

GENERAL LOCATION OF CONTRACT

# THE STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY  
UNITS



## CONSTRUCTION PLANS FOR: CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN END, FY16-18

CONTRACT NUMBER: T201407004  
FEDERAL AID PROJECT NUMBER: ESTP-2014(21)  
COUNTY: NEW CASTLE M.R. #: NA

| DESIGN DESIGNATION                     |                          |                                  |
|--|--------------------------|----------------------------------|
| FUNCTIONAL CLASS: N/A                  | D.H.V. PROJECTED: N/A    | YEAR: N/A                        |
| TYPE OF CONSTRUCTION: SIGN REPLACEMENT | DESIGN SPEED: N/A M.P.H. |                                  |
| A.A.D.T. CURRENT: N/A                  | YEAR: N/A                | TRUCKS: N/A %                    |
| A.A.D.T. PROJECTED: N/A                | YEAR: N/A                | DIRECTION OF DISTRIBUTION: N/A % |

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TOTAL SHEETS: 83

### APPROVED DESIGN EXCEPTIONS

| DESIGN PARAMETER | REQUIRED | PROVIDED | DATE |
|------------------|----------|----------|------|
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### ADDENDA & REVISIONS

| DESCRIPTION | NAME & DATE |
|-------------|-------------|
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### ASSOCIATED CONTRACTS

| CONTRACT NO. | CONTRACT NAME                               |
|--------------|---|
| 64-04-020    | I-95 (PART 'B') NINTH STREET TO BRANDYWINE  |
| 64-04-020    | I-95 (PART 'C') FOURTH STREET TO BRANDYWINE |
| 68-02-001    | I-95 SIGNING                                |
| 80-064-10    | DELAWARE AVENUE BRANDYWINE CEMETARY         |
| 64-01-062    | I-495 R/W                                   |
| 64-02-022    | I-495 PENN RR TO TERMINAL THOUROUGHFARE     |
| 64-04-024    | I-495 R/W                                   |
| 65-03-005    | I-495 R/W                                   |
| 76-07-017    | SIGNING - I-95, I-295 & I-495               |

SEE SHEET 2 FOR  
PROJECT LOCATION

PREPARED BY  
THE CONSULTING FIRM OF

**TYLIN INTERNATIONAL**  
DOVER, DELAWARE

*D. William Detwiler*  
RECOMMENDED (SHEETS 1-59)      08/27/2015  
DATE

PREPARED BY  
THE CONSULTING FIRM OF

RUMMEL, KLEPPER & KAHL, LLP

*Stuart Montgomery*  
RECOMMENDED (SHEETS 60-83)      08/26/15  
DATE

RECOMMENDED

*Vincent J. Foster*      08/28/2015  
SQUAD MANAGER, CONSTRUCTION      DATE

*John F. [Signature]*      08/28/2015  
GROUP ENGINEER, CONSTRUCTION      DATE

*James A. [Signature]*      08/31/2015  
ASSISTANT DIRECTOR, CONSTRUCTION      DATE

RECOMMENDED

STORMWATER ENGINEER

DATE \_\_\_\_\_ SEAL \_\_\_\_\_

RECOMMENDED

SQUAD MANAGER, BRIDGE DESIGN

DATE \_\_\_\_\_ SEAL \_\_\_\_\_

RECOMMENDED

*[Signature]*  
BRIDGE DESIGN ENGINEER

DATE 08/27/2015      SEAL \_\_\_\_\_

RECOMMENDED

GROUP ENGINEER, PROJECT DEVELOPMENT

DATE \_\_\_\_\_ SEAL \_\_\_\_\_

RECOMMENDED

*[Signature]*  
ASSISTANT DIRECTOR, BRIDGE

DATE 08/31/2015      SEAL \_\_\_\_\_

APPROVED

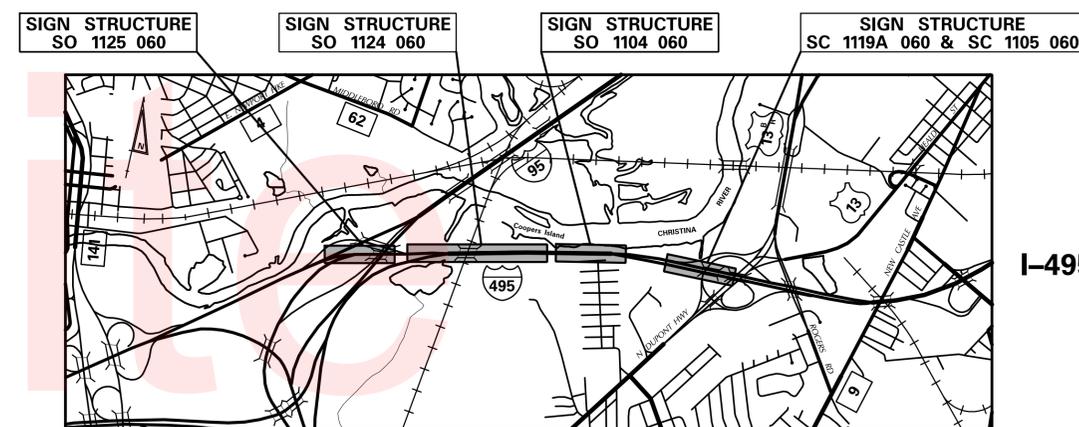
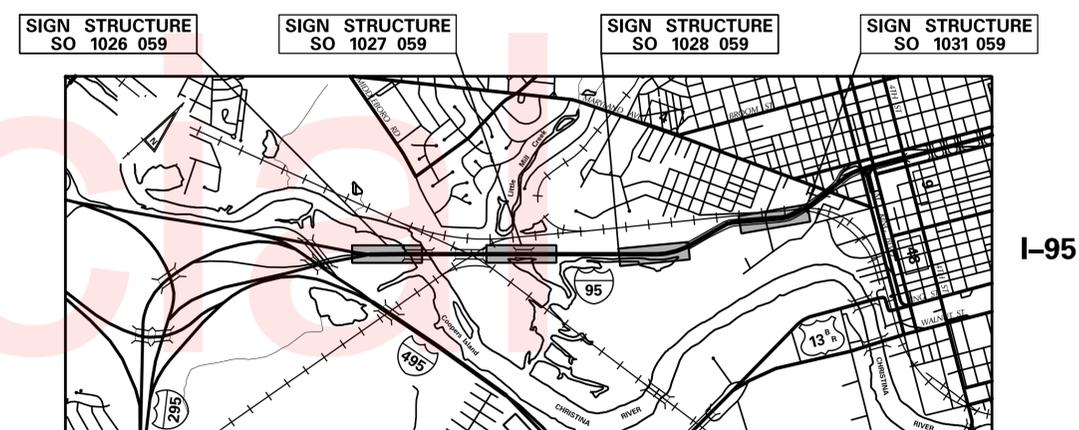
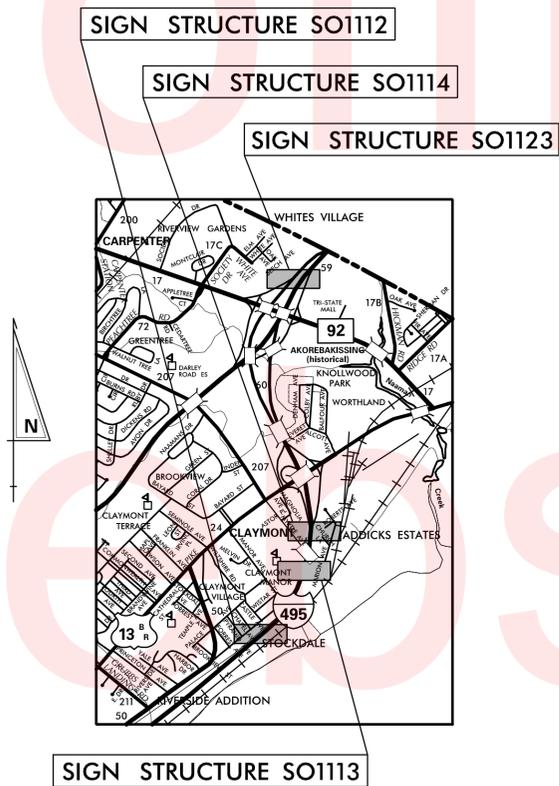
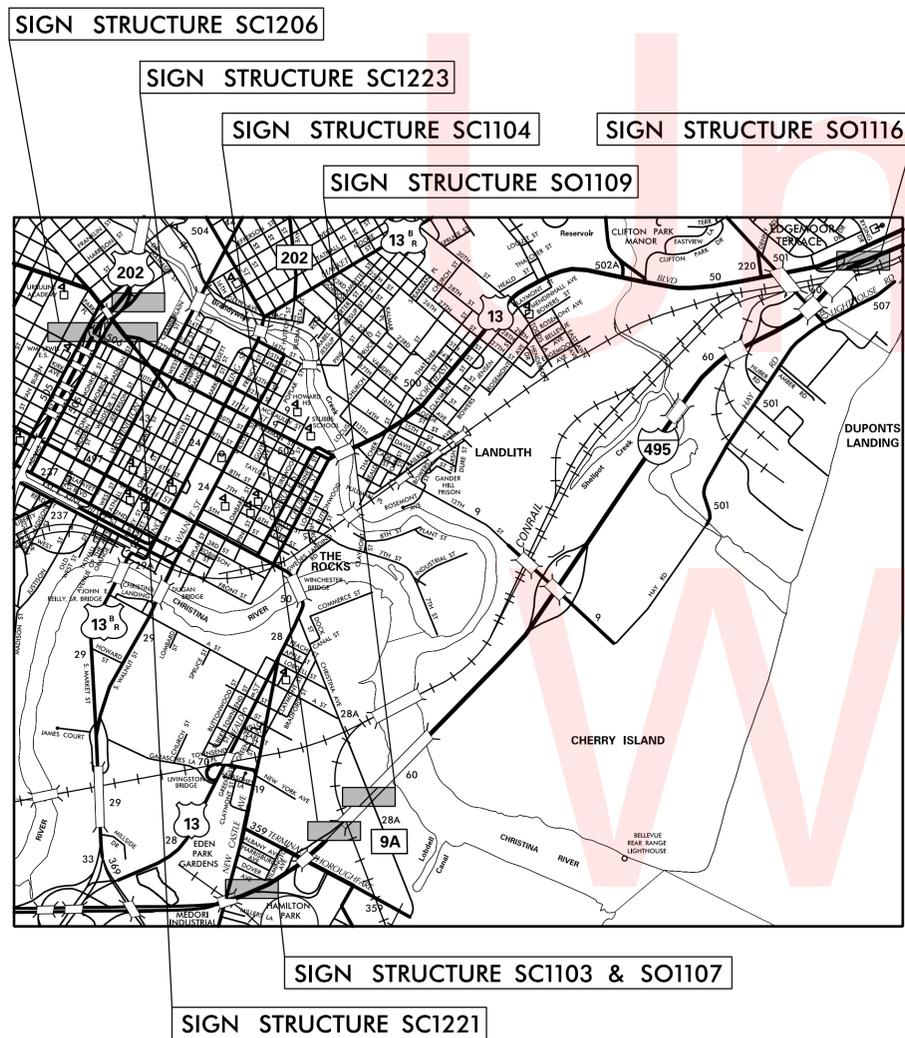
*Robert Brian McClary*  
CHIEF ENGINEER

DATE 08/31/2015      SEAL \_\_\_\_\_

SEAL

LAST REVISED: 8/24/2015  
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PROJECT LOCATION

