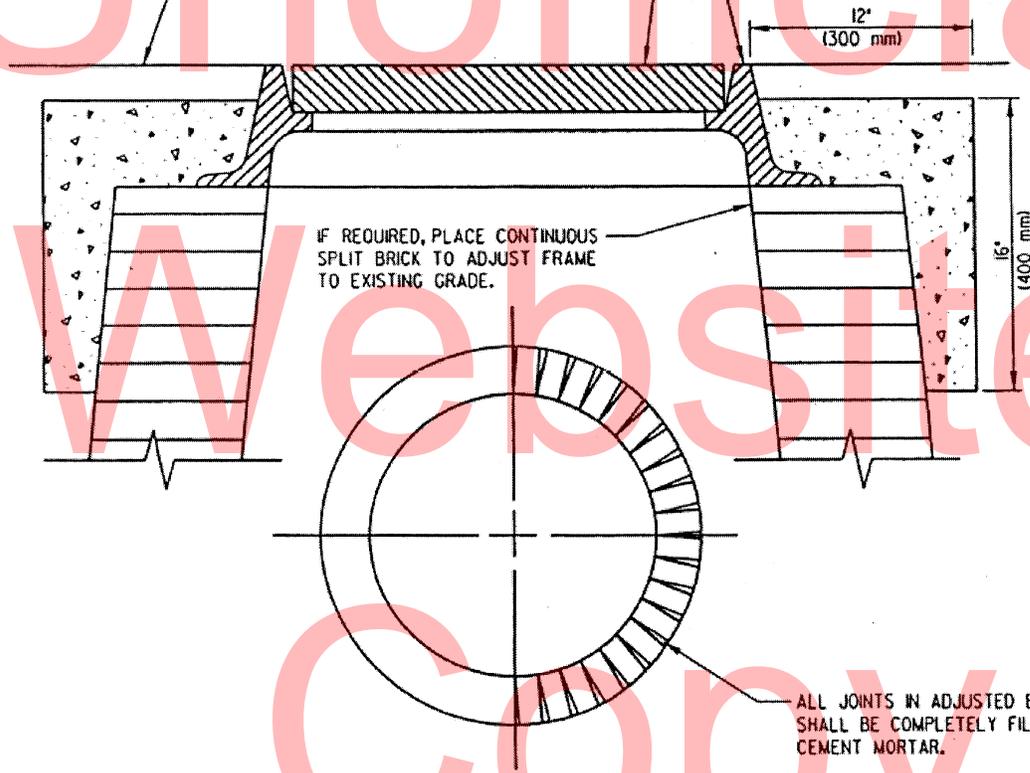
CANAL

CHAPMAN ROAD, RED LION ROAD, NEWTOWN ROAD, WALTHER R

DETAILS SHEET

REMOVE AND DISPOSE OF EXISTING FRAME AND COVER AS DIRECTED BY OWNER IF NOT UTILIZED IN THE ADJUSTMENT. PLACE NEW OR EXISTING FRAME AND COVER (AS REQUIRED) TO CORRECT ELEVATION AND RECOLLAR. NEW FRAME AND COVER (IF REQUIRED) TO BE PROVIDED BY OWNER.

REMOVE HOT MIX AND CONCRETE COLLAR FROM AROUND MANHOLE.

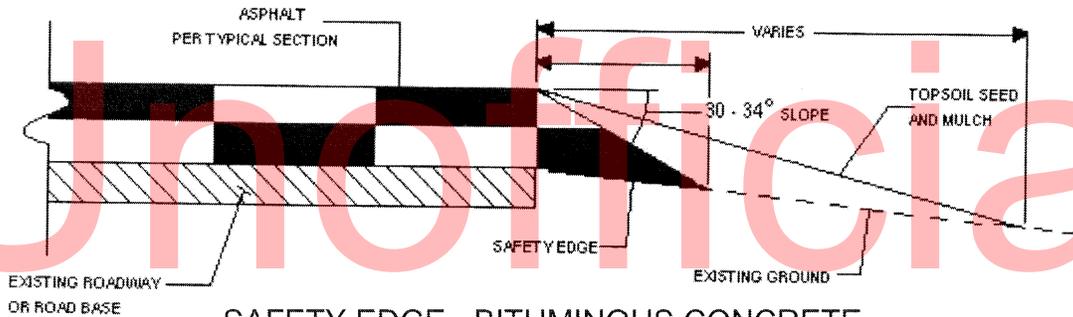


ADJUSTING AND REPAIRING MANHOLE

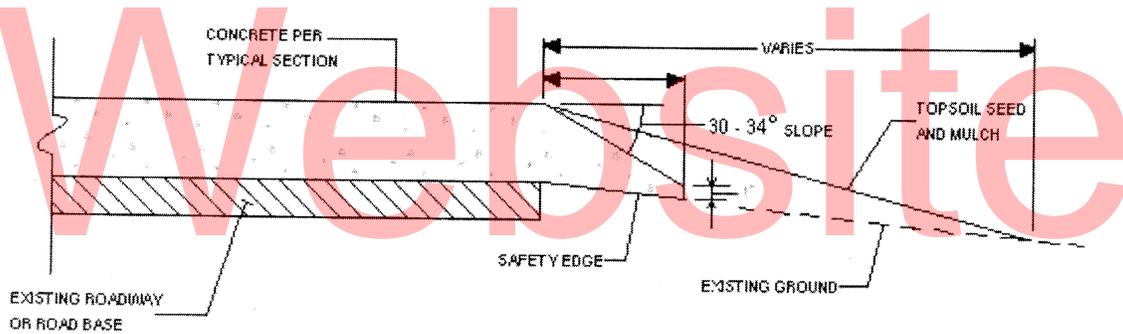
N.T.S.

NOTE: MANHOLES IN CONCRETE ROADWAYS SHALL BE SAWCUT FULL DEPTH TO THE SPECIFIED DIMENSION PRIOR TO ANY ADJUSTMENT BEING MADE. THE SAWCUTTING SHALL BE INCIDENTAL TO ITEM 710002.

DETAILS SHEET



SAFETY EDGE - BITUMINOUS CONCRETE



SAFETY EDGE - CONCRETE

GENERAL NOTES

1. THE SAFETY EDGE WILL BE CONSTRUCTED AS PART OF THE ROADWAY PAVEMENT. A SHOULDER WEDGE DEVICE WILL BE ADDED TO THE SCREED OF THE PAVING MACHINE.
2. SHORT SECTIONS OF HANDWORK WILL BE ALLOWED WHEN NECESSARY FOR TRANSITION AND TURNOUTS.
3. SITE PREPARATION AND ADDITIONAL EARTHWORK REQUIRED TO CONSTRUCT THE SAFETY EDGE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE WORK.

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 1 **26(OLD BALTIMORE PIKE) FROM OLD BALTIMORE PIKE (RD 26) FROM 500'
SOUTH OF ROUTE 273 TO ROUTE 273 (RD 3)**

LENGTH 152.4 m 500 L.F. AADT 13178

GENERAL IMPROVEMENTS

MEDIAN IMPROVEMENTS @ ROUTE 273 AND OLD BALTIMORE PIKE

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 1

202000	EXCAVATION AND EMBANKMENT	195.00 C.Y.
302007	GRADED AGGREGATE BASE COURSE, TYPE B	77.00 C.Y.
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	118.00 TON
401827	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (NON-CARBONATE STONE)	108.00 TON
701011	PORTLAND CEMENT CONCRETE CURB, TYPE 2	567.00 L.F.
743056	** FLAGGER, NEW CASTLE COUNTY, FEDERAL	80.00 HOUR
743065	** FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	8.00 HOUR
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	124.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	1,578.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	248.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	789.00 L.F.
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	127.00 S.Y.
760507	PROFILE MILLING, HOT-MIX	1,208.00 SY-IN
762001	SAW CUTTING, HOT MIX	1,309.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	40.00 L.F.
763501	CONSTRUCTION ENGINEERING	1.00 L.S.

** DENOTES FIXED PRICE ITEM

LOCATION 1 26(OLD BALTIMORE PIKE) FROM OLD BALTIMORE PIKE (RD 26) FROM 500' SOUTH OF ROUTE 273 TO ROUTE 273 (RD 3)

COMMENTS:

Recommended Traffic Control: TA-10

Restricted Work Hours: 8:00 AM to 4:00 PM

Curb Ramp Locations:

None

Entrance and Driveway Locations:

None

Manhole Adjustments:

None

General Notes:

Restricted work hours, may be adjusted if traffic conditions warrant

The contractor will be required to close one of the left turn lanes from WB Route 273 to Old Baltimore Pike when performing median work on Old Baltimore Pike.

Unofficial
Website
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DELAWARE DEPARTMENT OF TRANSPORTATION

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 2 346(WALTHER ROAD) FROM ROUTE 40 (RD32) TO OLD BALTIMORE PIKE (RD336)

LENGTH 3048 m 10000 L.F. AADT 10666

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C 70-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 2

401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	120.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	800.00 TON
401830	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)	5,300.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	50.00 TON
406001	BITUMINOUS CONCRETE PATCHING	14,600.00 SY-IN
705001	P.C.C. SIDEWALK, 4"	100.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	40.00 S.F.
705008	CURB RAMP, TYPE 1	150.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	350.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	2.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	2.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	6.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	2.00 EACH
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	10.00 EA-DY
743007	** TRAFFIC OFFICERS	200.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	20.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	1,000.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	100.00 HOUR
743550	TUBULAR MARKER	8.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	1,020.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	4,000.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	88,000.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	8,000.00 S.F.
748027	PERMANENT PAVEMENT STRIPING, ALKYD-THERMOPLASTIC, 12"	80.00 L.F.
748530	REMOVAL OF PAVEMENT STRIPING	600.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	44,000.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	480.00 L.F.

LOCATION 2

346(WALTHER ROAD) FROM ROUTE 40 (RD32) TO OLD BALTIMORE PIKE (RD336)

LENGTH 3048 m 10000 L.F. AADT 10666

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C 70-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 2

748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	9.00 EACH
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	75.00 EACH
749688	INSTALLATION OF 4" DIAMETER HOLE, LESS THAN OR EQUAL TO 6" DEPTH	4.00 EACH
758000	REMOVAL OF EXISTING PORTLAND CEMENTCONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	20.00 S.Y.
760507	PROFILE MILLING, HOT-MIX	96,400.00 SY-IN
762001	SAW CUTTING, HOT MIX	650.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	50.00 L.F.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	2.00 HOUR

** DENOTES FIXED PRICE ITEM

Unofficial Website Copy

LOCATION 2 346(WALTHER ROAD) FROM ROUTE 40 (RD32) TO OLD BALTIMORE PIKE (RD336)

COMMENTS:

Recommended Traffic Control: TA-10, TA-3

Restricted Work Hours: 8:00 AM to 4:00 PM

Curb Ramp Locations:

Cedar Ridge Court - 4

Entrance and Driveway Locations:

Valley Stream Village 25'

Cedar Ridge Court 35'

Old Walther Rd. 10'

Creekside Dr. 40'

Creekside Dr. 141'

Darling St. 10'

Barretts Way 65'

Manhole Adjustments:

2 - NCCDSS

2 - Water valves by others

General Notes:

School and concrete plant at this location

Unofficial
Website
Copy

DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT T201206105

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 3 347(CHAPMAN RD) FROM SALEM CHURCH RD (RD348) TO ROUTE 273 (RD18)

LENGTH 2249.424 m 7380 L.F. AADT 9492

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG 64-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 3

302005	GRADED AGGREGATE BASE COURSE, TYPE B	20.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	320.00 TON
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	200.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	840.00 TON
401827	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (NON-CARBONATE STONE)	7,000.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	120.00 TON
406001	BITUMINOUS CONCRETE PATCHING	15,300.00 SY-IN
701011	PORTLAND CEMENT CONCRETE CURB, TYPE 2	100.00 L.F.
705001	P.C.C. SIDEWALK, 4"	300.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	220.00 S.F.
705008	CURB RAMP, TYPE 1	300.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	1,400.00 S.F.
705500	P.C.C. CROSSABLE MEDIAN	140.00 S.Y.
705530	TRIANGULAR CHANNELIZING ISLANDS	2,130.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	12.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	12.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	25.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	12.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	2.00 EACH
743003	ARROWPANELS, TYPE C	100.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	10.00 EA-DY
743007	** TRAFFIC OFFICERS	200.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	60.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	1,500.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	150.00 HOUR
743550	TUBULAR MARKER	37.00 EACH
744530	CONCUIT JUNCTION WELL, TYPE 11, PRECAST CONCRETE/ POLYMER LID-FRAME	5.00 EACH

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 3 347(CHAPMAN RD) FROM SALEM CHURCH RD (RD348) TO ROUTE 273 (RD18)

LENGTH 2249.424 m 7380 L.F. AADT 9492

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG 64-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 3

744531	CONDUIT JUNCTION WELL, TYPE 14, PRECAST CONCRETE/ POLYMER LID-FRAME	1.00 EACH
744544	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	4.00 EACH
745602	FURNISH & INSTALL UP TO 4" SCHEDULE 80 HDPE CONDUIT (BORE)	100.00 L.F.
745603	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (OPEN CUT)	45.00 L.F.
745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	140.00 L.F.
746850	POLE BASE, TYPE 4	7.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	2,830.00 L.F.
747516	CABINET BASE, TYPE P	1.00 EACH
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	4,500.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	64,000.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	9,000.00 S.F.
748530	REMOVAL OF PAVEMENT STRIPING	155.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	32,000.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	3,700.00 L.F.
748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	7.00 EACH
748557	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 3"	1,200.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	135.00 EACH
749688	INSTALLATION OF 4" DIAMETER HOLE, LESS THAN OR EQUAL TO 6" DEPTH	19.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	28.00 S.F.
758000	REMOVAL OF EXISTING PORTLAND CEMENTCONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	155.00 S.Y.
760507	PROFILE MILLING, HOT-MIX	127,300.00 SY-IN
762001	SAW CUTTING, HOT MIX	1,145.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	200.00 L.F.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	10.00 HOUR

** DENOTES FIXED PRICE ITEM

LOCATION 3 347(CHAPMAN RD) FROM SALEM CHURCH RD (RD348) TO ROUTE 273 (RD18)

COMMENTS:

Restricted work Hours: 8:00 AM to 4:00 PM

Recommended Traffic Control: TA-10, TA-3, TA-33

Curb Ramp Locations:

Christiana High School - 1
Wakefield Dr. - 2
Hudson Village - 1 - signalized
Hudson Village - 1
BP Station - 1 - signalized
Route 273 ramp onto Chapman Rd. - 1
University Plaza east - 1
University Plaza west - 1
Glen Eagle Village - 1
Route 273 - 1 - signalized
Route to Chapman Rd. - 1

Triangular Channelizing Islands:

N. Wakefield Dr. - 2 - signalized
Hudson Village - 1 - signalized
University Plaza east - 1 - signalized
University Plaza west - 1 - signalized

Entrance and Driveway Locations:

Alexis Dr. 20'
Galleon Rd. 20'
North Wakefield Dr. south 20'
N. Wakefield Dr. north 45'
Cambridge Stockton 60'
Exxon east 15'
Exxon west 5'
University Plaza south 25'
University Plaza north 35'
#242 20'
Sherwood Forest 30'

Manhole Adjustments:

2 - DelDOT storm
12 - NCCDSS
7 - Water valves by others
2 - Delmarva Gas by other
2 - Delmarva Electric by others

General Notes:

Christiana High School at this location
DelDOT yard on Regal Blvd.

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 4 **35(RED LION ROAD) FROM 200' WEST OF LOVERS LANE TO DUPONT PARKWAY (RD82)**

LENGTH 6242.304 m 20480 L.F. AADT 4839

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG 70-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 4

302005	GRADED AGGREGATE BASE COURSE, TYPE B	20.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	340.00 TON
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	2,000.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	1,100.00 TON
401830	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)	12,000.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	140.00 TON
406001	BITUMINOUS CONCRETE PATCHING	20,000.00 SY-IN
701022	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8	40.00 L.F.
705001	P.C.C. SIDEWALK, 4"	400.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	80.00 S.F.
705008	CURB RAMP, TYPE 1	600.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	4.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	4.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	13.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	5.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	1.00 EACH
732004	TOPSOIL (TON)	100.00 TON
734013	PERMANENT GRASS SEEDING, DRY GROUND	1,000.00 S.Y.
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	30.00 EA-DY
743007	** TRAFFIC OFFICERS	400.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	65.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	2,200.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	220.00 HOUR
744505	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	2.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	1,230.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	1,400.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	164,290.00 L.F.

