

EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE INLET
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

MANMADE ROADSIDE FEATURES	
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

RIGHT-OF-WAY SYMBOLS	
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE
	HISTORIC RIGHT-OF-WAY BASELINE
	EXISTING RIGHT-OF-WAY
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING DENIAL OF ACCESS
	EXISTING R/W & DENIAL OF ACCESS

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC METER
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER VENT OR CLEANOUT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE GUY WIRE ANCHOR
	UTILITY POLE
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD
	MANHOLE - UNDETERMINED OWNER

UTILITY COMPANY FACILITIES	
	EXISTING UTILITY NAME

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	BRICK PATTERNED SURFACE
	BUTT JOINT
	CONSTRUCTION BASELINE
	CONSTRUCTION SAFETY FENCE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CLEAR ZONE
	DRAINAGE INLET
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPE 1
	GUARDRAIL, TYPE 2
	GUARDRAIL, TYPE 3
	GUARDRAIL END ANCHORAGE
	GUARDRAIL END TREATMENT, TYPE 1
	GUARDRAIL END TREATMENT, TYPE 2
	GUARDRAIL END TREATMENT, TYPE 3
	HORIZONTAL CLEARANCE
	IMPACT ATTENUATOR
	JUNCTION BOX - DRAINAGE
	LIMIT OF CONSTRUCTION
	MAILBOX
	MANHOLE
	PAVEMENT PATCH
	PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK - 4"
	P.C.C. SIDEWALK - 6" (USE 8" DEPTH FOR CHANNELIZATION ISLANDS.)
	UNDERDRAIN
	UNDERDRAIN OUTLET

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE

PROPOSED SYMBOLS

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	CONSTRUCTION SAFETY FENCE
	DRAINAGE INLET
	DO NOT DISTURB
	ENERGY DISSIPATOR
	FENCE
	FLARED END SECTION
	FILL WITH FLOWABLE FILL
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY OTHERS
	UNDERDRAIN / LENGTH
	UNDERDRAIN OUTLET PIPE

LANDSCAPING	
	LANDSCAPE PLANTINGS
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN

PAVEMENT SECTION(S)	
	OVERLAY PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	RECONSTRUCTED PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	DRIVEWAY AND ENTRANCE PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS

UTILITY COMPANY FACILITIES	
	PROPOSED UTILITY NAME

GENERAL NOTES

- THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
- THE CONTRACTOR SHALL GIVE TWO (2) WEEKS NOTICE TO THE PROPERTY OWNER WHEN ANY FIXTURE, SHRUB OR OTHER OBJECT MUST BE REMOVED FROM THE RIGHT OF WAY OR EASEMENT AREA. IF THE OWNER HAS NOT ATTEMPTED TO SALVAGE THIS PROPERTY, THE CONTRACTOR SHALL REMOVE IT WITHOUT OBLIGATION. COMPENSATION SHALL BE INCIDENTAL TO THE CONTRACT.
- THE ENDS OF ALL CURBS SHALL BE DEPRESSED FLUSH WITH THE PAVEMENT AT A RATIO OF FIFTEEN TO ONE (15:1) UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL PVC SLEEVES (4" INSIDE MINIMUM DIAMETER, 6" INSIDE MAXIMUM DIAMETER) IN PROPOSED CONCRETE SIDEWALKS, ISLANDS, AND MEDIANS FOR FUTURE TRAFFIC SIGN POSTS AS DIRECTED BY THE ENGINEER. THE LOWER END OF THE SLEEVE SHALL SIT ON THE TOP OF THE SUBBASE MATERIAL. THE COST SHALL BE INCIDENTAL TO THE CONTRACT.
- STAGING AREAS - PROPER EROSION AND SEDIMENT CONTROL MEASURES AS DETERMINED BY THE ENGINEER SHALL BE INSTALLED IN ALL STAGING AREAS. ALL AREAS USED BY THE CONTRACTOR FOR STAGING OPERATIONS SHALL BE FULLY RESTORED BY THE CONTRACTOR UPON COMPLETION OF THE CONTRACT. IF THE STAGING AREA IS PAVED, IT SHALL BE RESTORED TO ITS ORIGINAL CONDITION. IF THE AREA IS UNPAVED, IT SHALL BE RE-GRADED, TOPSOILED, SEEDED AND MULCHED IN ACCORDANCE WITH DELAWARE STANDARD SPECIFICATIONS 732, 734 AND 735, FOR TOPSOIL, SEED AND MULCH RESPECTIVELY, TO THE SATISFACTION OF THE ENGINEER. THE SEED SHALL ADHERE TO THE SPECIFICATIONS OF SECTION 734 FOR PERMANENT GRASS SEEDING - DRY GROUND. ALL COSTS ASSOCIATED WITH RESTORATION OF THE STAGING AREA SHALL BE AT THE CONTRACTOR'S EXPENSE. IF THE ENGINEER DETERMINES THAT A SATISFACTORY STAND OF GRASS DOES NOT EXIST AT THE TIME OF FINAL INSPECTION, ALL COSTS ASSOCIATED WITH REESTABLISHING A SATISFACTORY STAND OF GRASS SHALL BE AT THE CONTRACTOR'S EXPENSE.
- SITE REVIEWER - AN EROSION CONTROL SITE REVIEWER SHALL BE A PERSON FROM THE CONTRACTOR'S STAFF ASSIGNED TO EROSION AND SEDIMENT CONTROL IMPLEMENTATION AND MAINTENANCE AND SHALL BE REQUIRED ON SPECIFIC PROJECTS. THE NAME AND DNREC CERTIFICATION NUMBER OF EACH SITE REVIEWER SO REQUIRED SHALL BE SUBMITTED TO THE DEPARTMENT. THE NAME OF THE DELAWARE REGISTERED PROFESSIONAL ENGINEER PROVIDING DIRECTION AND SUPERVISION OF THE SITE REVIEWER, AS REQUIRED IN SECTION 12.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS, SHALL ALSO BE SUBMITTED TO THE DEPARTMENT. THE SITE REVIEWER REQUIREMENTS IN EFFECT ON THIS PROJECT SHALL BE MARKED WITH AN "X" BELOW:

EROSION POTENTIAL FOR THIS PROJECT	SITE REVIEWER REQUIREMENT
(X) INSIGNIFICANT	NONE
() MINOR	CONTRACTOR CERTIFICATION COURSE TRAINING ONLY, AS DEFINED IN SECTION 13 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MEDIUM	AT THE TIME OF BID OF THE CONTRACT, EITHER THE SUPERINTENDENT OR A SEPARATE INDIVIDUAL FROM THE CONTRACTOR'S STAFF SHALL BE A CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 12 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MAJOR	SUPERINTENDENT AND AN INDIVIDUAL FROM CONTRACTOR'S STAFF SHALL BE CCR. ONE INDIVIDUAL FROM THE CONTRACTOR'S STAFF MUST BE A CCR AT THE TIME OF BID OF THE CONTRACT. THE SUPERINTENDENT MUST BECOME A CCR WITHIN ONE YEAR AFTER THE AWARD OF CONTRACT.

- ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR INCLUDE:

(X)	NONE
()	ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
()	RASTER FILES, IN .CAL FILE FORMAT, FOR ALL PLAN SHEETS.
()	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

NOTE: THE DOCUMENT ENTITLED "RELEASE FOR DELIVERY OF DOCUMENTS IN ELECTRONIC FORM TO A CONTRACTOR" MUST BE SIGNED BY ALL PARTIES PRIOR TO THE DELIVERY OF ANY ELECTRONIC PROJECT FILES.

- AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

()	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
(X)	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR.
()	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR.

- THE DISTURBED AREA FOR THIS PROJECT IS __N/A____ ACRES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADHERING TO THE CONSTRUCTION SITE POLLUTION PREVENTION SPECIFICATIONS AS DETAILED IN SECTION 3.6 OF THE "DELAWARE EROSION AND SEDIMENT CONTROL HANDBOOK". ALL COSTS ASSOCIATED WITH ADHERING TO THE STANDARDS SHALL BE INCIDENTAL TO THE OVERALL CONTRACT COSTS.
- THE EROSION AND SEDIMENT CONTROL PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE EROSION AND SEDIMENT CONTROL PLANS ARE VALID FOR A THREE YEAR PERIOD, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR SHALL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE EROSION AND SEDIMENT CONTROL PLAN APPROVAL. DELDOT WILL REVIEW THE CURRENT EROSION AND SEDIMENT CONTROL PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.

PROJECT NOTES

SECTION 100

- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING AN AREA SUITABLE FOR STOCKPILING BORROW, TOPSOIL AND OTHER FILL MATERIAL REQUIRED FOR THE PROJECT IN ACCORDANCE WITH SECTIONS 110.07 AND 110.10 OF THE STANDARD SPECIFICATIONS. SUBJECT TO THE APPROVAL OF THE ENGINEER, STOCKPILE AREA MAY BE LOCATED OUTSIDE THE PROJECT LIMITS, IF NECESSARY. NO ADDITIONAL PAYMENT WILL OCCUR FOR OFF SITE STOCKPILING.
- ANY DAMAGE TO ITEMS NOTED TO BE RELOCATED OR RESET BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

SECTION 700

- UNLESS OTHERWISE NOTED, POINT GEOMETRY ADJACENT TO CURB AND GUTTER IS GIVEN TO EDGE OF PAVEMENT. PAVEMENT WIDTH DIMENSIONS ARE TO THE EDGE OF THE TRAVEL WAY/EDGE OF PAVEMENT. ALL CURB MEASUREMENTS, RADIAL OR TANGENT, SHALL BE TO THE FACE OF THE CURB.
- WHERE NEW CONCRETE SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, SAWCUT THE EXISTING SIDEWALK OR MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER AND SHALL BE PAID FOR IN ITEM 762001 SAWCUTTING, CONCRETE, FULL DEPTH.
- ALL PROPOSED CURB RAMPS AND ADJACENT SIDEWALK WITHIN THE LIMITS OF THE CURB RAMP SHALL BE FIVE (5) FEET WIDE. WHERE PROPOSED SIDEWALK MEETS EXISTING SIDEWALK WITH A DIFFERENT WIDTH THE PROPOSED EDGE OF SIDEWALK SHALL BE TAPERED AT 3:1 RATIO.
- ALL PAVED AREAS (INCLUDING DRIVEWAYS) TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE IN.
- ALL HOT-MIX SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.
- ALL PROPOSED 6-INCH PCC PATTERNED SIDEWALK ALONG ROSE HILL DRIVE, CHADDWYCK BOULEVARD AND HEDDINGTON ROAD SHALL BE STAMPED WITH A HERRINGBONE BRICK PATTERN AND COLORED BRICK RED IN ACCORDANCE WITH THE DELDOT STANDARD CONSTRUCTION DETAILS.(SHEET M-6).
- MAINTENANCE OF TRAFFIC
- ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN, AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.
- MAINTENANCE OF TRAFFIC DURING LANE CLOSURES ON TWO-LANE ROADWAYS AND SIDEWALK CLOSURES SHALL CONFORM TO CHAPTER 6H OF THE DEMUTCD. THE CONTRACTOR SHALL MAINTAIN A MINIMUM LANE WIDTH OF 10 FEET AT ALL TIMES.

MISCELLANEOUS

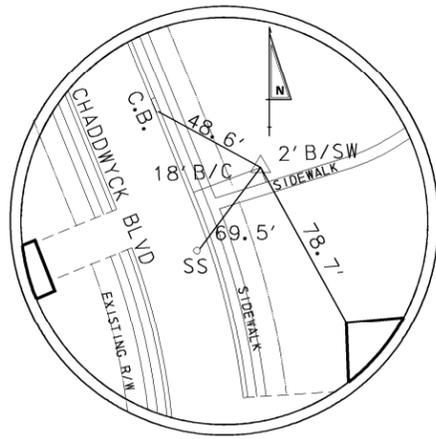
- NO BARE SOIL SHALL BE PERMITTED TO SHOW AT THE END OF ANY WORK DAY. ALL BARE SOIL SHALL BE COVERED WITH GRADED AGGREGATE BASE COURSE, OR SEED AND MULCH, OR AN EROSION CONTROL METHOD APPROVED BY THE ENGINEER UNLESS OTHERWISE NOTED ON THE PLANS.
- THE CONTRACTOR SHALL CONTACT THE CHIEF OF SCHEDULING FOR DART FIRST STATE (DTC) AT 302-577-3278 EXT. 346, FOURTEEN (14) DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT LOCAL POLICE AND FIRE EMERGENCY SERVICES FOURTEEN (14) DAYS PRIOR TO THE START OF CONSTRUCTION AND/OR ROAD CLOSURES. THE CONTRACTOR SHALL NOTIFY EMERGENCY SERVICES WITH ANY LANE CLOSURES DURING CONSTRUCTION.
- ALL EXISTING DRAINAGE SYSTEMS SHOWN WITHIN THE RIGHT OF WAY ARE TO REMAIN OPERATIONAL UNLESS DENOTED FOR ABANDONMENT OR REMOVAL.

- THE LOCATIONS OF THE EXISTING UTILITIES SHOWN ARE APPROXIMATE, THE CONTRACTOR SHALL NOTIFY "MISS UTILITY" AT TELEPHONE NUMBER 1-800-282-8555 PRIOR TO ANY EXCAVATION. ALL EXISTING UTILITIES SHALL BE PROTECTED, TEMPORARILY SUPPORTED AND/OR RELOCATED AS NECESSARY TO COMPLETE THE WORK IN ACCORDANCE WITH THE RESPECTIVE UTILITY COMPANY.
- THE CONSTRUCTION PLANS/RECORD DEVELOPMENT PLANS USED FOR THE RIGHT OF WAY APPROXIMATION ARE AS FOLLOWS:

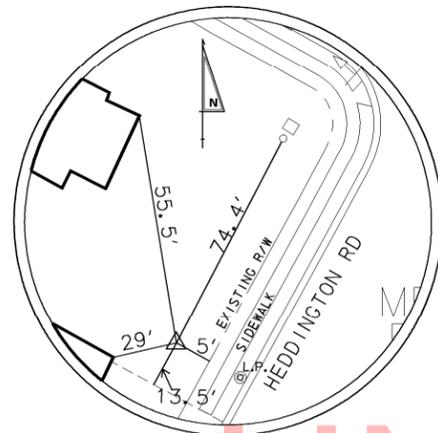
CHADDWYCK BOULEVARD AND HEDDINGTON DRIVE - TAX PARCEL NUMBER 10-040.00-022, "CONSTRUCTION PLANS FOR PUBLIC STREETS", DATED AUGUST, 1998 PROVIDED BY DELDOT.
NEW CASTLE COUNTY - MICROFILM # 013690

ROSE HILL DRIVE: NEW CASTLE COUNTY - MICROFILM # 011131
- THE CONTRACTOR IS ONLY PERMITTED TO WORK BETWEEN 8:00 AM AND 6:00 PM.
- ALL SIGN POSTS LOCATED IN THE MEDIAN OR CHICANE SHALL BE PLACED TWO (2) FEET FROM THE FACE OF THE CURB AS NOTED ON THE PLANS.
- ALL PROPOSED CROSSWALK SYMBOLS SHALL BE TWO (2) FEET WIDE AND SIX (6) FEET LONG WITH TWO (2) FOOT SPACING BETWEEN SYMBOLS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSTALL ALL REGULATORY, WARNING AND SPECIAL SIGNS AND POSTS WITHIN CONTRACT LIMITS UNLESS OTHERWISE NOTED. DELDOT TO FURNISH ALL REGULATORY, WARNING AND SPECIAL SIGNS AND POSTS TO CONTRACTOR FOR INSTALLATION. FOR COORDINATION AND PICK UP OF SIGNS AND POSTS, THE CONTRACTOR SHALL CONTACT DELDOT'S TRANSPORTATION SYSTEM SPECIALIST AT 302-739-4366.
- THE DELDOT PROJECT MANAGER IS RESPONSIBLE FOR ENSURING ANY REQUIRED DOCUMENTS AND ANALYSIS AS PART OF THE ADOPTED WORK ZONE SAFETY AND MOBILITY PROCEDURES AND GUIDELINES HAS BEEN COMPLETED PRIOR TO ANY WORK STARTING ON THIS CONTRACT
- IT IS ANTICIPATED THAT ALL WORK WILL OCCUR WITHIN DELDOT'S EXISTING RIGHT OF WAY OR EASEMENT AREAS. SHOULD THE NEED OCCUR TO TRESPASS ONTO PRIVATE PROPERTY, IT WILL BE THE RESPONSIBILITY OF THE DELDOT PROJECT MANAGER TO SECURE SUCH TRESPASS NEEDS.
- NO ENVIRONMENTAL PERMITS ARE REQUIRED FOR THE WORK PROVIDED THAT NO JURISDICTIONAL WETLANDS ARE ENCOUNTERED. SHOULD THE PROPOSED WORK AFFECT ANY JURISDICTIONAL WETLANDS, THE CONTRACTOR IS REQUIRED TO CONTACT THE DELDOT ENVIRONMENTAL SECTION AT 302-760-2264.
- NO UTILITY RELOCATION INVOLVEMENT IS ANTICIPATED. SHOULD ANY CONFLICTS WITH A PUBLIC UTILITY BE ENCOUNTERED DURING CONSTRUCTION REQUIRING ADJUSTMENT AND/OR RELOCATION OF THE UTILITY'S EXISTING FACILITIES, THE NECESSARY RELOCATION WORK SHALL BE ACCOMPLISHED BY THE AFFECTED UTILITY OR THEIR CONTRACTOR AT THEIR EXPENSE.
- THE DELDOT PROJECT MANAGER SHALL BE RESPONSIBLE FOR COORDINATING WITH THE TRAFFIC SECTION RELATING TO ANY IMPACTS TO TRAFFIC SECTION FACILITIES (INCLUDING BUT NOT LIMITED TO TRAFFIC LOOPS, JUNCTION WELLS ETC.) IN ADVANCE OF THE START OF ACTIVITY. PRIOR TO INITIATING ANY WORK ON THE CONTRACT (OR SITES), THE DELDOT PROJECT MANAGER SHALL BE RESPONSIBLE FOR PREPARING AND SUBMITTING FOR THE APPROVAL OF THE SAFETY SECTION, MAINTENANCE OF THE TRAFFIC PLAN. SUFFICIENT TIME SHALL BE PROVIDED FOR THE REVIEW AND APPROVAL OF THE PLANS. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE PROPOSED TIME RESTRICTIONS ON THE CLOSURE OF TRAVEL LANES SUBJECT TO THE APPROVAL OF THE SAFETY SECTION.