## EXISTING SYMBOLS

| DRAINAGE |                               |
|----------|-------------------------------|
|          | DITCH OR STREAM CENTERLINE    |
|          | DIRECTIONAL STREAM FLOW ARROW |
|          | DRAINAGE INLET                |
|          | DRAINAGE JUNCTION BOX         |
|          | DRAINAGE MANHOLE              |
|          | DRAINAGE PIPE AND FLOW ARROW  |
|          | DRAINAGE PIPE HEADWALL        |
|          | RIPRAP - AREA FEATURE         |
|          | RIPRAP - LINEAR FEATURE       |

| MANMADE ROADSIDE FEATURES |                                |
|---------------------------|--------------------------------|
|                           | BOLLARD - STEEL POLE           |
|                           | BOLLARD - WOOD POST            |
|                           | CURB                           |
|                           | CURB AND GUTTER                |
|                           | FENCE - CHAINLINK OR STRANDED  |
|                           | FENCE - STOCKADE OR SPLIT RAIL |
|                           | FLAG POLE                      |
|                           | GUARDRAIL - STEEL BEAM         |
|                           | GUARDRAIL - WIRE ROPE          |
|                           | LAMP AND POST - RESIDENTIAL    |
|                           | MAILBOX                        |
|                           | PARKING METER AND POST         |
|                           | PAVEMENT - FLEXIBLE            |
|                           | PAVEMENT - RIGID               |
|                           | PILE - BRIDGE                  |
|                           | PILLAR OR MISCELLANEOUS POST   |
|                           | TRAFFIC SIGN AND POST          |
|                           | WALL - BRICK OR BLOCK          |
|                           | WALL - STONE                   |

| NATURAL ROADSIDE FEATURES |                                  |
|---------------------------|----------------------------------|
|                           | GRASS LAWN                       |
|                           | HEDGEROW OR THICKET              |
|                           | MARSH BOUNDARY LINE              |
|                           | TREE - CONIFEROUS                |
|                           | TREE - DECIDUOUS                 |
|                           | TREE STUMP                       |
|                           | SHRUBBERY                        |
|                           | DELINEATED WETLAND BOUNDARY LINE |
|                           | WOODS LINE BOUNDARY              |

| RIGHT-OF-WAY SYMBOLS |                                 |
|----------------------|---------------------------------|
|                      | PROPERTY MARKER - CONCRETE MON. |
|                      | PROPERTY MARKER - IRON PIPE     |
|                      | HISTORIC RIGHT-OF-WAY BASELINE  |
|                      | EXISTING RIGHT-OF-WAY           |
|                      | EXISTING PROPERTY LINE          |
|                      | EXISTING EASEMENT               |
|                      | EXISTING DENIAL OF ACCESS       |
|                      | EXISTING R/W & DENIAL OF ACCESS |

| SURVEY CONTROL & MONUMENTATION |                                |
|--------------------------------|--------------------------------|
|                                | SURVEY BENCHMARK LOCATION      |
|                                | SURVEY TIE POINT LOCATION      |
|                                | SURVEY TRAVERSE POINT          |
|                                | POINT OF CURVATURE OR TANGENCY |
|                                | POINT OF INTERSECTING TANGENTS |

| UTILITY |                                  |
|---------|----------------------------------|
|         | SOIL BORING LOCATION             |
|         | UTILITY TEST HOLE LOCATION       |
|         | CABLE TV DISTRIBUTION BOX        |
|         | ELECTRIC MANHOLE                 |
|         | ELECTRIC METER                   |
|         | ELECTRIC TRANSFORMER             |
|         | POLE MOUNTED LUMINAIRE           |
|         | GAS MANHOLE                      |
|         | GAS METER                        |
|         | GAS VALVE                        |
|         | GAS PUMP - SERVICE STATION       |
|         | RAILROAD TRACKS                  |
|         | SANITARY SEWER MANHOLE           |
|         | SANITARY SEWER VALVE             |
|         | SANITARY SEWER VENT OR CLEANOUT  |
|         | SEPTIC DRAIN FIELD               |
|         | TELEPHONE BOOTH                  |
|         | TELEPHONE MANHOLE                |
|         | TELEPHONE TEST POINT             |
|         | TRAFFIC - CONDUIT JUNCTION WELL  |
|         | TRAFFIC - LIGHT POLE AND BASE    |
|         | TRAFFIC - PEDESTRIAN POLE & BASE |
|         | TRAFFIC - SIGNAL CABINET & BASE  |
|         | TRAFFIC - SIGNAL POLE AND BASE   |
|         | UTILITY BOX                      |
|         | UTILITY POLE GUY WIRE ANCHOR     |
|         | UTILITY POLE                     |
|         | WATER - FIRE HYDRANT             |
|         | WATER METER                      |
|         | WATER VALVE                      |
|         | WELL HEAD                        |
|         | MANHOLE - UNDETERMINED OWNER     |

| UTILITY COMPANY FACILITIES |                            |
|----------------------------|----------------------------|
|                            | DELMARVA POWER (ELECTRIC)  |
|                            | DELMARVA POWER (GAS)       |
|                            | CITY OF WILMINGTON (SEWER) |
|                            | CITY OF WILMINGTON (WATER) |
|                            | TRAFFIC CABLE              |
|                            | VERIZON                    |

| CONSTRUCTION |   |
|--------------|---|
|              | CONCRETE SAFETY BARRIER - PERMANENT                             |
|              | BIOFILTRATION SWALE   |
|              | BRICK PATTERNED SURFACE   |
|              | BUTT JOINT  |
|              | CONSTRUCTION BASELINE   |
|              | CONSTRUCTION SAFETY FENCE                                       |
|              | CURB, TYPE 1 & TYPE 3   |
|              | CURB, TYPE 2  |
|              | CURB & GUTTER, TYPE 1   |
|              | CURB & GUTTER, TYPE 2   |
|              | CURB & GUTTER, TYPE 3   |
|              | CURB & GUTTER, TYPE 4   |
|              | CLEAR ZONE  |
|              | DRAINAGE INLET  |
|              | DITCH   |
|              | FENCE - METAL   |
|              | FENCE - WOOD  |
|              | FLARED END SECTION  |
|              | GUARDRAIL, TYPE 1   |
|              | GUARDRAIL, TYPE 2   |
|              | GUARDRAIL, TYPE 3   |
|              | GUARDRAIL END ANCHORAGE   |
|              | GUARDRAIL END TREATMENT, TYPE 1                                 |
|              | GUARDRAIL END TREATMENT, TYPE 2                                 |
|              | GUARDRAIL END TREATMENT, TYPE 3                                 |
|              | IMPACT ATTENUATOR   |
|              | JUNCTION BOX - DRAINAGE   |
|              | LATERAL OFFSET  |
|              | LIMIT OF CONSTRUCTION   |
|              | MAILBOX   |
|              | MANHOLE   |
|              | PAVEMENT PATCH  |
|              | PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH                      |
|              | PIPE & DIRECTIONAL FLOW ARROW                                   |
|              | RIPRAP  |
|              | P.C.C. SIDEWALK - 4"  |
|              | P.C.C. SIDEWALK - 6" (USE 8" DEPTH FOR CHANNELIZATION ISLANDS.) |
|              | UNDERDRAIN  |
|              | UNDERDRAIN OUTLET   |

| LANDSCAPING |                     |
|-------------|---------------------|
|             | LANDSCAPE PLANTINGS |
|             | SHRUBBERY           |
|             | CONIFEROUS TREE     |
|             | DECIDUOUS TREE      |

## PROPOSED SYMBOLS

| IDENTIFIERS |   |
|-------------|---|
|             | ADJUST BY CONTRACTOR  |
|             | ADJUST BY OTHERS  |
|             | CONCRETE SAFETY BARRIER   |
|             | CURB OR CURB & GUTTER   |
|             | CONVERT TO JUNCTION BOX   |
|             | CONVERT TO DRAINAGE MANHOLE   |
|             | CURB OPENING  |
|             | CURB RAMP / TYPE  |
|             | CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM |
|             | CONSTRUCTION SAFETY FENCE   |
|             | DRAINAGE INLET  |
|             | DO NOT DISTURB  |
|             | ENERGY DISSIPATOR   |
|             | FENCE   |
|             | FLARED END SECTION  |
|             | FILL WITH FLOWABLE FILL   |
|             | FILTRATION STRUCTURE  |
|             | GUARDRAIL   |
|             | JUNCTION BOX  |
|             | MANHOLE   |
|             | MONUMENT - RIGHT-OF-WAY   |
|             | PIPE  |
|             | RELOCATE BY CONTRACTOR  |
|             | RELOCATE BY OTHERS  |
|             | REMOVE BY CONTRACTOR  |
|             | REMOVE BY OTHERS  |
|             | UNDERDRAIN / LENGTH   |
|             | UNDERDRAIN OUTLET PIPE  |

| CONSTRUCTION PHASING & M.O.T |                                    |
|------------------------------|------------------------------------|
|                              | BARRICADE, TYPE 3                  |
|                              | CONCRETE SAFETY BARRIER - PORTABLE |
|                              | CONSTRUCTION SAFETY FENCE / LENGTH |
|                              | CONSTRUCTION SAFETY FENCE          |
|                              | CONSTRUCTION WARNING SIGN LOCATION |
|                              | CONSTRUCTION WARNING SIGN          |
|                              | CRASH CUSHION ARRAY                |
|                              | DRUM - TRAFFIC CONTROL             |
|                              | FLAGGER LOCATION                   |
|                              | PHASING TRAFFIC FLOW ARROW         |
|                              | TEMPORARY CONSTRUCTION             |
|                              | TEMPORARY PAVEMENT MARKING ARROW   |
|                              | TRUCK WITH MOUNTED ATTENUATOR      |
|                              | WORK AREA - ACTIVE PHASE           |

| PAVEMENT SECTION(S) |  |
|---------------------|--|
|                     | OVERLAY PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS               |
|                     | RECONSTRUCTED PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS         |
|                     | DRIVEWAY AND ENTRANCE PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS |

| RIGHT-OF-WAY SYMBOLS |                                 |
|----------------------|---------------------------------|
|                      | PROPOSED RIGHT-OF-WAY MONUMENT  |
|                      | PROPOSED DENIAL OF ACCESS       |
|                      | PROPOSED PERMANENT EASEMENT     |
|                      | PROPOSED RIGHT-OF-WAY           |
|                      | PROPOSED R/W & DENIAL OF ACCESS |
|                      | TEMPORARY CONSTRUCTION EASEMENT |
|                      | PROPOSED RIGHT-OF-WAY BASELINE  |

| TRAFFIC |                       |
|---------|-----------------------|
|         | ITMS CONDUIT          |
|         | SIGNAL CONDUIT        |
|         | CONDUIT JUNCTION WELL |
|         | LUMINAIRE             |
|         | PAVEMENT MARKINGS     |
|         | PAVEMENT STRIPING     |
|         | TRAFFIC SIGN          |

| EROSION & SEDIMENT CONTROL |                                    |
|----------------------------|------------------------------------|
|                            | DEWATERING BAG                     |
|                            | DEWATERING BASIN                   |
|                            | EARTH DIKE                         |
|                            | INLET SEDIMENT CONTROL             |
|                            | PERIMETER DIKE/SWALE               |
|                            | PORTABLE SEDIMENT TANK             |
|                            | SANDBAG DIKE                       |
|                            | SANDBAG DIVERSION                  |
|                            | STONE CHECK DAM                    |
|                            | STABILIZED CONSTRUCTION ENTRANCE   |
|                            | SILT FENCE / LENGTH                |
|                            | SILT FENCE                         |
|                            | SILT FENCE - REINFORCED            |
|                            | SUMP PIT, TYPE 1                   |
|                            | SUMP PIT, TYPE 2                   |
|                            | SEDIMENT TRAP                      |
|                            | SEDIMENT TRAP                      |
|                            | SEDIMENT TRAP WITH INLET AS OUTLET |
|                            | SEDIMENT TRAP PIPE OUTLET          |
|                            | STILLING WELL                      |
|                            | TEMPORARY SWALE                    |
|                            | TEMPORARY SLOPE DRAIN              |
|                            | TURBIDITY CURTAIN / LENGTH         |
|                            | TURBIDITY CURTAIN                  |