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 4 **35(RED LION ROAD) FROM 200' WEST OF LOVERS LANE TO DUPONT PARKWAY (RD82)**

LENGTH 6242.304 m 20480 L.F. AADT 4839

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG 70-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 4

748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	2,800.00 S.F.
748502	RAISED/RECESSED PAVEMENT MARKER	440.00 EACH
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	82,145.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	2,500.00 L.F.
748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	28.00 EACH
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	91.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	32.00 S.F.
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	60.00 S.Y.
760507	PROFILE MILLING, HOT-MIX	220,000.00 SY-IN
762001	SAW CUTTING, HOT MIX	1,250.00 L.F.
763502	MAINTENANCE OF RAILROAD TRAFFIC	1.00 L.S.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	5.00 HOUR

** DENOTES FIXED PRICE ITEM

LOCATION 4 35(RED LION ROAD) FROM 200' WEST OF LOVERS LANE TO DUPONT PARKWAY (RD82)

COMMENTS:

Restricted work Hours; 8:00 AM to 4:00 PM

Recommended Traffic Control: TA-10, TA-3

Curb Ramp Locations:

Baywatch Rd. - 2
Jester St. - 2
Delilah Dr. - 2
Red Lion Christian Academy - 2

Entrance and Driveway Locations:

Old Kirkwood Rd. south 55'
Old Kirkwood Rd. north 40'
Delilah Dr. 25'
Christian Academy 25'
Church Rd. east & west 30'
Bear-Corbitt Rd. east 50'
Baywatch Rd. 60'
Rutledge 40'
Old Porter Rd. 30'
Jester Crossing 20'
Rice Dr. 30'

Manhole Adjustments:

3 - NCCDSS
2 - Verizon
10 - Water valves by others

General Notes:

Conrail railroad tracks, Red Lion Christian Academy, Red Lion Evangelical Church and Red Lion Methodist Church at this location

DELAWARE DEPARTMENT OF TRANSPORTATION

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 5 68(NEWTOWN RD) FROM ROUTE 7 (RD5) TO SMALLEY'S DAM RD (RD345)

LENGTH 670.56 m 2200 L.F. AADT 9360

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG 64-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 5

302005	GRADED AGGREGATE BASE COURSE, TYPE B	20.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	20.00 TON
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	100.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	520.00 TON
401827	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (NON-CARBONATE STONE)	1,400.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	25.00 TON
406001	BITUMINOUS CONCRETE PATCHING	9,500.00 SY-IN
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	10.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	175.00 S.F.
711001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	1.00 EACH
743003	ARROWPANELS, TYPE C	15.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	10.00 EA-DY
743007	** TRAFFIC OFFICERS	90.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	10.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	200.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	20.00 HOUR
743550	TUBULAR MARKER	12.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	520.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	600.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	19,600.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	1,200.00 S.F.
748530	REMOVAL OF PAVEMENT STRIPING	256.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	9,800.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	146.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	29.00 EACH
749689	INSTALLATION OF 4" DIAMETER HOLE, GREATER THAN 6" DEPTH	1.00 EACH
760507	PROFILE MILLING, HOT-MIX	25,100.00 SY-IN

DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT T201206105

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

LOCATION 5 68(NEWTOWN RD) FROM ROUTE 7 (RD5) TO SMALLEY'S DAM RD (RD345)

LENGTH 670.56 m 2200 L.F. AADT 9360

GENERAL IMPROVEMENTS

2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE TYPE C PG
64-22

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 5

762001	SAW CUTTING, HOT MIX	360.00 L.F.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	5.00 HOUR

** DENOTES FIXED PRICE ITEM

Unofficial
Website
Copy

LOCATION 5 68(NEWTOWN RD) FROM ROUTE 7 (RD5) TO SMALLEY'S DAM RD (RD345)

COMMENTS:

Restricted work Hours: 8:00 AM to 4:00 PM

Recommended Traffic Control: TA-10, TA-3

Curb Ramp Locations:

Smalleys Dam Rd. - 1

Entrance and Driveway Locations:

Smalleys Dam Rd. 15'

Manhole Adjustments:

No manhole adjustments anticipated at this location

Unofficial
Website
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CANAL PAVEMENT & REHABILITATION, NORTH V, 2012

CONTRACT QUANTITIES SUMMARY SHEET

202000	EXCAVATION AND EMBANKMENT	195.00 C.Y.
302005	GRADED AGGREGATE BASE COURSE, TYPE B	60.00 TON
302007	GRADED AGGREGATE BASE COURSE, TYPE B	77.00 C.Y.
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	800.00 TON
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	2,300.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	3,378.00 TON
401827	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22 (NON-CARBONATE STONE)	8,508.00 TON
401830	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)	17,300.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	335.00 TON
406001	BITUMINOUS CONCRETE PATCHING	59,400.00 SY-IN
701011	PORTLAND CEMENT CONCRETE CURB, TYPE 2	667.00 L.F.
701022	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8	40.00 L.F.
705001	P.C.C. SIDEWALK, 4"	800.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	350.00 S.F.
705008	CURB RAMP, TYPE 1	1,050.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	1,925.00 S.F.
705500	P.C.C. CROSSABLE MEDIAN	140.00 S.Y.
705530	TRIANGULAR CHANNELIZING ISLANDS	2,130.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	18.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	18.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	45.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	19.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	3.00 EACH
732004	TOPSOIL (TON)	100.00 TON
734013	PERMANENT GRASS SEEDING, DRY GROUND	1,000.00 S.Y.
743003	ARROWPANELS, TYPE C	115.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	60.00 EA-DY
743007	** TRAFFIC OFFICERS	890.00 HOUR

DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT: T201206105

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CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

CONTRACT QUANTITIES SUMMARY SHEET

743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	155.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	4,980.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	498.00 HOUR
743550	TUBULAR MARKER	57.00 EACH
744505	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	2.00 EACH
744530	CONCUIT JUNCTION WELL, TYPE 11, PRECAST CONCRETE/ POLYMER LID-FRAME	5.00 EACH
744531	CONDUIT JUNCTION WELL, TYPE 14, PRECAST CONCRETE/ POLYMER LID-FRAME	1.00 EACH
744544	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	4.00 EACH
745602	FURNISH & INSTALL UP TO 4" SCHEDULE 80 HDPE CONDUIT (BORE)	100.00 L.F.
745603	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (OPEN CUT)	45.00 L.F.
745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	140.00 L.F.
746850	POLE BASE, TYPE 4	7.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	5,600.00 L.F.
747516	CABINET BASE, TYPE P	1.00 EACH
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	10,624.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	337,468.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	21,248.00 S.F.
748027	PERMANENT PAVEMENT STRIPING, ALKYD-THERMOPLASTIC, 12"	80.00 L.F.
748502	RAISED/RECESSED PAVEMENT MARKER	440.00 EACH
748530	REMOVAL OF PAVEMENT STRIPING	1,011.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	168,734.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	6,826.00 L.F.
748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	44.00 EACH
748557	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 3"	1,200.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	330.00 EACH
749688	INSTALLATION OF 4" DIAMETER HOLE, LESS THAN OR EQUAL TO 6" DEPTH	23.00 EACH
749689	INSTALLATION OF 4" DIAMETER HOLE, GREATER THAN 6" DEPTH	1.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	60.00 S.F.

DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT: T201206105

PAGE 33 OF 33

CANAL

PAVEMENT & REHABILITATION, NORTH V, 2012

CONTRACT QUANTITIES SUMMARY SHEET

758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	362.00 S.Y.
760507	PROFILE MILLING, HOT-MIX	470,008.00 SY-IN
762001	SAW CUTTING, HOT MIX	4,714.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	290.00 L.F.
763000	INITIAL EXPENSE	LUMP SUM
763501	CONSTRUCTION ENGINEERING	1.00 L.S.
763502	MAINTENANCE OF RAILROAD TRAFFIC	1.00 L.S.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	22.00 HOUR
763643	MAINTENANCE OF TRAFFIC, ALL INCLUSIVE	LUMP SUM

** DENOTES FIXED PRICE ITEM

Unofficial
Website
Copy

T201206105

Old Baltimore Pike @ SR 273

Website

Copy

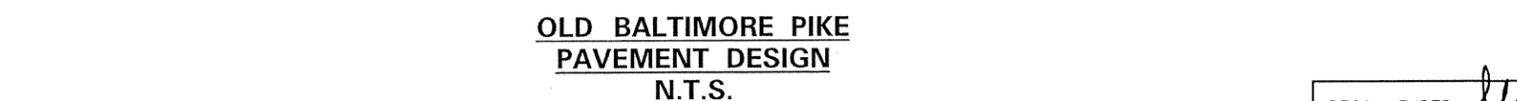
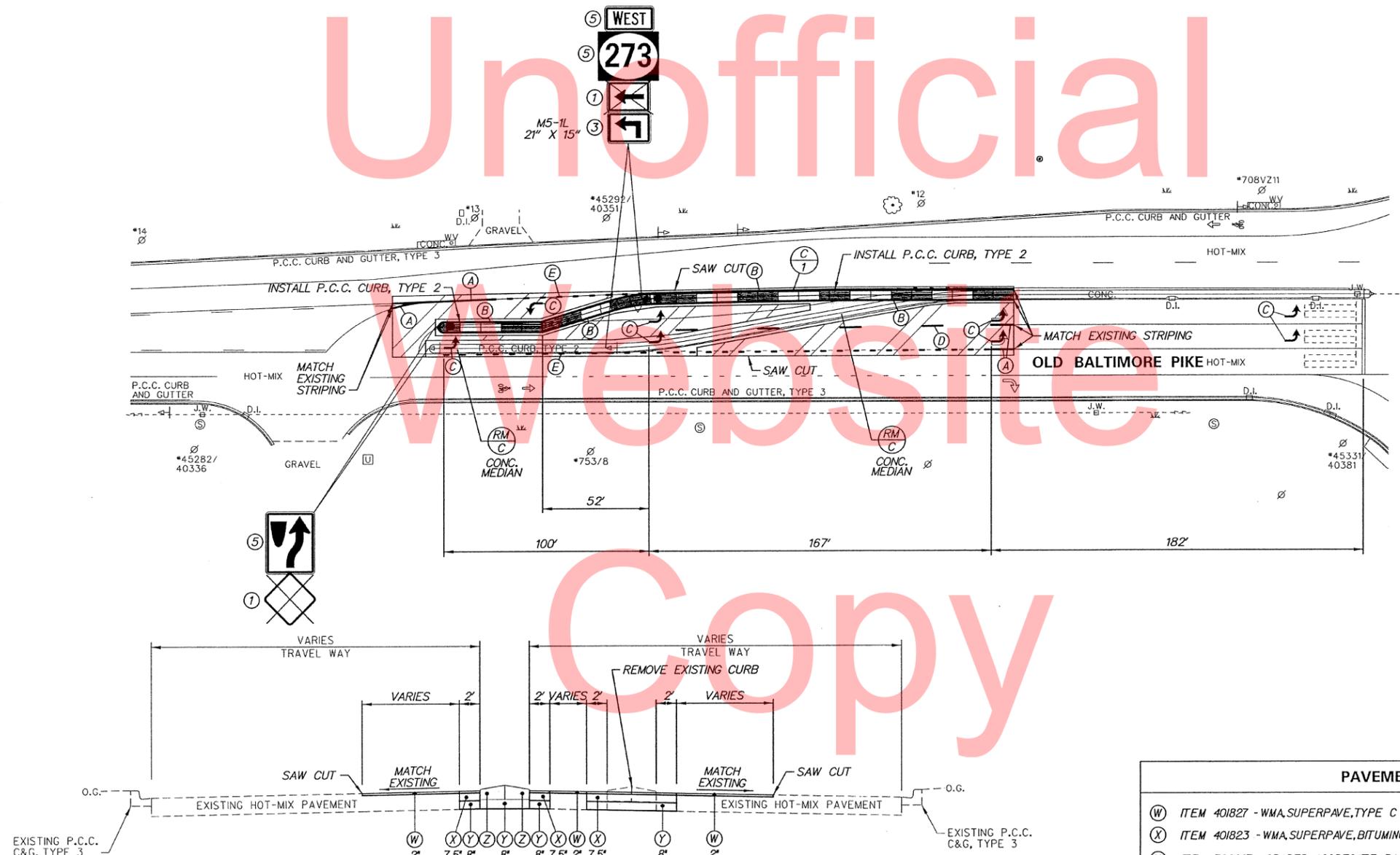
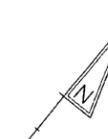
SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAVEMENT, WHITE, (ITEM 748548)	90 LF
(B)	5" PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAVEMENT, YELLOW, (ITEM 748548)	571 LF
(C)	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND, ALKYD-THERMOPLASTIC (ITEM 748015)	124 SF
(D)	5" BROKEN PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, 10' LINE & 30' GAP, WHITE, (ITEM 748548)	40 LF
(E)	5" DOTTED PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, 2' LINE & 6' GAP, WHITE, (ITEM 748548)	88 LF

*QUANTITY REPRESENTS ACTUAL AMOUNT OF MATERIAL

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P. C. C. CURB, TYPE 2	567 LF

PAVEMENT SECTIONS	
	FULL-DEPTH RECONSTRUCTED PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	MILL AND OVERLAY - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS

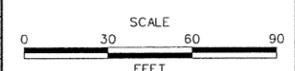


PAVEMENT DESIGN LEGEND	
(W)	ITEM 401827 - WMA, SUPERPAVE, TYPE C HOT-MIX, 160 GYRATIONS, PG 64-22
(X)	ITEM 401823 - WMA, SUPERPAVE, BITUMINOUS CONCRETE BASECOURSE, 160 GYRATIONS, PG 64-22, PATCHING
(Y)	ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
(Z)	ITEM 701011 - P.C.C. CURB, TYPE 2

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DELAWARE
 DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



PAVEMENT & REHABILITATION,
 2012, NORTH V

CONTRACT	T201206105
COUNTY	NEW CASTLE
BRIDGE NO.	N/A
DESIGNED BY:	B.S.S. (WR&A)
CHECKED BY:	M.J.B. (WR&A)

OLD BALTIMORE PIKE
 @ SR 273 MEDIAN
 IMPROVEMENTS

SHEET NO.	1
TOTAL SHTS.	1

RECOMMENDED DATE: 5/9/14 APPROVED TRAFFIC ENGINEER DATE: 5/19/2014

T201206105

Signal Plans

Website

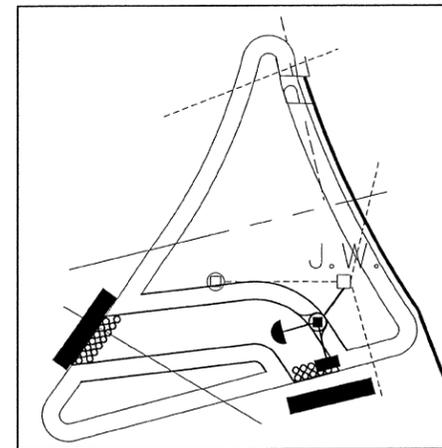
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CONDUIT RUN SCHEDULE				
CR#	# OF CONDUITS	SIZE	LENGTH	AMOUNT AND TYPE OF CABLE / WIRE
1	1	2.5"	11'	(2) 16/*14 (4) 4/*18 [NEW (1)*6 AWG]
2	1	1.5"	37'	(1) 2/*8 U.F.W./ GROUND [NEW (1)*6 AWG]
3	1	2.5"		<REMOVE (1) 9/*14 (3) 4/*18 [NEW (4) 9/*14 (6) 4/*18 (1)*6 AWG]
4	1	1.5"	UNK	(1) 2/*8 U.F.W./ GROUND [NEW (1)*6 AWG]
5	1	2.5"	46'	<REMOVE (1) 9/*14 [NEW (1) 9/*14 (2) 4/*18 (1)*6 AWG]
6	1	2.5"	3'	(1) 9/*14 (1)*6 AWG
7	1	2.5"	152'	<REMOVE (2) 4/*18 [NEW (3) 9/*14 (4) 4/*18 (1)*6 AWG]
8	1	2.5"	61'	<REMOVE (1) 9/*14 [NEW (3) 9/*14 (3) 4/*18 (1)*6 AWG]
9	1	2.5"	99'	(1) 9/*14 (3) 4/*18 (1)*6 AWG
10	1	2.5"	4'	(1) 9/*14 (1)*6 AWG
11	1	2.5"	30'	<REMOVE (1) 9/*14 [NEW (2) 4/*18 (1)*6 AWG]
12	1	1.5"	38'	<REMOVE (1) 4/*18 [NEW (1) 4/*18 (1)*6 AWG]
13	1	2.5"	59'	<REMOVE (1) 9/*14 (2) 4/*18 [NEW (1)*6 AWG]

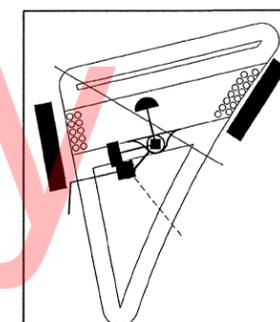
* DENOTES EXISTING

GENERAL SIGNAL NOTES (CON'T)

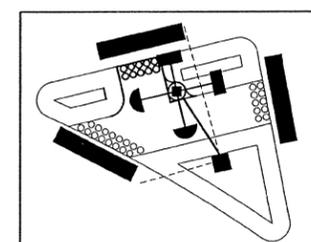
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50" OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
- ROW BASED OFF OF SIGNAL PLAN DATED 3/26/02, CONTRACT NO. 22-116-01.
- ALL PEDESTRIAN SIGNALS SHALL CONTAIN COUNTDOWN HEADS.
- A SURVEY WAS NOT CONDUCTED. MEASUREMENTS AND LOCATION OF TRAFFIC EQUIPMENT BASED OFF OF FIELD MEASUREMENTS, AERIALS, AND OLD SIGNAL PLAN DATED MARCH 26, 2002.
- ALL EXISTING PEDESTRIAN EQUIPMENT SHALL BE REMOVED.
- THE CONTRACTOR SHALL REPLACE THE EXISTING TYPE 4 POLE BASE WITH A TYPE 1 JUNCTION WELL.
- ACCESS TO THE SOUTHERN PROPERTY LIMITS PROVIDED BY TRAFFIC SIGNAL AND MAINTENANCE AGREEMENT DATED AUGUST 7, 1986.
- THE TRAFFIC CONTRACTOR SHALL REMOVE THE EXISTING CABINET AND REPLACE WITH AN EPAC CONTROLLER CABINET.
- THE TRAFFIC CONTRACTOR SHALL REMOVE EXISTING HEADS 4 AND 7 AND REPLACE WITH NEW INVERTED-T HEADS.
- DELDOT'S TRAFFIC SECTION SHALL INSTALL A CDMA IN THE CONTROLLER CABINET.



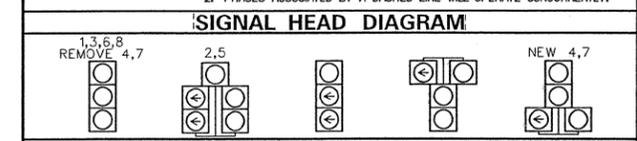
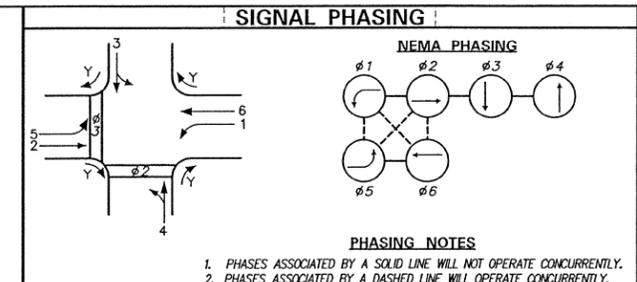
DETAIL 1
SCALE
10
FEET



DETAIL 3
SCALE
10
FEET



DETAIL 2
SCALE
10
FEET



LEGEND			
(AB)	ABANDON	(OH)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)	(OH)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
(CA)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)	(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)	(PL)	EXISTING POLE IDENTIFIER (# OF POLE)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(PL)	PROPOSED POLE IDENTIFIER (# OF POLE)
(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(RM)	REMOVE BY CONTRACTOR
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM)	REMOVE BY OTHERS
(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	◇	◇
MAST ARM	▶	▶
MICROWAVE DETECTION	▶	▶
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	—	—
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	→	→
RIGHT-OF-WAY	---	R/W
SERVICE PEDESTAL	□	□
SIGNAL CABINET	□	□
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	—	—
UTILITY POLE	⊙	⊙
VIDEO DETECTION	▶	▶

- GENERAL SIGNAL NOTES:
- ALL SIGNAL POLES WILL BE 28 FEET, EXCEPT WHERE SHOWN.
 - CO #3 IS NOT DRAWN TO SCALE, NOR IS THE DIRECTION NECESSARILY CORRECT.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

RECOMMENDED _____ DATE: _____

RECOMMENDED *ave* DATE: 9/11/12

RECOMMENDED *Yvonne C. H...* DATE: 9/14/12

APPROVED TRAFFIC ENGINEER *Will Z...* DATE: 9/14/12

APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER *Debra* DATE: 9/17/12

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS

1	CHANGED CONTRACT # T201206112 TO T201206105, MS-3/14
---	--

SCALE 0 30 60 90 FEET

PAVEMENT & REHABILITATION NORTH V, 2012

CONTRACT	T201206105	PERMIT NO.	N060P
COUNTY	NEW CASTLE	DESIGNED BY:	GG
		CHECKED BY:	

SIGNAL PLAN

CHAPMAN ROAD @ NORTH WAKEFIELD DRIVE

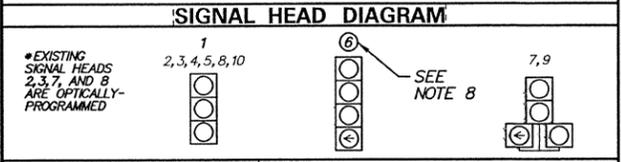
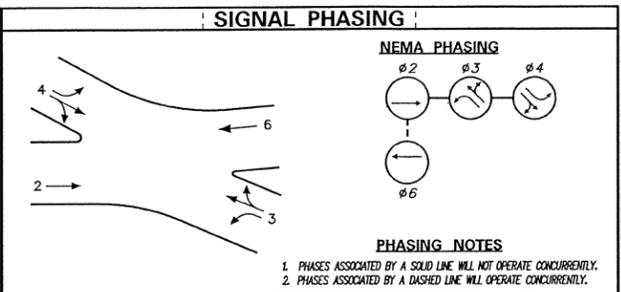
SHEET NO. 1