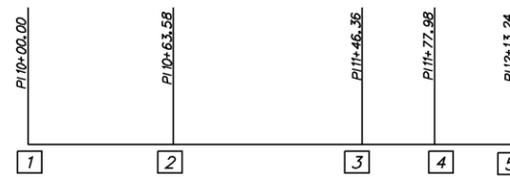
CHADDWYCK BOULEVARD HORIZONTAL AND VERTICAL CONTROL



TRAVERSE POINT RKK1

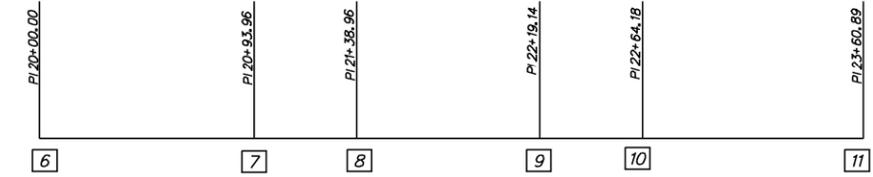


TRAVERSE POINT RKK2



CHADDWYCK BLVD AT SHETLAND DR

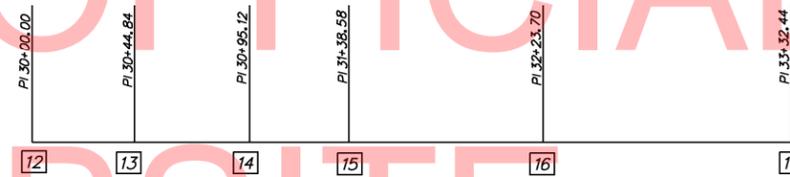
CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
1	10+00.00	0.00'	594934.3291	596585.3610
2	10+63.58	0.00'	594969.5057	596638.3243
3	11+46.36	0.00'	595018.4214	596705.1024
4	11+77.98	0.00'	595040.5282	596727.7166
5	12+13.24	0.00'	595068.8572	596748.6979



CHADDWYCK BLVD AT WALDEN PL

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
6	20+00.00	0.00'	596940.1072	597137.6565
7	20+93.96	0.00'	597034.0543	597139.1803
8	21+38.96	0.00'	597078.5292	597146.0352
9	22+19.14	0.00'	597155.8164	597167.3650
10	22+64.18	0.00'	597197.4604	597184.5394
11	23+60.89	0.00'	597285.2542	597225.0876

HORIZONTAL / VERTICAL CONTROL DATA					
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
RKK1			596390.9726	597390.3931	55.8234
RKK2			595964.2051	597225.1212	55.1246



CHADDWYCK BLVD AT HEDDINGTON RD

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
12	30+00.00	0.00'	596133.6396	597150.1406
13	30+44.84	0.00'	596103.5228	597183.3566
14	30+95.12	0.00'	596076.1119	597225.5163
15	31.38.58	0.00'	596055.0665	597263.5366
16	32.23.70	0.00'	596014.8547	597338.5629
17	33+32.44	0.00'	595962.8768	597434.0777

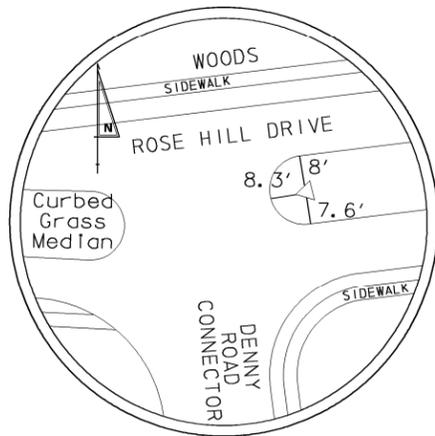


HEDDINGTON RD - NECKDOWN

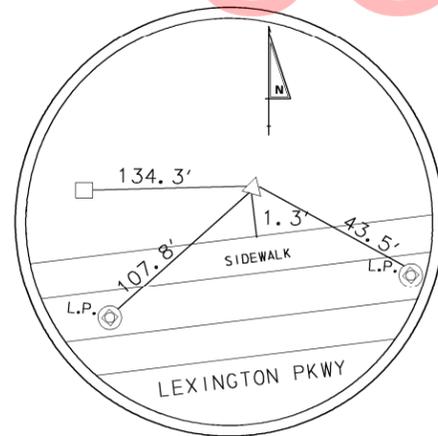
CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
18	40+00.00	0.00'	595887.4247	597573.6196
19	41+84.68	0.00'	595799.3986	597735.9668

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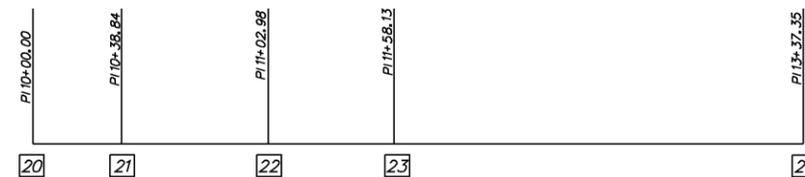
ROSE HILL DRIVE HORIZONTAL AND VERTICAL CONTROL



TRAVERSE POINT RKK3

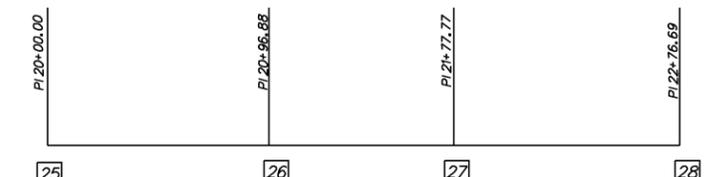


TRAVERSE POINT RKK4



ROSE HILL DR AT CRIMSON KING DR

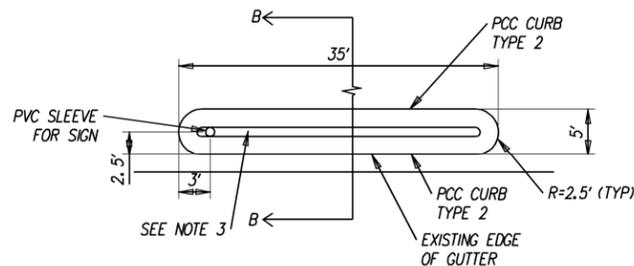
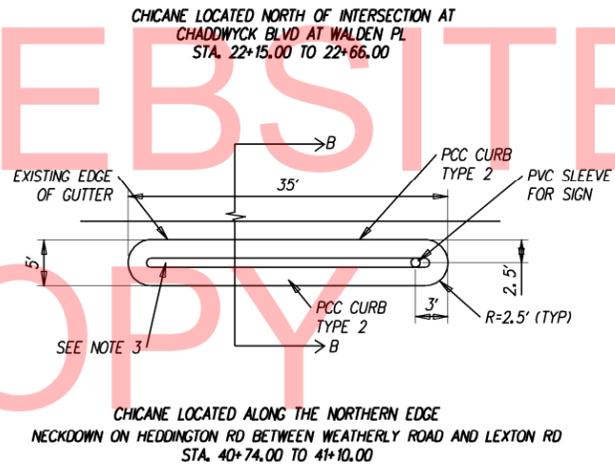
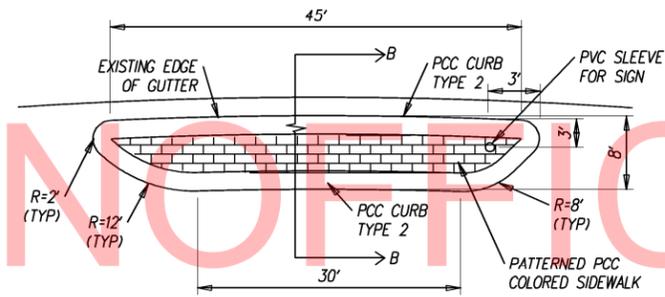
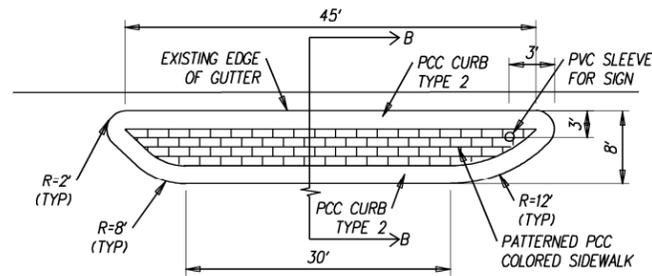
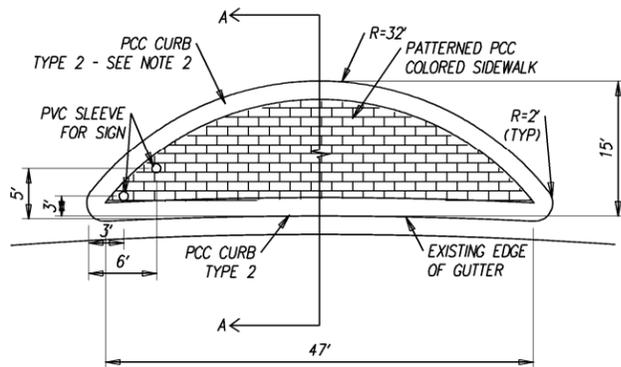
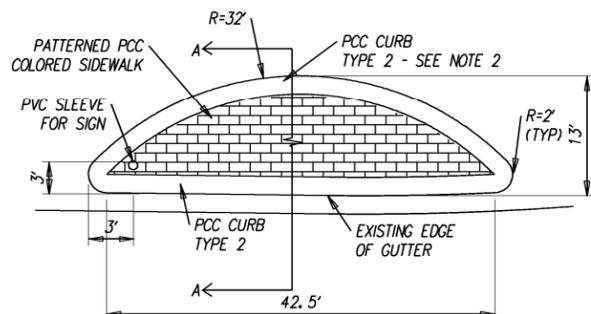
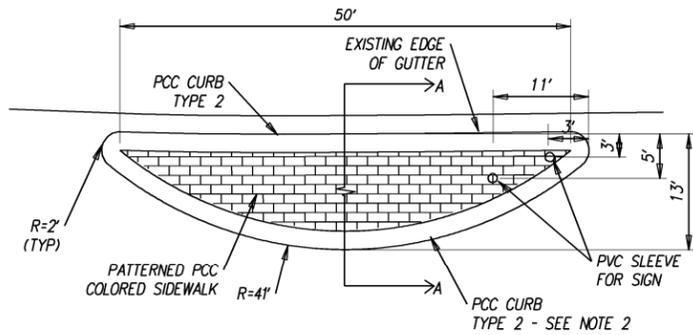
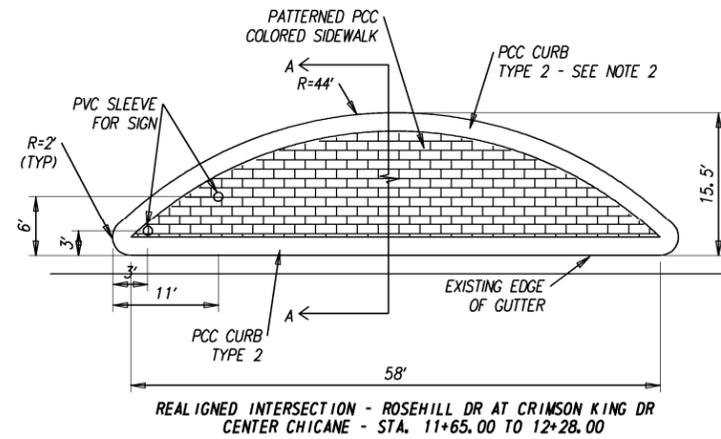
CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
20	10+00.00	0.00'	572979.5888	559896.7365
21	10+38.84	0.00'	572972.1815	559934.8586
22	11+02.98	0.00'	572966.1624	559998.7245
23	11+58.13	0.00'	572970.1226	560053.7265
24	13+37.35	0.00'	572983.2988	560232.4676



ROSE HILL DR AT NASHUA CT

CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
25	20+00.00	0.00'	572936.5162	560813.1240
26	20+96.88	0.00'	572938.6122	560909.9802
27	21+77.77	0.00'	572951.9855	560989.7533
28	22+76.69	0.00'	572970.5506	561086.9236

HORIZONTAL / VERTICAL CONTROL DATA					
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
RKK3			573111.7442	562082.8456	75.4198
RKK4			573242.9874	562863.7890	74.8686

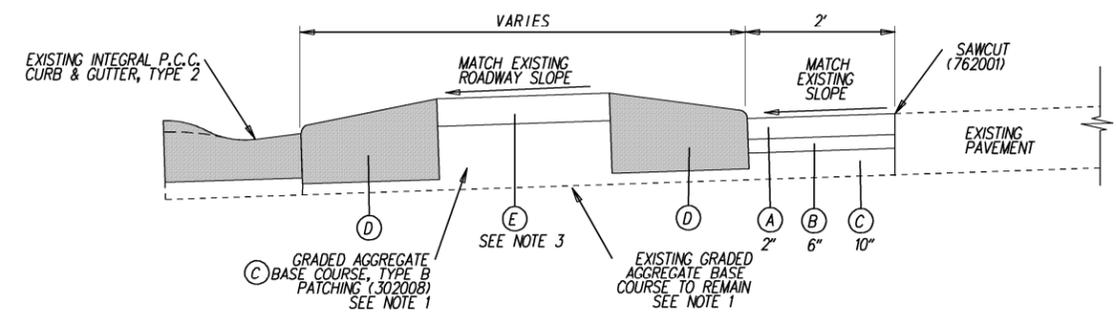
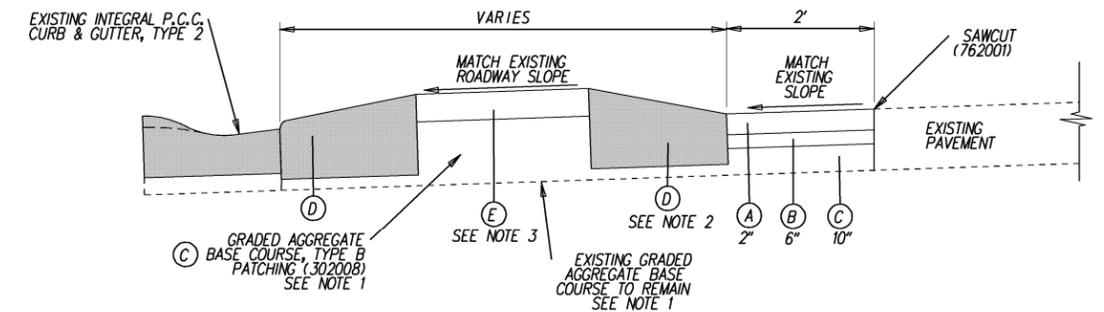


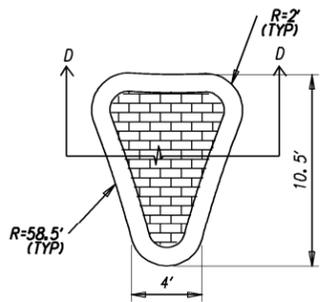
LEGEND

- (A) ITEM 401665 - SUPERPAVE TYPE C HOT-MIX, 160 GYRATIONS, PG 64-22, PATCHING
- (B) ITEM 401667 - SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING
- (C) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING
- (D) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2
- (E) ITEM 705519 - PATTERNED PORTLAND CEMENT CONCRETE SIDEWALK, 6"

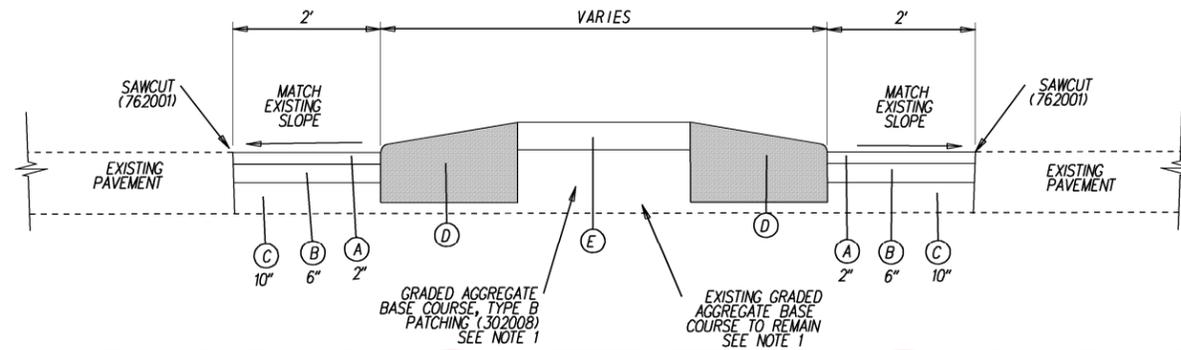
- NOTES:
1. THE EXISTING HOT-MIX PAVEMENT SECTION SHALL BE REMOVED AS NECESSARY TO CONSTRUCT THE PROPOSED CHICANES. PAYMENT FOR HOT-MIX PAVEMENT REMOVAL SHALL BE MADE UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT. THE EXISTING SUBBASE MATERIAL BELOW THE PROPOSED ISLAND SHALL REMAIN IN PLACE AS DIRECTED BY THE ENGINEER. ANY WORK NECESSARY TO RECOMPACT THE EXISTING SUBBASE MATERIAL SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT. ANY VOIDS RESULTING FROM EXCAVATING THE EXISTING PAVEMENT SECTION SHALL BE BACKFILLED WITH GABC TYPE B AS DIRECTED BY THE ENGINEER AND PAID FOR UNDER ITEM 302008 - GABC TYPE B PATCHING.
 2. WHEN TYPE 2 CURB OF THE CENTER CHICANE AT A REALIGNED INTERSECTION IS PLACED ADJACENT TO THE TRAVEL LANE, THAT SECTION OF CURB SHALL BE DEPRESSED FLUSH WITH PAVEMENT, TAPERING BACK TO FULL HEIGHT AT A SLOPE OF 6:1.
 3. NO PATTERNED CEMENT CONCRETE SHALL BE USED AT THE NECKDOWN ISLANDS ALONG HEDDINGTON ROAD BETWEEN WEATHERLY ROAD AND LEXTON ROAD.