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## GENERAL NOTES

- THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
- THE CONTRACTOR SHALL GIVE TWO (2) WEEKS NOTICE TO THE PROPERTY OWNER WHEN ANY FIXTURE, SHRUB OR OTHER OBJECT MUST BE REMOVED FROM THE RIGHT OF WAY OR EASEMENT AREA. IF THE OWNER HAS NOT ATTEMPTED TO SALVAGE THIS PROPERTY, THE CONTRACTOR SHALL REMOVE IT WITHOUT OBLIGATION. COMPENSATION SHALL BE INCIDENTAL TO THE CONTRACT.
- THE END OF ALL CURBS SHALL BE DEPRESSED FLUSH WITH THE PAVEMENT AT A RATIO OF TWELVE TO ONE (12:1) UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- LANE CLOSURES ALONG I-495 AND DELAWARE AVENUE ARE PERMITTED BETWEEN 8:00PM AND 5:00AM, SUNDAY NIGHT THROUGH FRIDAY MORNING.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL PVC SLEEVES (4" INSIDE MINIMUM DIAMETER, 6" INSIDE MAXIMUM DIAMETER) IN PROPOSED CONCRETE SIDEWALKS, ISLANDS, AND MEDIANS FOR FUTURE TRAFFIC SIGN POSTS AS DIRECTED BY THE ENGINEER. THE LOWER END OF THE SLEEVE SHALL SIT ON THE TOP OF THE SUBBASE MATERIAL. THE COST SHALL BE INCIDENTAL TO THE CONTRACT.
- STAGING AREAS - PROPER EROSION AND SEDIMENT CONTROL MEASURES AS DETERMINED BY THE ENGINEER SHALL BE INSTALLED IN ALL STAGING AREAS. ALL AREAS USED BY THE CONTRACTOR FOR STAGING OPERATIONS SHALL BE FULLY RESTORED BY THE CONTRACTOR UPON COMPLETION OF THE CONTRACT. IF THE STAGING AREA IS PAVED, IT SHALL BE RESTORED TO ITS ORIGINAL CONDITION. IF THE AREA IS UNPAVED, IT SHALL BE RE-GRADED, TOPSOILED, SEEDDED AND MULCHED IN ACCORDANCE WITH DELAWARE STANDARD SPECIFICATIONS 732, 734 AND 735, FOR TOPSOIL, SEE AND MULCH RESPECTIVELY, TO THE SATISFACTION OF THE ENGINEER. THE SEED SHALL ADHERE TO THE SPECIFICATIONS OF SECTION 734 FOR PERMANENT GRASS SEEDING - DRY GROUND. ALL COSTS ASSOCIATED WITH RESTORATION OF THE STAGING AREA SHALL BE AT THE CONTRACTOR'S EXPENSE. IF THE ENGINEER DETERMINES THAT A SATISFACTORY STAND OF GRASS DOES NOT EXIST AT THE TIME OF FINAL INSPECTION, ALL COSTS ASSOCIATED WITH REESTABLISHING A SATISFACTORY STAND OF GRASS SHALL BE AT THE CONTRACTOR'S EXPENSE.
- SITE REVIEWER - AN EROSION CONTROL SITE REVIEWER SHALL BE A PERSON FROM THE CONTRACTOR'S STAFF ASSIGNED TO EROSION AND SEDIMENT CONTROL IMPLEMENTATION AND MAINTENANCE AND SHALL BE REQUIRED ON SPECIFIC PROJECTS. THE NAME AND DNREC CERTIFICATION NUMBER OF EACH SITE REVIEWER SO REQUIRED SHALL BE SUBMITTED TO THE DEPARTMENT PRIOR TO THE EXECUTION OF THE CONTRACT. THE NAME OF THE DELAWARE REGISTERED PROFESSIONAL ENGINEER PROVIDING DIRECTION AND SUPERVISION OF THE SITE REVIEWER, AS REQUIRED IN SECTION 12.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS, SHALL ALSO BE SUBMITTED TO THE DEPARTMENT PRIOR TO THE EXECUTION OF THE CONTRACT. THE SITE REVIEWER REQUIREMENTS IN EFFECT ON THIS PROJECT SHALL BE MARKED WITH AN "X" BELOW:

| EROSION POTENTIAL FOR THIS PROJECT | SITE REVIEWER REQUIREMENT   |
|------------------------------------|---|
| ( X ) INSIGNIFICANT                | NONE  |
| ( ) MINOR                          | CONTRACTOR CERTIFICATION COURSE TRAINING ONLY, AS DEFINED IN SECTION 13 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.  |
| ( ) MEDIUM                         | THE SUPERINTENDENT OR A SEPARATE INDIVIDUAL FROM THE CONTRACTOR'S STAFF SHALL BE A CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 12 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.   |
| ( ) MAJOR                          | SUPERINTENDENT AND AN INDIVIDUAL FROM CONTRACTOR'S STAFF SHALL BE CCR. ONE INDIVIDUAL FROM THE CONTRACTOR'S STAFF MUST BE A CCR PRIOR TO THE EXECUTION OF THE CONTRACT. THE SUPERINTENDENT MUST BECOME A CCR WITHIN ONE YEAR AFTER THE AWARD OF CONTRACT. |

- ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR, INCLUDE:

|       |  |
|-------|--|
| ( X ) | NONE   |
| ( )   | ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.                        |
| ( )   | ALL PLAN SHEETS, IN PDF FORMAT.  |
| ( )   | EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.                  |
| ( )   | PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.                  |
| ( )   | DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM). |

NOTE: THE DOCUMENT ENTITLED "RELEASE FOR DELIVERY OF DOCUMENTS IN ELECTRONIC FORM TO A CONTRACTOR" MUST BE SIGNED BY ALL PARTIES PRIOR TO THE DELIVERY OF ANY ELECTRONIC PROJECT FILES.

- AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

|       |  |
|-------|--|
| ( )   | THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.   |
| ( X ) | THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR IS INCIDENTAL TO ITEM 743000.   |
| ( )   | THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR SHALL BE PAID FOR UNDER ITEM 743031. |

- THE DISTURBED AREA FOR THIS PROJECT IS 1.34 ACRES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADHERING TO THE CONSTRUCTION SITE POLLUTION PREVENTION SPECIFICATIONS AS DETAILED IN SECTION 3.6 OF THE "DELAWARE EROSION AND SEDIMENT CONTROL HANDBOOK". ALL COSTS ASSOCIATED WITH ADHERING TO THE STANDARDS SHALL BE INCIDENTAL TO THE OVERALL CONTRACT COSTS.

- THE EROSION AND SEDIMENT CONTROL PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE EROSION AND SEDIMENT CONTROL PLANS ARE VALID FOR A THREE YEAR PERIOD FOR EACH WORK ORDER, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR SHALL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE EROSION AND SEDIMENT CONTROL PLAN APPROVAL. DELDOT WILL REVIEW THE CURRENT EROSION AND SEDIMENT CONTROL PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.
- CONTRACT TERM: THIS CONTRACT SHALL BE FOR A PERIOD OF THREE YEARS (1,095 CALENDAR DAYS) AND SHALL BEGIN FROM THE DATE OF NOTICE TO PROCEED. THE PERFORMANCE BOND SHALL BE SUBMITTED AT THE BEGINNING OF EACH FISCAL YEAR. FAILURE ON THE PART OF THE CONTRACTOR TO SUBMIT THE PERFORMANCE BOND FOR THE EXTENSION PERIOD PRIOR TO THE LAST WORKING DAY BEFORE END OF THE PREVIOUS PERIOD SHALL RESULT IN THE CONTRACT BEING CANCELLED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE FORMS NECESSARY TO RENEW THE PERFORMANCE BOND EACH YEAR THE CONTRACT IS IN FORCE.
- THE DEPARTMENT ANTICIPATES ADDING SEVERAL MORE SIGN LOCATIONS THROUGHOUT THE CONTRACT TERM; HOWEVER, NO ADDITIONAL WORK IS GUARANTEED.
- ADVANCE UTILITY RELOCATION INVOLVEMENT IS ANTICIPATED AND WILL BE ADDRESSED IN FUTURE WORK ORDER SUBMITTALS PRIOR TO ISSUING A NOTICE TO PROCEED FOR THAT WORK ORDER. SHOULD ANY CONFLICTS BE ENCOUNTERED DURING CONSTRUCTION REQUIRING ADJUSTMENT AND/OR RELOCATION OF THE EXISTING FACILITIES, THE NECESSARY RELOCATION WORK SHALL BE ACCOMPLISHED BY THE RESPECTIVE UTILITY, AS DIRECTED BY THE CONSTRUCTION ENGINEER. ANY ADJUSTMENTS AND/OR RELOCATIONS ON MUNICIPALLY OWNED FACILITIES SHALL BE DONE BY THE STATE'S CONTRACTOR IN ACCORDANCE WITH THE RESPECTIVE UTILITY'S STANDARD SPECIFICATIONS AS DIRECTED BY THE CONSTRUCTION ENGINEER.
- EACH LOCATION SHALL BE CONSIDERED A SINGLE UNIT OF WORK (WORK ORDER). UPON COMPLETION OF EACH SINGLE UNIT OF WORK AND ACCEPTANCE BY THE DEPARTMENT, ONE ITEMIZED ESTIMATE WILL BE PREPARED BY THE DEPARTMENT FOR THAT WORK. THE CONTRACTOR SHALL REVIEW AND APPROVE THE INVOICE. PAYMENT WILL BE MONTHLY FOR THE COMPLETED INVOICES AS OUTLINED IN SECTION 109.07.
- PROSECUTION AND PROGRESS OF WORK:

THE CONTRACTOR SHALL COMMENCE WORK INDICATED ON THE WORK ORDER NO LATER THAN THE THIRTIETH (30) BUSINESS DAY AFTER ISSUANCE UNLESS REQUIRED MATERIALS ARE NOT AVAILABLE. NON-AVAILABILITY OF MATERIALS SHALL BE VERIFIED BY AT LEAST THREE (3) DIFFERENT SOURCES. VERIFICATION SHALL BE FORMAL AND SUPPLIED BY THE CONTRACTOR IN WRITING.

ISSUANCE OF EACH WORK ORDER CONSTITUTES THE NOTICE TO PROCEED WITH THE WORK DESCRIBED ON THE WORK ORDER. IF THERE IS A VERIFIED NON-AVAILABILITY OF MATERIALS, TIME CHARGES SHALL COMMENCE ON THE FIRST (1ST) WORKING DAY FOLLOWING THE DELIVERY DATE OF SAID MATERIALS.

FAILURE TO START ASSIGNED WORK ORDERS IN THE ALLOWED TIME CONSTITUTES "FAILURE TO PURSUE THE WORK" AND SUBJECTS THE CONTRACTOR TO LIQUIDATED DAMAGES AS OUTLINED IN SUBSECTION 108.09 OF THE STANDARD SPECIFICATIONS. IF WORK ON A SPECIFIC WORK ORDER IS NOT COMPLETED WITHIN THE ALLOTTED TIME, LIQUIDATED DAMAGES WILL BE ASSESSED IN ACCORDANCE WITH SUBSECTION 108.09 AND BASED ON THE TOTAL VALUE OF THAT WORK ORDER.

A FINAL INSPECTION WILL BE PERFORMED IN ACCORDANCE WITH SUBSECTION 105.20 ON EACH WORK ORDER AND IF THE WORK IS SATISFACTORILY COMPLETED IT WILL BE ACCEPTED BY THE DEPARTMENT.

- THE CONTRACTOR MAY SIMULTANEOUSLY WORK ON MULTIPLE LOCATIONS WITH APPROVAL FROM THE TRAFFIC ENGINEER AND PROVIDING THE DETOURS AND MOT DO NOT OVERLAP.
- PLANS SHOWN ARE SUBJECT TO MINOR CHANGES, FINAL APPROVED PLANS WILL BE ISSUED WHEN THE WORK ORDER IS GIVEN TO THE CONTRACTOR. PAYMENT WILL BE MADE ON THE ACTUAL QUANTITIES USED.
- THE INITIAL EXPENSE WILL BE PAID ONE TIME FOR THE FIRST WORK ORDER. JOB ORDER MOBILIZATION (ITEM 763612) WILL BE UTILIZED TO PAY FOR MOBILIZATION TO ALL OTHER WORK ORDER LOCATIONS.

## PROJECT NOTES

### SECTION 100

- ANY DAMAGE TO ITEMS NOT DESIGNATED TO BE REPLACED BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

### SECTION 200

- IN AREAS WHERE TREES OR SHRUBS WILL BE OVERHANGING THE PROPOSED SIDEWALK, PRUNING MAY BE NECESSARY TO ACHIEVE A VERTICAL CLEAR SPACE OF 10 FEET ABOVE THE PROPOSED SIDEWALK ELEVATION. THE CONTRACTOR SHALL PRUNE EXISTING TREE AND SHRUB BRANCHES, WHICH OVERHANG THE SIDEWALK, IN ACCORDANCE WITH I.S.A. STANDARDS. THE CONTRACTOR SHALL NOTIFY DELDOT'S ROADSIDE ENVIRONMENTALIST ADMINISTRATOR, EUGENE 'CHIP' ROSAN, JR. AT (302) 760-2185 AND/OR HIS DESIGNEE, AT LEAST TWO (2) DAYS PRIOR TO THE PRUNING OPERATION. ALL COSTS ASSOCIATED WITH THE ABOVE WORK TO BE PAID UNDER ITEM 201000 - CLEARING AND GRUBBING.

- THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXCAVATE TEST PITS AT POINTS OF POSSIBLE UTILITY CONFLICTS, TO DETERMINE IF A CONFLICT EXISTS. ANY CONFLICTS SHALL BE COORDINATED BY THE CONTRACTOR, WITH THE ENGINEER AND THE UTILITY COMPANY INVOLVED. THE ENGINEER SHALL ULTIMATELY DETERMINE THE SOLUTION TO THE UTILITY CONFLICT. TEST HOLES SHALL BE INCIDENTAL TO ITEM 618537-DRILLED SHAFT, 54", 618538-DRILLED SHAFT, 60", 618539-DRILLED SHAFT ROCK SOCKET, 48".