TOTAL SHTS. 1

W:\MSV\8\CELLS\PROJDEV\SB\CEL

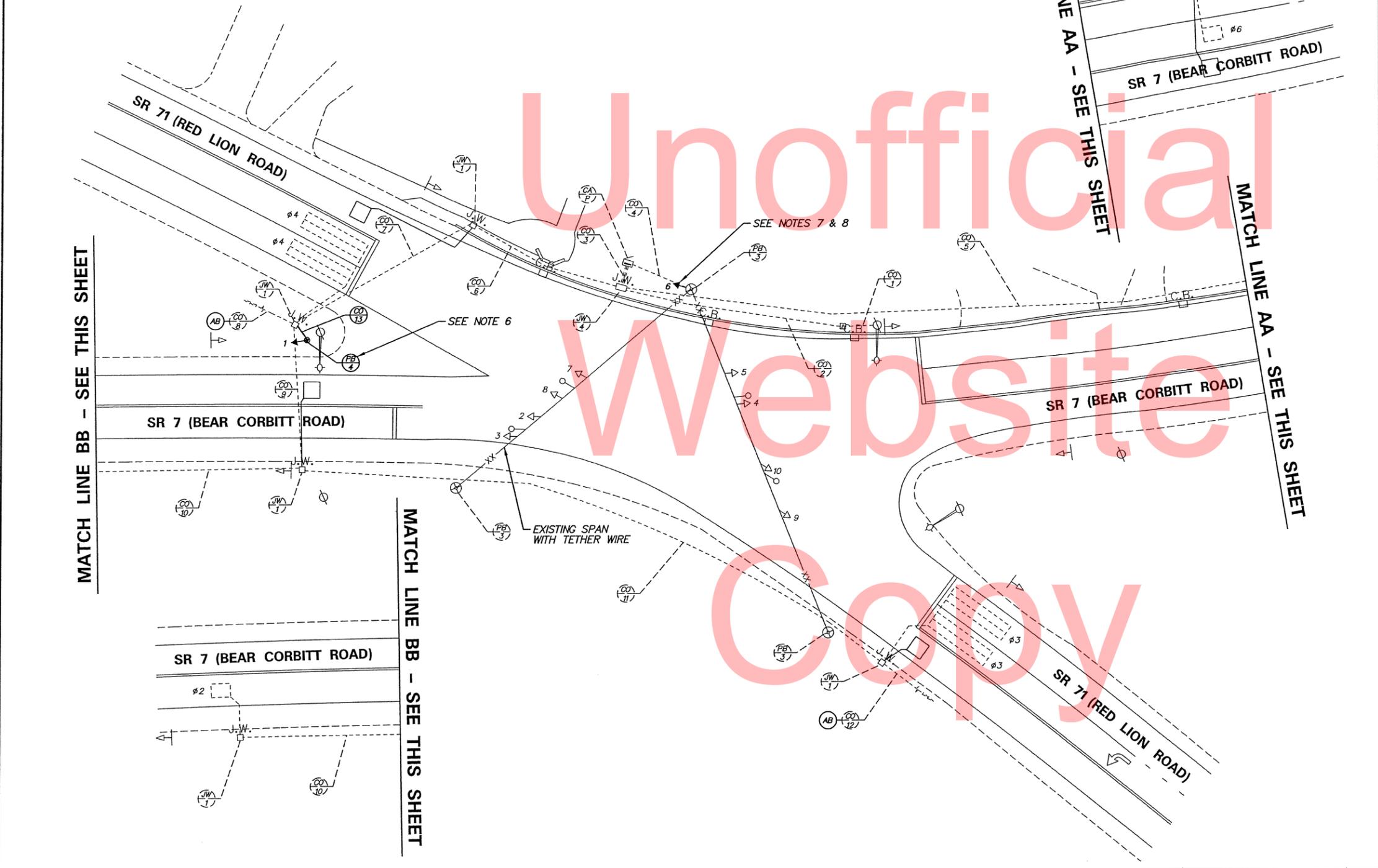
CONDUIT RUN SCHEDULE					
CO#	OF CONDUITS	SIZE	LENGTH	B/T/O	AMOUNT AND TYPE OF CABLE/ WIRE
1*	1	1.5 IN	11 FT	-	EX. (2) POWER CABLES
2*	1	1.5 IN	80 FT	-	EX. (2) POWER CABLES
3*	3	1.5 IN	7 FT	-	EX. (2) POWER CABLES
		2.5 IN	7 FT	-	EX. (4) 4/*18 - TO REMAIN, <REMOVE EX. (2) 4/*18 - (CUT)>
4*	1	2.5 IN	22 FT	-	[NEW (1) 4/*14] [NEW (4) 4/*18]
5*	1	2.5 IN	271 FT	-	EX. (2) 16/*14, EX. (4) 4/*18, [NEW (1) 4/*14]
6*	1	2.5 IN	57 FT	-	EX. (1) 4/*18 [NEW (1) 4/*14]
7*	1	2.5 IN	74 FT	-	EX. (3) 4/*18 - TO REMAIN, <REMOVE EX. (2) 4/*18 - (CUT)>, [NEW (1) 4/*14]
8*	1	2.5 IN	XX FT	-	<ABANDON - EX. (1) 4/*18 - (CUT)>
9*	1	2.5 IN	51 FT	-	EX. (2) 4/*18 - TO REMAIN, <REMOVE EX. (1) 4/*18 - (CUT)> [NEW (1) 4/*18]
10*	1	2.5 IN	132 FT	-	EX. (1) 4/*18
11*	1	2.5 IN	228 FT	-	EX. (1) 4/*18 - TO REMAIN, <REMOVE EX. (1) 4/*18 - (CUT)> [NEW (1) 4/*18]
12*	1	2.5 IN	XX FT	-	<ABANDON - EX. (1) 4/*18 - (CUT)>
13	1	2.5 IN	6 FT	7	[NEW (1) 4/*14]

NOTES:
 6. THE CONTRACTOR SHALL INSTALL A PEDESTAL POLE ON THE PROPOSED POLE BASE, TYPE 4 WITH A POST TOP-MOUNT ONE-WAY ASSEMBLY FOR PROPOSED SIGNAL HEAD 1.
 7. THE CONTRACTOR SHALL INSTALL PROPOSED SIGNAL HEAD 6 TO THE EXISTING SIGNAL POLE USING A STAND-OFF BRACKET.
 8. THE CONTRACTOR SHALL INSTALL TUNNEL SHADES ON PROPOSED SIGNAL HEAD 6.

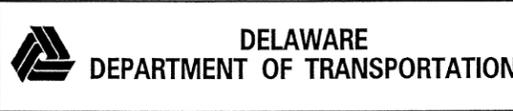


LEGEND	
■ PROPOSED SIGNAL CABINET	⊖ REMOVE BY CONTRACTOR
□ EXISTING SIGNAL CABINET	⊖ REMOVE BY OTHERS
⊙ PROPOSED SIGNAL POLE BASE	⊖ ABANDON
⊙ EXISTING SIGNAL POLE BASE	⊖ PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
⊙ PROPOSED PEDESTRIAN POLE BASE	⊖ EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
⊙ EXISTING PEDESTRIAN POLE BASE	⊖ PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊙ PROPOSED WOOD POLE	⊖ EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
⊙ EXISTING UTILITY POLE	⊖ PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
⊙ PROPOSED JUNCTION WELL	⊖ EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)
J.W. EXISTING JUNCTION WELL	⊖ PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
→ PROPOSED SIGNAL HEAD	⊖ EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
→ EXISTING SIGNAL HEAD	⊖ PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
→ PROPOSED PEDESTRIAN SIGNAL HEAD	⊖ EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
→ EXISTING PEDESTRIAN SIGNAL HEAD	⊖ PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
→ PROPOSED PEDESTRIAN PUSHBUTTON	⊖ EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
→ EXISTING PEDESTRIAN PUSHBUTTON	— PROPOSED SPAN WIRE
→ PROPOSED VIDEO DETECTION	— XX EXISTING SPAN WIRE
→ EXISTING VIDEO DETECTION	--- RIGHT-OF-WAY OR PROPERTY LINE
→ PROPOSED MICROWAVE DETECTION	◆ PROPOSED SPAN INSULATOR
→ EXISTING MICROWAVE DETECTION	◇ EXISTING SPAN INSULATOR
→ OVERHEAD SIGNING	⊖ SERVICE PEDESTAL
→ PROPOSED OPTICOM RECEIVER	
→ EXISTING OPTICOM RECEIVER	
→ PROPOSED MAST ARM	
→ EXISTING MAST ARM	
→ PROPOSED LUMINAIRE	
→ EXISTING LUMINAIRE	
□ PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	
□ EXISTING LOOP DETECTOR (TYPE 1 OR 2)	

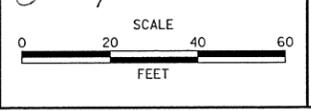
GENERAL SIGNAL NOTES:
 1. DETECTION - 35 M.P.H. NORTHBOUND SR 7 - 3.3 SECONDS PASSAGE TIME AT 171 FEET FROM THE STOP LINE. SOUTHBOUND SR 7 - 3.2 SECONDS PASSAGE TIME AT 166 FEET FROM THE STOP LINE.
 2. LOOP DETECTORS - (TO REMAIN): TYPE #1 - 5' x 7' - SR 7 THROUGH MOVEMENTS. TYPE #2 - 6' x 25' - ALL SR 71 MOVEMENTS.
 3. ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 4. ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 5. ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.



RECOMMENDED _____ DATE: _____ RECOMMENDED _____ DATE: _____ RECOMMENDED *[Signature]* DATE: 11/3/09 APPROVED TRAFFIC ENGINEER *[Signature]* DATE: 11/3/09 APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER *[Signature]* DATE: 11/3/09



ADDENDUM / REVISIONS
1. INSTALLED SUPPLEMENTAL SIGNAL HEADS FOR PHASES 2 & 4. D.W.C. (WR&A) 10-09 (CONTRACT # 29-010-02)
2. INSTALLED SYSTEM LOOPS GG (4/9/12) - T201206105



HSIP 2008, SITE C

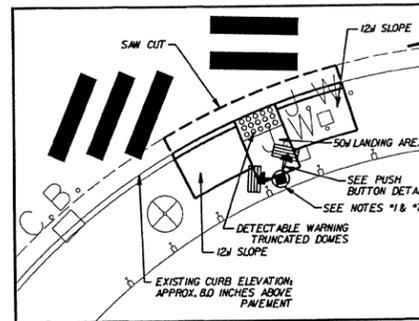
CONTRACT	PERMIT NO.	N-137
29-010-02	DESIGNED BY: D.W.C. (WR&A)	
COUNTY	CHECKED BY: M.J.B. (WR&A)	
NEW CASTLE		

SIGNAL PLAN	SHEET NO.
SR 7 (BEAR CORBITT ROAD) @ SR 71 (RED LION ROAD)	1
	TOTAL SHTS.
	1

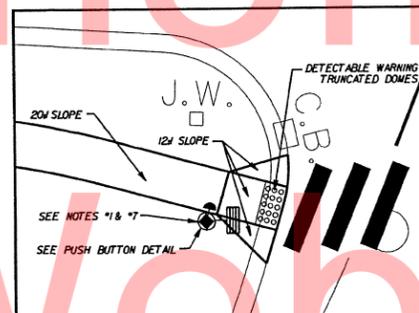
CONDUIT RUN SCHEDULE

CO#	*OF CON	SIZE	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE		
				TO REMAIN	REMOVE	INSTALL
*1	1	2.0"	UNK	(1)2/*8 U.F.W. GROUND		
*2	3	2.5"	33'	(2)16/*14 (3)4/*18 COMM. CABLE	(1)4/*18	(4)9/*14 (4)4/*18
*3	1	2.5"	6'	(3)4/*18 COMM. CABLE	(2)4/*18	(1)9/*14 [NEW] (2) 4/#18]
*4	1	2.5"	17'	(2)16/*14		(1)9/*14 (4)4/*18
*5	1	2.5"	5'	(3)4/*18 COMM. CABLE	(1)4/*18	(3)9/*14
*6	1	2.5"	48'	(3)4/*18 COMM. CABLE	(1)4/*18	(3)9/*14
*7	1	1.5"	3'	(1)4/*18		
*8	1	2.5"	66'	(2)4/*18 COMM. CABLE	(1)4/*18	(3)9/*14
*9	1	2.5"	37'	(2)4/*18 COMM. CABLE	(1)4/*18	(1)9/*14
*10	1	1.5"	72'	(2)4/*18		
*11	1	1.5"	2'	(2)4/*18		
*12	1	2.5"	243'	COMM. CABLE	(1)4/*18	
*13	1	1.5"	3'	(1)4/*18		
*14	1	1.5"	5'	(2)4/*18		
*15	1	UNK	54'	UNKNOWN		[NEW] (2) 4/#18]
*16	1	UNK	116'	UNKNOWN		[NEW] (2) 4/#18]
*17	1	UNK	65'	UNKNOWN		
*18	1	UNK	UNK	UNKNOWN		
*19	1	2.5"	111'	(1)4/*18 COMM. CABLE	(2)4/*18	(1)9/*14
*20	1	2.5"	72'	(1)4/*18		
*21	1	1.5"	3'	(1)4/*18		
*22	1	2.5"	86'	COMM. CABLE	(2)4/*18	
*23	1	2.5"	96'	COMM. CABLE	(2)4/*18	
*24	1	2.5"	138'	COMM. CABLE	(2)4/*18	
*25	1	1.5"	4'		(2)4/*18	
*26	1	2.5"	4'			(1)9/*14
*27	1	UNK	UNK	COMM. CABLE		
*28	1	2.5"	67'			(1)9/*14
*29	1	2.5"	5'			(1)9/*14
*30	1	2.5"	12'			(1)9/*14
*31	1	2.5"	7'			(1)9/*14
*32	1	2.5"	11'			(1)9/*14
*33	1	UNK	UNK	COMM. CABLE		
*34	1	UNK	UNK	COMM. CABLE		

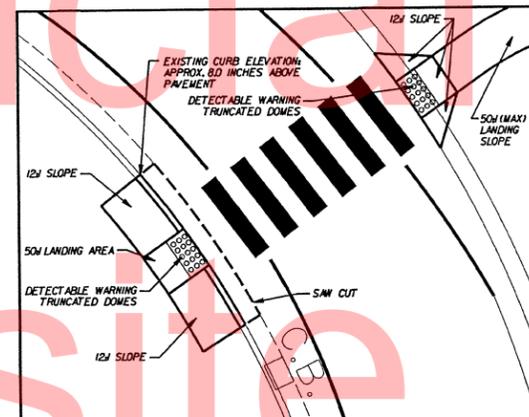
* DENOTES EXISTING



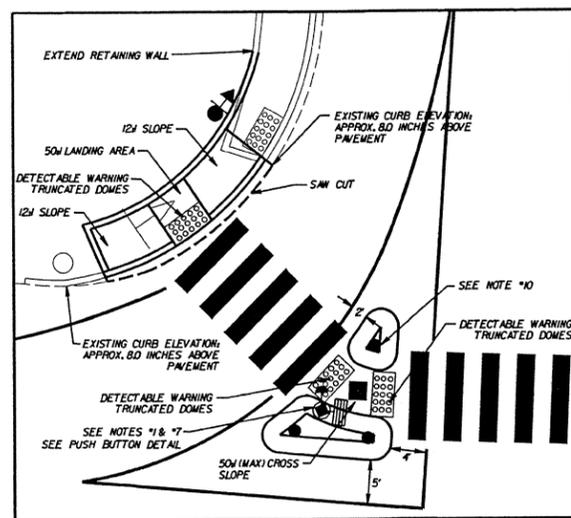
CURB DETAIL 1 (FIELD ADJUSTMENTS AS REQUIRED)
SCALE 1" = 10'



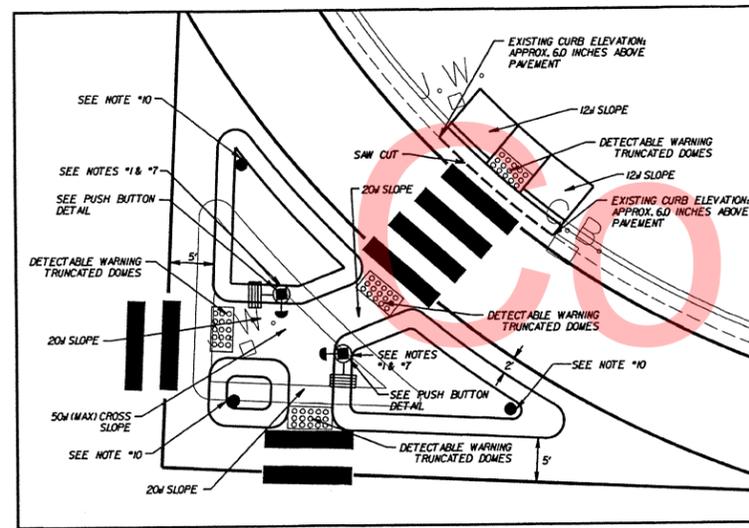
CURB DETAIL 2 (FIELD ADJUSTMENTS AS REQUIRED)
SCALE 1" = 10'



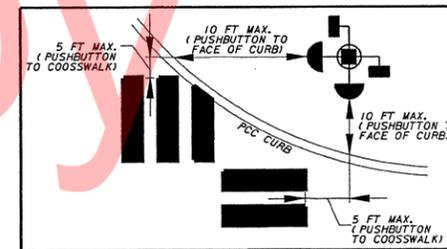
CURB DETAIL 3 (FIELD ADJUSTMENTS AS REQUIRED)
SCALE 1" = 10'



CURB DETAIL 4 (FIELD ADJUSTMENTS AS REQUIRED)
SCALE 1" = 10'

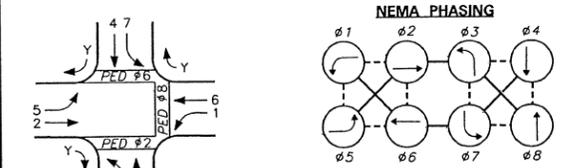


CURB DETAIL 5 (FIELD ADJUSTMENTS AS REQUIRED)
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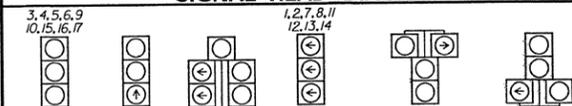