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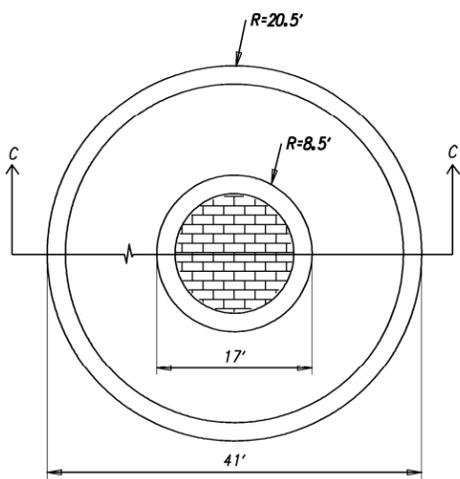




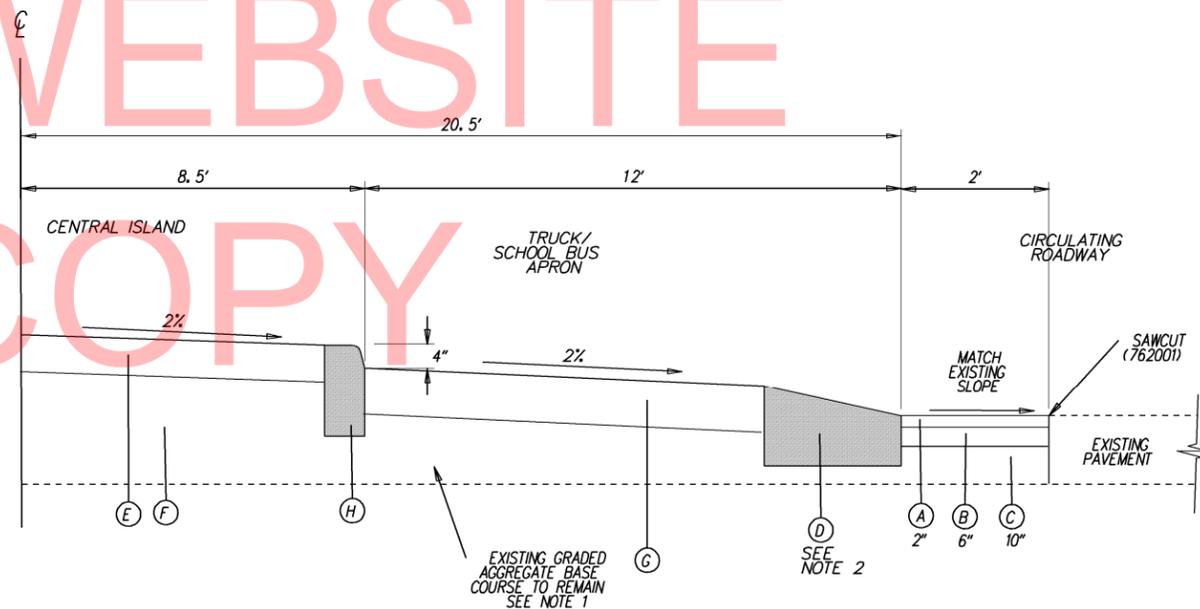
SPLITTER ISLAND - ROUNDABOUT AT CHADDWYCK BLVD AND HEDDINGTON RD



TYPICAL SECTION - SPLITTER ISLAND D-D



CENTRAL ISLAND AND APRON ROUNDABOUT AT CHADDWYCK BLVD AND HEDDINGTON RD



TYPICAL SECTION - ROUNDABOUT C-C

LEGEND

- (A) ITEM 401665 - SUPERPAVE TYPE C HOT-MIX, 160 GYRATIONS, PG 64-22, PATCHING
- (B) ITEM 401667 - SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING
- (C) ITEM 302008 - GRADED AGGREGATE BASE COURSE, TYPE B, PATCHING
- (D) ITEM 701011 - PORTLAND CEMENT CONCRETE CURB, TYPE 2
- (E) ITEM 705519 - PATTERNED PORTLAND CEMENT CONCRETE SIDEWALK, 6"
- (F) ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
- (G) ITEM 501527 - PATTERNED PORTLAND CEMENT CONCRETE PAVEMENT, 9"
- (H) ITEM 701012 - PORTLAND CEMENT CONCRETE CURB, TYPE 1-4

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- NOTES:
1. THE EXISTING HOT-MIX PAVEMENT SECTION SHALL BE REMOVED AS NECESSARY TO CONSTRUCT THE PROPOSED ROUNDABOUT. PAYMENT FOR HOT-MIX PAVEMENT REMOVAL SHALL BE MADE UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT. THE EXISTING SUBBASE MATERIAL BELOW THE PROPOSED ISLAND SHALL REMAIN IN PLACE AS DIRECTED BY THE ENGINEER. ANY WORK NECESSARY TO RECOMPACT THE EXISTING SUBBASE MATERIAL SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT. ANY VOIDS RESULTING FROM EXCAVATING THE EXISTING PAVEMENT SECTION SHALL BE BACKFILLED WITH GABC TYPE B AS DIRECTED BY THE ENGINEER AND PAID FOR UNDER ITEM 302008 - GABC TYPE B PATCHING.
 2. TYPE 2 CURB LOCATED ALONG THE EDGE OF THE TRUCK/SCHOOL BUS APRON SHALL BE DEPRESSED FLUSH WITH PAVEMENT, TAPERING BACK TO FULL HEIGHT AT A SLOPE OF 6:1.

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	11+69.64	32.34	572938.70	560067.55
2	11+68.24	28.90	572942.03	560065.91
3	12+28.96	28.62	572946.72	560126.45
4	12+27.60	32.07	572943.18	560125.34

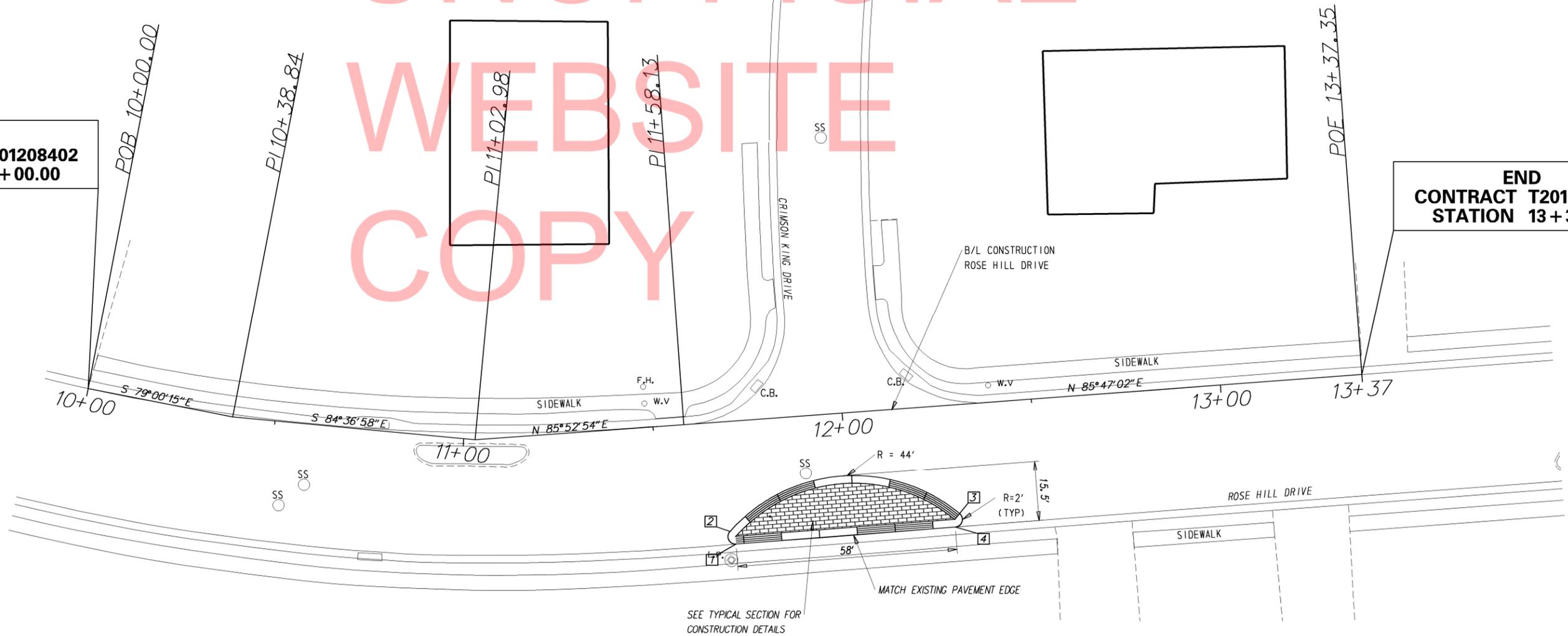
NOTE: ALL OFFSETS ARE RIGHT OF BASELINE, UNLESS OTHERWISE NOTED.



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BEGIN
CONTRACT T201208402
STATION 10+00.00

END
CONTRACT T201208402
STATION 13+37.35



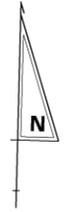
CONSTRUCTION SEQUENCING NOTES

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE UTILIZING TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD.
3. IMMEDIATELY FOLLOWING CURB AND HOT-MIX PATCHING COMPLETION, INSTALL ALL INTERSECTION STRIPING AND RAISED PAVEMENT MARKERS. THE CONTRACTOR MUST MAKE SURE THAT THE DELDOT SIGN SHOP WILL INSTALL ALL STOP SIGNS AND OBJECT MARKERS AT THE SAME TIME AS THE INTERSECTION STRIPING IS BEING INSTALLED.
4. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND TRAFFIC CONTROL DEVICES.

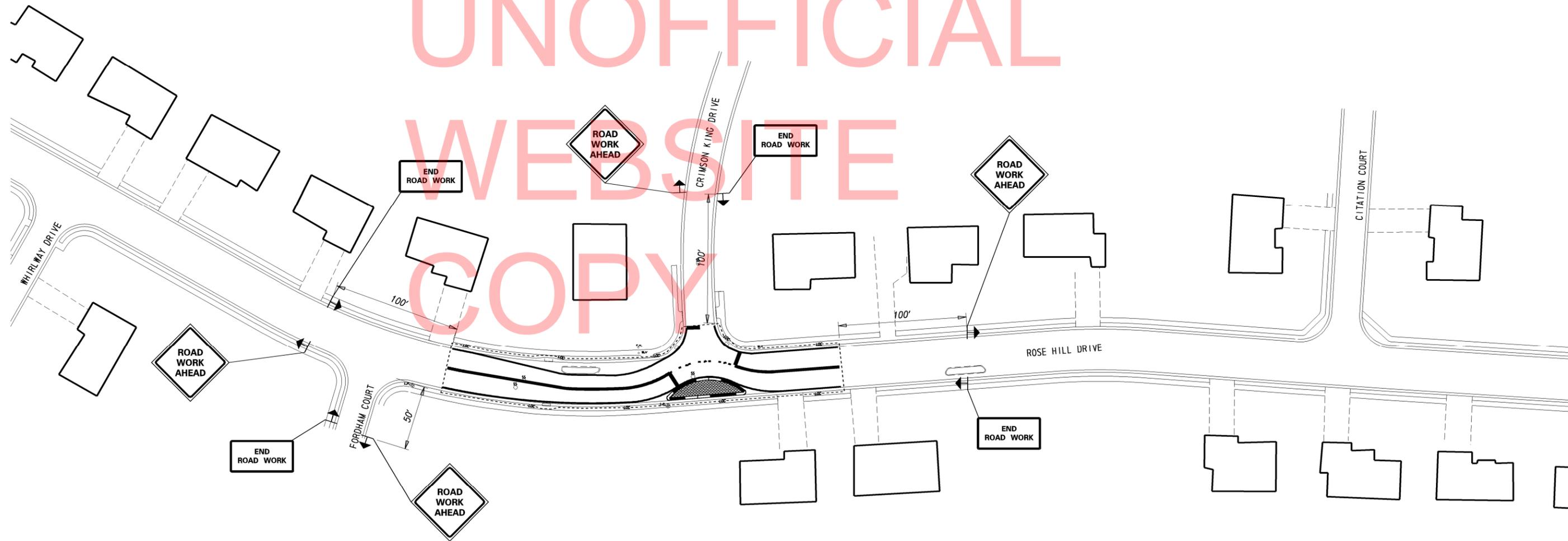
→ WARNING SIGN

NOTES:

1. PLACE ALL TEMPORARY WARNING SIGNS (ITEM 743525) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION
2. ALL LOCATIONS SHOWN ARE APPROXIMATE



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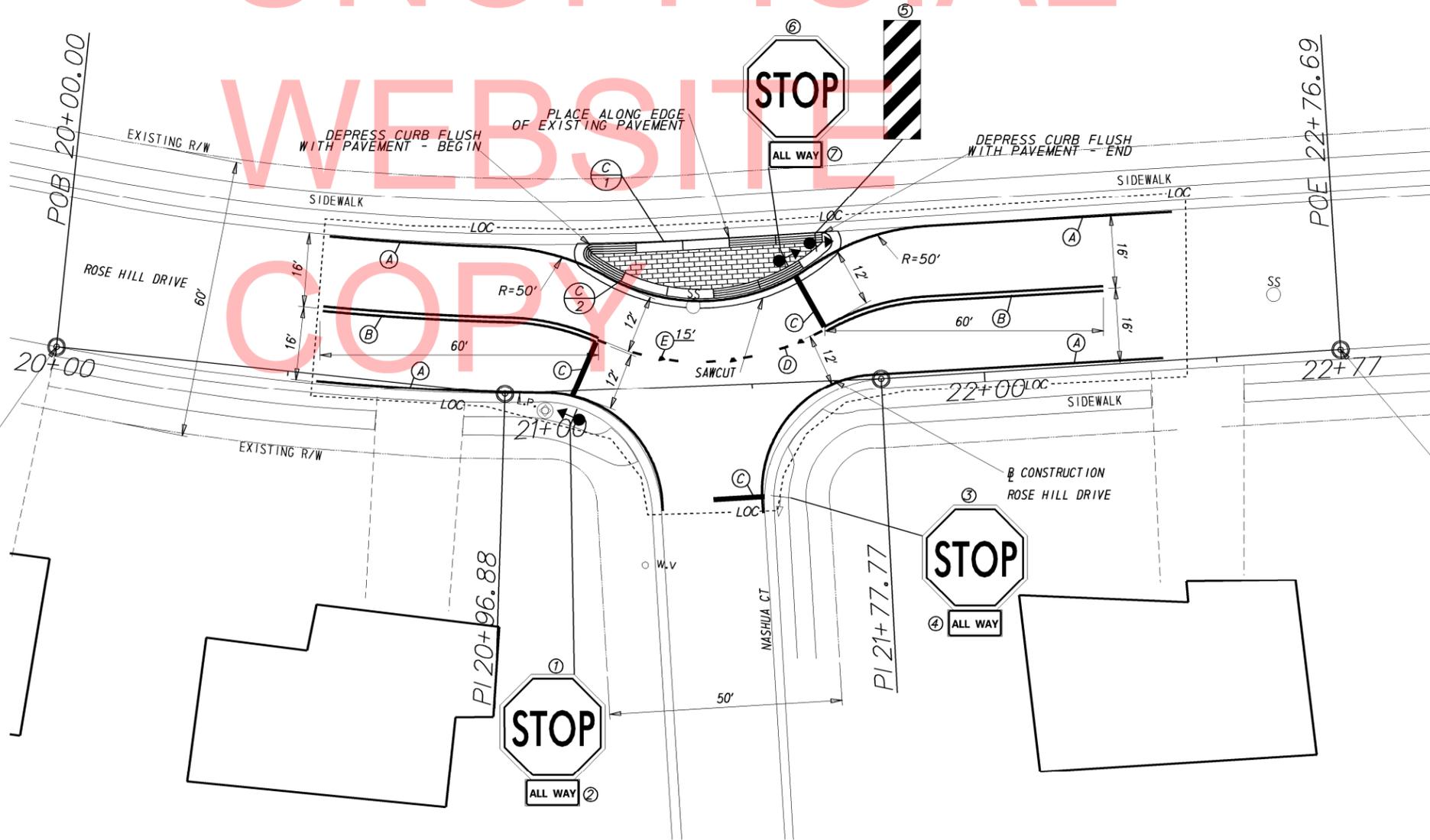
PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	380 LF
(B)	5" SOLID DOUBLE YELLOW EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	120 LF
(C)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	50 SF
(D)	5" YELLOW EPOXY RESIN PAVEMENT AUXILIARY LANE LINE STRIPING, 2' LINE & 6' GAP (ITEM 748548)	50 LF
(E)	RAISED YELLOW PAVEMENT MARKER (SPACING AS NOTED) (ITEM 748502)	3 EA

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P. C. C. CURB TYPE 2	50 LF
2	P. C. C. CURB TYPE 2	66 LF

NOTES:
1. STRIPE ADJACENT TO GUTTER



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BEGIN
CONTRACT T201208402
STATION 20+00.00

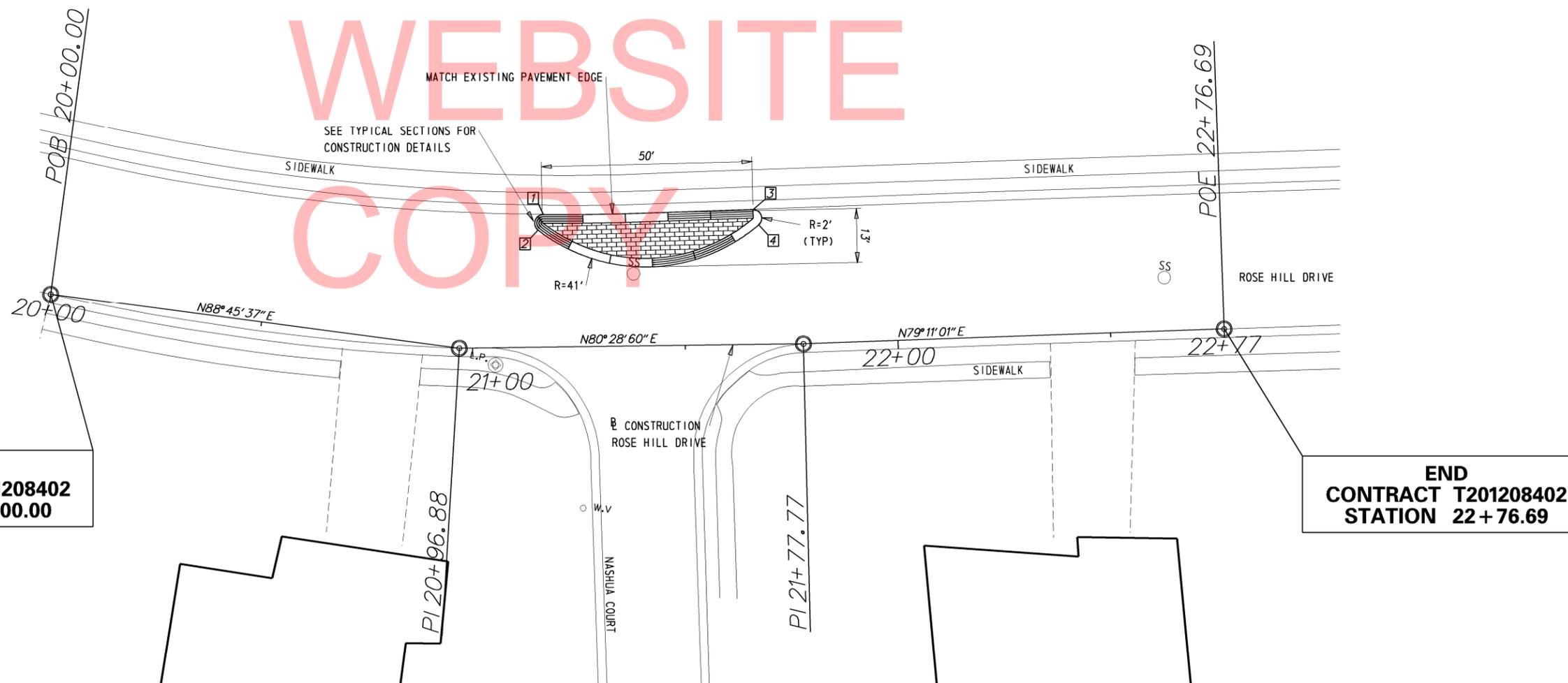
END
CONTRACT T201208402
STATION 22+76.69

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	21+17.03	31.10	572972.61	560924.71
2	21+15.73	27.54	572968.88	560924.02
3	21+66.28	31.65	572981.30	560973.20
4	21+67.61	28.12	572978.04	560975.09

NOTES:
 1. ALL OFFSETS ARE LEFT OF BASELINE UNLESS OTHERWISE NOTED.



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BEGIN
 CONTRACT T201208402
 STATION 20+00.00

END
 CONTRACT T201208402
 STATION 22+76.69

CONSTRUCTION SEQUENCING NOTES

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE UTILIZING TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD.
3. IMMEDIATELY FOLLOWING CURB AND HOT-MIX PATCHING COMPLETION, INSTALL ALL INTERSECTION STRIPING AND RAISED PAVEMENT MARKERS. THE CONTRACTOR MUST MAKE SURE THAT THE DELDOT SIGN SHOP WILL INSTALL ALL STOP SIGNS AND OBJECT MARKERS AT THE SAME TIME AS THE INTERSECTION STRIPING IS BEING INSTALLED.
4. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT.