- ITEMS TO BE REMOVED UNDER ITEM 21000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:
  - EXISTING SIGN STRUCTURE SC1206
  - EXISTING SIGN STRUCTURE SC1221
  - EXISTING SIGN STRUCTURE SC1223
  - EXISTING SIGN STRUCTURE SC1227
  - EXISTING SIGN STRUCTURE SC1107
  - EXISTING SIGN STRUCTURE SC1108
  - EXISTING SIGN STRUCTURE SC1109
  - EXISTING SIGN STRUCTURE SC1112
  - EXISTING SIGN STRUCTURE SC1113
  - EXISTING SIGN STRUCTURE SC1114
  - EXISTING SIGN STRUCTURE SC1116
  - EXISTING SIGN STRUCTURE SC1123
 EXISTING SIGN STRUCTURES AND PANELS SHALL BE THE PROPERTY OF THE CONTRACTOR.

- EXISTING SIGN STRUCTURE FOUNDATIONS SHALL BE CUT 1' BELOW EXISTING GRADE. PAYMENT SHALL BE INCLUDED IN ITEM 21000 - REMOVAL OF STRUCTURES AND OBSTRUCTIONS. AREA AROUND EXISTING STRUCTURE SHALL BE RE-GRADED, TOPSOILED, SEEDDED AND MULCHED IN ACCORDANCE WITH DELAWARE STANDARD SPECIFICATIONS 908004 AND 908014.

### SECTION 600

- THE SIGN STRUCTURES SHALL BE SHOP-ASSEMBLED TO ENSURE PROPER FIT OF SPLICE BOLTS IN THE FIELD.
- ALL WELDING SHALL BE SUBJECT TO NON-DESTRUCTIVE TESTING IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE ANSI/AASHTO/AWS D1.1 STRUCTURAL WELDING CODE - STEEL. WELDS IN MAIN MEMBERS SHALL BE ULTRASONICALLY INSPECTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- SIGN STRUCTURES SHALL BE FABRICATED SO THAT AFTER ERECTION, THE MASTS AND MAST ARMS SHALL CONFORM TO THE CAMBER DIAGRAMS SHOWN IN THE DRAWINGS.
- THE CONTRACTOR SHALL INCLUDE LEAD TIME OR DELIVERY DATE ON SHOP DRAWINGS.

### SECTION 700

- THE CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY ALL EXISTING CONDITIONS, LAYOUT DIMENSIONS, AND CLEARANCES THAT ARE SHOWN ON THE DRAWINGS PRIOR TO ANY CONSTRUCTION WORK. THE COST OF THIS ITEM SHALL BE INCIDENTAL TO ITEM 763501 - CONSTRUCTION ENGINEERING.
- THE CONTRACTOR SHALL SURVEY THE LAYOUT OF PROPOSED FOUNDATIONS AND ANCHOR BOLTS AND OBTAIN ENGINEER'S APPROVAL OF THE FOUNDATION LOCATIONS PRIOR TO PREPARING SHOP DRAWINGS FOR SIGN SUPPORTS TO BE ERECTED THEREON. THE SHOP DRAWINGS SHALL INCLUDE THE MEASUREMENTS OBTAINED FROM THE SURVEY. ALL SURVEY DATA WILL BE BASED ON THE NATIONAL GEODETIC SURVEY VERTICAL DATUM (NAVD88) AND THE NATIONAL GEODETIC SURVEY HORIZONTAL DATUM (NAD83) PAVEMENT FOR ALL THE SURVEY WORK WILL BE INCIDENTAL TO ITEM 763501 - CONSTRUCTION ENGINEERING.
- THE CONTRACTOR SHALL ENSURE THAT THERE ARE NO GAPS BETWEEN SECTIONS OF SIGN.
- IN AREAS WHERE PROPOSED CURB MEETS EXISTING CURB AND THE TWO CURB TYPES ARE NOT SIMILAR, THE PROPOSED CURB SHALL BE TRANSITIONED IN 10 LINEAR FEET, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK, INCLUDING SAW CUTTING EXISTING CURB SHALL BE INCIDENTAL TO THE PROPOSED CURB ITEM.
- WHERE PROPOSED CONCRETE SIDEWALK IS CONSTRUCTED TO MEET EXISTING SIDEWALK, THE EXISTING SIDEWALK SHALL BE SAWCUT AT THE TIE-IN POINT OR MEET THE NEAREST EXISTING SIDEWALK JOINT. ALL SAW CUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER AND SHALL BE PAID FOR UNDER ITEM 762002 - SAWCUTTING, CONCRETE, FULL DEPTH.
- ALL PAVED AREAS TO BE RECONSTRUCTED OR WIDENED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT.

### MISCELLANEOUS

- THE CONTRACTOR SHALL COVER ALL SIGN FOUNDATION EXCAVATIONS AT THE END OF EACH WORKDAY WITH STEEL PLATES CAPABLE OF SUPPORTING PEDESTRIAN LOADS OR AS DIRECTED BY THE ENGINEER. PAYMENT SHALL BE INCIDENTAL TO ITEM 618537-DRILLED SHAFT, 54", 618538-DRILLED SHAFT, 60", 618539-DRILLED SHAFT ROCK SOCKET, 48".
- IF ANY PUMPING IS NECESSARY TO EXCAVATE FOR THE SIGN STRUCTURE FOUNDATIONS, PLEASE REFER TO SECTION 111 FOR THE PUMPING REQUIREMENTS. PAYMENT SHALL BE INCIDENTAL TO ITEM 618537-DRILLED SHAFT, 54", 618538-DRILLED SHAFT, 60", 618539-DRILLED SHAFT ROCK SOCKET, 48".
- ALL WORK SHALL BE PERFORMED WITHIN THE EXISTING RIGHT-OF-WAY.
- HEIDI E. KROFFT, DELDOT ARCHAEOLOGIST, MUST BE CONTACTED AT (302)760-2125 OR HEIDI.KROFFT@STATE.DE.US TWO WEEKS PRIOR TO CONSTRUCTION. HEIDI KROFFT OR A DELDOT ARCHAEOLOGIST MUST BE ON SITE DURING THE EXCAVATION PHASE AT SIGN STRUCTURE SC1221 TO MONITOR WORK ADJACENT TO THE WILMINGTON AND BRANDYWINE CEMETERY. IF GRAVE SHAFTS OR HUMAN REMAINS ARE ENCOUNTERED, ALL WORK SHALL STOP IMMEDIATELY UNTIL BOTH DELDOT AND THE STATE HISTORIC PRESERVATION OFFICE HAVE CONCURRED THAT THE CONSTRUCTION WORK WILL NOT IMPACT ANY INTACT ARCHAEOLOGICAL DEPOSITS. THIS CONSULTATION SHALL TAKE PLACE NO MORE THAN 24 HOURS AFTER CONSTRUCTION HAS STOPPED. ALL WORK ON THE PROJECT SHALL COMPLY WITH THE DELAWARE STATE UNMARKED HUMAN REMAINS LAW (7 DEL.C. CH. 54).
- THE CONTRACTOR SHALL VERIFY IF THE SIGN STRUCTURE LOCATIONS ARE WITHIN A SUPERFUND SITE. REFER TO SPECIAL PROVISION 202560 FOR GUIDANCE AND MORE INFORMATION.
- THE CONTRACTOR SHALL CONTACT MICHAEL ELLER, CHIEF OF SCHEDULING FOR DART FIRST STATE, AT 302-576-6061 14 DAYS PRIOR TO THE START OF CONSTRUCTION OF A WORK ORDER.
- QUANTITIES HAVE BEEN INCREASED TO ACCOUNT FOR THE POTENTIAL ADDITION OF FUTURE SIGN STRUCTURE LOCATIONS. CONTRACTOR WILL BE PAID FOR THE ACTUAL QUANTITY USED FOR EACH ITEM.

|            |                  |            |             |
|------------|------------------|------------|-------------|
| CONTRACT   | BRIDGE NO.       | <b>N/A</b> | SHEET NO.   |
| T201407004 | DESIGNED BY: JWS |            | 4           |
| COUNTY     | CHECKED BY: DEF  |            | TOTAL SHTS. |
| NEW CASTLE |                  |            | 83          |

| HORIZONTAL /VERTICAL CONTROL DATA |          |          |             |             |           |
|-----------------------------------|----------|----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET   | NORTHING    | EASTING     | ELEVATION |
| TP-5                              | 10+02.36 | 4.65 LT. | 625682.6142 | 620097.8445 | 37.28     |
| TP-6                              | 11+42.79 | 0.30 RT. | 625736.3479 | 620227.6780 | 35.48     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |  |
|--------------------------------|----------|--------|-------------|-------------|--|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |  |
| POB                            | 10+00.00 | 0.00   | 625677.4094 | 620097.6246 |  |
| POE                            | 12+60.00 | 0.00   | 625785.2206 | 620334.2186 |  |

**DATUM REFERENCE:**

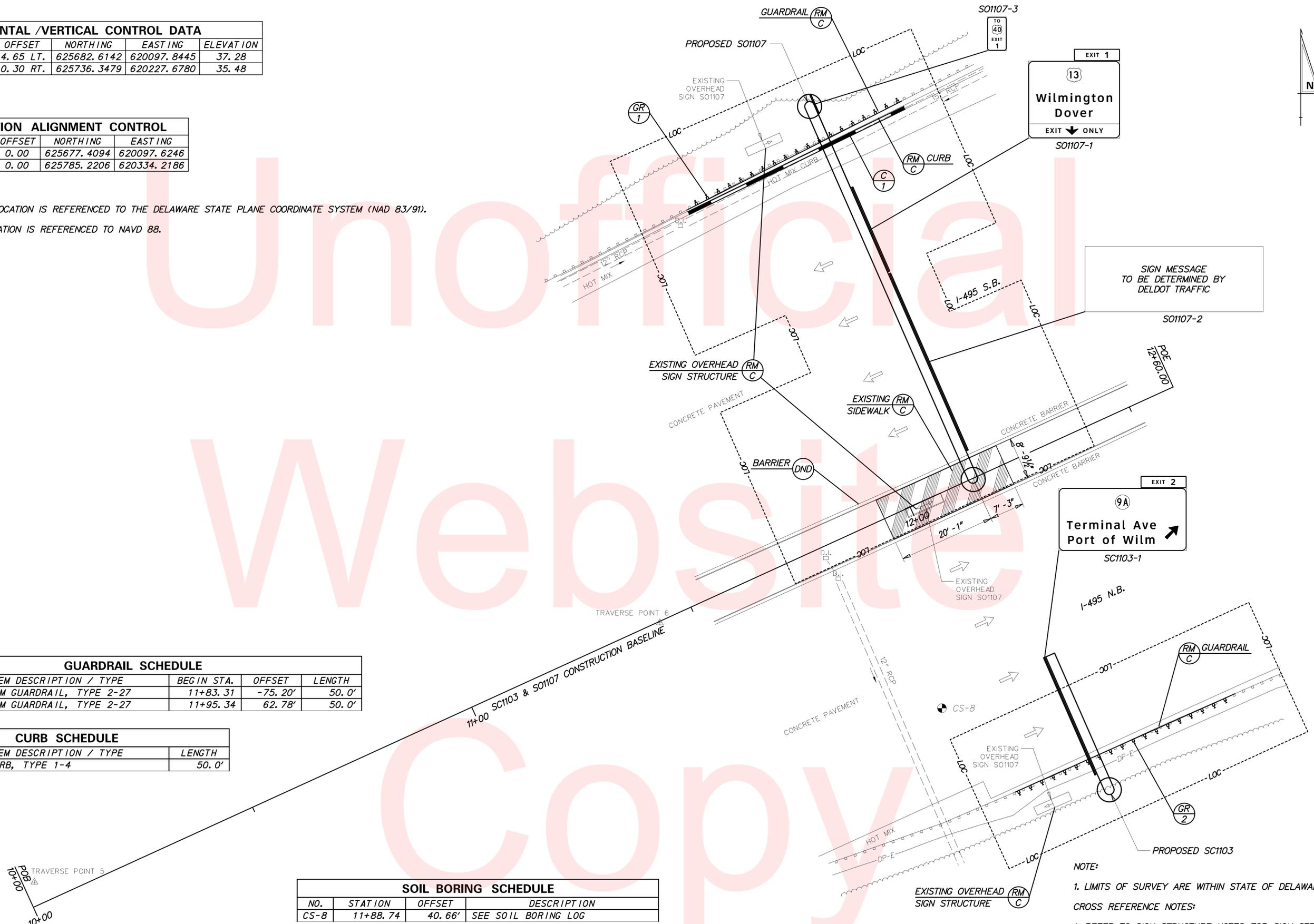
HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.

| GUARDRAIL SCHEDULE |                                 |            |         |        |
|--------------------|---------------------------------|------------|---------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET  | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 11+83.31   | -75.20' | 50.0'  |
| 2                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 11+95.34   | 62.78'  | 50.0'  |

| CURB SCHEDULE |                         |        |
|---------------|-------------------------|--------|
| NO.           | ITEM DESCRIPTION / TYPE | LENGTH |
| 1             | P. C. C. CURB, TYPE 1-4 | 50.0'  |

| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-8                 | 11+88.74 | 40.66' | SEE SOIL BORING LOG |