PUSH BUTTON DETAIL (TYPICAL)
NOT TO SCALE

SIGNAL PHASING



SIGNAL HEAD DIAGRAM



LEGEND

- PROPOSED SIGNAL CABINET
- EXISTING SIGNAL CABINET
- PROPOSED SIGNAL POLE BASE
- ⊙ EXISTING SIGNAL POLE BASE
- PROPOSED PEDESTRIAN POLE BASE
- ⊙ EXISTING PEDESTRIAN POLE BASE
- PROPOSED WOOD POLE
- ⊙ EXISTING UTILITY POLE
- PROPOSED JUNCTION WELL
- ⊙ EXISTING JUNCTION WELL
- J.W. EXISTING JUNCTION WELL
- ➔ PROPOSED SIGNAL HEAD
- ➔ EXISTING SIGNAL HEAD
- ➔ PROPOSED PEDESTRIAN SIGNAL HEAD
- ➔ EXISTING PEDESTRIAN SIGNAL HEAD
- ➔ PROPOSED PEDESTRIAN PUSHBUTTON
- ➔ EXISTING PEDESTRIAN PUSHBUTTON
- ➔ PROPOSED VIDEO DETECTION
- ➔ EXISTING VIDEO DETECTION
- ➔ PROPOSED MICROWAVE DETECTION
- ➔ EXISTING MICROWAVE DETECTION
- ➔ OVERHEAD SIGNING
- ➔ PROPOSED OPTICOM RECEIVER
- EXISTING OPTICOM RECEIVER
- ➔ PROPOSED MAST ARM
- ➔ EXISTING MAST ARM
- ➔ PROPOSED LUMINAIRE
- ➔ EXISTING LUMINAIRE
- PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
- EXISTING LOOP DETECTOR (TYPE 1 OR 2)
- REMOVE BY CONTRACTOR
- REMOVE BY OTHERS
- ⊙ ABANDON
- ⊙ PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
- ⊙ EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
- ⊙ PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
- ⊙ EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
- ⊙ PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
- ⊙ EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
- ⊙ PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
- ⊙ EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
- ⊙ PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
- ⊙ EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
- ⊙ PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)
- ⊙ EXISTING CABINET IDENTIFIER (TYPE OF CABINET)
- PROPOSED SPAN WIRE
- XX — EXISTING SPAN WIRE
- RIGHT-OF-WAY OR PROPERTY LINE
- ◆ PROPOSED SPAN INSULATOR
- ◇ EXISTING SPAN INSULATOR
- SERVICE PEDESTAL
- ➔ EXISTING HIGHWAY SIGN
- ➔ PROPOSED HIGHWAY SIGN
- TUBULAR MARKER

GENERAL SIGNAL NOTES

RECOMMENDED Lisa Delyaur DATE: 10/08/2010

RECOMMENDED _____ DATE: _____

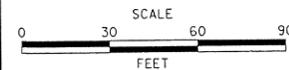
RECOMMENDED _____ DATE: _____

APPROVED TRAFFIC ENGINEER Mike Z... DATE: 10/11/10

APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER Paul DATE: 10/11/10



ADDENDUM / REVISIONS
 [] INSTALLED SYSTEM LOOPS
 GG - 4/9/12 - T201206105



PAVEMENT & REHABILITATION, NORTH VI, 2010

CONTRACT T201006106
 COUNTY NEW CASTLE

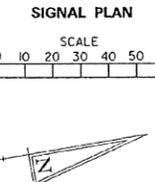
PERMIT NO. N367
 DESIGNED BY: ECS
 CHECKED BY: LYD

SR 273 (CHRISTIANA BYPASS)
 @ EAGLE RUN ROAD/
 CHAPMAN ROAD

SHEET NO. 2
 TOTAL SHTS. 2

SIGNAL NOTES

- Detection - 40 MPH - 4 seconds passage time at 200 feet from stop bar.
- Loop detectors:
 - Type #1 - 5' x 7' - to be installed on main street through movements.
 - Type #2 - 6' x 25' - to be installed on main street left turn movements.
 - Type #3 - 6' x 25' - to be installed on side street through and left turn movements.
- All signal poles will be 32 feet, except where shown.
- Electric service to be provided from Conduit run #102.
- All signal equipment removed from a project is to be returned to DelDOT Traffic - Dover, Delaware.
- Pole bases, cabinet bases, and conduit junction wells to be removed in accordance with Section 201 and 202 of the standard specifications or as directed by engineer. Existing conduit is to be abandoned.
- All galvanized conduit (GCR) shall be reamed and threaded. All GCR shall be threaded together with approved couplings. Set screw, bolted, and compression fitting are not acceptable.
- All underground and overhead utilities shown on these plans are schematic only and may not be complete. The contractor shall be responsible for notifying Miss Utility, and/or the appropriate utility prior to the beginning of construction for the utility markouts. If the contractor perceives that a conflict between utilities and the traffic signal will occur, the contractor shall notify DelDOT Traffic immediately before construction.
- Refer to striping plan sheets for pavement marking details.
- Install metallic identification tape 12 to 18 inches below grade above all non-metallic conduits.
- Existing signal conduit is not drawn to scale, and the actual conduit sized and locations have not been verified.



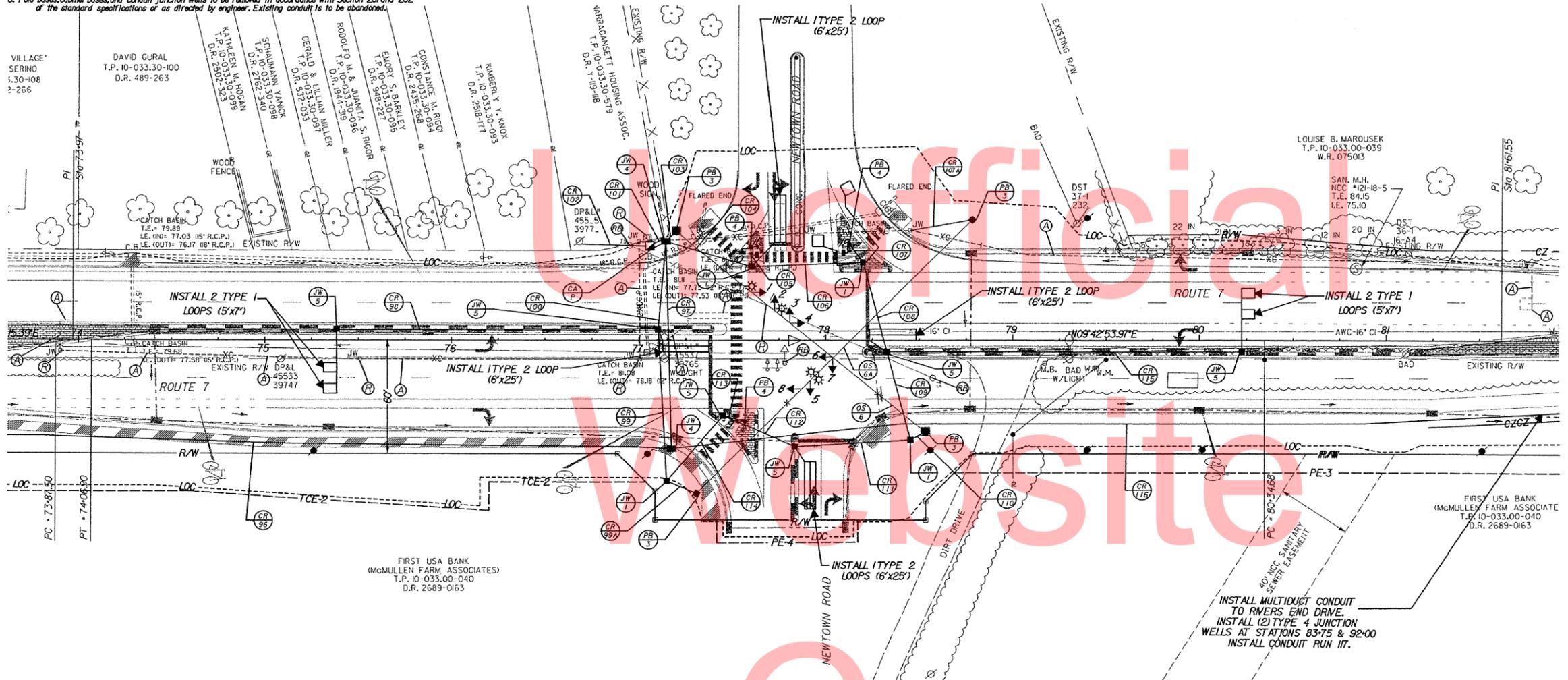
PERMIT NO. N-485	CONTRACT 20-126-01	COUNTY NEW CASTLE	SHEET NO. 142	TOTAL SHTS. 163
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**SR 7, US 40 TO
NEWTOWN ROAD
DEL RT 7 @ NEWTOWN ROAD**

REVISIONS

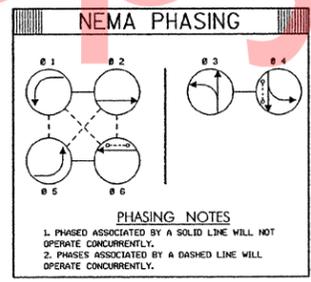
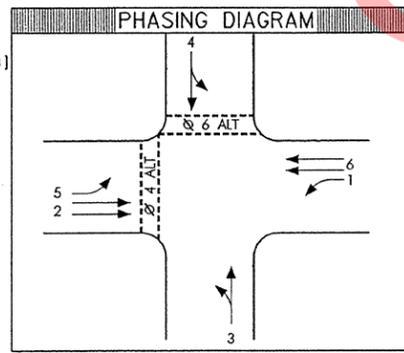
1) INSTALLED SYSTEM LOOPS
GG - 4/10/12 - T201206105

LEGEND	
	OPTICOM RECEIVER (PRE-EMPTION DEVICE)
	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
	EXISTING POLE BASE
	PROPOSED PEDESTAL BASE
	EXISTING PEDESTAL BASE
	PROPOSED POLE
	EXISTING D.P. & L. POLE
	PROPOSED CABINET BASE
	EXISTING CABINET BASE
	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
	RIGHT-OF-WAY OR PROPERTY LINE
	PROPOSED LOOP DETECTOR (TYPE 1 OR 2)
	EXISTING LOOP DETECTOR
	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
	EXISTING JUNCTION WELL
	PROPOSED JUNCTION WELL
	PROPOSED SPAN WIRE
	EXISTING SPAN WIRE
	PROPOSED SIGNAL HEAD
	EXISTING SIGNAL HEAD
	PROPOSED PEDESTRIAN SIGNAL HEAD
	EXISTING PEDESTRIAN SIGNAL HEAD
	PROPOSED MAST ARM
	REMOVE
	ABANDON
	REMOVE EXISTING CONCRETE BASE 16" BELOW GROUND LEVEL AND COVER, OR AS DIRECTED BY SIGNAL PLAN
	CONDUIT RUN IDENTIFIER (RUN NUMBER)
	OVERHEAD SPAN IDENTIFIER (SPAN NUMBER)
	EXISTING CONDUIT
	PROPOSED CONDUIT
	PROPOSED OVERHEAD SIGN
	PROPOSED PEDESTRIAN PUSH BUTTON



CR#	# OF CONDUITS	SIZE (IN)	LENGTH (FT)	AMOUNT AND TYPE OF CABLE / WIRE
96	1	4MCR	516	12COMM.CABLE
97	1	4MCR	110	12COMM.CABLE
98	1	2.5CR	172	114"/18
99	1	2.5CR	81	114"/14 (2)4"/18 (1)9"/14 (1)16"/14
99A	1	2.5CR	18	114"/18 116"/14
100	1	2.5CR	47	114"/14 (4)4"/18 (1)9"/14 (1)16"/14
101	3	12.5CR/11MCR	9	144"/14 (8)4"/18 (3)9"/14 (2)16"/14 12COMM.CABLE NEW (2) 4/#18
102	1	1.5CR	38	112"/8 U.F.W./GROUND
103	1	2.5CR	8	114"/18 116"/14
104	1	2.5CR	49	134"/14 (3)4"/18 (2)9"/14 [NEW (2) 4/#18]
105	1	2.5CR	11	124"/14 119"/14
106	1	2.5CR	60	114"/14 (2)4"/18 (1)9"/14 [NEW (2) 4/#18]
107A	1	2.5CR	34	EMPTY
107	1	2.5CR	13	114"/14 (1)9"/14
108	1	2.5CR	49	124"/18
109	1	2.5CR	49	EMPTY
110	1	2.5CR	9	EMPTY
111	1	2.5CR	62	EMPTY
112	1	2.5CR	42	114"/18
113	1	2.5CR	12	114"/14 (1)9"/14
114	1	2.5CR	47	114"/14 (1)4"/18 (1)9"/14
115	1	2.5CR	190	114"/18
116	1	4MCR	659	12COMM.CABLE
117	1	4MCR	825	12COMM.CABLE
OS6			171	114"/18 116"/14
OS6A			207	114"/18 116"/14

CR - GALVANIZED OR PVC CONDUIT; MCR - MULTIDUCT CONDUIT; OS - OVERHEAD SPAN WIRE

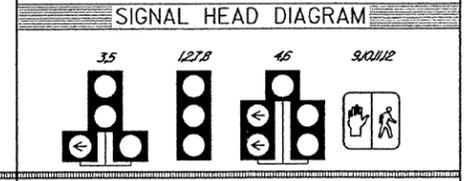


PHASING NOTES

- PHASE ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.
- PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.

OR&A
Orth - Rodgers & Associates, Inc.
TRANSPORTATION ENGINEERS and PLANNERS
301 Lindenwood Drive, Suite 100, Mohrville, PA 17055
(610)401-9100

APPROVED BY
John S. Doh
2/27/04
DATE



RECOMMENDED _____ 200 _____
RECOMMENDED _____ 200 _____
RECOMMENDED _____ 200 _____

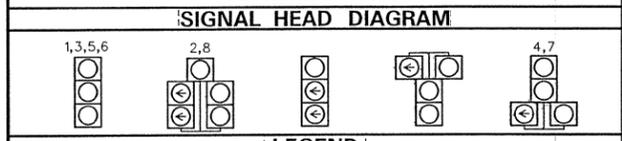
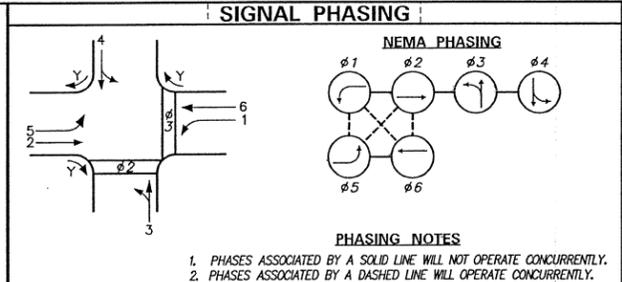
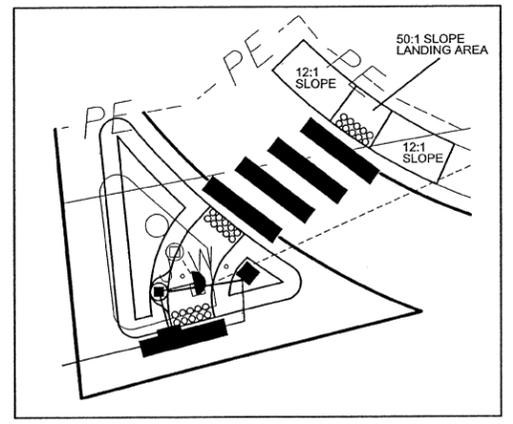
APPROVED TRAFFIC ENGINEER
Jacob Lewin Darbi
4/19/04
DATE

APPROVED for INSTL.
Chula
4/19/04
DATE
CHIEF TRAFFIC ENGINEER

CONDUIT RUN SCHEDULE

CR#	# OF CONDUITS	SIZE	LENGTH	AMOUNT AND TYPE OF CABLE / WIRE
1	1	1.5"	15'	(1)2#8 U.F.W./ GROUND [NEW (1)*6 AWG]
2	3	2.5"	20'	(2)1/2#14 <REMOVE (1)1/2#14> [NEW (4)1/2#18 (1)*6 AWG]
3	1	2.5"	10'	(6)1/2#18 (2)1/2#14 <REMOVE (2)1/2#14> [NEW (3)1/2#14 (1)*6 AWG]
4	1	2.5"	18'	(2)1/2#14 (1)*6 AWG
5	1	2.5"	109'	(1)1/2#18 <REMOVE (1)1/2#18 (1)1/2#14> [NEW (1)1/2#14 (1)*6 AWG]
6	1	2.5"	45'	<REMOVE (1)1/2#14> [NEW (1)1/2#14 (1)*6 AWG]
7	1	2.5"	3'	(1)1/2#14 (1)*6 AWG
8	1	2.5"	57'	(3)1/2#18 <REMOVE (1)1/2#14> [NEW (1)1/2#14 (1)*6 AWG]
9	1	2.5"	52'	(2)1/2#18 <REMOVE (1)1/2#14> [NEW (1)1/2#14 (1)*6 AWG]
10	1	2.5"	33'	(1)1/2#18 <REMOVE (1)1/2#14> [NEW (1)1/2#14 (1)*6 AWG]
11	1	2.5"	9'	(1)1/2#14 (1)*6 AWG

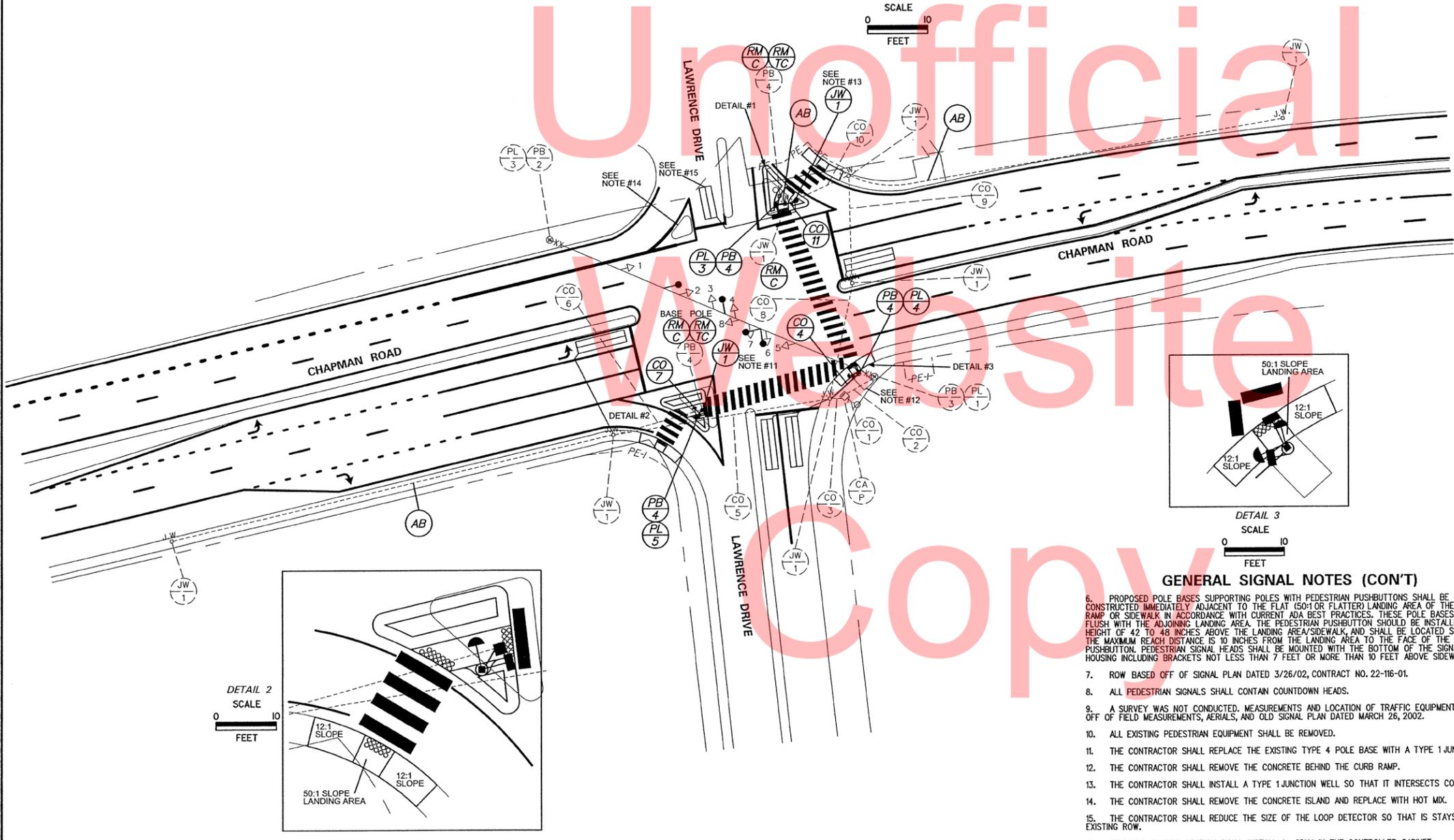
* DENOTES EXISTING



LEGEND

(AB)	ABANDON	(OH)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)	(OH)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
(CA)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)	(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)	(PL)	EXISTING POLE IDENTIFIER (# OF POLE)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(PL)	PROPOSED POLE IDENTIFIER (# OF POLE)
(JW)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(RM)	REMOVE BY CONTRACTOR
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM)	REMOVE BY OTHERS
(MA)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	□	□
LUMINAIRE	◆	◆
MAST ARM	▶	▶
MICROWAVE DETECTION	▲	▲
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	⊥	⊥
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	⊥	⊥
RIGHT-OF-WAY	---	---R/W---
SERVICE PEDESTAL	□	□
SIGNAL CABINET	□	□
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	—XX—	—XX—
UTILITY POLE	⊗	⊗
VIDEO DETECTION	⊞	⊞