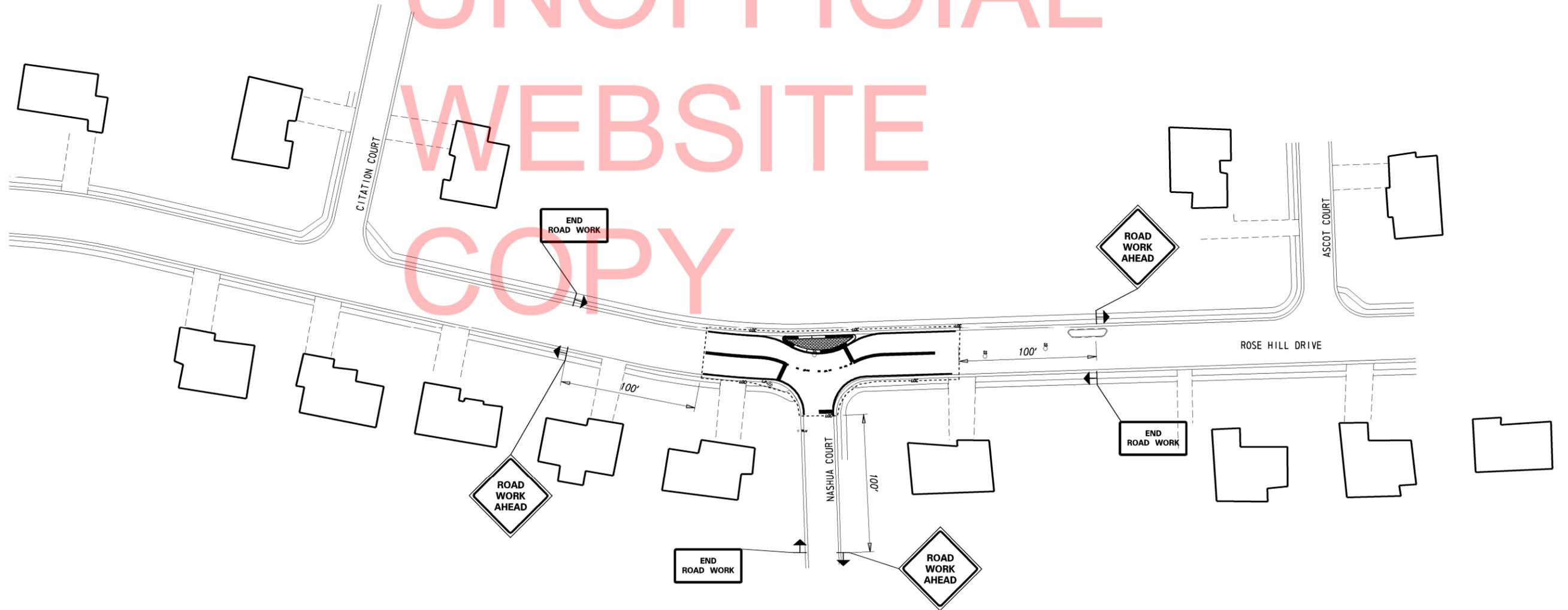
→ WARNING SIGN

NOTES:

1. PLACE ALL TEMPORARY WARNING SIGNS (ITEM 734525) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION
2. ALL LOCATIONS SHOWN ARE APPROXIMATE



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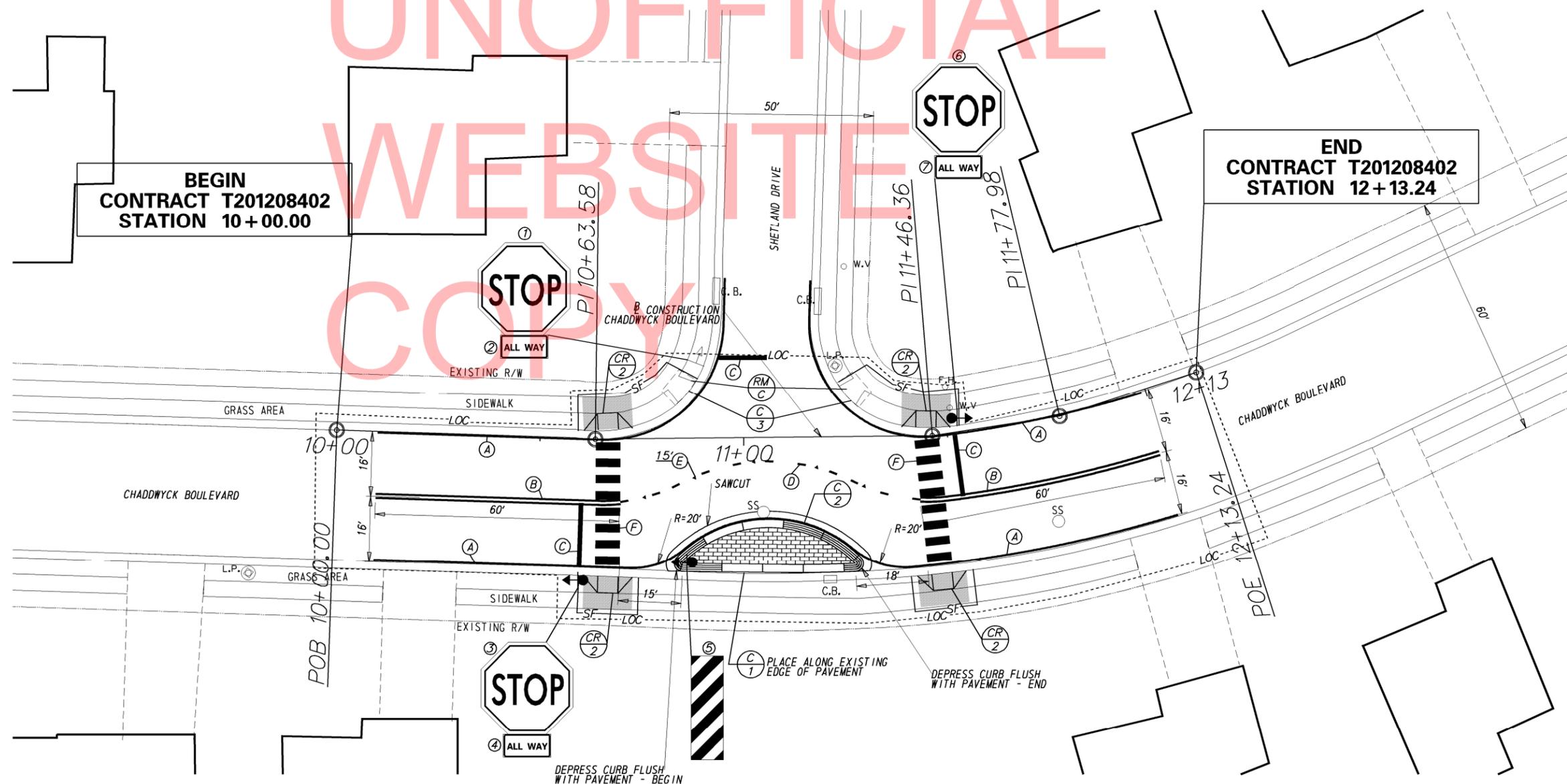
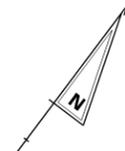


PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	426 LF
(B)	5" SOLID DOUBLE YELLOW EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	120 LF
(C)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	60 SF
(D)	5" YELLOW EPOXY RESIN PAVEMENT AUXILIARY LANE LINE STRIPING, 2' LINE & 6' GAP (ITEM 748548)	78 LF
(E)	RAISED YELLOW PAVEMENT MARKER (SPACING AS NOTED) (ITEM 748502)	5 EA
(F)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	192 SF

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P. C. C. CURB TYPE 2	53 LF
2	P. C. C. CURB TYPE 2	50 LF
3	INTEGRAL PCC CURB AND GUTTER, TYPE 2	20 LF

NOTES:

1. STRIPE ADJACENT TO GUTTER.
2. AFTER THE REMOVAL OF EXISTING CURB RAMPS, THE CONTRACTOR SHALL REPLACE THE SECTION WITH INTEGRAL CURB AND GUTTER TO MATCH THE SURROUNDING CURB AND GUTTER SECTION.

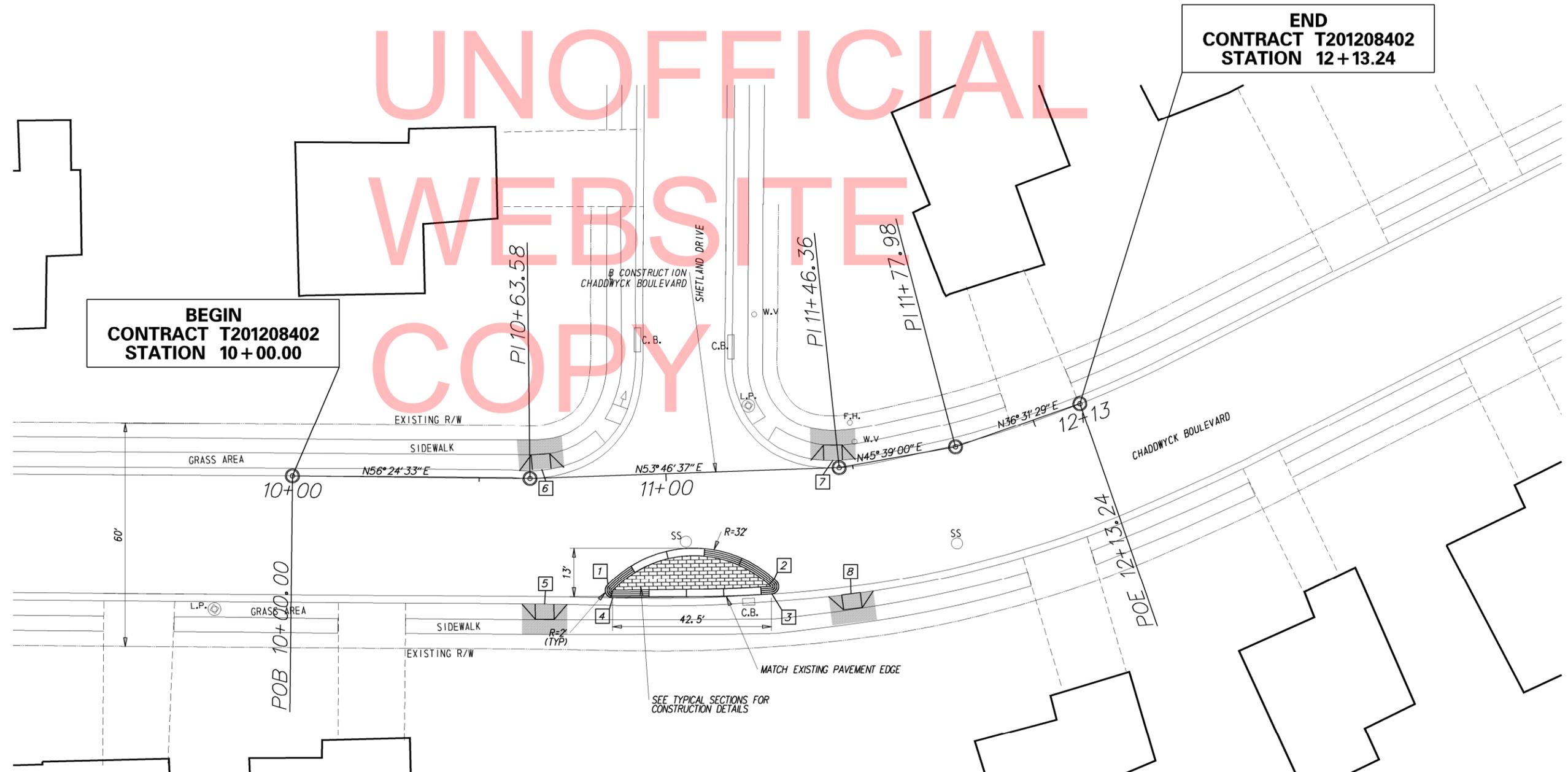
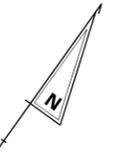


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COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	10+83.40	29.10	594957.74	596671.51
2	11+27.94	29.14	594984.03	596707.47
3	11+27.18	33.11	594980.38	596709.19
4	10+84.61	32.67	594955.59	596674.59
5	10+66.79	33.78	594943.88	596660.50
6	10+66.79	2.36	594973.30	596639.53
7	11+44.81	2.09	594019.19	596702.62
8	11+48.37	34.10	594992.21	596726.80

NOTES:
 1. ALL OFFSETS ARE RIGHT OF BASELINE UNLESS OTHERWISE NOTED.
 2. OFFSETS FOR POINTS 6 AND 7 ARE LEFT OF BASELINE.

NOTE:
 1. INSTALL ONE 2 FOOT WIDE SIDEWALK SURFACE DETECTABLE WARNING SYSTEM (705007) STRIP PARALLEL TO VEHICULAR TRAFFIC CENTERED WITHIN THE CURB RAMP. CURB RAMP SHALL BE SLOPED TO ENSURE THAT NO LOW POINTS EXIST WITHIN THE CURB RAMP. THAT SLOPES IN EVERY DIRECTION SHALL NOT EXCEED 2%.
 2. POINTS 5-8 REPRESENT THE CENTERLINES OF THE CURB RAMP.



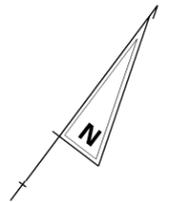
CONSTRUCTION SEQUENCING NOTES

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN ON THE CONSTRUCTION PLAN AND AS DIRECTED BY THE ENGINEER.
3. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE AND CURB RAMPS UTILIZING TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD. DURING THE CONSTRUCTION OF CURB RAMPS, PEDESTRIANS USING SIDEWALKS MUST BE ACCOMMODATED BY FLAGGERS WORKING ON THE PROJECT.
4. IMMEDIATELY FOLLOWING CURB AND HOT-MIX PATCHING COMPLETION, INSTALL ALL INTERSECTION STRIPING AND RAISED PAVEMENT MARKERS. THE CONTRACTOR MUST MAKE SURE THAT THE DELDOT SIGN SHOP WILL INSTALL ALL STOP SIGNS AND OBJECT MARKERS AT THE SAME TIME AS THE INTERSECTION STRIPING IS BEING INSTALLED.
5. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL DEVICES.

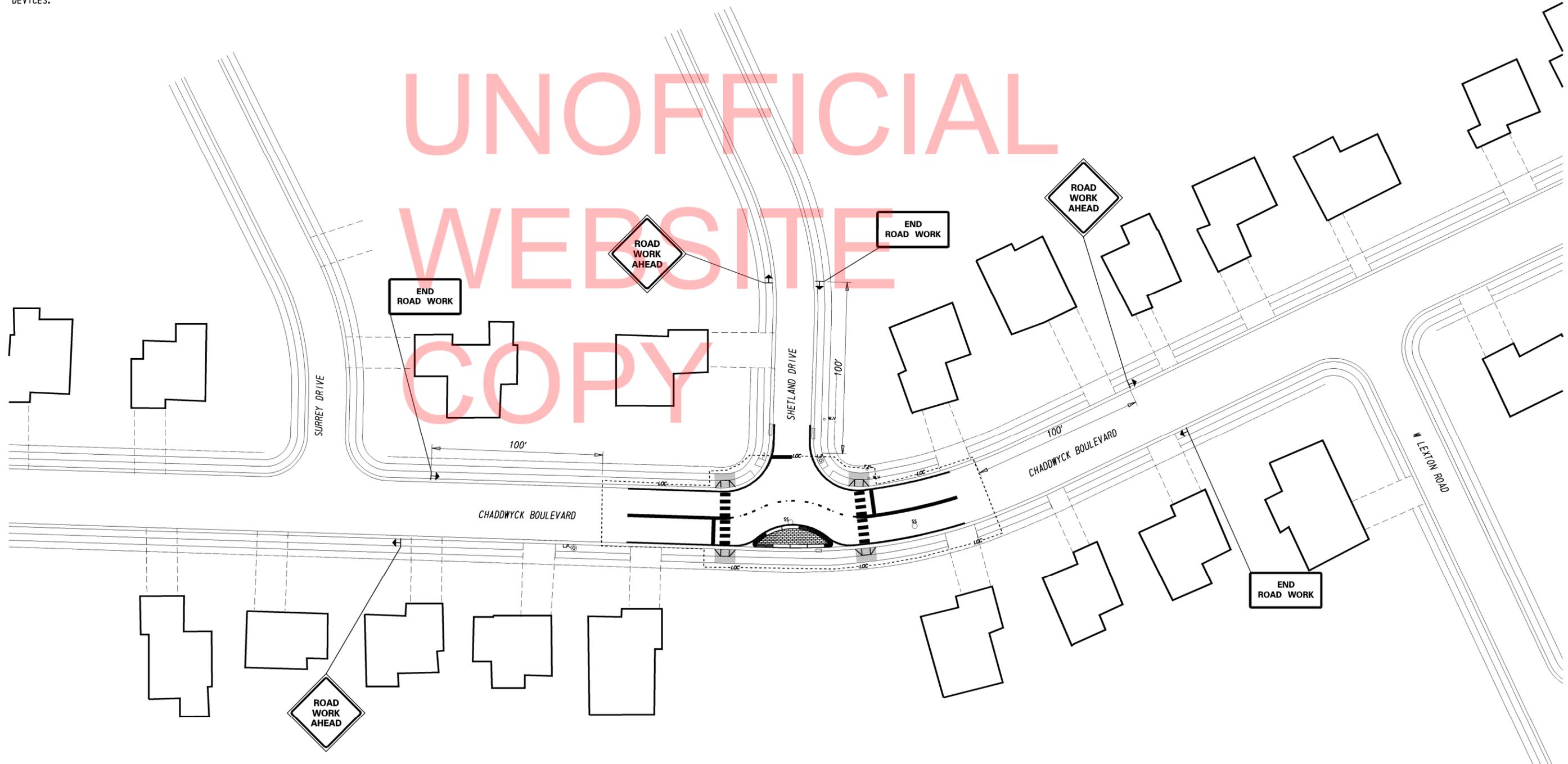
← WARNING SIGN

NOTES:

1. PLACE ALL TEMPORARY WARNING SIGNS (ITEM 743525) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION
2. ALL LOCATIONS SHOWN ARE APPROXIMATE