8/28/2015 12:29:15 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc\_L-495\_Task2\_Amnt\_1673\Drawings\Plans\CP03-SO1107.dgn

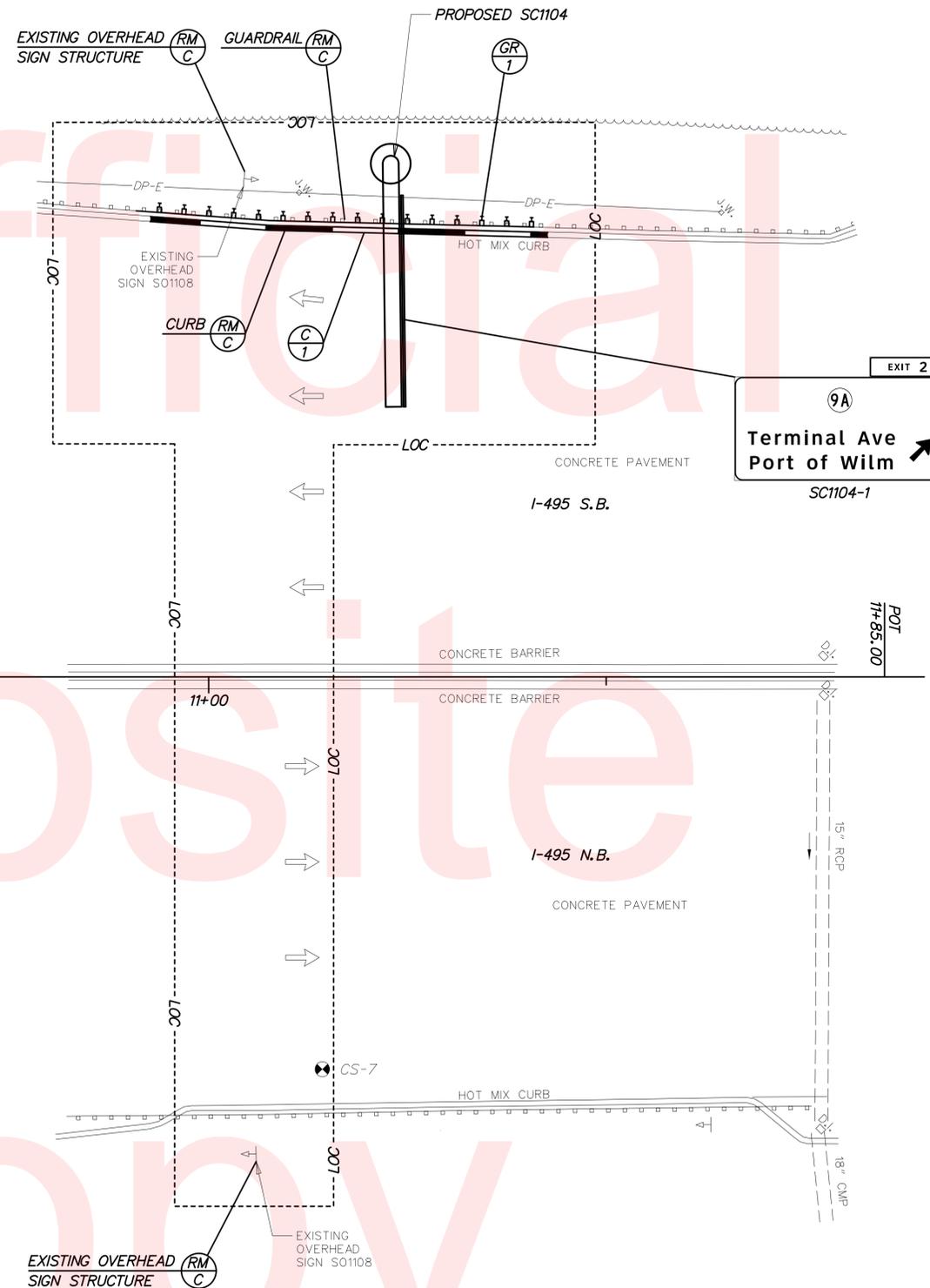
| HORIZONTAL /VERTICAL CONTROL DATA |          |            |             |             |           |
|-----------------------------------|----------|------------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET     | NORTHING    | EASTING     | ELEVATION |
| TP-7                              | 10+05.23 | 52.25 RT.  | 626794.7864 | 621746.7946 | 35.37     |
| TP-8                              | 15+51.24 | 51.00' RT. | 627181.9110 | 622131.8490 | 39.44     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |
|--------------------------------|----------|--------|-------------|-------------|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |
| POB                            | 10+00.00 | 0.00   | 626828.0193 | 621706.1350 |
| POT                            | 11+85.00 | 0.00   | 626958.8860 | 621836.8978 |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



POB  
10+00

SC1104 CONSTRUCTION BASELINE

11+00

POT  
11+85.00

TRAVERSE POINT 7

| GUARDRAIL SCHEDULE |                                 |            |         |        |
|--------------------|---------------------------------|------------|---------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET  | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 10+90.89   | -58.45' | 50.0'  |

| CURB SCHEDULE |                         |        |
|---------------|-------------------------|--------|
| NO.           | ITEM DESCRIPTION / TYPE | LENGTH |
| 1             | P. C. C. CURB, TYPE 1-4 | 50.0'  |

| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-7                 | 11+14.30 | 49.21' | SEE SOIL BORING LOG |

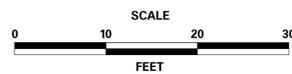
**NOTE:**

1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.

**CROSS REFERENCE NOTES:**

1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

ADDENDUMS / REVISIONS



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |              |     |
|------------|--------------|-----|
| CONTRACT   | BRIDGE NO.   | NA  |
| T201407004 | DESIGNED BY: | RMB |
| COUNTY     | CHECKED BY:  | DEF |
| NEW CASTLE |              |     |

**CONSTRUCTION PLAN  
SIGN STRUCTURE  
SC1104 AND SO1108**

|             |    |
|-------------|----|
| SHEET NO.   | 6  |
| TOTAL SHTS. | 83 |

| HORIZONTAL / VERTICAL CONTROL DATA |         |           |           |           |           |
|------------------------------------|---------|-----------|-----------|-----------|-----------|
| POINT                              | STATION | OFFSET    | NORTHING  | EASTING   | ELEVATION |
| TP-1                               | 0+05.39 | 77.52 LT. | 4874.2328 | 4916.1552 | 48.13     |
| TP-2                               | 1+54.55 | 53.04 LT. | 5000.0000 | 5000.0000 | 50.00     |

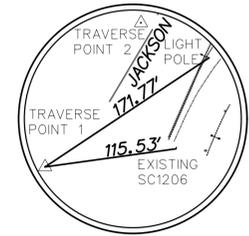
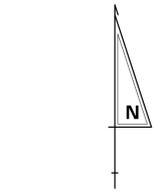
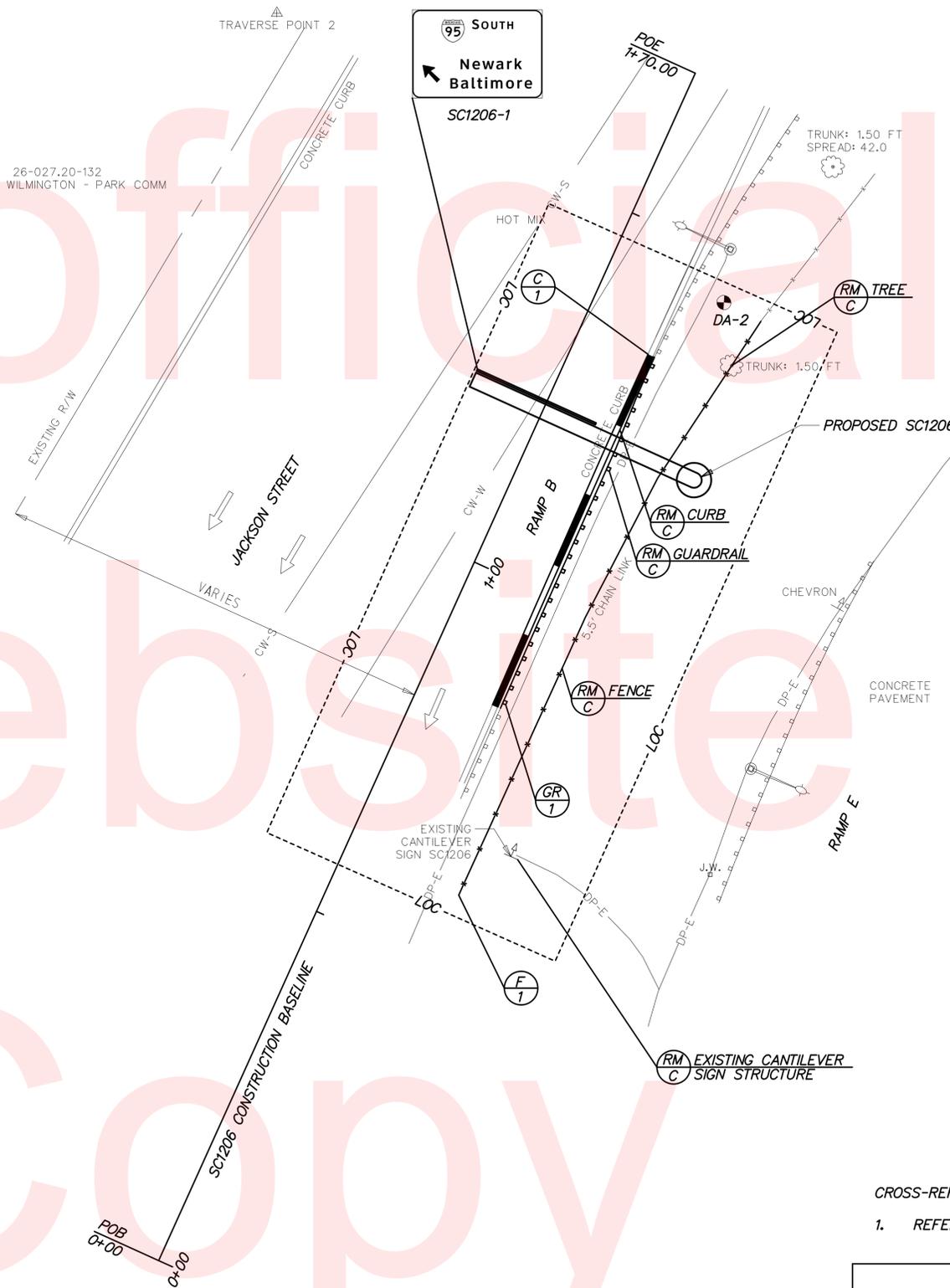
| CONSTRUCTION ALIGNMENT CONTROL |         |        |           |           |           |
|--------------------------------|---------|--------|-----------|-----------|-----------|
| POINT                          | STATION | OFFSET | NORTHING  | EASTING   | ELEVATION |
| POB                            | 0+00.00 | 0.00   | 4837.3355 | 4984.5404 |           |
| POE                            | 1+70.00 | 0.00   | 4992.1887 | 5054.6867 |           |

**DATUM REFERENCE:**

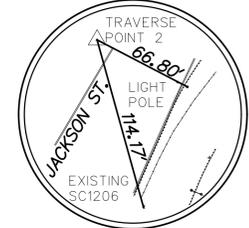
HORIZONTAL - THIS LOCATION IS BASED OFF AN ASSUMED HORIZONTAL DATUM.

VERTICAL - THIS LOCATION IS BASED OFF AN ASSUMED VERTICAL DATUM.

26-027.20-132  
CITY OF WILMINGTON - PARK COMM



TRAVERSE POINT #1  
N.T.S.