- GENERAL SIGNAL NOTES (CON'T)**
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSHBUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (50:1 OR FLATTER) LANDING AREA OF THE CURB RAMP OR SIDEWALK IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSHBUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 TO 48 INCHES ABOVE THE LANDING AREA/SIDEWALK, AND SHALL BE LOCATED SUCH THAT THE MAXIMUM REACH DISTANCE IS 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSHBUTTON. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE SIDEWALK LEVEL.
 - ROW BASED OFF OF SIGNAL PLAN DATED 3/26/02, CONTRACT NO. 22-116-01.
 - ALL PEDESTRIAN SIGNALS SHALL CONTAIN COUNTDOWN HEADS.
 - A SURVEY WAS NOT CONDUCTED. MEASUREMENTS AND LOCATION OF TRAFFIC EQUIPMENT BASED OFF OF FIELD MEASUREMENTS, AERIALS, AND OLD SIGNAL PLAN DATED MARCH 26, 2002.
 - ALL EXISTING PEDESTRIAN EQUIPMENT SHALL BE REMOVED.
 - THE CONTRACTOR SHALL REPLACE THE EXISTING TYPE 4 POLE BASE WITH A TYPE 1 JUNCTION WELL.
 - THE CONTRACTOR SHALL REMOVE THE CONCRETE BEHIND THE CURB RAMP.
 - THE CONTRACTOR SHALL INSTALL A TYPE 1 JUNCTION WELL SO THAT IT INTERSECTS CO#10.
 - THE CONTRACTOR SHALL REMOVE THE CONCRETE ISLAND AND REPLACE WITH HOT MIX.
 - THE CONTRACTOR SHALL REDUCE THE SIZE OF THE LOOP DETECTOR SO THAT IS STAYS WITHIN THE EXISTING ROW.
 - DELDOT'S TRAFFIC SECTION SHALL INSTALL A CDMA IN THE CONTROLLER CABINET.

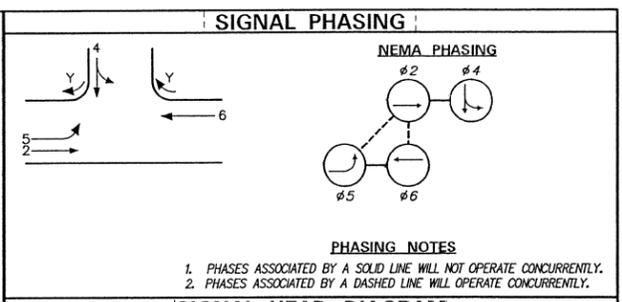
- GENERAL SIGNAL NOTES**
- ALL SIGNAL POLES WILL BE 28 FEET, EXCEPT WHERE SHOWN.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

RECOMMENDED _____ DATE: _____	RECOMMENDED <i>[Signature]</i> DATE: 9/14/12	RECOMMENDED <i>[Signature]</i> DATE: 9/14/12	APPROVED TRAFFIC ENGINEER <i>[Signature]</i> DATE: 9/14/12	APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER <i>[Signature]</i> DATE: 9/17/12
<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p> <p>ADDENDUM / REVISIONS</p> <p>1 CHANGED CONTRACT# T201206112 TO T201206105, MS-3/14</p>		<p>PAVEMENT & REHABILITATION NORTH V, 2012</p> <p>SCALE 0 30 60 90 FEET</p>		<p>CONTRACT T201206105 COUNTY NEW CASTLE</p> <p>PERMIT NO. N492</p> <p>DESIGNED BY: GG</p> <p>CHECKED BY:</p> <p>SIGNAL PLAN CHAPMAN ROAD @ LAWRENCE DRIVE</p> <p>SHEET NO. 1 TOTAL SHTS. 1</p>

W:\MSV\CELLS\PROJ\DEV\SB.CEL

CONDUIT RUN SCHEDULE				
CR#	# OF CONDUITS	SIZE	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE
1	1	1.5"	50'	(1)2/*B U.F.W/ GROUND
2	1	2.5"	8'	(2)16/*H (1)4/*B [NEW (3)4/*B (1)*6 AWG]
3	1	2.5"	10'	<REMOVE (1)4/*B> [NEW (4)4/*B (1)*6 AWG]
4	1	4.0"	100'	(3)4/*B (1)*6 AWG
5	1	4.0"	78'	(2)4/*B (1)*6 AWG

* DENOTES EXISTING

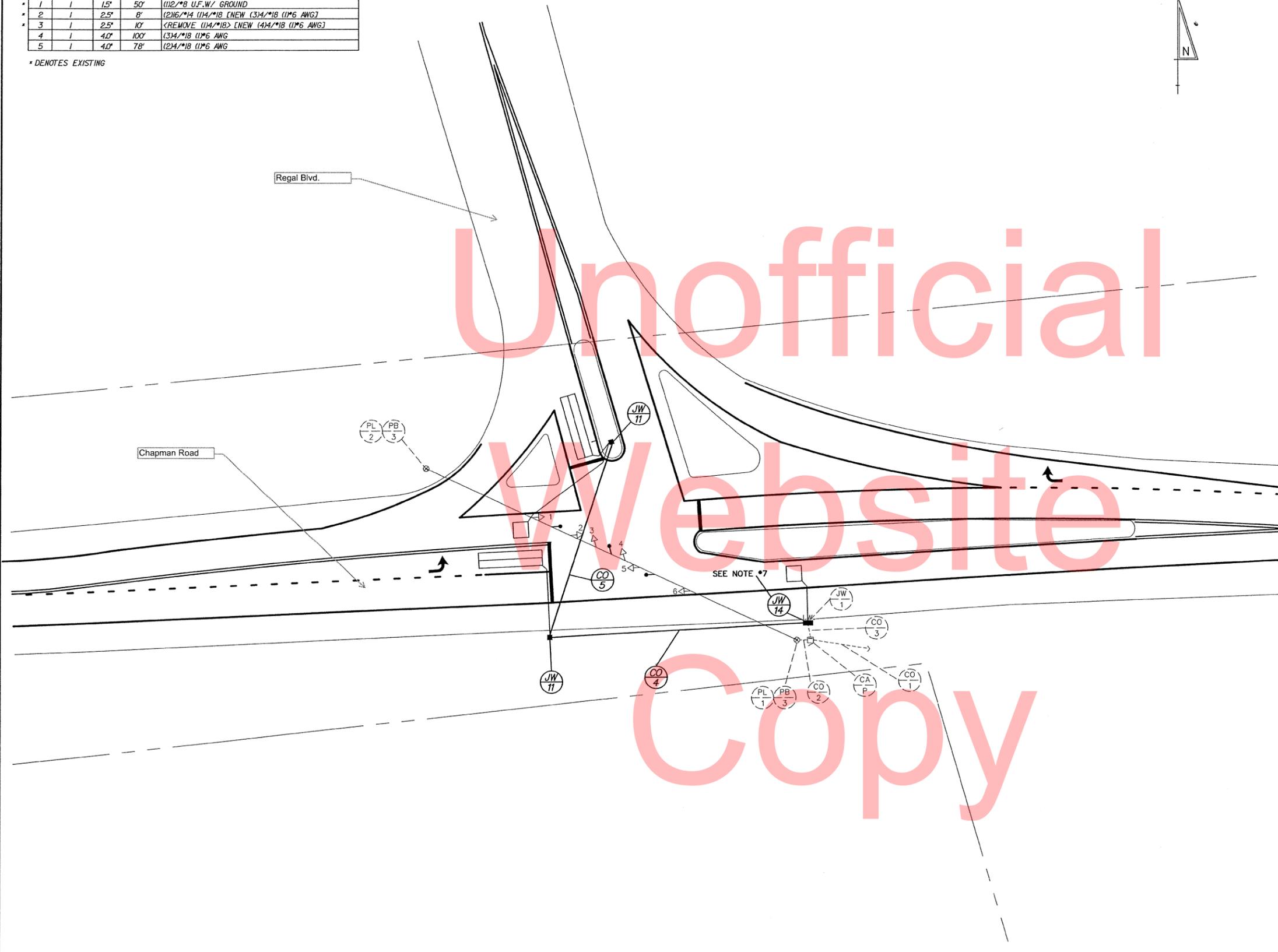


LEGEND

(AB)	ABANDON	(OP1)	EXISTING OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(CA)	EXISTING CABINET IDENTIFIER (TYPE OF CABINET)	(OP2)	PROPOSED OVERHEAD RUN IDENTIFIER (* OF OVERHEAD RUN)
(CB)	PROPOSED CABINET IDENTIFIER (TYPE OF CABINET)	(PB)	EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO)	EXISTING CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)	(PB2)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
(CO2)	PROPOSED CONDUIT RUN IDENTIFIER (* OF CONDUIT RUN)	(PL)	EXISTING POLE IDENTIFIER (* OF POLE)
(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(PL2)	PROPOSED POLE IDENTIFIER (* OF POLE)
(JW2)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)	(RM)	REMOVE BY CONTRACTOR
(MA)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM2)	REMOVE BY OTHERS
(MA2)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)	(RM3)	REMOVE BY TRAFFIC CONTRACTOR

	EXISTING SYMBOL	PROPOSED SYMBOL
JUNCTION WELL	J.W.	■
LOOP DETECTOR, TYPE 1	□	□
LOOP DETECTOR, TYPE 2	▤	▤
LUMINAIRE	⊙	⊙
MAST ARM	↗	↗
MICROWAVE DETECTION	⊙	⊙
OPTICOM RECEIVER	○	○
OVERHEAD SIGNING	⊥	⊥
PEDESTRIAN POLE/BASE	⊙	⊙
PEDESTRIAN PUSHBUTTON	→	→
PEDESTRIAN SIGNAL HEAD	⊥	⊥
RIGHT-OF-WAY	---	-R/W-
SERVICE PEDESTAL	□	□
SIGNAL CABINET	▣	▣
SIGNAL HEAD	→	→
SIGNAL POLE/BASE	⊙	⊙
SPAN INSULATOR	◇	◇
SPAN WIRE	—XX—	—XX—
UTILITY POLE	⊙	⊙
VIDEO DETECTION	⊙	⊙

- GENERAL SIGNAL NOTES**
- ALL SIGNAL POLES WILL BE 28 FEET, EXCEPT WHERE SHOWN.
 - CO #1 IS NOT DRAWN TO SCALE, NOR IS THE DIRECTION NECESSARILY CORRECT.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
 - CONTRACTOR SHALL REPLACE THE TYPE 1 JUNCTION WELL WITH A TYPE 14 JUNCTION WELL.
 - DELDOT'S TRAFFIC SECTION SHALL INSTALL A CDMA IN THE CONTROLLER CABINET.

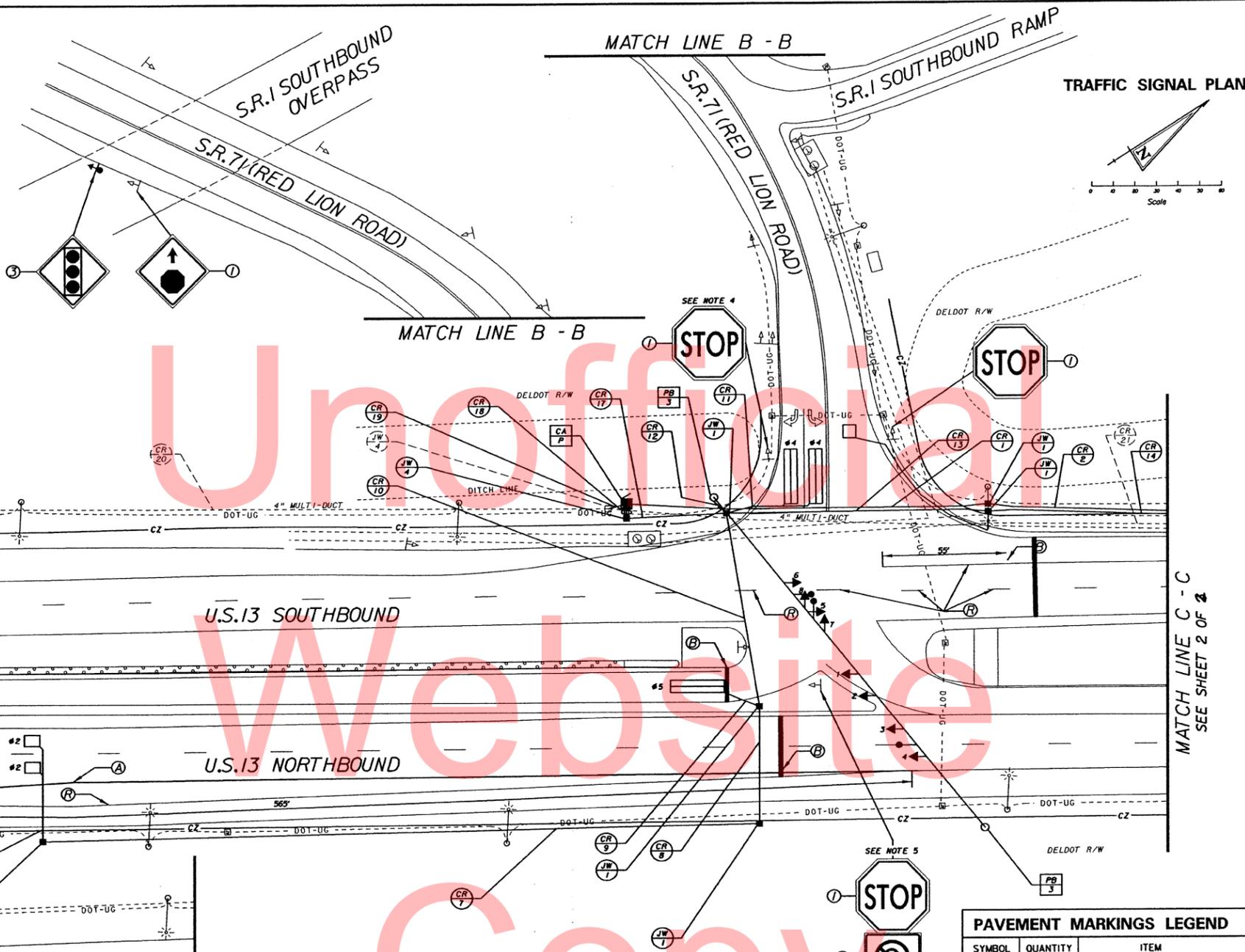


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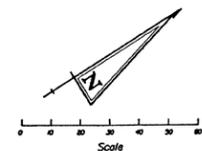
RECOMMENDED _____ DATE: _____	RECOMMENDED <i>W</i> DATE: 9/14/12	RECOMMENDED <i>M. C. Adams</i> DATE: 9/14/12	APPROVED TRAFFIC ENGINEER <i>Neil J. ...</i> DATE: 9/14/12	APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER <i>Quill</i> DATE: 9/17/12		
<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>		<p>ADDENDUM / REVISIONS</p> <p>1) CHANGED CONTRACT# T201206112 TO T201206105, MS-3/14</p>	<p>SCALE</p> <p>0 20 40 60</p> <p>FEET</p>	<p>CONTRACT T201206105</p> <p>COUNTY NEW CASTLE</p> <p>PERMIT NO. N493</p> <p>DESIGNED BY: GG</p> <p>CHECKED BY:</p>	<p>SIGNAL PLAN</p> <p>CHAPMAN ROAD</p> <p>@ REGAL BOULEVARD</p>	<p>SHEET NO. 1</p> <p>TOTAL SHTS. 1</p>

SIGNAL NOTES

1. DETECTION - 55 MPH - 4 SECONDS PASSAGE TIME AT 350 FEET FROM STOP BAR.
2. LOOP DETECTORS:
TYPE "1" - 5' x 7' - TO BE INSTALLED ON MAIN STREET THROUGH MOVEMENTS.
TYPE "2" - 6' x 25' - TO BE INSTALLED ON MAIN STREET LEFT TURN MOVEMENTS.
TYPE "2" - 6' x 25' - TO BE INSTALLED ON SIDE STREET THROUGH AND LEFT TURN MOVEMENTS.
3. ALL SIGNAL POLES WILL BE 32 FEET.
4. REMOVE EXISTING RH "STOP" SIGN. EXISTING D3 SIGNS TO REMAIN.
5. REMOVE EXISTING RH "STOP" SIGN. MOVE EXISTING R3-4 "NO U-TURN" SIGN TO TOP OF EXISTING POST.
6. ALL EXISTING SIGNS TO REMAIN UNLESS OTHERWISE NOTED.
7. ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
8. ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.
9. REMOVE EXISTING "EXPRESSWAY ENDS WATCH FOR CROSSING TRAFFIC" SIGN.
10. INSTALL "EXPRESSWAY ENDS SIGNAL 500 FEET" SIGN AS SHOWN.