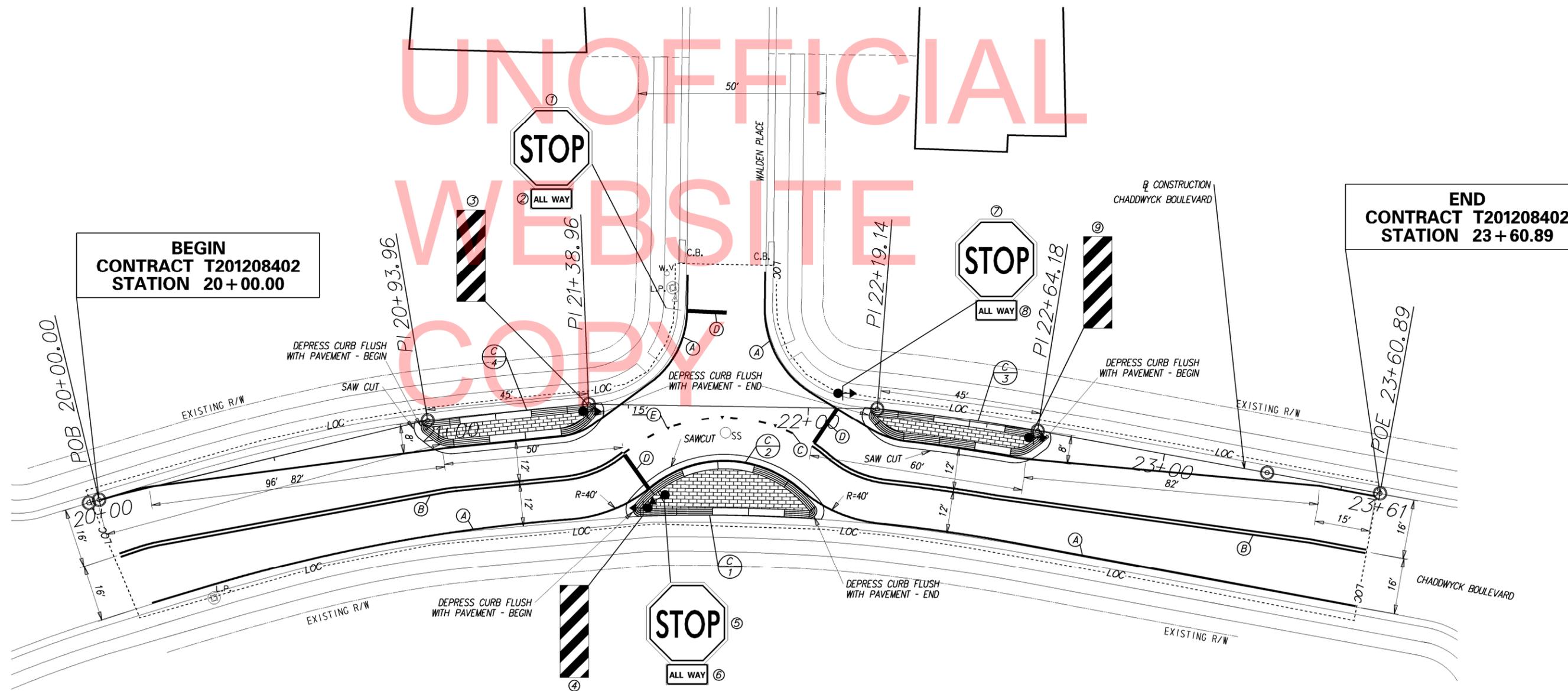
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PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	740 LF
(B)	5" SOLID DOUBLE YELLOW EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	303 LF
(C)	5" YELLOW EPOXY RESIN PAVEMENT AUXILIARY LANE LINE STRIPING, 2' LINE & 6' GAP (ITEM 748548)	56 LF
(D)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	50 SF
(E)	RAISED YELLOW PAVEMENT MARKER (SPACING AS NOTED) (ITEM 748502)	3 EA

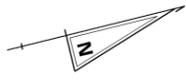
CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P. C. C CURB TYPE 2	47 LF
2	P. C. C CURB TYPE 2	67 LF
3	P. C. C CURB TYPE 2	105 LF
4	P. C. C CURB TYPE 2	105 LF

NOTES:
1. STRIPE ADJACENT TO GUTTER.



BEGIN
CONTRACT T201208402
STATION 20+00.00

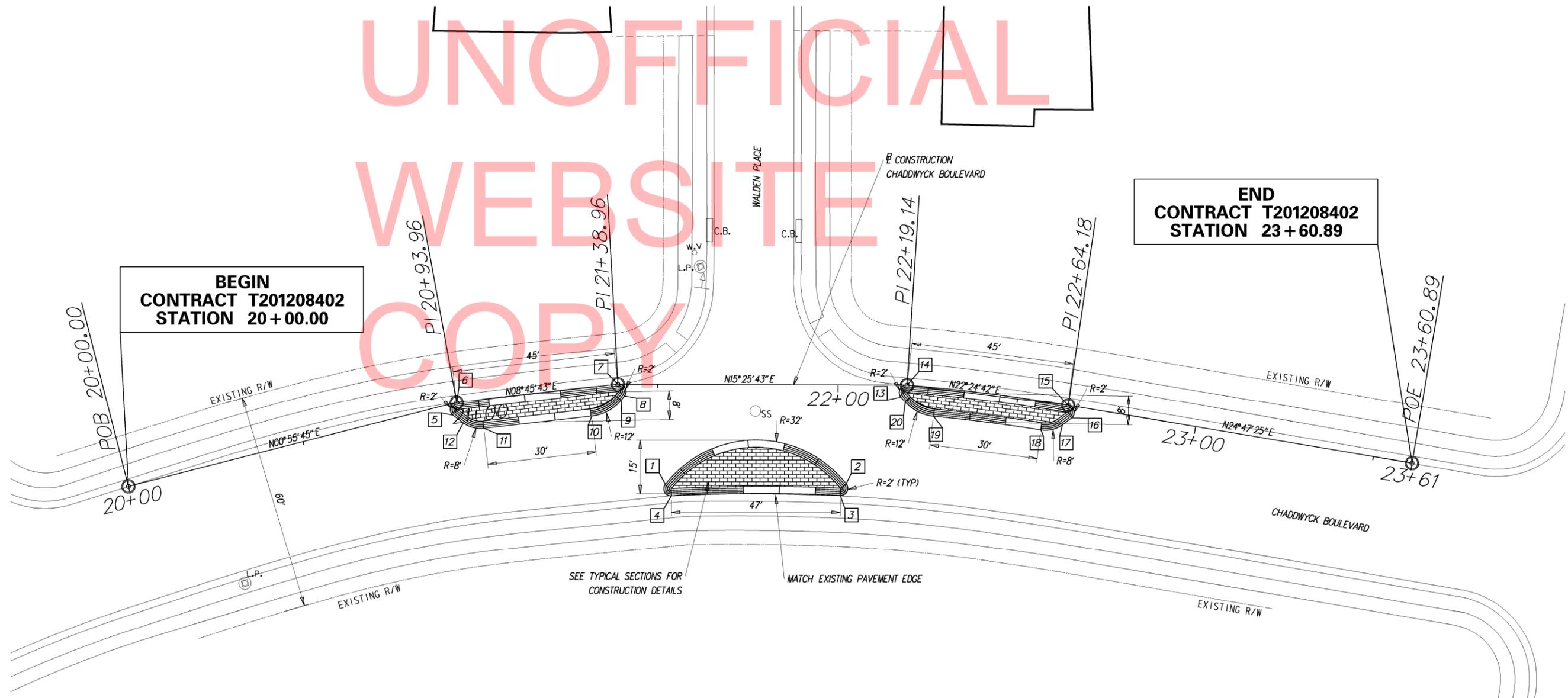
END
CONTRACT T201208402
STATION 23+60.89



COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	21+52.21	27.68	597083.94	597176.24
2	22+02.35	27.38	597132.35	597189.30
3	22+00.71	30.63	597129.90	597191.99
4	21+53.89	30.91	597084.70	597179.80
5	20+92.52	3.27	597032.18	597142.40
6	20+93.96	0.00	597034.05	597139.18
7	21+38.96	0.00	597078.53	597146.04
8	21+40.76	3.23	597079.41	597149.63
9	21+38.10	4.42	597078.19	597150.52
10	21+30.82	8.00	597069.26	597152.70

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
11	21+00.61	8.00	597039.41	597148.10
12	20+95.16	5.85	597034.35	597145.15
13	22+17.48	3.32	597153.34	597170.12
14	22+19.14	0.00	597155.82	597167.37
15	22+64.18	0.00	597197.46	597184.54
16	22+65.63	3.39	597197.36	597188.22
17	22+63.30	5.54	597194.53	597189.32
18	22+57.51	7.75	597188.34	597189.16
19	22+27.54	7.70	597160.65	597177.69
20	22+18.65	4.65	597154.12	597171.72

NOTES:
 1. ALL OFFSETS ARE RIGHT OF BASELINE UNLESS OTHERWISE NOTED.



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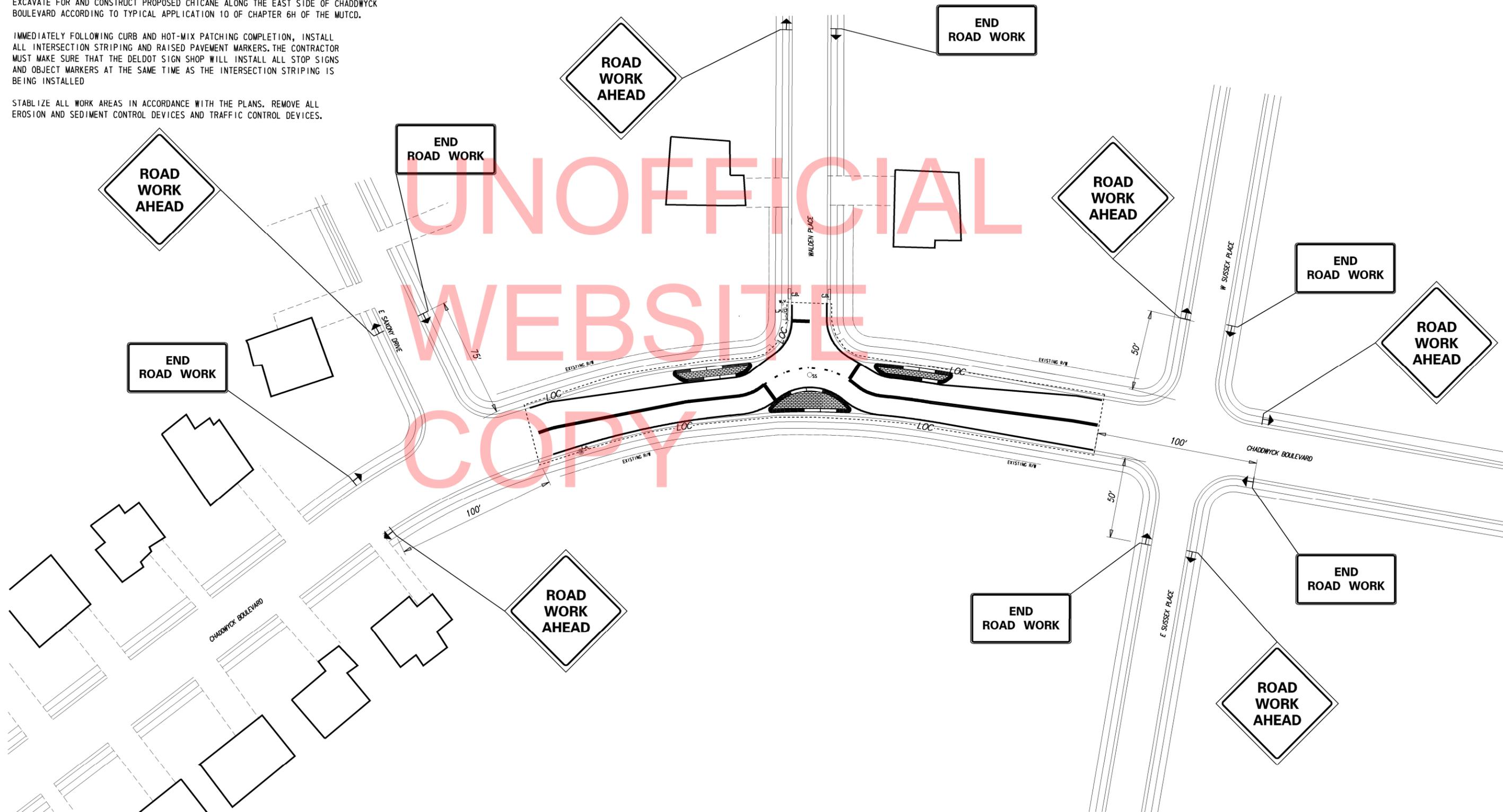
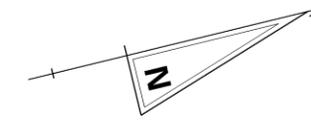
CONSTRUCTION SEQUENCING NOTES

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. MAINTENANCE OF TRAFFIC DURING LANE SHIFTS FOR THE CONSTRUCTION OF CHICANES CHADDWYCK BOULEVARD/ WALDEN PL. INTERSECTION SHALL CONFORM TO CHAPTER 6H OF THE MUTCD.
3. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANES ALONG THE WEST SIDE OF CHADDWYCK BOULEVARD ACCORDING TO TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE MUTCD.
4. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE ALONG THE EAST SIDE OF CHADDWYCK BOULEVARD ACCORDING TO TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE MUTCD.
5. IMMEDIATELY FOLLOWING CURB AND HOT-MIX PATCHING COMPLETION, INSTALL ALL INTERSECTION STRIPING AND RAISED PAVEMENT MARKERS. THE CONTRACTOR MUST MAKE SURE THAT THE DELDOT SIGN SHOP WILL INSTALL ALL STOP SIGNS AND OBJECT MARKERS AT THE SAME TIME AS THE INTERSECTION STRIPING IS BEING INSTALLED
6. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND TRAFFIC CONTROL DEVICES.

→ WARNING SIGN

NOTES:

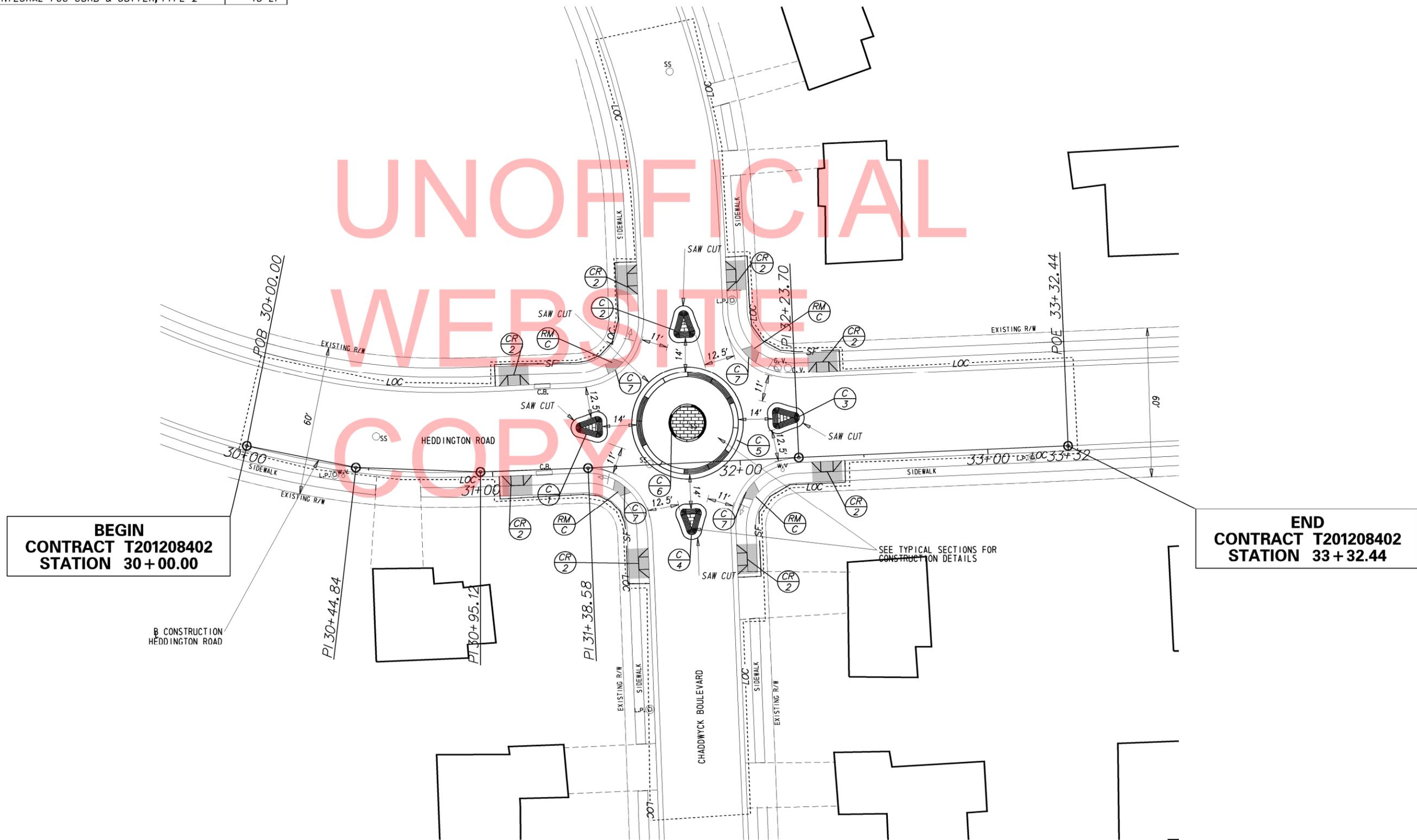
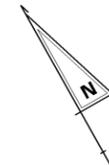
1. PLACE ALL TEMPORARY WARNING SIGNS (ITEM 743525) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION
2. ALL LOCATIONS SHOWN ARE APPROXIMATE



CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P.C.C. CURB TYPE 2	32 LF
2	P.C.C. CURB TYPE 2	32 LF
3	P.C.C. CURB TYPE 2	32 LF
4	P.C.C. CURB TYPE 2	32 LF
5	P.C.C. CURB TYPE 2	130 LF
6	P.C.C. CURB TYPE 1-4	48 LF
7	INTEGRAL PCC CURB & GUTTER, TYPE 2	40 LF

NOTES:

1. AFTER THE REMOVAL OF EXISTING CURB RAMPS, THE CONTRACTOR SHALL REPLACE THE SECTION WITH INTEGRAL CURB AND GUTTER TO MATCH THE SURROUNDING CURB AND GUTTER SECTION.