TRAVERSE POINT #2  
N.T.S.

| GUARDRAIL SCHEDULE |                                 |            |        |        |
|--------------------|---------------------------------|------------|--------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 1-27 | 0+84.07    | 10.97  | 50.0'  |

| CURB SCHEDULE |                         |        |
|---------------|-------------------------|--------|
| NO.           | ITEM DESCRIPTION / TYPE | LENGTH |
| 1             | P. C. C. CURB, TYPE 1-8 | 50.0'  |

| FENCE SCHEDULE |       |  |  |
|----------------|-------|--|--|
| NO.            | QTY.  | DESCRIPTION                                  | NOTES  |
| 1              | 85 LF | CHAIN-LINK FENCE, 5.5 FT. HIGH (ITEM 727004) | STA. 0+60, 16' RT. TO STA. 1+44, 21' RT. (+/-) |

**CROSS-REFERENCE NOTES:**

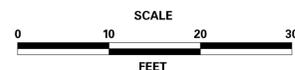
- REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

| SOIL BORING SCHEDULE |         |        |                     |
|----------------------|---------|--------|---------------------|
| NO.                  | STATION | OFFSET | DESCRIPTION         |
| DA-2                 | 1+44.29 | 15.75' | SEE SOIL BORING LOG |

8/28/2015 12:31:09 AM U:\425009\03 - Critical Cantilever\_Sgn\_Task\_3\_Amnl\_1673\Drawings\PLANS\CP01-SC1206.dgn



ADDENDUMS / REVISIONS



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: RMB |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

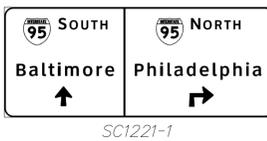
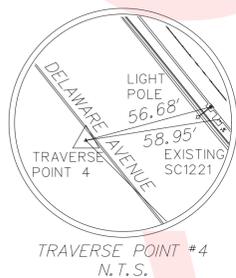
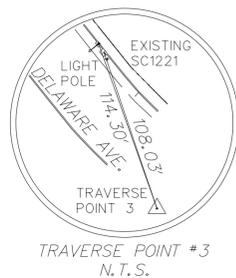
**CONSTRUCTION PLAN  
SIGN STRUCTURE SC1206**

|             |    |
|-------------|----|
| SHEET NO.   | 7  |
| TOTAL SHTS. | 83 |

| HORIZONTAL / VERTICAL CONTROL DATA |         |           |           |           |           |
|------------------------------------|---------|-----------|-----------|-----------|-----------|
| POINT                              | STATION | OFFSET    | NORTHING  | EASTING   | ELEVATION |
| TP-3                               | 0+01.07 | 25.91 LT. | 5206.9709 | 4793.0284 | 49.19     |
| TP-4                               | 1+32.16 | 37.07 LT. | 5300.0000 | 4700.0000 | 50.00     |

| CONSTRUCTION ALIGNMENT CONTROL |         |        |           |           |           |
|--------------------------------|---------|--------|-----------|-----------|-----------|
| POINT                          | STATION | OFFSET | NORTHING  | EASTING   | ELEVATION |
| POB                            | 0+00.00 | 0.00   | 5222.8546 | 4813.5261 |           |
| POE                            | 2+00.00 | 0.00   | 5375.7655 | 4684.6144 |           |

DATUM REFERENCE:  
 HORIZONTAL - THIS LOCATION IS BASED OFF AN ASSUMED HORIZONTAL DATUM.  
 VERTICAL - THIS LOCATION IS BASED OFF AN ASSUMED VERTICAL DATUM.



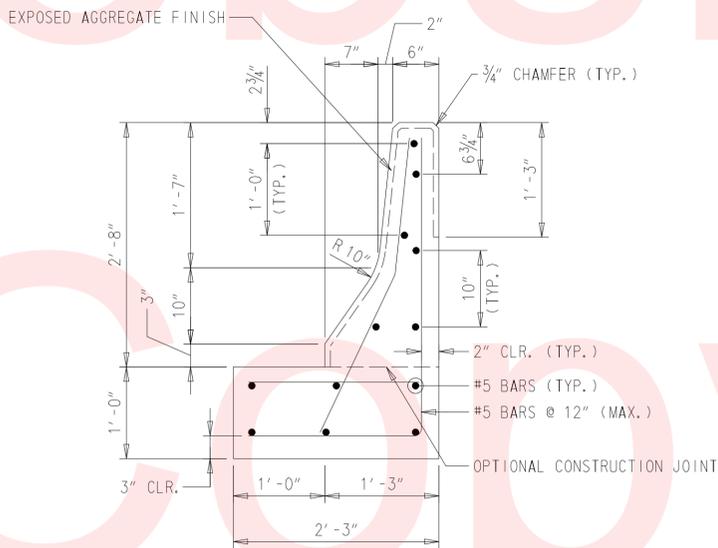
CROSS-REFERENCE NOTES:  
 1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

| COORDINATE LIST |         |        |           |           |
|-----------------|---------|--------|-----------|-----------|
| POINT NO.       | STATION | OFFSET | NORTHING  | EASTING   |
| 10001           | 1+66.79 | 21.87  | 5364.4773 | 4722.7411 |
| 10002           | 1+28.40 | 21.17  | 5334.6684 | 4746.9528 |
| 10003           | 1+28.51 | 15.29  | 5330.9591 | 4742.3860 |
| 10004           | 1+33.50 | 15.40  | 5334.8494 | 4739.2451 |
| 10005           | 1+42.53 | 11.13  | 5338.9965 | 4730.1661 |
| 10006           | 1+52.53 | 11.29  | 5346.7428 | 4723.8423 |
| 10007           | 1+61.90 | 16.02  | 5356.9631 | 4721.4203 |
| 10008           | 1+66.90 | 16.14  | 5360.8588 | 4718.2861 |

| BARRIER SCHEDULE |                                |        |
|------------------|--------------------------------|--------|
| NO.              | ITEM DESCRIPTION / TYPE        | LENGTH |
| 1                | MODIFIED P.C.C. SAFETY BARRIER | 36.0'  |

| CURB SCHEDULE |                         |        |
|---------------|-------------------------|--------|
| NO.           | ITEM DESCRIPTION / TYPE | LENGTH |
| 1             | P.C.C. CURB, TYPE 1-8   | 15.5'  |
| 2             | P.C.C. CURB, TYPE 1-8   | 15.0'  |

| SOIL BORING SCHEDULE |         |        |                     |
|----------------------|---------|--------|---------------------|
| NO.                  | STATION | OFFSET | DESCRIPTION         |
| DA-4                 | 1+49.96 | -1.23' | SEE SOIL BORING LOG |



MODIFIED P.C.C. SAFETY BARRIER (720653)  
 NOT TO SCALE



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| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
|                       |  |



|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: RMB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

|             |    |
|-------------|----|
| SHEET NO.   | 8  |
| TOTAL SHTS. | 83 |

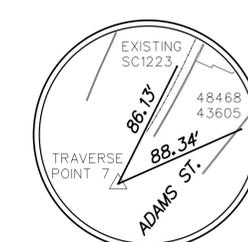
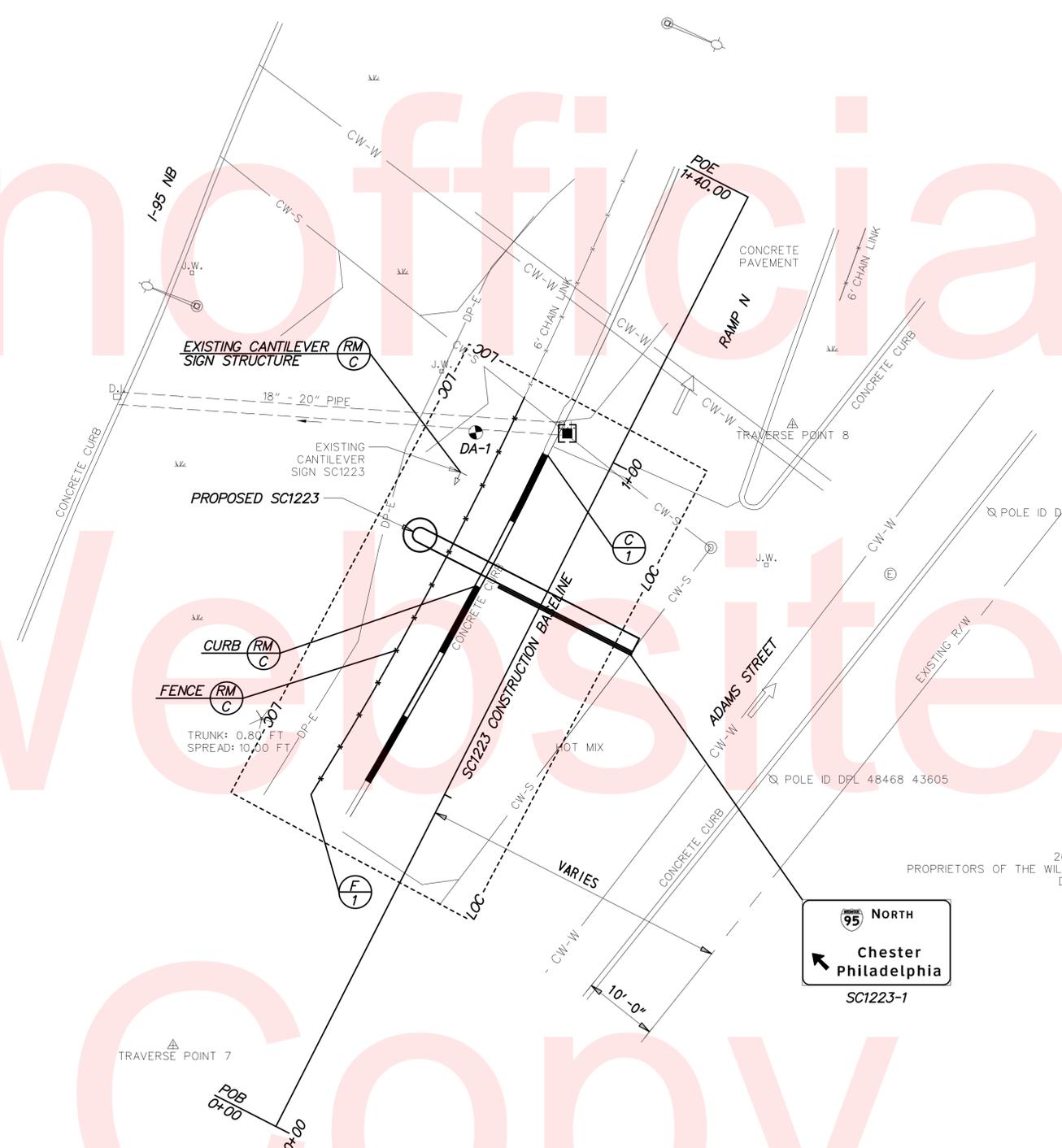
| HORIZONTAL / VERTICAL CONTROL DATA |         |           |           |           |           |
|------------------------------------|---------|-----------|-----------|-----------|-----------|
| POINT                              | STATION | OFFSET    | NORTHING  | EASTING   | ELEVATION |
| TP-7                               | 0+03.40 | 17.28 LT. | 4916.6504 | 4916.6501 | 50.68     |
| TP-8                               | 1+15.49 | 19.18 RT. | 5000.0000 | 5000.0000 | 50.00     |

| CONSTRUCTION ALIGNMENT CONTROL |         |        |           |           |           |
|--------------------------------|---------|--------|-----------|-----------|-----------|
| POINT                          | STATION | OFFSET | NORTHING  | EASTING   | ELEVATION |
| POB                            | 0+00.00 | 0.00   | 4905.7804 | 4930.5062 |           |
| POE                            | 1+40.00 | 0.00   | 5030.5389 | 4994.0302 |           |

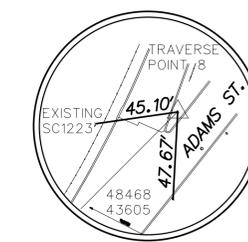
**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS BASED OFF AN ASSUMED HORIZONTAL DATUM.

VERTICAL - THIS LOCATION IS BASED OFF AN ASSUMED VERTICAL DATUM.



TRAVERSE POINT #7  
N.T.S.



TRAVERSE POINT #8  
N.T.S.