TRAFFIC SIGNAL PLAN



DELAWARE DEPARTMENT OF TRANSPORTATION
TRAFFIC SECTION

DOVER DELAWARE

PERMIT NO. N712	CONTRACT NO. 23-00-01	FED. AID NO.	SHEET NO. 1	TOTAL 2
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U.S.13 @ S.R.71

COUNTY OF NEW CASTLE

PRELTRACING	DATE	DESIGN LYD	DATE 10/13/2005
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REVISION

INSTALLED SYSTEM LOOP
GG - 41012 - T201206105

LEGEND

PROPOSED SIGNAL CABINET	(P)	REMOVE
EXISTING SIGNAL CABINET	(A)	ABANDON
PROPOSED POLE BASE	(PB)	REMOVE EXISTING CONCRETE BASE
EXISTING POLE BASE	(PB)	PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE)
PROPOSED PEDESTAL BASE	(PB)	EXISTING PEDESTAL BASE IDENTIFIER (TYPE OF POLE BASE)
EXISTING PEDESTAL BASE	(PB)	PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
PROPOSED WOOD POLE	(JW)	EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL)
EXISTING WOOD POLE	(JW)	PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
UTILITY POLE	(JW)	EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN)
PROPOSED JUNCTION WELL	(CR)	PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
EXISTING JUNCTION WELL	(CR)	EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN)
PROPOSED SIGNAL HEAD	(CR)	PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM)
EXISTING SIGNAL HEAD	(CR)	EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM)
PROPOSED PEDESTRIAN SIGNAL HEAD	(MA)	PROPOSED CABINET BASE IDENTIFIER (TYPE OF CABINET)
EXISTING PEDESTRIAN SIGNAL HEAD	(MA)	EXISTING CABINET BASE IDENTIFIER (TYPE OF CABINET)
PROPOSED VIDEO DETECTION	(CA)	METERED SERVICE PEDESTAL
EXISTING VIDEO DETECTION	(CA)	EXISTING HIGHWAY SIGN
PROPOSED MICROWAVE DETECTION	(XX)	PROPOSED HIGHWAY SIGN
EXISTING MICROWAVE DETECTION	(XX)	
OVERHEAD SIGNING	(XX)	
PROPOSED OPTION RECEIVER	(XX)	
EXISTING OPTION RECEIVER	(XX)	
PROPOSED MAST ARM	(XX)	
EXISTING MAST ARM	(XX)	
PROPOSED LUMINAIRE	(XX)	
EXISTING LUMINAIRE	(XX)	
PROPOSED SPAN WIRE	(XX)	
EXISTING SPAN WIRE	(XX)	
RIGHT-OF-WAY OR PROPERTY LINE	(XX)	
PROPOSED LOOP DETECTOR (TYPE 1 OR 2)	(XX)	
EXISTING LOOP DETECTOR	(XX)	

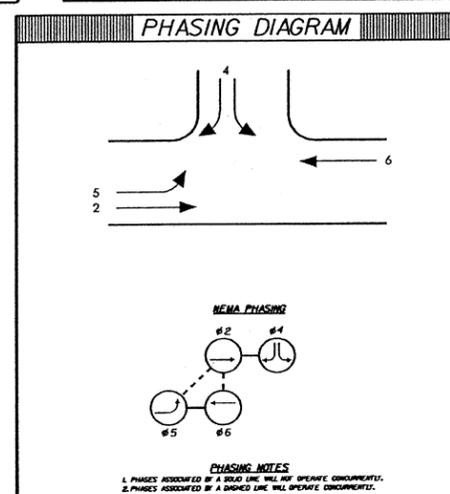
PAVEMENT MARKINGS LEGEND

SYMBOL	QUANTITY	ITEM
(A)	565 LF.	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE, 4"
(B)	115 S.F.	PERMANENT PAVEMENT STRIPING, ALKYD THERMOPLASTIC, WHITE, 16"

CONDUIT RUN SCHEDULE

CR#	# OF CONDUITS	SIZE	LENGTH	AMOUNT AND TYPE OF CABLE/ WIRE
1	1	2.0"	166'	(1) 1/2" x 6 U.F.W./ GROUND
2	1	2.0"	315'	(1) 1/2" x 6 U.F.W./ GROUND
3	1	2.0"	185'	(1) 1/2" x 6 U.F.W./ GROUND
4	1	2.0"	6'	(1) 1/2" x 6 U.F.W./ GROUND
5	1	2.0"	26'	(1) 1/2" x 6 U.F.W./ GROUND
6	1	1.5"	7'	LOOP DETECTOR WIRE
7	1	2.5"	332'	(1) 1/4" x 1/8
8	1	2.5"	54'	(1) 1/4" x 1/8
9	1	1.5"	8'	LOOP DETECTOR WIRE
10	1	2.5"	90'	(1) 1/4" x 1/8
11	1	1.5"	13'	LOOP DETECTOR WIRE
12	1	2.5"	9'	(2) 1/8" x 1/4 (3) 1/4" x 1/8
13	1	2.5"	121'	(1) 1/4" x 1/8 (1) 1/4" x 1/4 (NEW) (1) 4#18
14	1	2.5"	315'	(1) 1/4" x 1/8 (1) 1/4" x 1/4
15	1	1.5"	2'	LOOP DETECTOR WIRE
16	1	2.5"	139'	(1) 1/4" x 1/4
17	1	2.5"	46'	(2) 1/8" x 1/4 (1) 1/4" x 1/8 (1) 1/4" x 1/4 (NEW) (1) 4#18
18	3	2.5"	7'	(2) 1/8" x 1/4 (1) 1/4" x 1/8 (1) 1/4" x 1/4 (NEW) (1) 4#18
19	1	2.5"	8'	SINGLE-MODE FIBER OPTIC CABLE, 12 COUNTS
20	1	4.0"	-	SINGLE-MODE FIBER OPTIC CABLE, 12 COUNTS
21	1	4.0"	800'	SINGLE-MODE FIBER OPTIC CABLE, 12 COUNTS
22	1	4.0"	1100'	SINGLE-MODE FIBER OPTIC CABLE, 12 COUNTS
23	1	2.5"	7'	(1) 1/4" x 1/4

- SIGNING LEGEND By State Forces
1. REMOVE EXISTING SIGN
 2. EXISTING SIGN TO REMAIN
 3. PLACE NEW SIGN
 4. RENEW EXISTING SIGN
 5. REPOSITION EXISTING SIGN



SIGNAL HEAD DIAGRAM

RECOMMENDED Feb 13 2007

RECOMMENDED 20

RECOMMENDED 20

APPROVED TRAFFIC ENGINEER: *Whit Jozz* DATE: 2/13/07

APPROVED for INSTL. CHIEF TRAFFIC ENGINEER: *Calvin* DATE: 3/22/07

MATCH LINE A - A

MATCH LINE C - C
SEE SHEET 2 OF 2

MATCH LINE A - A

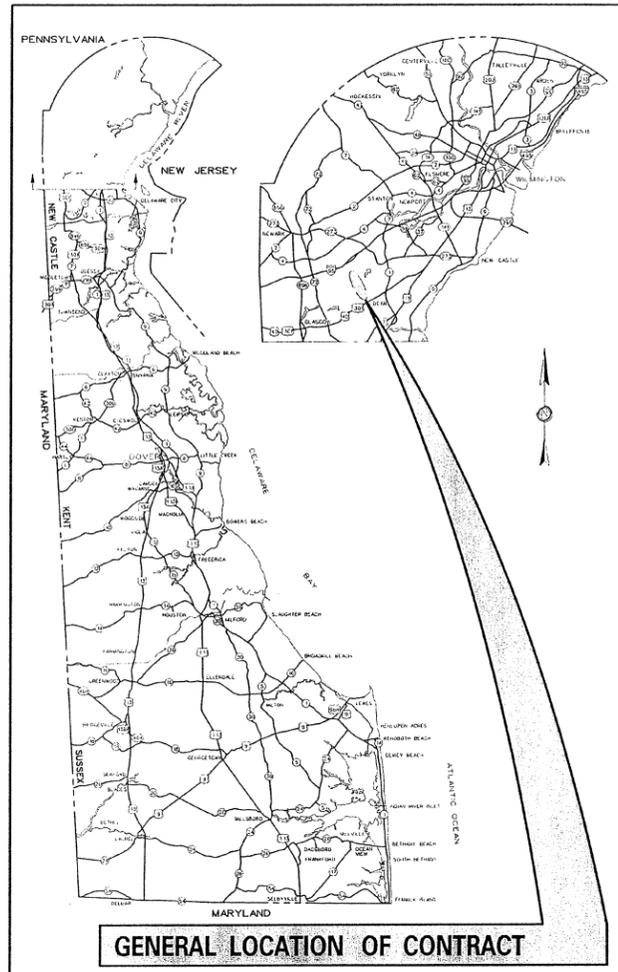
* DENOTES EXISTING CONDUITS

T201206105

Striping Plans

Website

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THE STATE OF DELAWARE
DEPARTMENT OF TRANSPORTATION



PROPOSED SIGNING AND STRIPING PLANS FOR:
PAVEMENT AND REHABILITATION
NORTH V, 2012
WALTHER ROAD,
FROM ROUTE 40 TO OLD BALTIMORE PIKE
CONTRACT NUMBER: T201206105
FEDERAL AID PROJECT NUMBER: N/A
COUNTY: NEW CASTLE M.R. #: N346

U.S. CUSTOMARY
UNITS
FINAL PLANS

INDEX OF SHEETS	
SHEET NO	TABLE OF CONTENTS
1	TITLE SHEET
2	PLAN SHEET INDEX
3	NOTES AND DETAILS
4-13	PROPOSED SIGNING AND STRIPING PLANS
14-16	SIGN SCHEDULE

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TOTAL SHEETS: 16

APPROVED DESIGN EXCEPTIONS			
DESIGN PARAMETER	REQUIRED	PROVIDED	DATE

ADDENDA & REVISIONS	
DESCRIPTION	NAME & DATE

PREPARED BY
THE CONSULTING FIRM OF



1352 MARROWS ROAD
SUITE 100
NEWARK, DE 19711
TELEPHONE: (302) 731-9176
FAX: (302) 731-7807

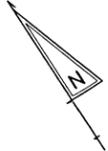
Tiffany T. Lawrence
RECOMMENDED DATE: June 9, 2014

RECOMMENDED _____ DATE _____	APPROVED <i>John SAINTIL</i> DATE: 06/09/2014				
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L-AS - REVISED: 8/7/2008
W:\MS\REF FRENCES\SPR\REF V\T.C.DGN

PLAN SHEET INDEX CROSS REFERENCE

PROPOSED SIGNING AND STRIPING PLANS	4 13
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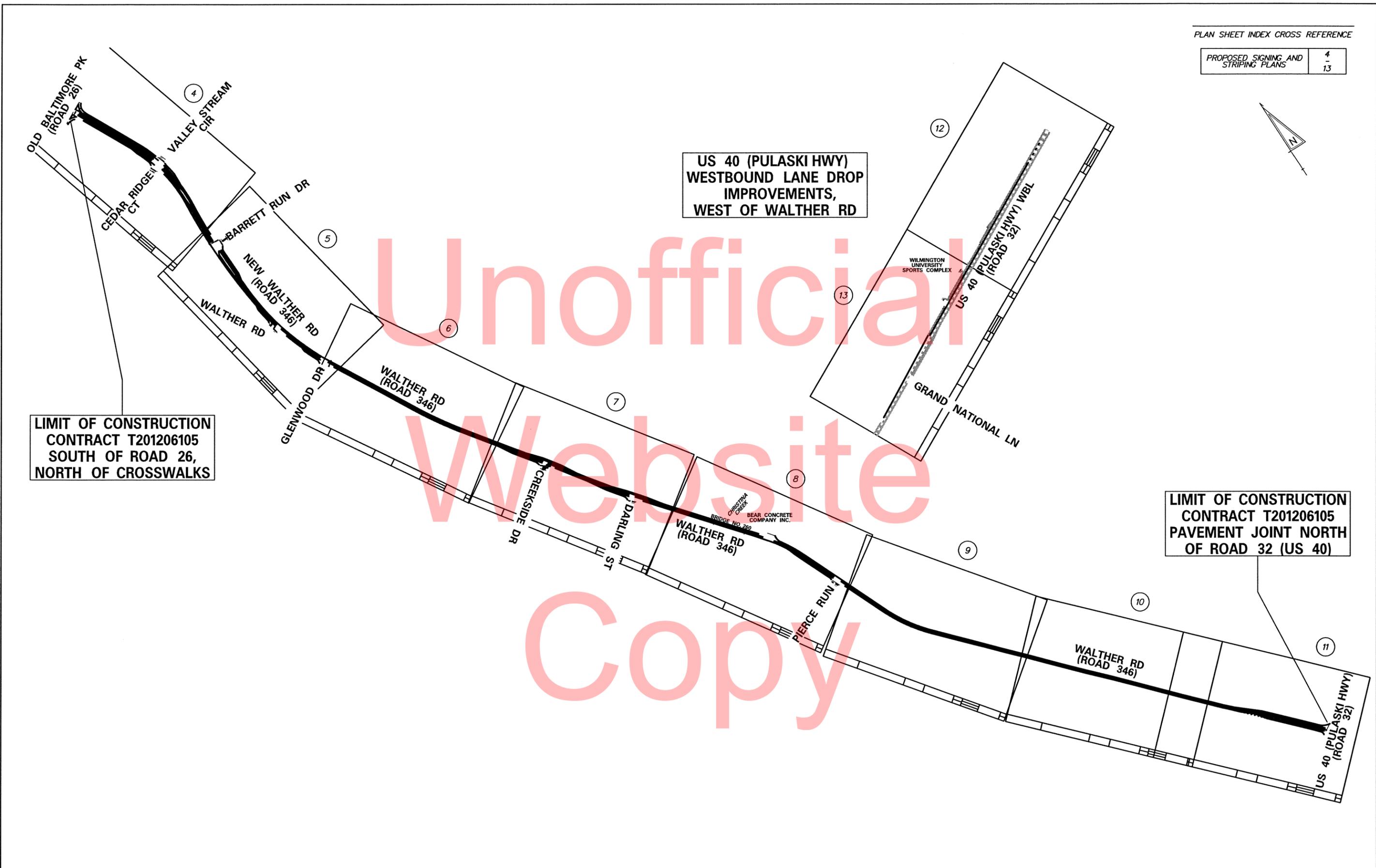


US 40 (PULASKI HWY)
WESTBOUND LANE DROP
IMPROVEMENTS,
WEST OF WALTHER RD

LIMIT OF CONSTRUCTION
CONTRACT T201206105
SOUTH OF ROAD 26,
NORTH OF CROSSWALKS

LIMIT OF CONSTRUCTION
CONTRACT T201206105
PAVEMENT JOINT NORTH
OF ROAD 32 (US 40)

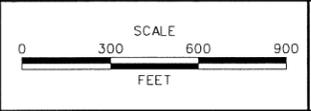
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DELAWARE
DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



PAVEMENT AND REHABILITATION
NORTH V, 2012
WALTHER ROAD,
FROM ROUTE 40 TO OLD BALTIMORE PIKE

CONTRACT	T201206105
COUNTY	NEW CASTLE
BRIDGE NO.	
DESIGNED BY:	AF, JVB
CHECKED BY:	JFL, JAB

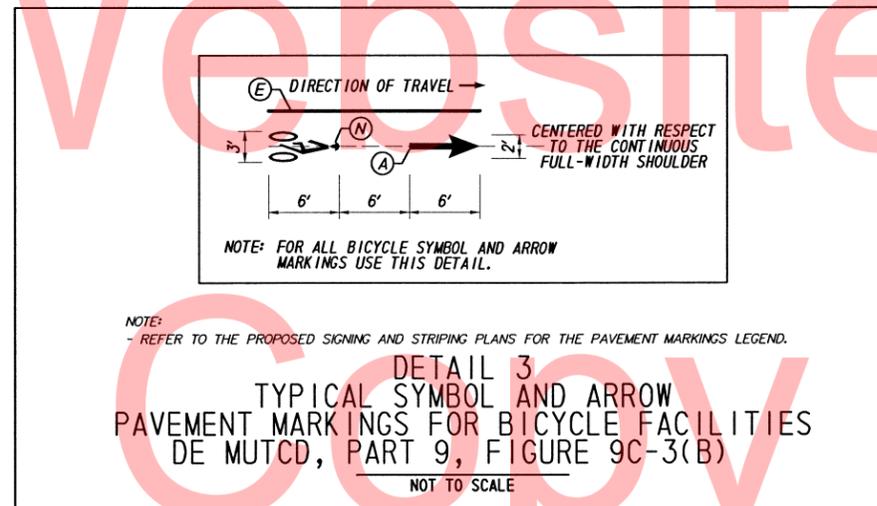
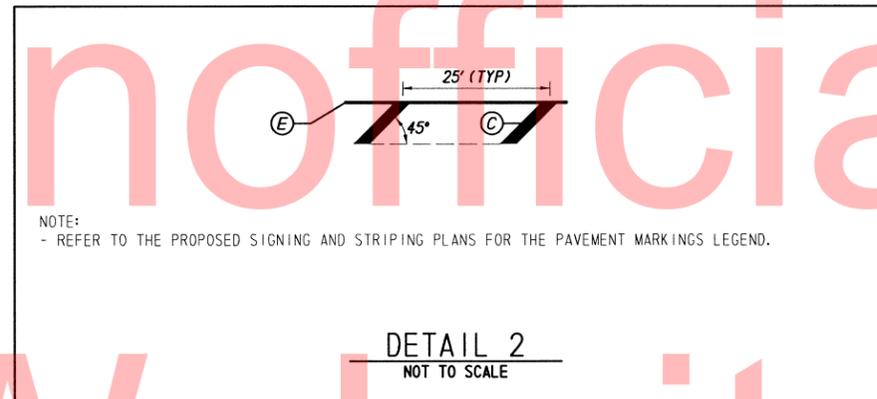
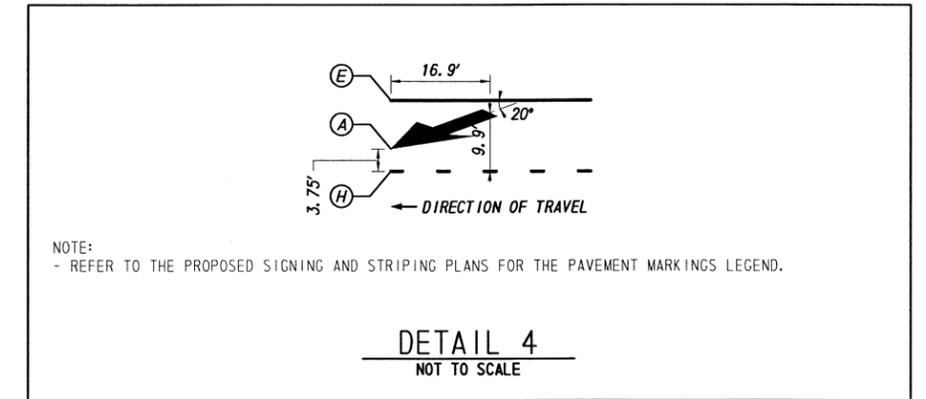
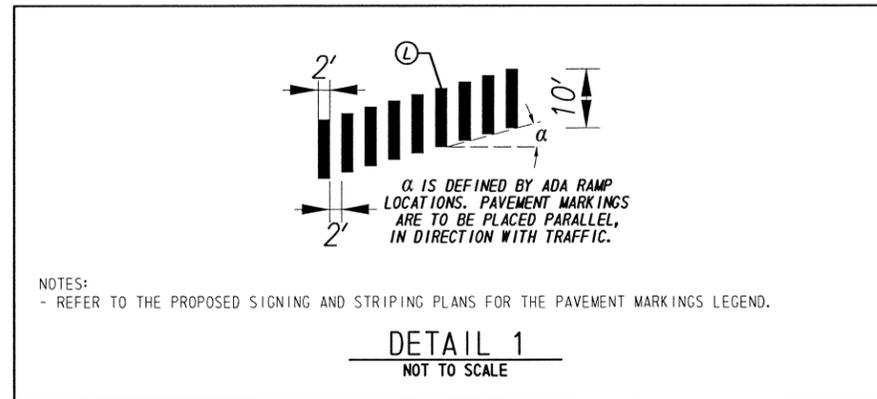
PLAN SHEET INDEX

SHEET NO.	2
TOTAL SHTS.	16

GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS

1. ROADWAY GEOMETRY IS BASED ON AERIAL PHOTOGRAPHY AND FIELD VERIFICATION.
2. ALL PAVEMENT MARKINGS SHOWN ARE PROPOSED. CONTRACTOR TO FOLLOW PROPOSED SIGNING AND PAVEMENT MARKINGS LEGEND. EXISTING PAVEMENT MARKINGS SHOWN ON AERIAL ARE NOT TO BE REPRODUCED UNLESS CALLED OUT AS SUCH, PER THIS PLAN.
3. REFERENCE CENTER LINE OR MEDIAN EDGE LINE FOR PAVEMENT MARKINGS LAYOUT.
4. PROPOSED RAMPS ARE SHOWN FOR FUTURE CROSSWALK TIE-IN LOCATIONS.
5. THE CONTRACTOR SHALL BE GOVERNED BY THE STANDARDS AND REQUIREMENTS OF THE FOLLOWING PUBLICATIONS, EXCEPT WHEN MODIFIED BY SPECIAL PROVISIONS OF THIS CONTRACT OR THROUGH WRITTEN APPROVAL BY THE ENGINEER.
 - DELDOT - "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", 2011 EDITION AND SUBSEQUENT REVISIONS (M.U.T.C.D.).
 - A.A.S.H.T.O. - "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS", 5TH EDITION WITH 2010 AND 2011 INTERIM REVISIONS.
 - F.H.W.A. - "STANDARD HIGHWAY SIGNS", 2004 EDITION AND SUBSEQUENT REVISIONS.