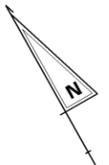
COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	31+35.81	14.29	597268.91	597268.04
2	31+35.80	18.16	596072.31	597269.90
3	31+42.40	20.21	596071.07	597276.45
4	31+42.35	18.21	596068.03	597277.90
5	31+45.11	14.16	596064.47	597275.97
6	31+45.09	12.20	596064.04	597272.62
7	31+77.95	50.52	596081.00	597322.10
8	31+76.00	53.26	596084.33	597321.68
9	31+78.15	59.74	596089.03	597326.64
10	31+82.02	59.73	596087.19	597330.04
11	31+84.13	53.18	596080.42	597328.81
12	31+82.12	50.46	596078.97	597325.75
13	32+16.69	12.06	596028.79	597338.08
14	32+13.98	14.04	596031.82	597336.63
15	32+13.98	18.19	596035.48	597338.58

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
16	32+16.72	20.17	596035.93	597341.94
17	32+23.21	17.98	596030.93	597346.62
18	32+23.17	14.11	596027.54	597344.76
19	31+81.06	27.44	596010.81	597288.02
20	31+77.19	27.42	596012.66	597284.62
21	31+75.16	21.08	596019.21	597285.82
22	31+77.16	18.36	596020.66	597288.87
23	31+81.19	18.40	596018.72	597292.40
24	31+83.14	21.14	596015.39	597292.83
25	31+79.54	16.06	596049.87	597307.22
26	31+79.54	7.56	596043.26	597303.21
27	31+72.04	16.06	596053.41	597299.73
28	31+79.54	23.56	596056.48	597311.24
29	31+87.04	16.06	596046.33	597314.72
30	31+79.54	4.44	596031.80	597297.54

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
31	31+59.04	16.06	596059.55	597289.16
32	31+79.54	36.56	596067.93	597316.91
33	32+00.04	16.06	596040.19	597325.29
34	31+09.80	1.96	596067.29	597237.40
35	31+10.00	34.24	596098.86	597255.11
36	31+62.13	74.78	596109.85	597319.62
37	31+97.99	74.38	596092.55	597351.03
38	32+34.92	34.00	596039.36	597364.67
39	32+34.87	2.12	596007.65	597347.36
40	31+96.90	39.81	595992.43	597296.13
41	31+61.08	39.48	596009.64	597264.72

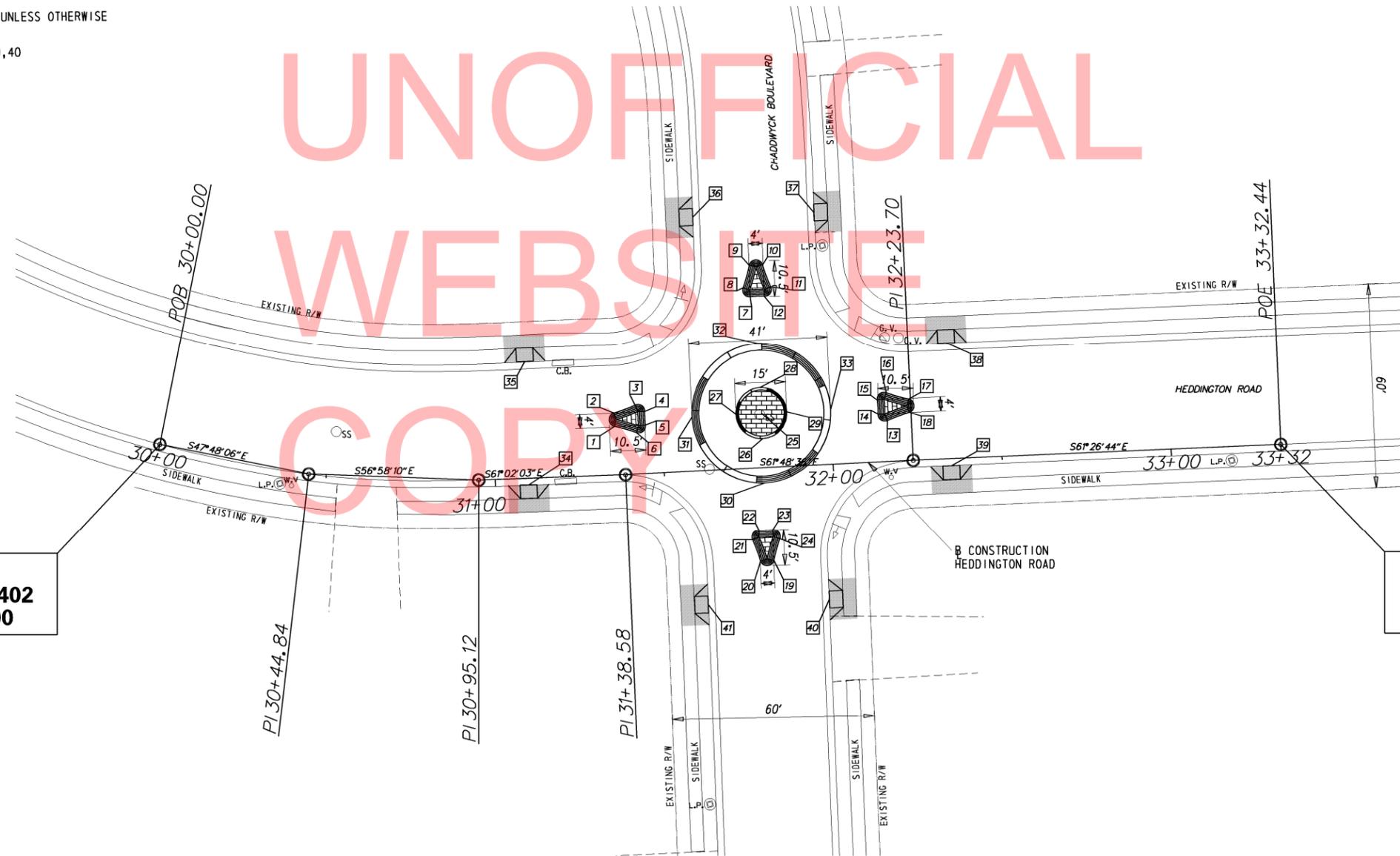
NOTE:

- INSTALL ONE 2 FOOT WIDE SIDEWALK SURFACE DETECTABLE WARNING SYSTEM (705007) STRIP PARALLEL TO VEHICULAR TRAFFIC CENTERED WITHIN THE CURB RAMPS. CURB RAMPS SHALL BE SLOPED TO ENSURE THAT NO LOW POINTS EXIST WITHIN THE CURB RAMP. THAT SLOPES IN EVERY DIRECTION SHALL NOT EXCEED 2%.
- POINTS 34-41 REPRESENT THE CENTER LINES OF THE CURB RAMPS



NOTES:

- ALL OFFSETS ARE LEFT OF BASELINE UNLESS OTHERWISE NOTED.
- OFFSET FOR POINTS 19-24, 30, 34, 39, 40 AND 41 ARE RIGHT OF BASELINE.



**BEGIN
CONTRACT T201208402
STATION 30+00.00**

**END
CONTRACT T201208402
STATION 33+32.44**

ADDITIONAL NOTES

1. MAINTAIN ACCESS TO ALL RESIDENCES IN THE COMMUNITY AT ALL TIMES.
2. ALL DETOUR SIGNING INCLUDING TRAILBLAZERS ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE MUTCD).
3. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN THE DE MUTCD - PART 6 FOR LIGHTS, BARRICADES AND SIGNS (AS PER LATEST REVISION)
4. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF THE DE MUTCD WILL PREVAIL.
5. WARNING SIGNS SHOULD BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
6. BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF ROADWAY.
7. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

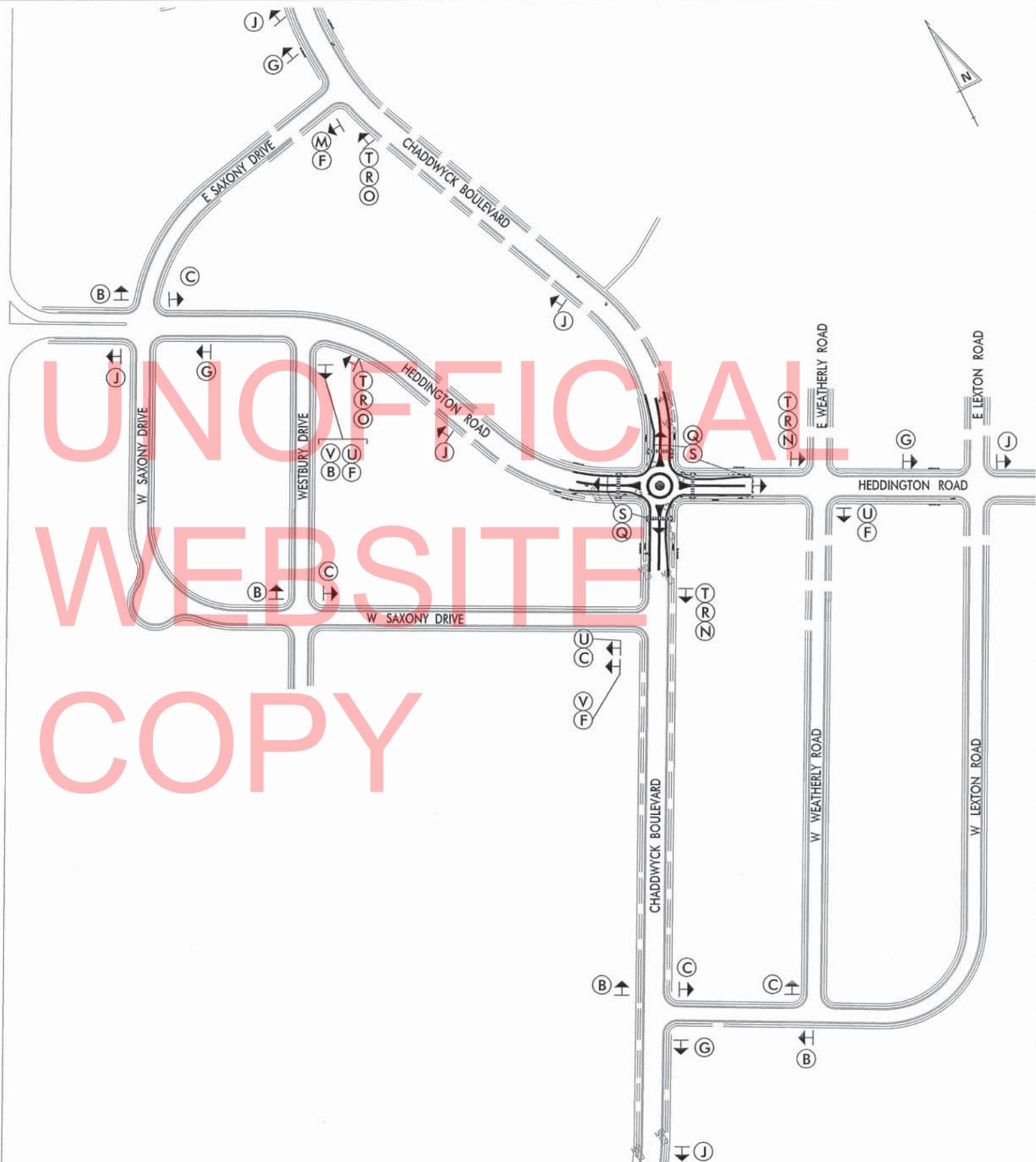
CONSTRUCTION SEQUENCING NOTES

1. CONSTRUCT ALL EIGHT CURB RAMPS.
2. CLOSE THE INTERSECTION OF CHADDWYCK BLVD. AT HEDDINGTON ROAD ON ALL FOUR SIDES WITH BARRICADES AS SHOWN TO CONSTRUCT THE ROUNDABOUT AND THE FOUR SPLITTER ISLANDS. THE EXISTING MANHOLE LOCATED AT THE CENTER OF THE ROUNDABOUT SHOULD BE ADJUSTED FOR VERTICAL HEIGHT AS REQUIRED (ITEM 710506)
3. CONSTRUCT PROPOSED CURB RAMPS UTILIZING TYPICAL APPLICATION 29 OF CHAPTER 6H OF THE DE MUTCD. IF ESTABLISHING OR MAINTAINING AN ALTERNATIVE PEDESTRIAN ROUTE IS NOT FEASIBLE, ADA COMPLIANT TEMPORARY CURB RAMPS MUST BE PROVIDED TO ASSIST PEDESTRIANS WITH DISABILITIES THROUGH THE PROJECT LIMITS.
4. FOLLOWING ISLAND AND HOT-MIX PATCHING COMPLETION, INSTALL ALL ROUNDABOUT STRIPING. THE CONTRACTOR MUST MAKE SURE THAT DELDOT SIGN SHOP WILL INSTALL ALL YIELD SIGNS AND ROUNDABOUT SIGNING AT THE SAME TIME AS THE ROUNDABOUT STRIPING IS BEING INSTALLED.
5. DURING CONSTRUCTION, PEDESTRIANS USING THE SIDEWALKS MUST BE ACCOMMODATED BY FLAGGERS WORKING ON THE PROJECT.
6. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL DEVICES.

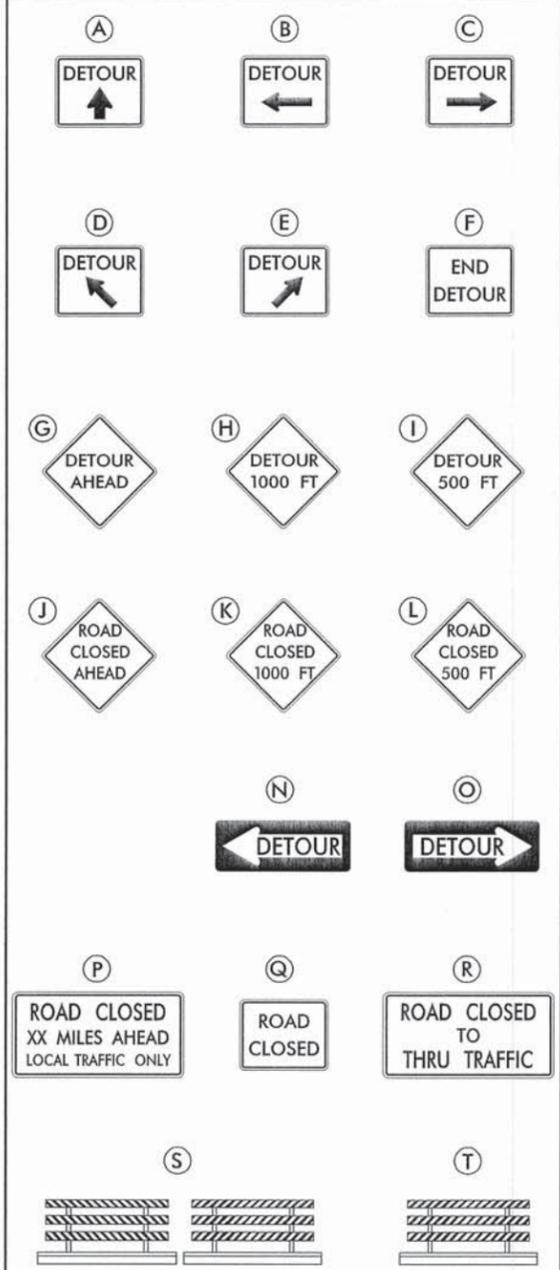
SPECIAL SIGNS



ALL TEMPORARY TRAFFIC CONTROL SIGNS SHALL HAVE A BLACK LEGEND AND BORDER ON A FLUORESCENT ORANGE BACKGROUND THAT COMPLY WITH THE DE MUTCD 2011.



LEGEND



GENERAL NOTES

1. ALL DETOUR SIGNING INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE TO THE DE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE- MUTCD PART 6) FOR LIGHTS, BARRICADES AND SIGNS (AS PER LATEST REVISION)
3. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES WILL PREVAIL.
4. SIGNS J THROUGH L AND P THROUGH R ,THE WORD (ROAD) SHOULD BE CHANGED TO RAMP, RR OR BRIDGE WHERE APPLICABLE.
5. WARNING SIGNS SHOULD BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT SHEETING.
6. "S" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF ROADWAY.
7. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED *[Signature]* DATE: 7/11/12 RECOMMENDED *[Signature]* DATE: 7/18/12 RECOMMENDED _____ DATE: _____ APPROVED CHIEF SAFETY OFFICER *[Signature]* DATE: 7/17/12 APPROVED TRAFFIC ENGINEER *[Signature]* DATE: 7/17/12

DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUM / REVISIONS	

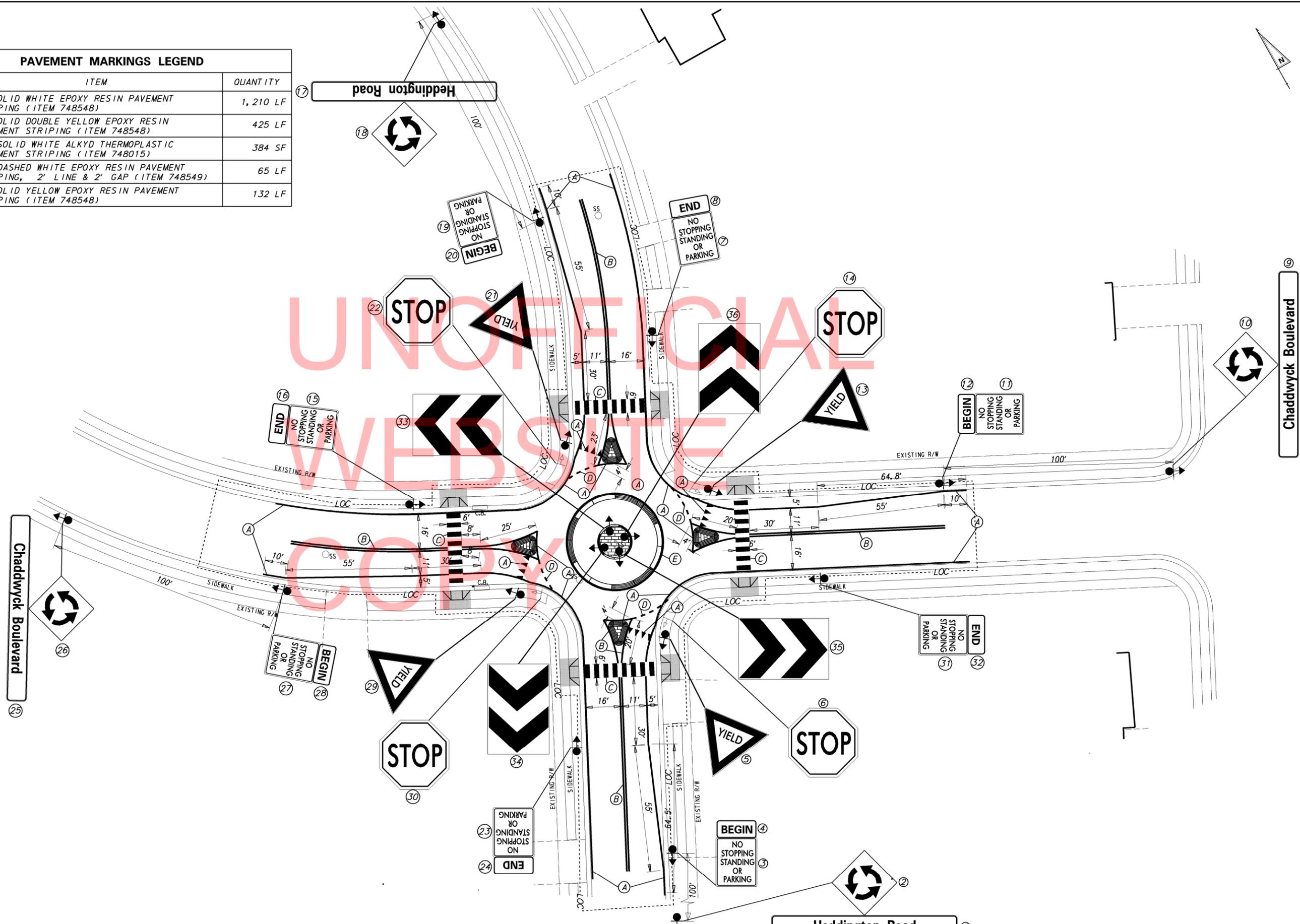
NOT TO SCALE

TRAFFIC CALMING MISCELLANEOUS LOCATIONS NEW CASTLE COUNTY, 2012

CONTRACT	PERMIT NO.	X
T201208402	DESIGNED BY: SM	
COUNTY	CHECKED BY: ATD	
NEW CASTLE		

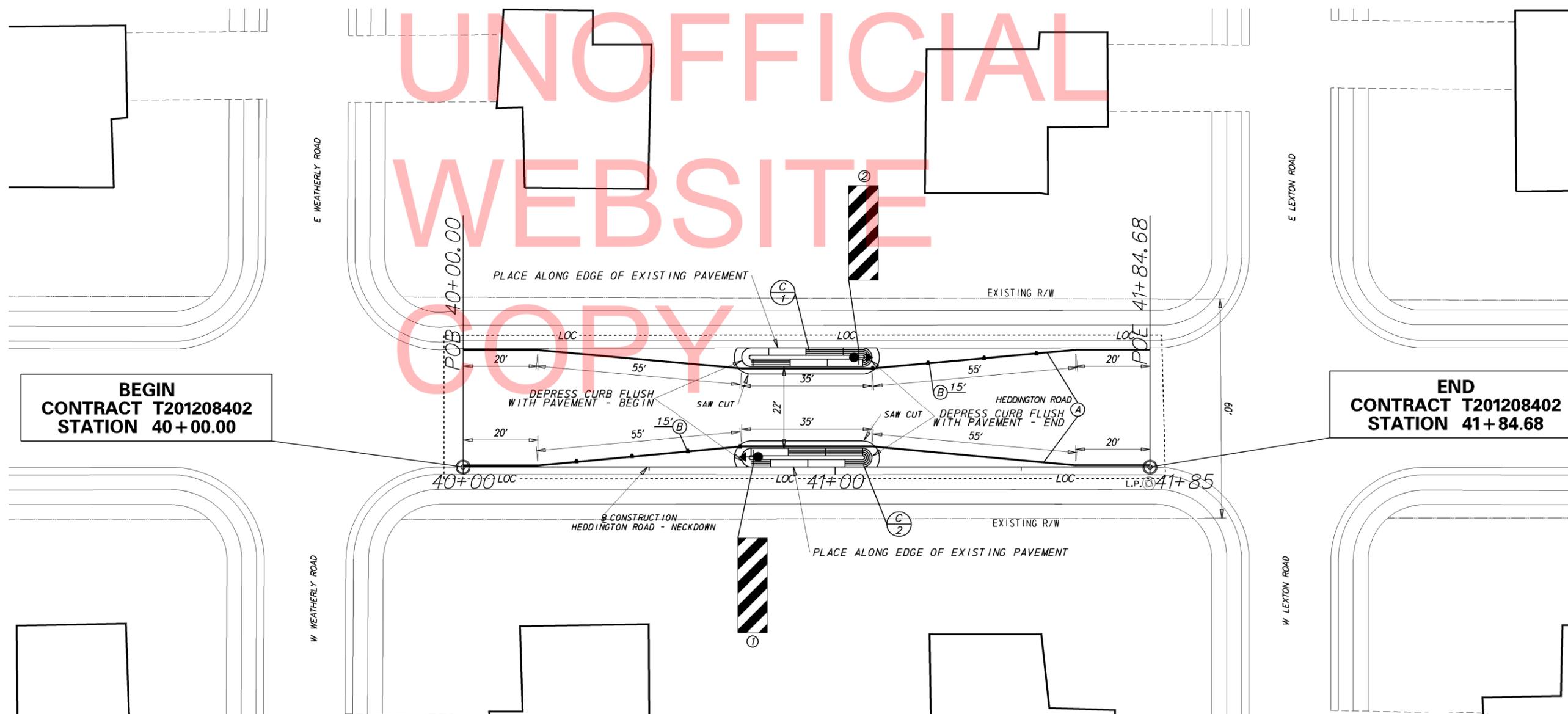
DETOUR PLAN	SHEET NO.
CHADDWYCK BLVD AT HEDDINGTON ROAD	21
	TOTAL SHTS.
	29

PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	1,210 LF
(B)	5" SOLID DOUBLE YELLOW EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	425 LF
(C)	24" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	384 SF
(D)	10" DASHED WHITE EPOXY RESIN PAVEMENT STRIPING, 2" LINE & 2" GAP (ITEM 748549)	65 LF
(E)	5" SOLID YELLOW EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	132 LF



PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	5" SOLID WHITE EPOXY RESIN PAVEMENT STRIPING (ITEM 748548)	370 LF
(B)	RAISED WHITE PAVEMENT MARKER (SPACING AS NOTED) (ITEM 748502)	8 EA

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	P.C.C. CURB TYPE 2	76 LF
2	P.C.C. CURB TYPE 2	76 LF

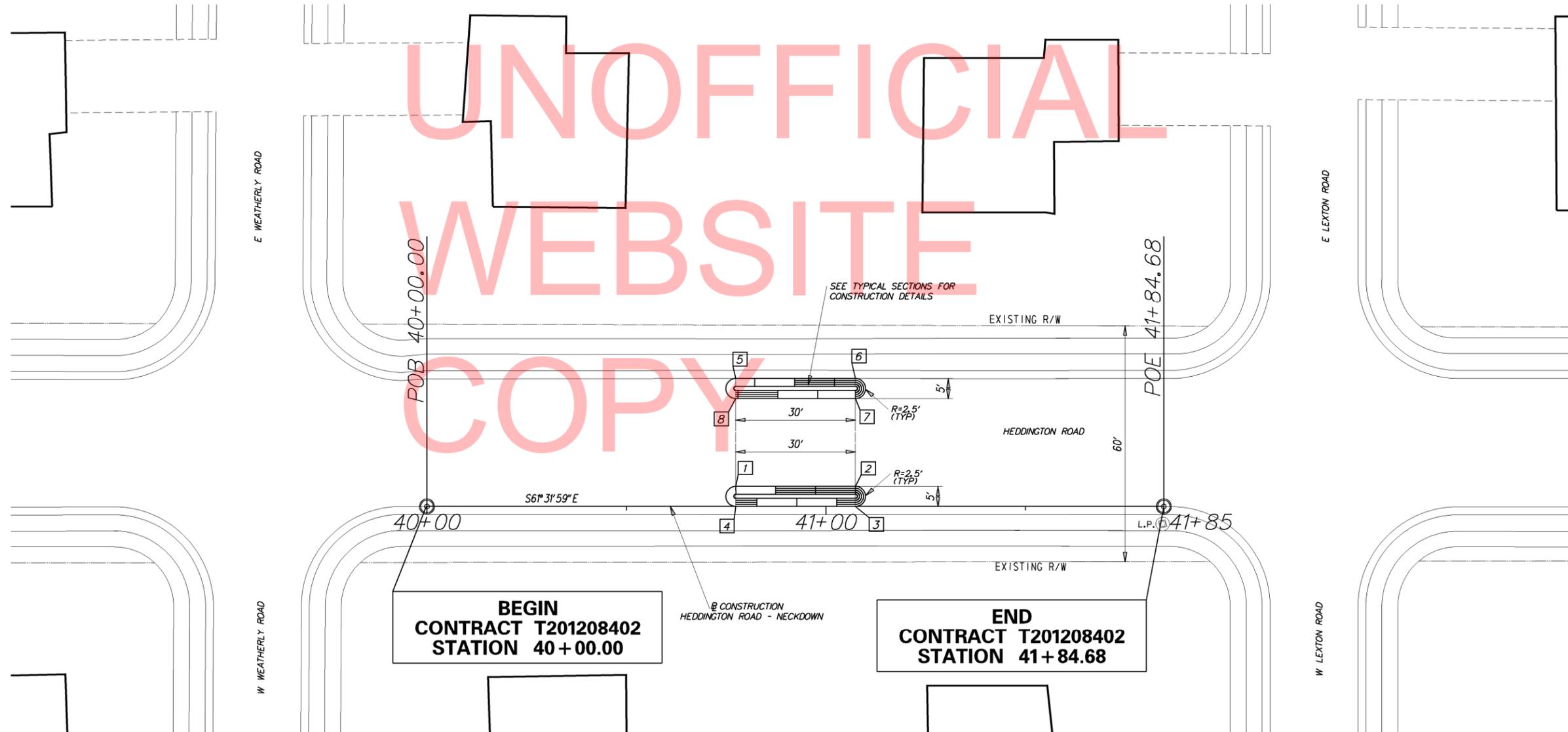
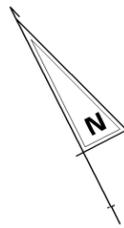


BEGIN
CONTRACT T201208402
STATION 40+00.00

END
CONTRACT T201208402
STATION 41+84.68

COORDINATE LIST				
POINT NO.	STATION	OFFSET	NORTHING	EASTING
1	40+77.41	5.00	595854.93	597644.05
2	41+07.41	5.00	595840.63	597670.42
3	41+07.41	0.00	595836.23	597688.04
4	40+77.41	0.00	595850.53	597641.67
5	40+77.41	32.00	595878.66	597656.92
6	41+07.41	32.0	595864.36	597683.30
7	41+07.41	27.00	595859.97	597680.91
8	40+77.41	27.0	595874.27	597654.54

NOTES:
 1. ALL OFFSETS ARE LEFT OF BASELINE UNLESS OTHERWISE NOTED.



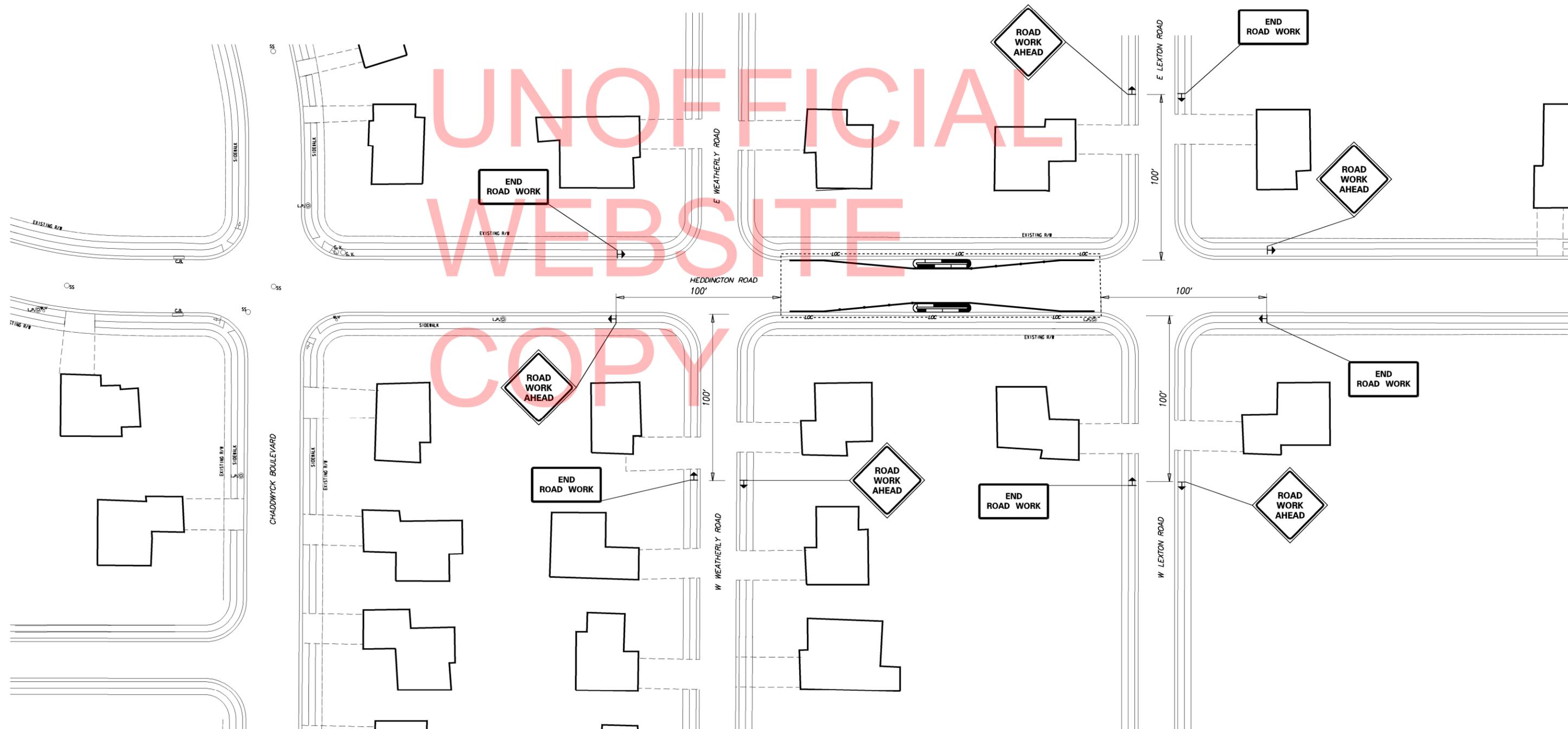
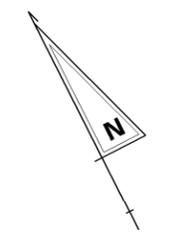
CONSTRUCTION SEQUENCING NOTES

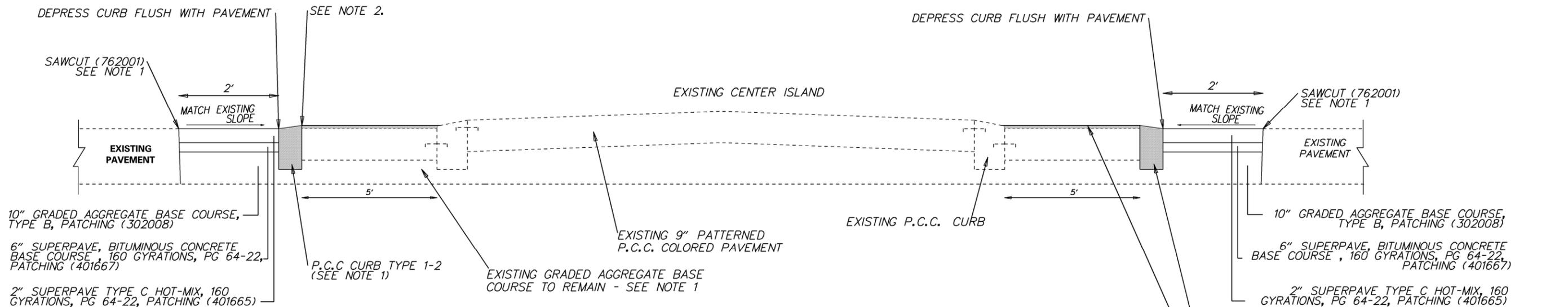
1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES AS SHOWN ON THE CONSTRUCTION PLAN AND AS DIRECTED BY THE ENGINEER.
3. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE ON THE NORTH SIDE OF THE ROADWAY UTILIZING TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD.
4. EXCAVATE FOR AND CONSTRUCT PROPOSED CHICANE ON THE SOUTH SIDE OF THE ROADWAY UTILIZING CHAPTER 6H OF THE DE MUTCD.
5. IMMEDIATELY FOLLOWING CURB AND HOT-MIX PATCHING COMPLETION, INSTALL ALL INTERSECTION STRIPING AND RAISED PAVEMENT MARKERS. THE CONTRACTOR MUST MAKE SURE THAT THE DELDOT SIGN SHOP WILL INSTALL ALL STOP SIGNS AND OBJECT MARKERS AT THE SAME TIME AS THE INTERSECTION STRIPING IS BEING INSTALLED.
6. STABILIZE ALL WORK AREAS IN ACCORDANCE WITH THE PLANS. REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL DEVICES.

→ WARNING SIGN

NOTES:

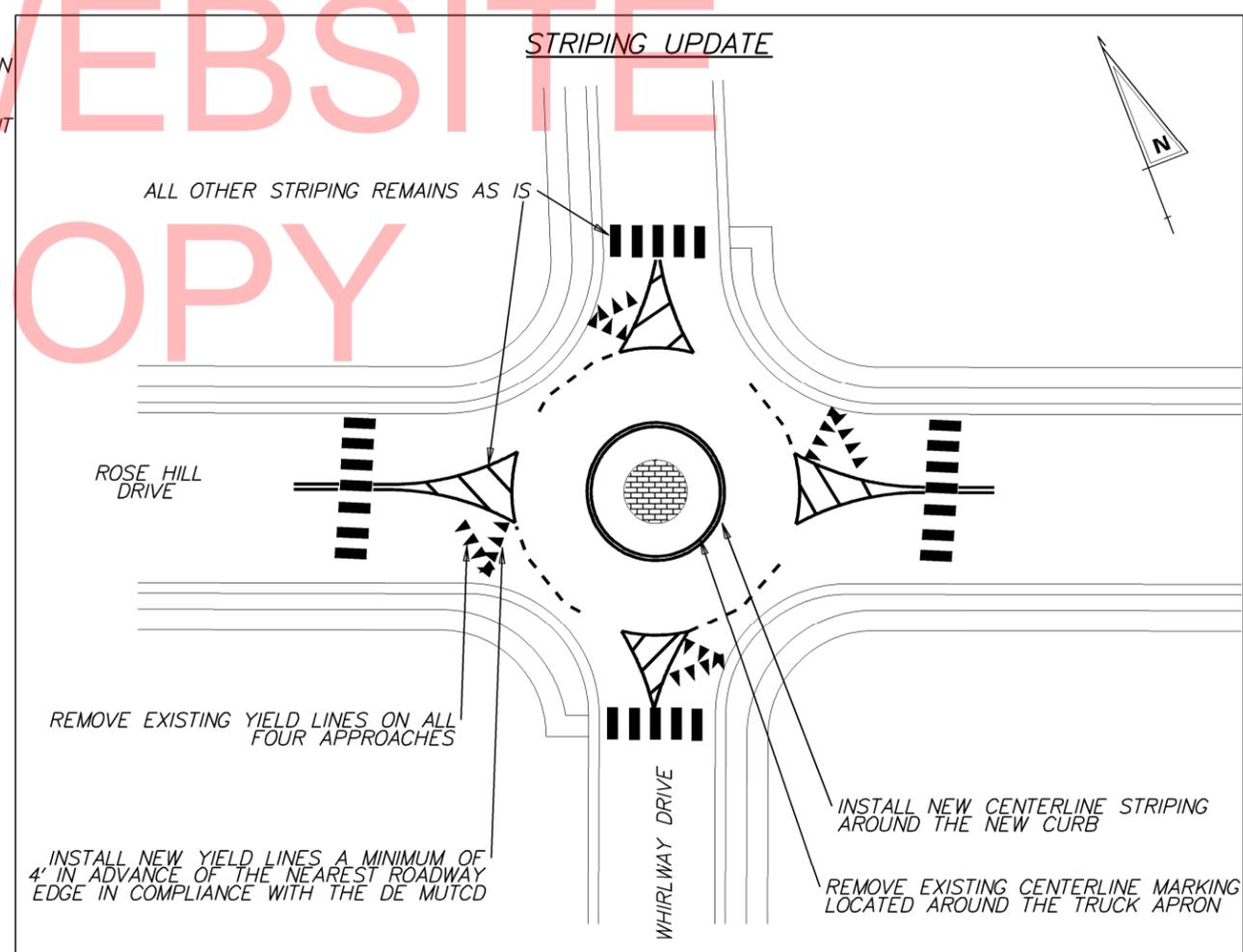
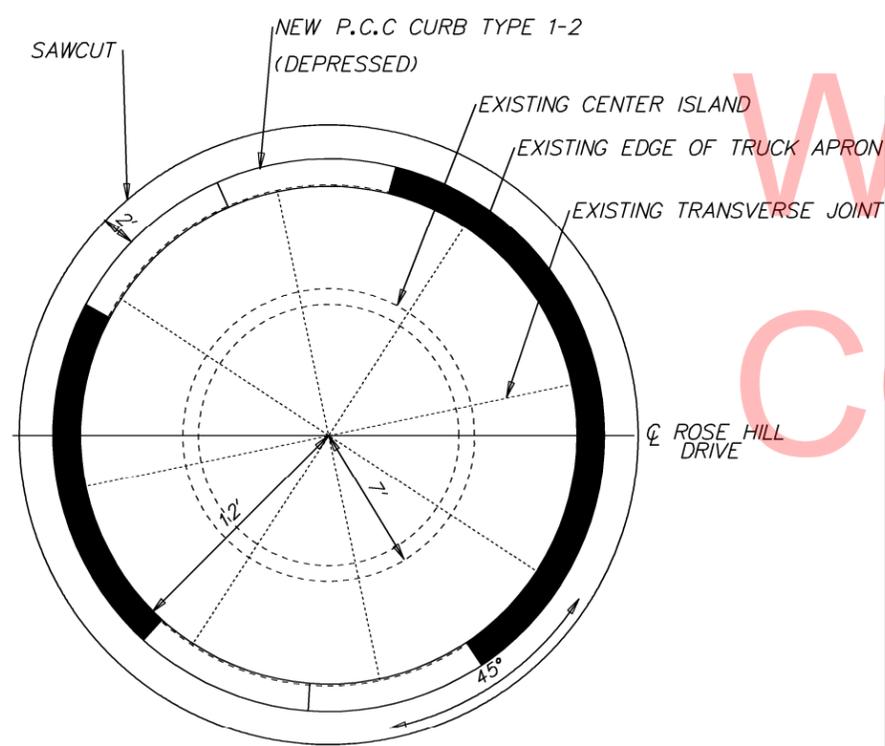
1. PLACE ALL TEMPORARY WARNING SIGNS (ITEM 743525) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION
2. ALL LOCATIONS SHOWN ARE APPROXIMATE