26-028.10-072  
PROPRIETORS OF THE WILMINGTON AND BRANDYWINE CEMETARY  
D.R. B-114-69

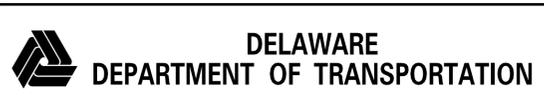
| CURB SCHEDULE |                         |        |
|---------------|-------------------------|--------|
| NO.           | ITEM DESCRIPTION / TYPE | LENGTH |
| 1             | P. C. C. CURB, TYPE 1-8 | 50.0'  |

| FENCE SCHEDULE |       |  |  |
|----------------|-------|--|--|
| NO.            | QTY.  | DESCRIPTION                                  | NOTES  |
| 1              | 61 LF | CHAIN-LINK FENCE, 6.0 FT. HIGH (ITEM 727004) | STA. 0+42, 16' LT. TO STA. 1+02, 15' LT. (+/-) |

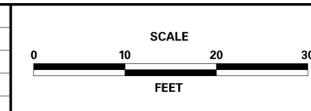
CROSS-REFERENCE NOTES:  
1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

| SOIL BORING SCHEDULE |         |         |                     |
|----------------------|---------|---------|---------------------|
| NO.                  | STATION | OFFSET  | DESCRIPTION         |
| DA-1                 | 0+95.22 | -18.31' | SEE SOIL BORING LOG |

8/28/2015 12:33:03 AM U:\425009\03 - Critical Cantilever\_Sgn\_Task\_3\_Amnl\_1673\Drawings\PLANS\CP04-SC1223.dgn



| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: RMB |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

**CONSTRUCTION PLAN  
SIGN STRUCTURE SC1223**

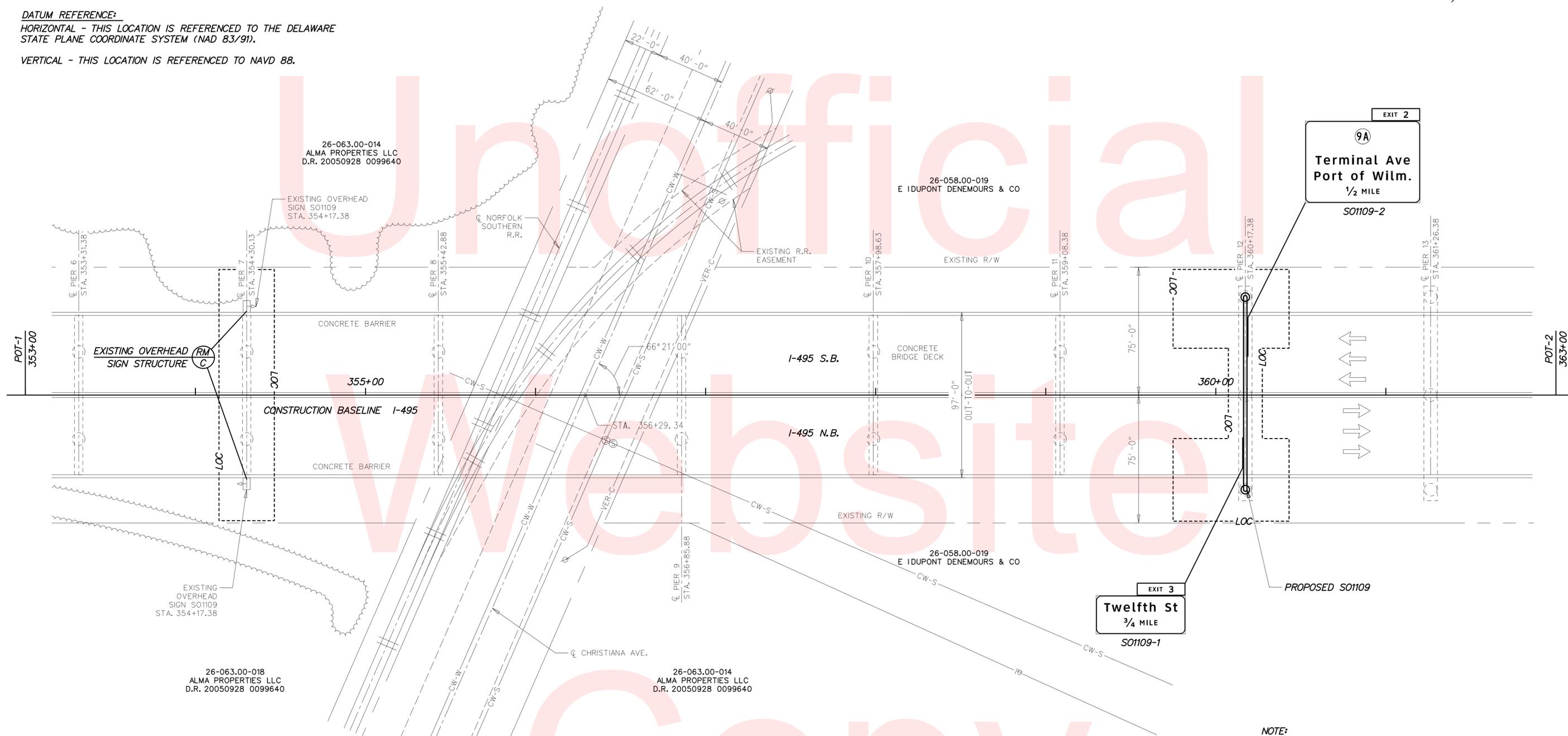
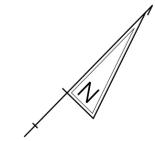
|             |    |
|-------------|----|
| SHEET NO.   | 9  |
| TOTAL SHTS. | 83 |

| CONSTRUCTION ALIGNMENT CONTROL |         |        |             |             |
|--------------------------------|---------|--------|-------------|-------------|
| POINT                          | STATION | OFFSET | NORTHING    | EASTING     |
| POT-1                          | 353+00  | 0.00   | 627585.1794 | 622477.7621 |
| POT-2                          | 363+00  | 0.00   | 628214.4534 | 623121.2016 |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



**NOTE:**

1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.

**CROSS REFERENCE NOTES:**

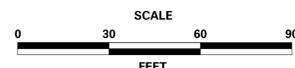
2. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

3. THE CONTRACTOR SHALL SUBMIT THE PROPOSED METHODS OF REMOVAL TO DELDOT ENGINEER FOR APPROVAL. THE TECHNIQUES CHOSEN MUST NOT BE DETRIMENTAL TO THE REMAINING STRUCTURE ELEMENTS. ANY DAMAGE TO THE EXISTING STRUCTURE ELEMENTS TO REMAIN, CAUSED BY THE CONTRACTOR OPERATIONS, SHALL BE REPAIRED TO THE SATISFACTION OF DELDOT ENGINEER AT NO COST TO DELDOT.

8/28/2015 12:34:50 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\CP05-SO1109.dgn



ADDENDUMS / REVISIONS



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: JLS |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

**CONSTRUCTION PLAN  
SIGN STRUCTURE SO1109**

|             |    |
|-------------|----|
| SHEET NO.   | 10 |
| TOTAL SHTS. | 83 |

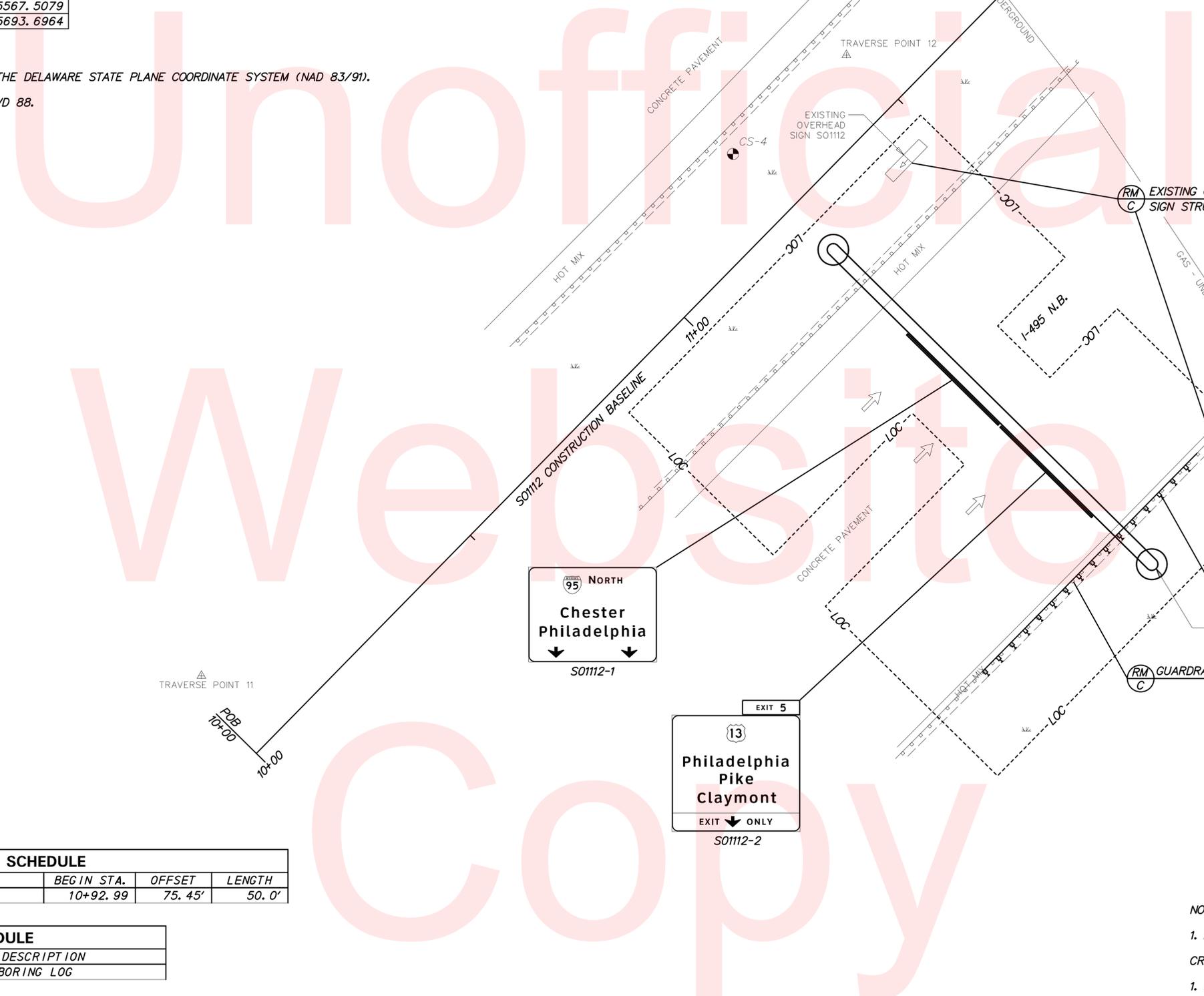
| HORIZONTAL /VERTICAL CONTROL DATA |          |           |             |             |           |
|-----------------------------------|----------|-----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET    | NORTHING    | EASTING     | ELEVATION |
| TP-11                             | 10+02.75 | 15.30 LT. | 654450.2856 | 645558.5219 | 21.39     |
| TP-12                             | 11+49.21 | 11.08 LT. | 654551.7683 | 645664.2074 | 21.46     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |
|--------------------------------|----------|--------|-------------|-------------|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |
| POB                            | 10+00.00 | 0.00   | 654437.6004 | 645567.5079 |
| POE                            | 11+80.00 | 0.00   | 654565.9611 | 645693.6964 |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



| GUARDRAIL SCHEDULE |                                 |            |        |        |
|--------------------|---------------------------------|------------|--------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 10+92.99   | 75.45' | 50.0'  |

| SOIL BORING SCHEDULE |          |         |                     |
|----------------------|----------|---------|---------------------|
| NO.                  | STATION  | OFFSET  | DESCRIPTION         |
| CS-4                 | 11+24.64 | -13.02' | SEE SOIL BORING LOG |

**NOTE:**  
 1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.  
**CROSS REFERENCE NOTES:**  
 1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

8/28/2015 12:35:22 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\CP06-S0112.dgn

|   |                       |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
|---|-----------------------|--------------------------|--|---|----------|------------|--------|------------|--|------------|-----------|--------------|-----|-------------|-----|--|---|-----------|----|-------------|----|
| <p><b>DELAWARE DEPARTMENT OF TRANSPORTATION</b></p> | ADDENDUMS / REVISIONS | <p>SCALE</p> <p>FEET</p> | <p><b>CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18</b></p> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>CONTRACT</td> <td>T201407004</td> </tr> <tr> <td>COUNTY</td> <td>NEW CASTLE</td> </tr> </table> | CONTRACT | T201407004 | COUNTY | NEW CASTLE | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>BRIDGE NO.</td> <td style="text-align: center;"><b>NA</b></td> </tr> <tr> <td>DESIGNED BY:</td> <td>RMB</td> </tr> <tr> <td>CHECKED BY:</td> <td>DEF</td> </tr> </table> | BRIDGE NO. | <b>NA</b> | DESIGNED BY: | RMB | CHECKED BY: | DEF | <p><b>CONSTRUCTION PLAN SIGN STRUCTURE S0112</b></p> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>SHEET NO.</td> <td style="text-align: center;">11</td> </tr> <tr> <td>TOTAL SHTS.</td> <td style="text-align: center;">83</td> </tr> </table> | SHEET NO. | 11 | TOTAL SHTS. | 83 |
|   | CONTRACT              | T201407004               |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
|   | COUNTY                | NEW CASTLE               |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
| BRIDGE NO.  | <b>NA</b>             |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
| DESIGNED BY:  | RMB                   |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
| CHECKED BY:   | DEF                   |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
| SHEET NO.   | 11                    |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |
| TOTAL SHTS.   | 83                    |                          |  |   |          |            |        |            |  |            |           |              |     |             |     |  |   |           |    |             |    |