PAVEMENT MARKING DETAILS



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<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS		NOT TO SCALE	PAVEMENT AND REHABILITATION NORTH V, 2012 WALTHER ROAD, FROM ROUTE 40 TO OLD BALTIMORE PIKE	CONTRACT	BRIDGE NO.	SHEET NO.	
					T201206105			3
					COUNTY	DESIGNED BY: AF, JVB		TOTAL SHTS.
					NEW CASTLE	CHECKED BY: JFL, JAB		16
					NOTES AND DETAILS			

NOTE

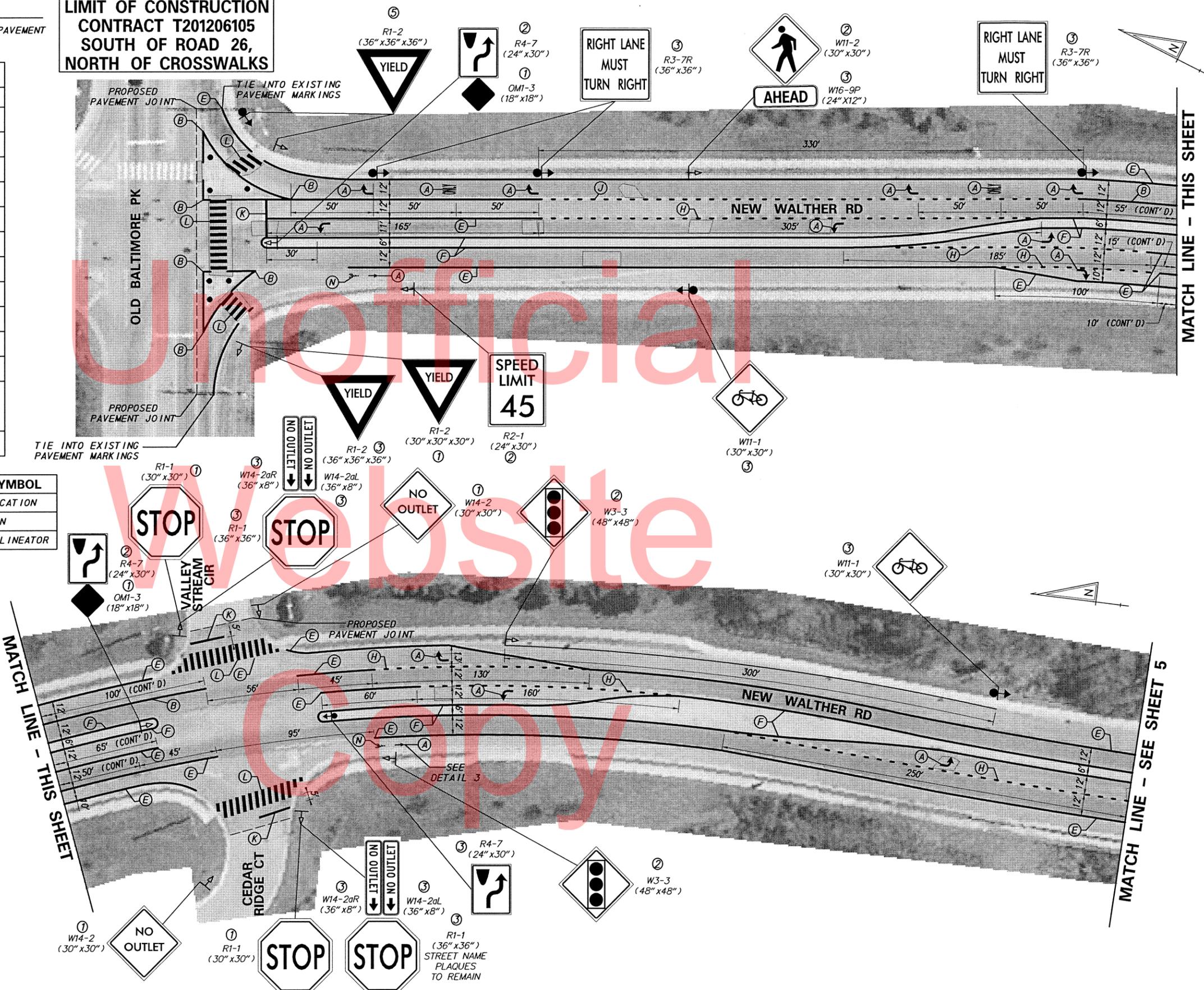
REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

**LIMIT OF CONSTRUCTION
CONTRACT T201206105
SOUTH OF ROAD 26,
NORTH OF CROSSWALKS**

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
(B)	10" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748549)
(C)	12" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(D)	12" SOLID YELLOW ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(E)	5" SOLID WHITE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(F)	5" SOLID YELLOW EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(G)	5" SOLID YELLOW DOUBLE LINE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(H)	5" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING, (ITEM 748548)
(J)	10" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748549)
(K)	16" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(L)	24" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(M)	3" SOLID BLACK LINE EPOXY RESIN PAINT PAVEMENT STRIPING, (ALONG LEFT-HAND SIDE OF WHITE LINE ONLY; CLOSEST TO TRAFFIC) (ITEM 748557)
(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
←	EXISTING SIGN LOCATION
←●	NEW SIGN LOCATION
●	FLEXIBLE POST DELINEATOR



MATCH LINE - THIS SHEET

MATCH LINE - SEE SHEET 5

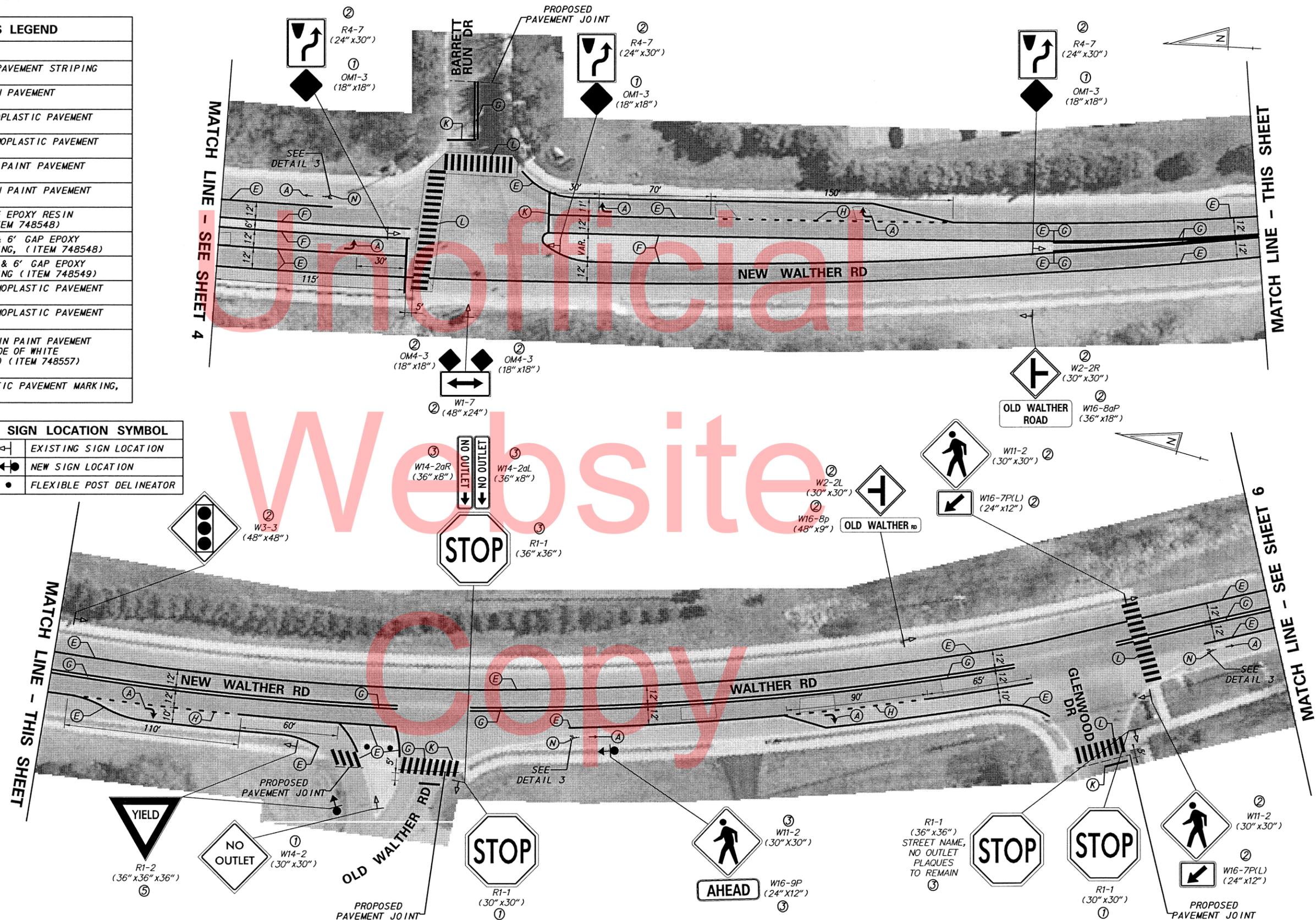
NOTE

REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
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(H)	5" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING, (ITEM 748548)
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(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
◀	EXISTING SIGN LOCATION
▶	NEW SIGN LOCATION
•	FLEXIBLE POST DELINEATOR



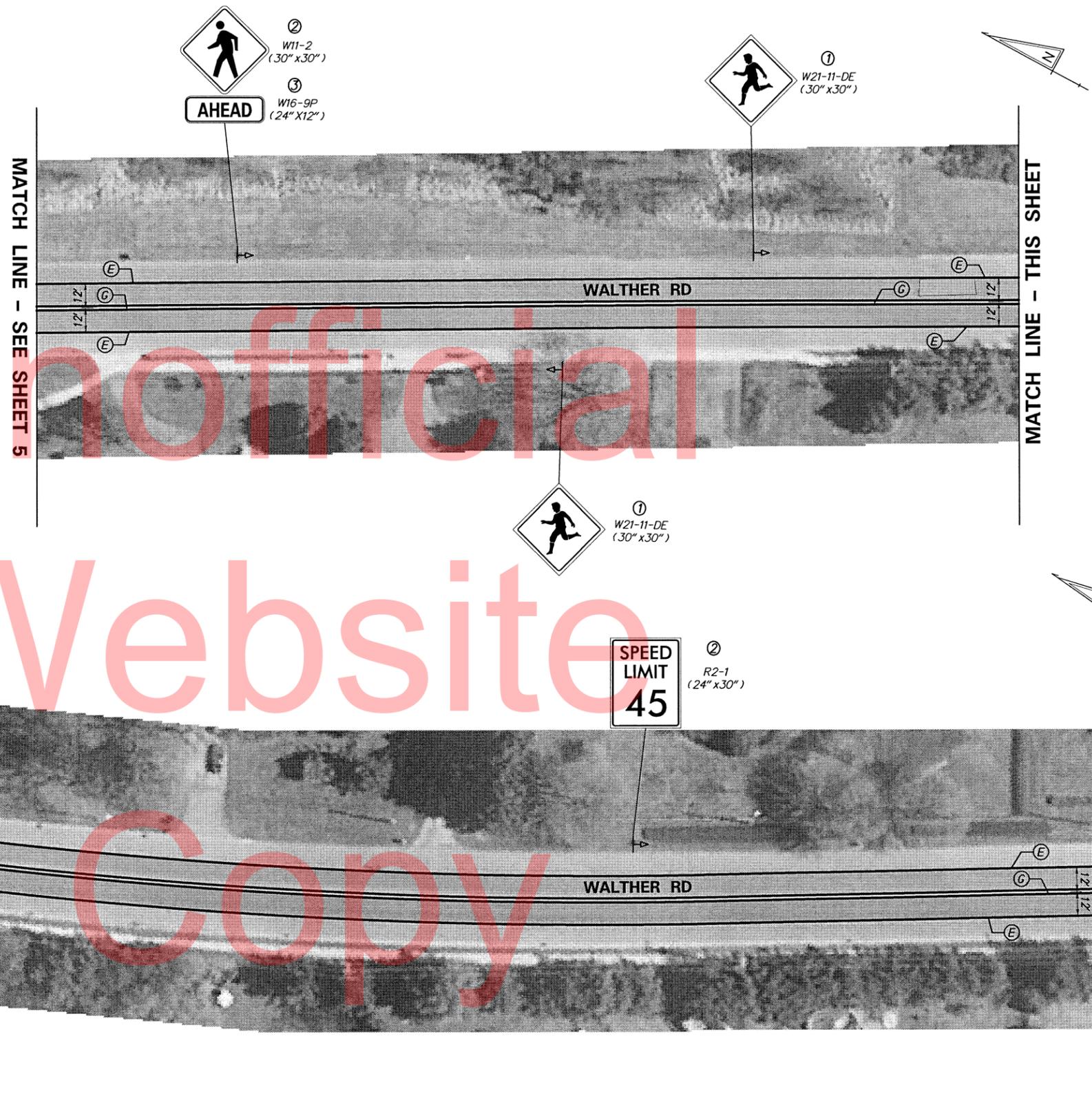
NOTE

REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
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SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
	EXISTING SIGN LOCATION
	NEW SIGN LOCATION
	FLEXIBLE POST DELINEATOR



MATCH LINE - THIS SHEET

MATCH LINE - SEE SHEET 7

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<p>DELAWARE DEPARTMENT OF TRANSPORTATION</p>	ADDENDUMS / REVISIONS	<p>SCALE</p> <p>FEET</p>	<p>PAVEMENT AND REHABILITATION NORTH V, 2012 WALTHER ROAD, FROM ROUTE 40 TO OLD BALTIMORE PIKE</p>	CONTRACT	BRIDGE NO.	<p>PROPOSED SIGNING AND STRIPING PLAN</p>	SHEET NO.
					T201206105		
				COUNTY	CHECKED BY: JFL, JAB	TOTAL SHTS.	16
				NEW CASTLE			

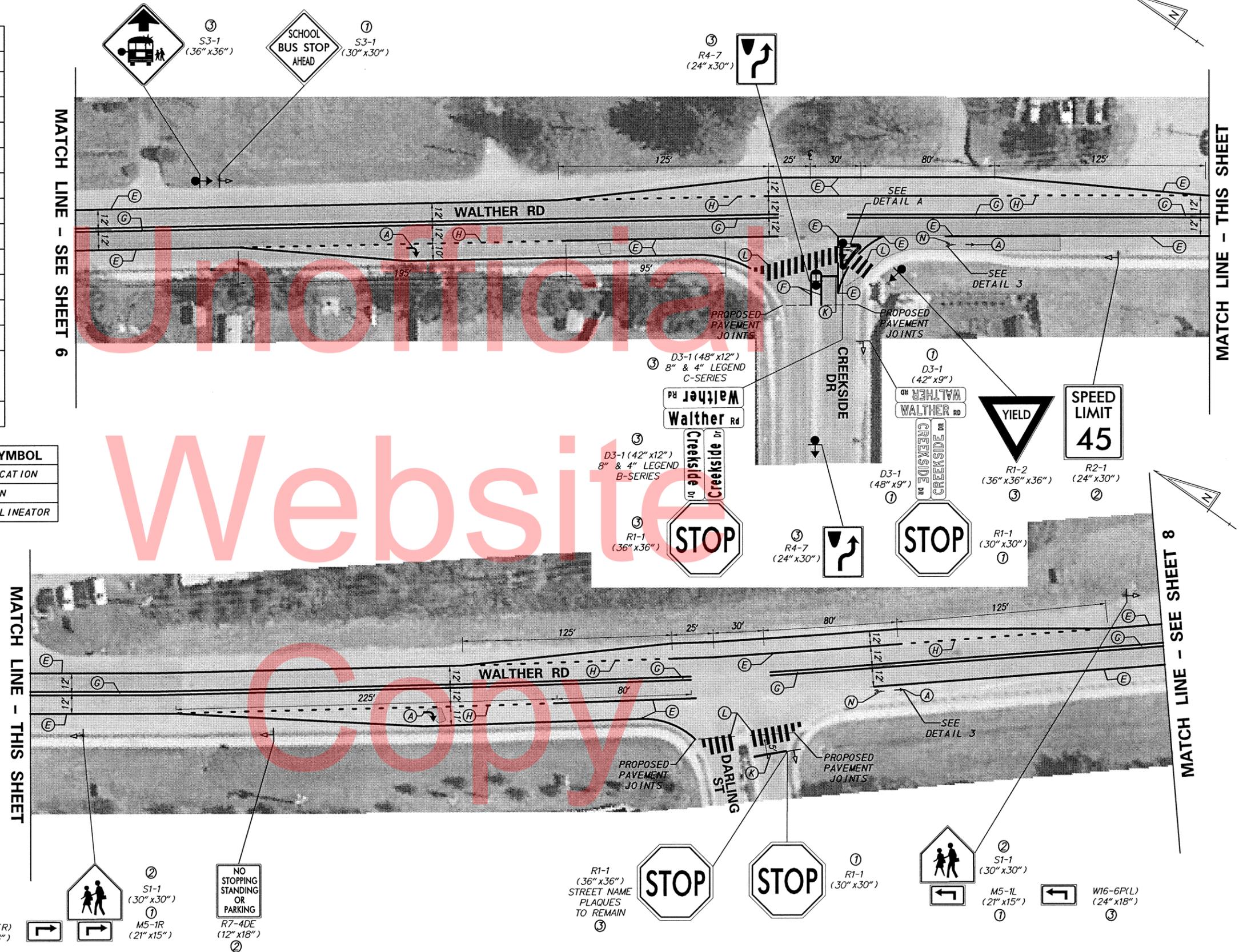
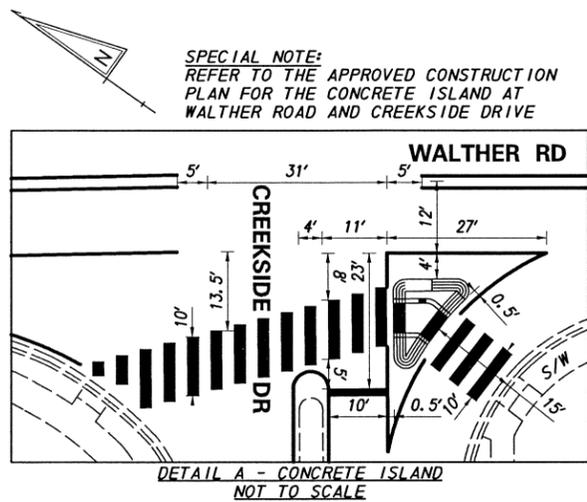
NOTE

REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
(B)	10" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748549)
(C)	12" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(D)	12" SOLID YELLOW ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(E)	5" SOLID WHITE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(F)	5" SOLID YELLOW EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(G)	5" SOLID YELLOW DOUBLE LINE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(H)	5" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING, (ITEM 748548)
(J)	10" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748549)
(K)	16" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(L)	24" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(M)	3" SOLID BLACK LINE EPOXY RESIN PAINT PAVEMENT STRIPING, (ALONG LEFT-HAND SIDE OF WHITE LINE ONLY; CLOSEST TO TRAFFIC) (ITEM 748557)
(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

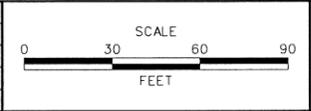
SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
◀	EXISTING SIGN LOCATION
▶	NEW SIGN LOCATION
•	FLEXIBLE POST DELINEATOR



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ADDENDUMS / REVISIONS



PAVEMENT AND REHABILITATION
 NORTH V, 2012
 WALTHER ROAD,
 FROM ROUTE 40 TO OLD BALTIMORE PIKE

CONTRACT T201206105	BRIDGE NO.
COUNTY NEW CASTLE	DESIGNED BY: AF, JVB
	CHECKED BY: JFL, JAB

PROPOSED SIGNING AND STRIPING PLAN

SHEET NO.	7
TOTAL SHTS.	16

NOTE

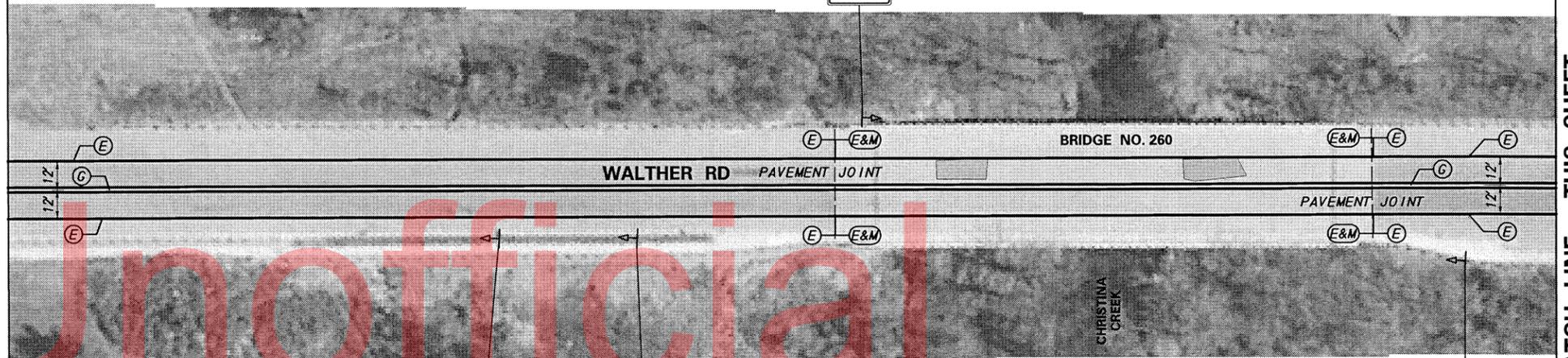
REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
(B)	10" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748549)
(C)	12" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(D)	12" SOLID YELLOW ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748027)
(E)	5" SOLID WHITE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(F)	5" SOLID YELLOW EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(G)	5" SOLID YELLOW DOUBLE LINE EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748548)
(H)	5" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING, (ITEM 748548)
(J)	10" DOTTED WHITE - 2' LINE & 6' GAP EPOXY RESIN PAINT PAVEMENT STRIPING (ITEM 748549)
(K)	16" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(L)	24" SOLID WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)
(M)	3" SOLID BLACK LINE EPOXY RESIN PAINT PAVEMENT STRIPING, (ALONG LEFT-HAND SIDE OF WHITE LINE ONLY; CLOSEST TO TRAFFIC) (ITEM 748557)
(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
◀	EXISTING SIGN LOCATION
◀●	NEW SIGN LOCATION
●	FLEXIBLE POST DELINEATOR

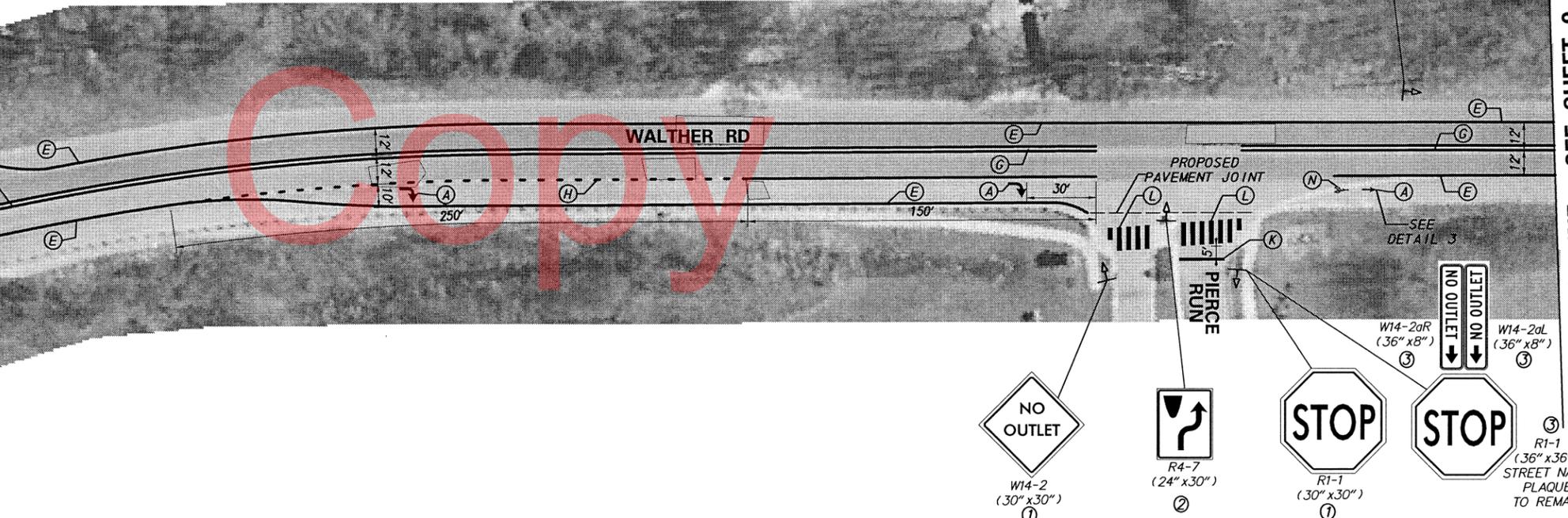
MATCH LINE - SEE SHEET 7



MATCH LINE - THIS SHEET

Unofficial

Website



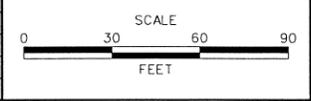
MATCH LINE - THIS SHEET

MATCH LINE - SEE SHEET 9

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DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



PAVEMENT AND REHABILITATION
 NORTH V, 2012
 WALTHER ROAD,
 FROM ROUTE 40 TO OLD BALTIMORE PIKE

CONTRACT	T201206105
COUNTY	NEW CASTLE
BRIDGE NO.	
DESIGNED BY:	AF, JVB
CHECKED BY:	JFL, JAB

PROPOSED SIGNING AND STRIPING PLAN

SHEET NO.	8
TOTAL SHTS.	16

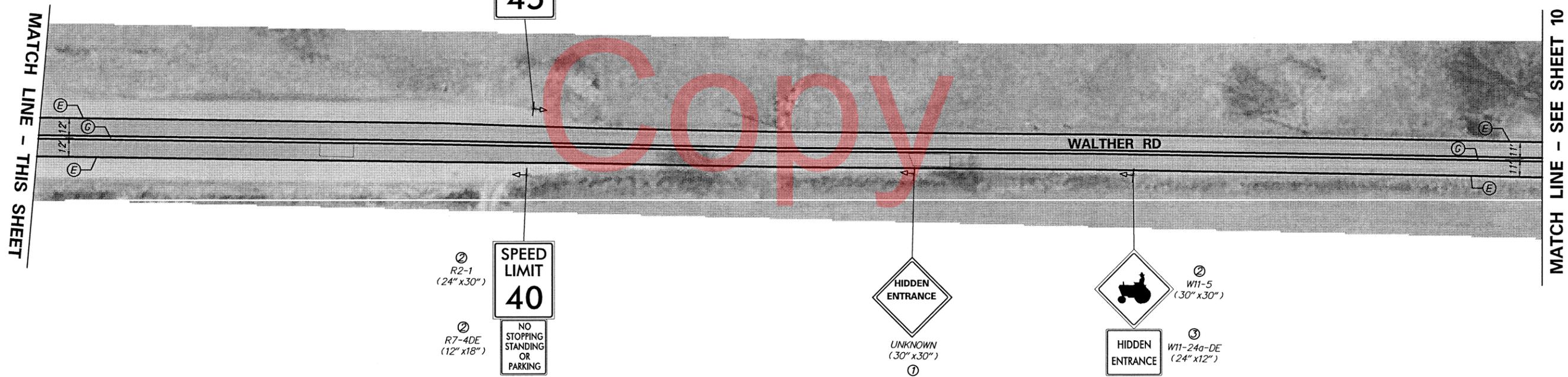
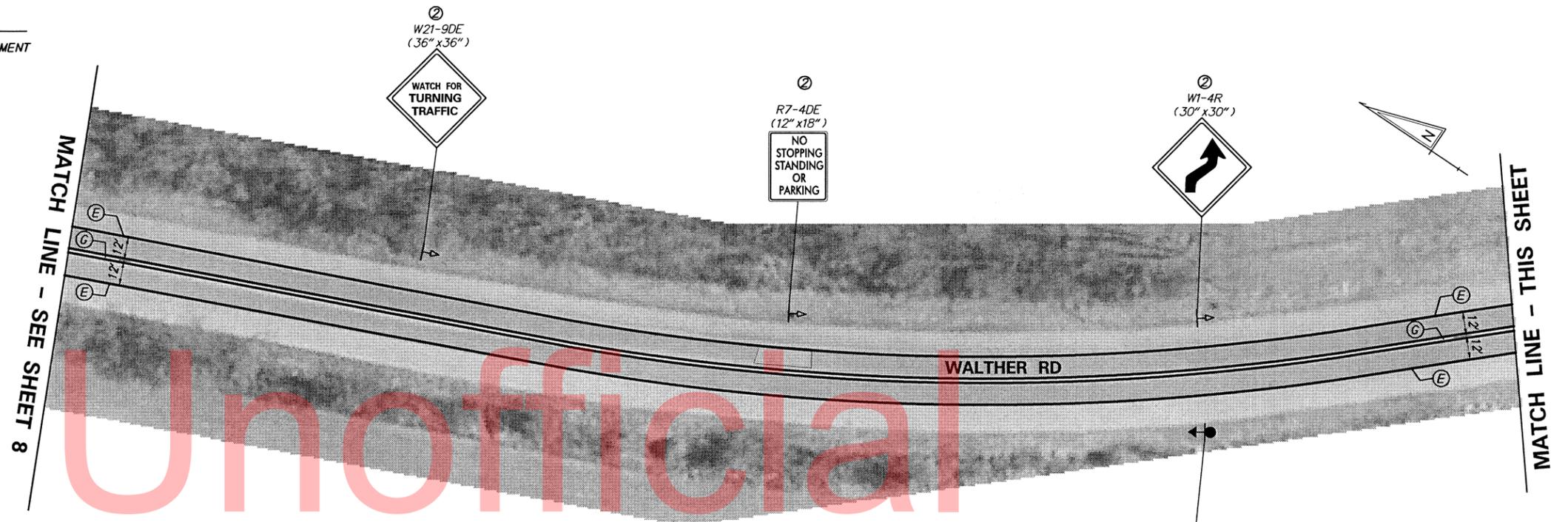
NOTE

REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
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(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

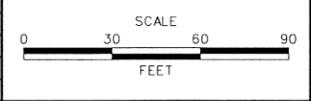
SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

SIGN LOCATION SYMBOL	
◀	EXISTING SIGN LOCATION
▶	NEW SIGN LOCATION
•	FLEXIBLE POST DELINEATOR



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ADDENDUMS / REVISIONS



CONTRACT T201206105	BRIDGE NO.
COUNTY NEW CASTLE	DESIGNED BY: AF,JVB
	CHECKED BY: JFL,JAB

SHEET NO.	9
TOTAL SHTS.	16

NOTE

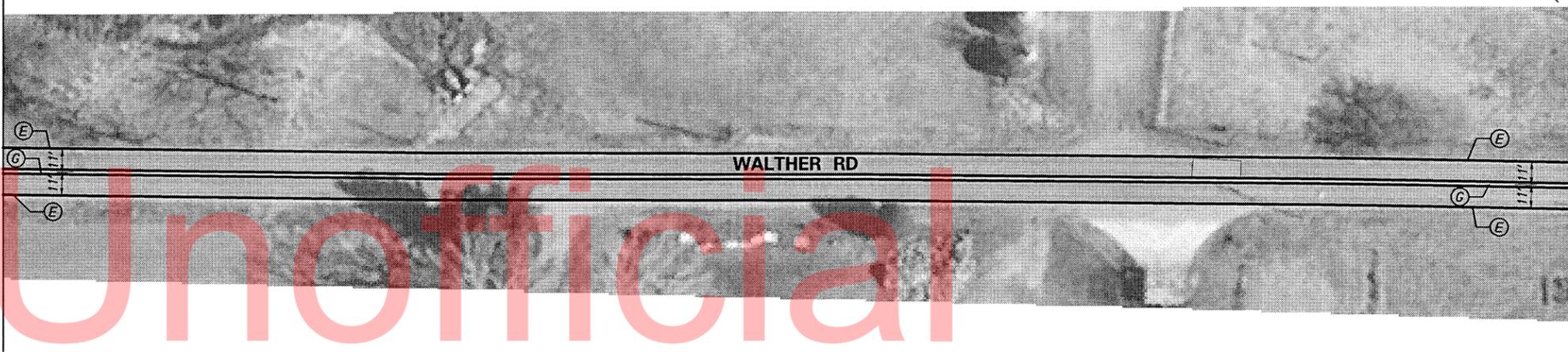
REFER TO SHEET 3 FOR THE GENERAL NOTES FOR SIGNING AND PAVEMENT MARKINGS AND PAVEMENT MARKING DETAILS.

PAVEMENT MARKINGS LEGEND	
SYMBOL	ITEM
(A)	WHITE ALKYD-THERMOPLASTIC PAVEMENT STRIPING SYMBOL (ITEM 748015)
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(N)	WHITE PREFORMED THERMOPLASTIC PAVEMENT MARKING, BIKE SYMBOL (ITEM 748553)

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN

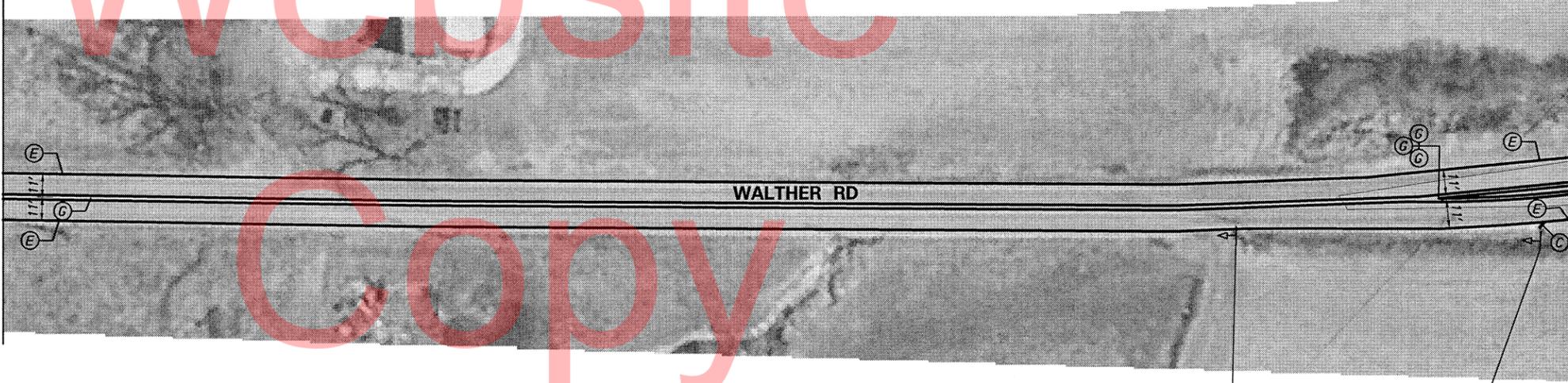
SIGN LOCATION SYMBOL	
	EXISTING SIGN LOCATION
	NEW SIGN LOCATION
	FLEXIBLE POST DELINEATOR

MATCH LINE - SEE SHEET 9

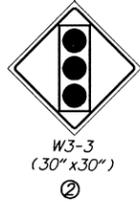


MATCH LINE - THIS SHEET

MATCH LINE - THIS SHEET



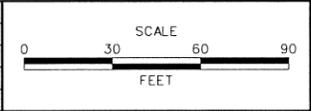
MATCH LINE - SEE SHEET 11



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DELAWARE
DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



PAVEMENT AND REHABILITATION
NORTH V, 2012
WALTHER ROAD,
FROM ROUTE 40 TO OLD BALTIMORE PIKE

CONTRACT	BRIDGE NO.
T201206105	
COUNTY	DESIGNED BY: AF,JVB
NEW CASTLE	CHECKED BY: JFL,JAB

PROPOSED SIGNING AND STRIPING PLAN

SHEET NO.	10
TOTAL SHTS.	16