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WEBSITE
COPY

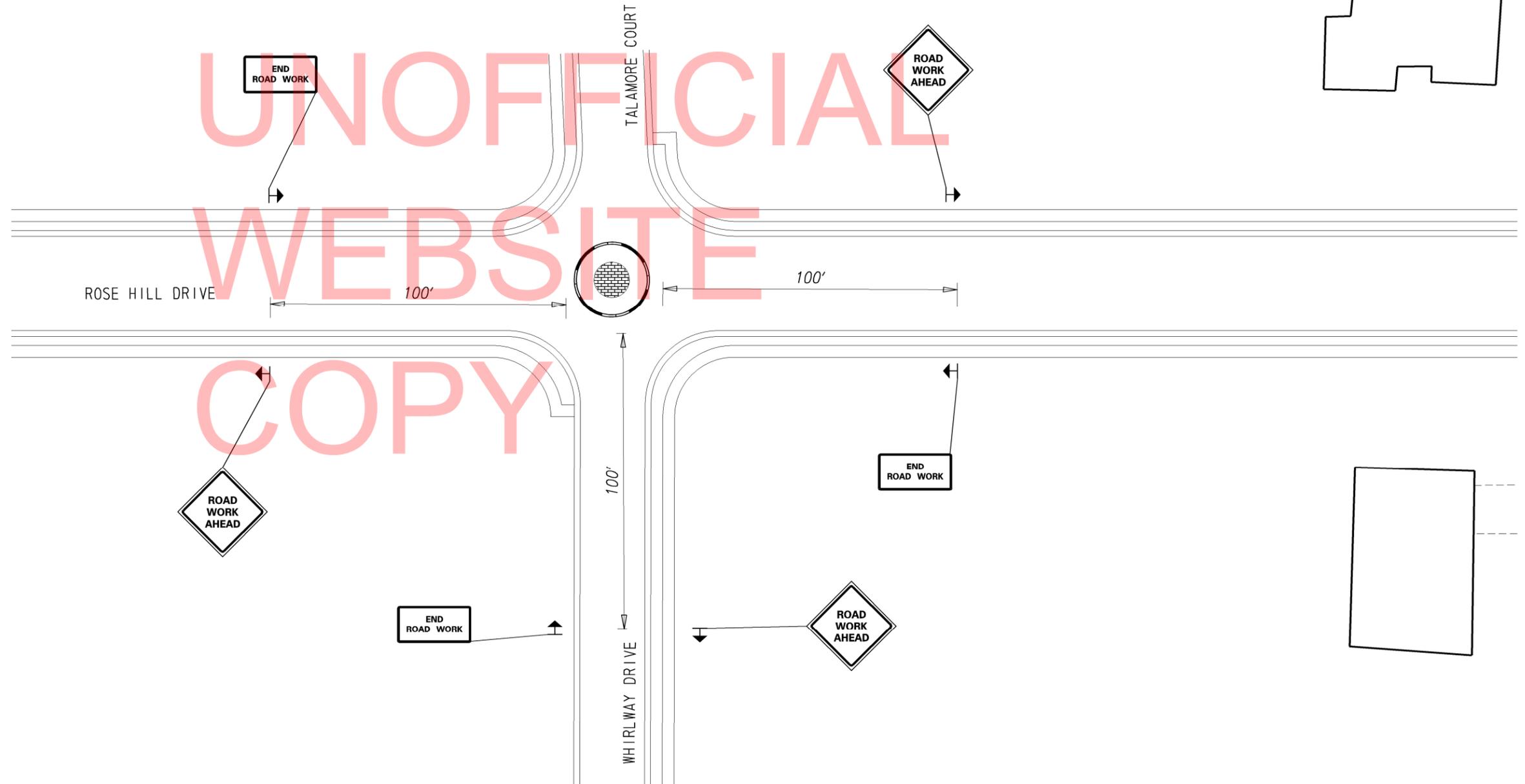
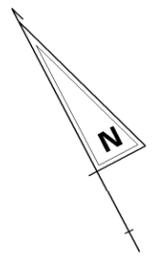
ROSE HILL DRIVE
TRAFFIC CIRCLE AT ASSAULT COURT / WHIRLWAY DRIVE



- NOTES:
1. THE EXISTING HOT-MIX PAVEMENT AROUND THE PERIMETER OF THE EXISTING TRUCK APRON SHALL BE SAWCUT AND REMOVED AS NECESSARY TO CONSTRUCT THE PROPOSED CURB SECTIONS. PAYMENT FOR HOT-MIX PAVEMENT REMOVAL SHALL BE MADE UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT. THE EXISTING SUBBASE MATERIAL BELOW THE PROPOSED CURB SHALL REMAIN IN PLACE AS DIRECTED BY THE ENGINEER. ANY WORK NECESSARY TO RECOMPACT THE EXISTING SUBBASE MATERIAL SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT. ANY VOIDS RESULTING FROM EXCAVATING THE EXISTING PAVEMENT SECTION SHALL BE BACKFILLED WITH GABC TYPE B AS DIRECTED BY THE ENGINEER AND PAID FOR UNDER ITEM 302008 - GABC TYPE B PATCHING.
 2. INSTALL P.C.C. CURB TYPE 1-2 ALONG THE OUTER EDGE OF THE EXISTING TRUCK APRON. THE OUTER EDGE OF THE CURB (FACING TRAFFIC) SHOULD BE DEPRESSED TO BE FLUSH WITH EXISTING PAVEMENT, RISING 1.5" OVER 9" TO THE BACK OF THE CURB.
 3. SANDBLAST THE SURFACE OF THE EXISTING TRUCK APRON TO REMOVE ANY COATINGS ON THE EXISTING CONCRETE.
 4. THE EXISTING TRUCK APRON AREA SHOULD BE BACKFILLED FROM THE FACE OF EXISTING CURB TO THE BACK OF THE NEW CURB (APPROXIMATELY 1.5") WITH CLASS B CONCRETE (WITH #8 AGGREGATE ALONG WITH POLYPROPYLENE FIBERS). THE DOSAGE RATE FOR THE FIBERS IN THE CONCRETE SHALL BE 1.5 PCY. USE AN EPOXY BONDING COMPOUND CONFORMING TO AASHTO M235 TO CREATE A BOND BETWEEN THE OLD AND NEW CONCRETE.
 5. WHEN INSTALLING THE NEW CONCRETE SECTION, THE NEW PAVEMENT JOINTS SHOULD MATCH THE EXISTING TRANSVERSE JOINTS.
 6. TRANSVERSE JOINTS SHALL BE AS PER THE STANDARD CONSTRUCTION DETAILS AND UTILIZE 1 1/4" DOWEL BARS.

CONSTRUCTION SEQUENCING NOTES:

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. EXCAVATE FOR AND CONSTRUCT PROPOSED CURB AROUND THE EXISTING TRUCK APRON.
3. SANDBLAST THE EXISTING TRUCK APRON AREA TO REMOVE ANY EXISTING COATINGS.
4. PLACE BACKFILL BETWEEN THE FACE OF EXISTING CURB OF THE CENTRAL ISLAND AND THE BACK OF NEW CURB.
5. USE AN EPOXY BONDING COMPOUND CONFORMING TO AASHTO M235 TO CREATE A BOND BETWEEN THE OLD AND NEW CONCRETE PAVEMENTS.
6. WHEN PLACING THE BACKFILL, MATCH THE NEW PAVEMENT JOINTS WITH EXISTING JOINTS.
7. TRAFFIC CONTROL FOR THE CONSTRUCTION OF THE NEW CURB AND BACKFILL OVER EXISTING TRUCK APRON SHALL UTILIZE TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD.



ADDENDUMS / REVISIONS	

NOT TO SCALE

**TRAFFIC CALMING
MISCELLANEOUS LOCATIONS
NEW CASTLE COUNTY, 2012**

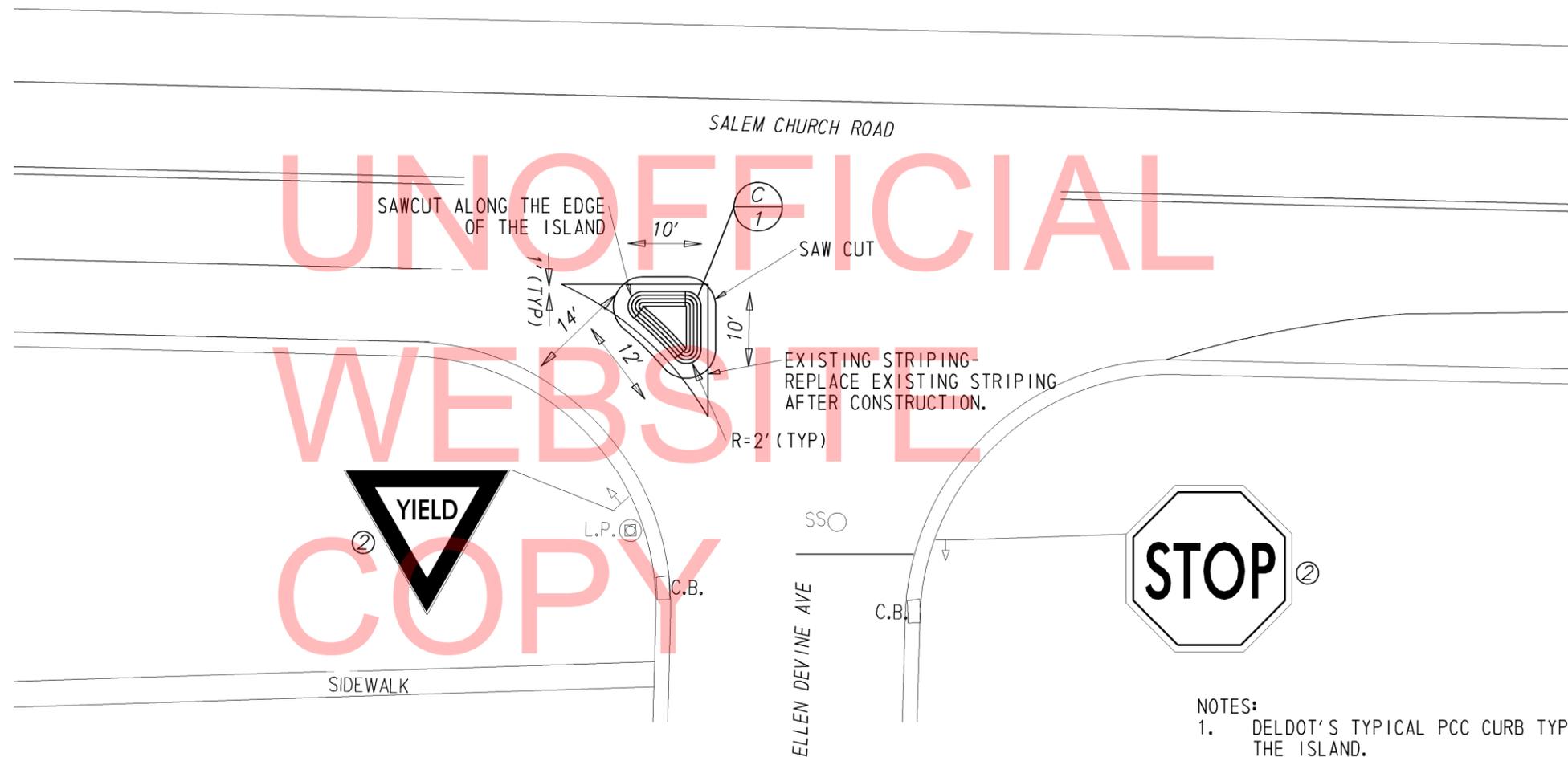
CONTRACT	BRIDGE NO.	X
T201208402	DESIGNED BY:	SM
COUNTY	CHECKED BY:	RMB
NEW CASTLE		

**CONSTRUCTION PHASING,
AND M.O.T
ROSE HILL TRAFFIC CIRCLE**

SHEET NO.	27
TOTAL SHTS.	29

CURB SCHEDULE		
NO.	ITEM DESCRIPTION / TYPE	LENGTH
1	PCC CURB TYPE 2	33 LF

SIGNING LEGEND	
①	REMOVE EXISTING SIGN
②	EXISTING SIGN TO REMAIN
③	PLACE NEW SIGN
④	RENEW EXISTING SIGN
⑤	REPOSITION EXISTING SIGN



NOTES:

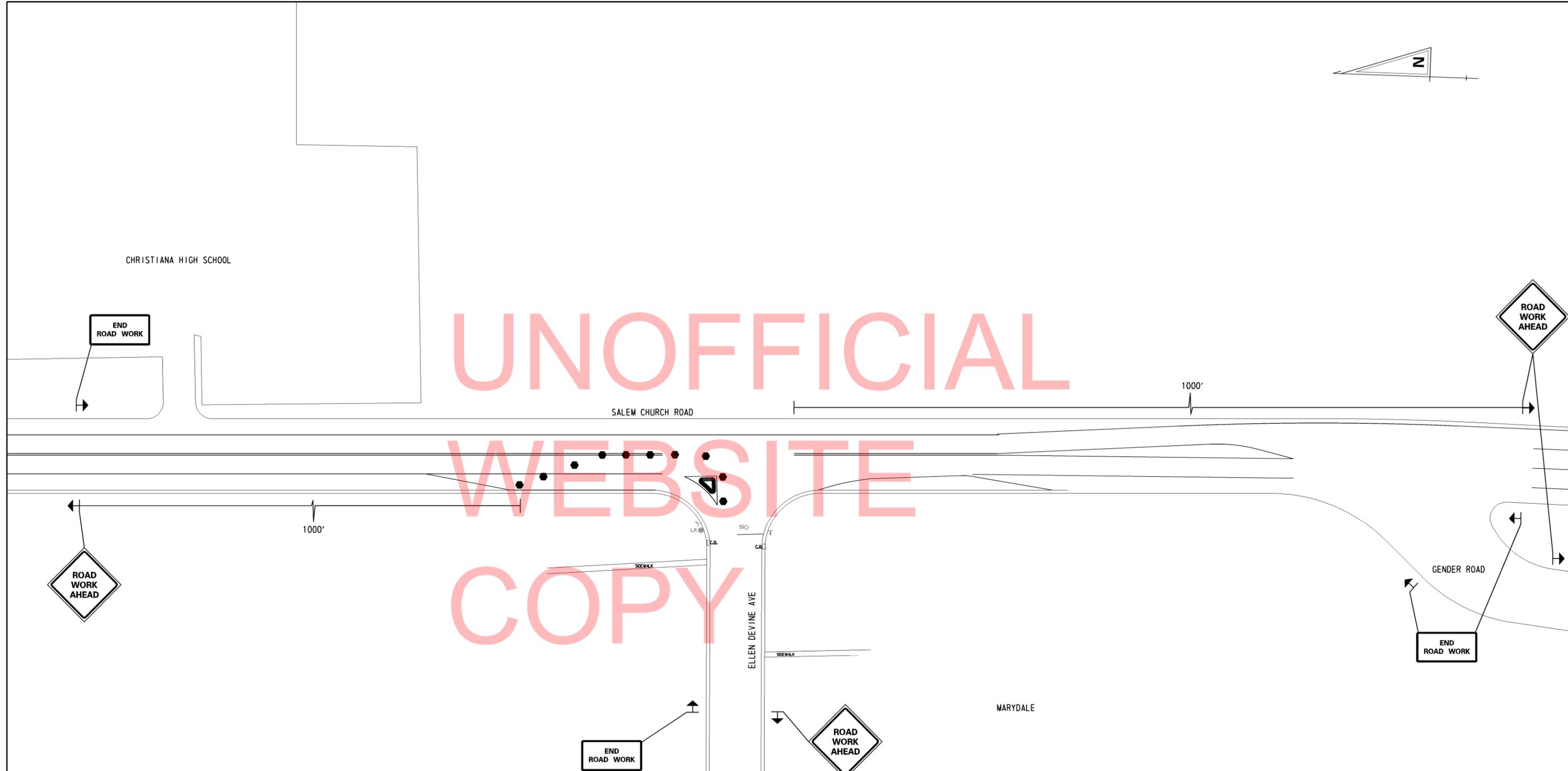
1. DELDOT'S TYPICAL PCC CURB TYPE 2 SHALL BE USED TO CONSTRUCT THE ISLAND.
2. THE CONTRACTOR IS RESPONSIBLE FOR SETTING THE ELEVATIONS IN THE FIELD.



CHRISTIANA HIGH SCHOOL

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LEGEND:

- WARNING SIGN
- DRUMS

CONSTRUCTION SEQUENCING NOTES:

1. PLACE ALL TEMPORARY WARNING SIGNS AS DEPICTED ON THIS SHEET.
2. EXCAVATE FOR AND CONSTRUCT PROPOSED ISLAND UTILIZING TYPICAL APPLICATION 10 OF CHAPTER 6H OF THE DE MUTCD FOR TRAFFIC CONTROL. PLACE AN ADDITIONAL FLAGGER FOR TRAFFIC CONTROL ON ELLEN DEVINE AVE.
3. ALL CONSTRUCTION SHALL BE COMPLETED DURING OFF-PEAK HOURS BETWEEN 9 AM AND 3 PM ONLY.
4. ANY EXPOSED SAW CUT AREA SHALL BE CORDONED OFF WITH DRUMS DURING NIGHT TIMES.
5. IF THE CONCRETE IS NOT INSTALLED ON THE SAME DAY OF EXCAVATION, EXCAVATED AREAS SHALL BE FILED WITH GRADED AGGREGATE FOR TEMPORARY ROADWAY MATERIAL (305001) TO MATCH EXISTING SURFACE DURING NIGHT TIMES.
6. IMMEDIATELY FOLLOWING CURB COMPLETION, INSTALL ALL INTERSECTION STRIPING, AS SHOWN.

ADDENDUMS / REVISIONS	

NOT TO SCALE

TRAFFIC CALMING
MISCELLANEOUS LOCATIONS
NEW CASTLE COUNTY, 2012

CONTRACT	BRIDGE NO.	X
T201208402	DESIGNED BY:	SM
COUNTY	CHECKED BY:	RMB
NEW CASTLE		

CONSTRUCTION PHASING
AND M.O.T
SALEM CHURCH ROAD AT
ELLEN DEVINE AVE

SHEET NO.	29
TOTAL SHTS.	29