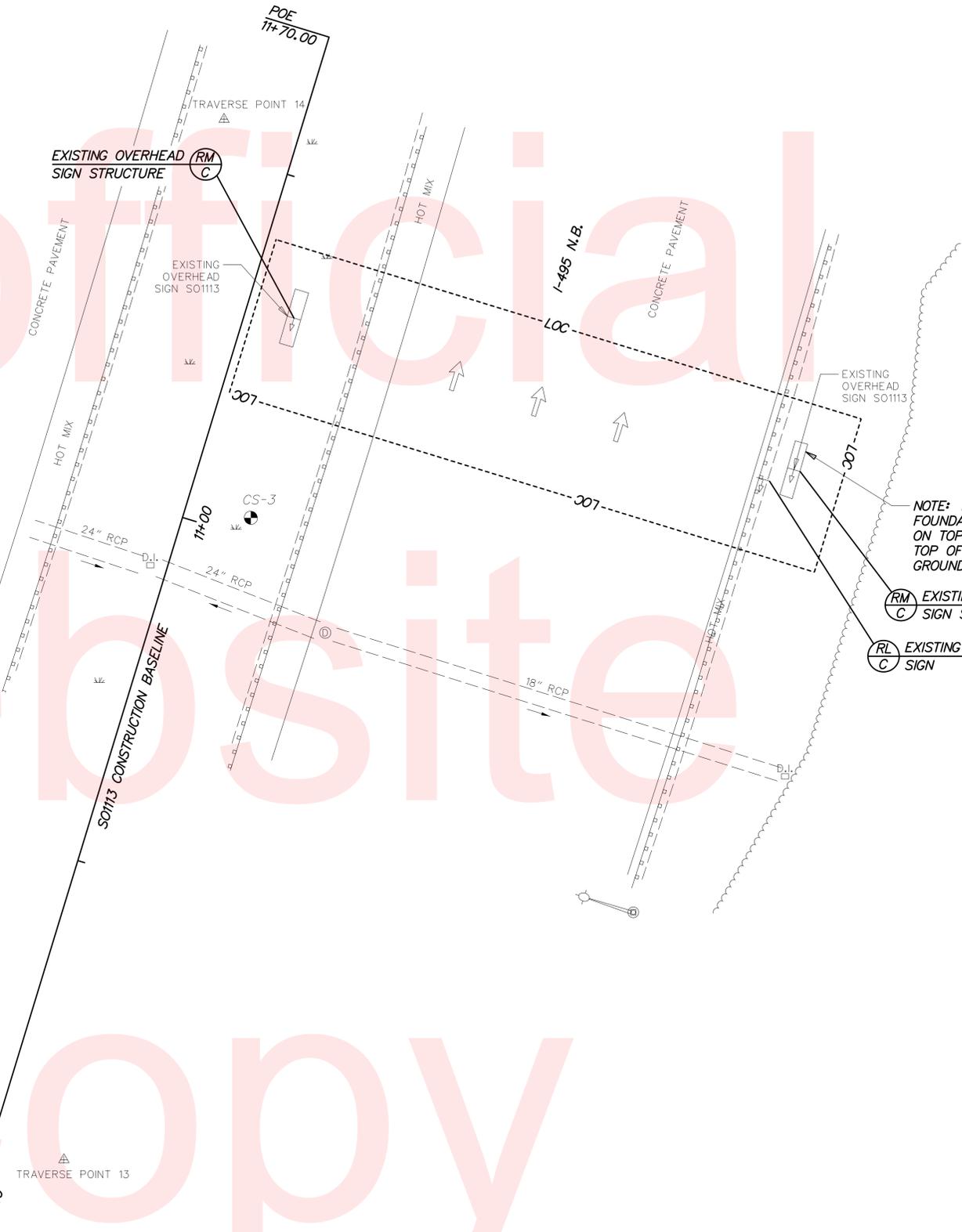
| HORIZONTAL /VERTICAL CONTROL DATA |          |           |             |             |           |
|-----------------------------------|----------|-----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET    | NORTHING    | EASTING     | ELEVATION |
| TP-13                             | 10+09.56 | 10.35 RT. | 655776.9594 | 646471.4137 | 25.96     |
| TP-14                             | 11+54.75 | 10.66 LT. | 655921.9427 | 646493.7858 | 26.44     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |           |
|--------------------------------|----------|--------|-------------|-------------|-----------|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     | ELEVATION |
| POB                            | 10+00.00 | 0.00   | 655770.8408 | 646458.7242 |           |
| POE                            | 11+70.00 | 0.00   | 655933.4092 | 646508.4388 |           |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



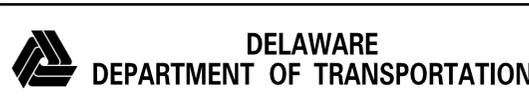
NOTE: DELDOT ELECTRIC FEEDS THROUGH EXISTING FOUNDATION. CONSTRUCT JUNCTION WELL, TYPE 1, ON TOP OF REMAINING PORTION OF FOUNDATION. TOP OF JUNCTION WELL SHALL BE AT FINISHED GROUND LEVEL.

- (RM/C) EXISTING OVERHEAD SIGN STRUCTURE
- (RL/C) EXISTING SEPTA SIGN

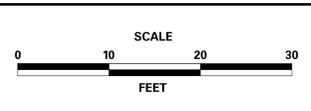
| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-3                 | 11+02.74 | 9.15'  | SEE SOIL BORING LOG |

NOTE:  
1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.

8/28/2015 12:35:43 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\CP07-S01113.dgn



| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
|                       |  |



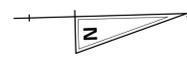
**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |            |
|------------|------------------|------------|
| CONTRACT   | BRIDGE NO.       | <b>N/A</b> |
| T201407004 | DESIGNED BY: RMB |            |
| COUNTY     | CHECKED BY: DEF  |            |
| NEW CASTLE |                  |            |

**CONSTRUCTION PLAN  
SIGN STRUCTURE S01113**

|             |    |
|-------------|----|
| SHEET NO.   | 12 |
| TOTAL SHTS. | 83 |

| HORIZONTAL /VERTICAL CONTROL DATA |          |          |             |             |           |
|-----------------------------------|----------|----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET   | NORTHING    | EASTING     | ELEVATION |
| TP-15                             | 10+29.99 | 8.02 LT. | 656611.5610 | 646605.3540 | 34.10     |
| TP-16                             | 12+77.35 | 2.79 LT. | 656858.9619 | 646606.9810 | 32.21     |

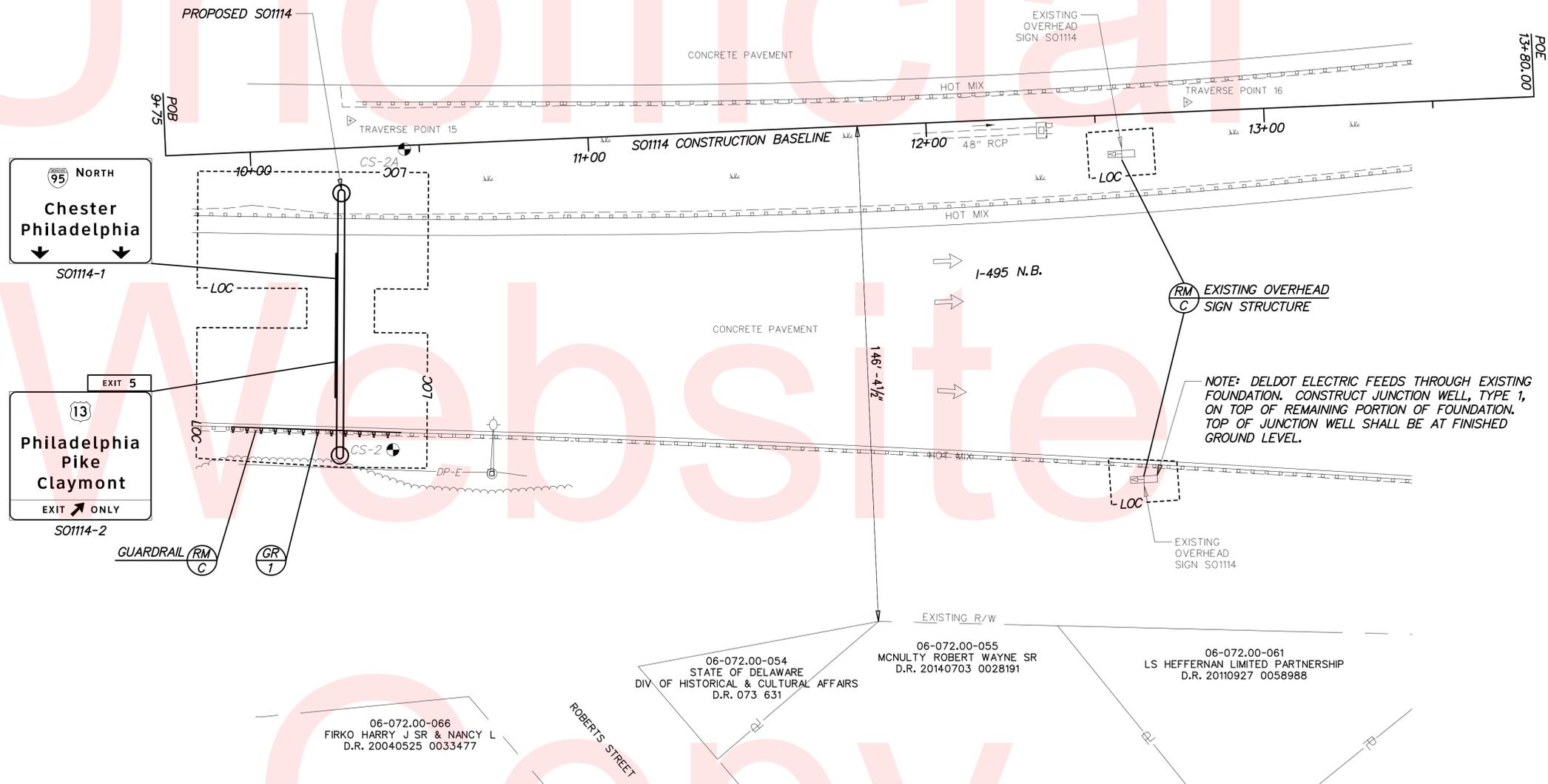


| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |
|--------------------------------|----------|--------|-------------|-------------|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |
| POB                            | 9+75.00  | 0.00   | 656556.6888 | 646614.1699 |
| POE                            | 13+80.00 | 0.00   | 656961.6460 | 646608.2835 |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



| GUARDRAIL SCHEDULE |                                 |            |        |        |
|--------------------|---------------------------------|------------|--------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 9+91.10    | 80.78' | 50.0'  |

| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-2                 | 10+38.40 | 89.57' | SEE SOIL BORING LOG |
| CS-2A                | 10+45.63 | 1.10'  | SEE SOIL BORING LOG |

- NOTE:**
- LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.
- CROSS REFERENCE NOTES:**
- REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

8/28/2015 12:36:47 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\CP08-SO1114.dgn

|   |                       |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
|---|-----------------------|--------------------------|--|---|----------|------------|-----------|------------|------------------|--|--------|-----------------|--|------------|--|--|--|---|-----------|----|-------------|----|
| <p><b>DELAWARE</b><br/>DEPARTMENT OF TRANSPORTATION</p> | ADDENDUMS / REVISIONS | <p>SCALE</p> <p>FEET</p> | <p><b>CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18</b></p> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>CONTRACT</td> <td>BRIDGE NO.</td> <td style="text-align: center;"><b>NA</b></td> </tr> <tr> <td>T201407004</td> <td>DESIGNED BY: RMB</td> <td></td> </tr> <tr> <td>COUNTY</td> <td>CHECKED BY: DEF</td> <td></td> </tr> <tr> <td>NEW CASTLE</td> <td></td> <td></td> </tr> </table> | CONTRACT | BRIDGE NO. | <b>NA</b> | T201407004 | DESIGNED BY: RMB |  | COUNTY | CHECKED BY: DEF |  | NEW CASTLE |  |  | <p><b>CONSTRUCTION PLAN</b><br/><b>SIGN STRUCTURE SO1114</b></p> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>SHEET NO.</td> <td style="text-align: center;">13</td> </tr> <tr> <td>TOTAL SHTS.</td> <td style="text-align: center;">83</td> </tr> </table> | SHEET NO. | 13 | TOTAL SHTS. | 83 |
|   | CONTRACT              | BRIDGE NO.               | <b>NA</b>  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
|   | T201407004            | DESIGNED BY: RMB         |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
| COUNTY  | CHECKED BY: DEF       |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
| NEW CASTLE  |                       |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
| SHEET NO.   | 13                    |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
| TOTAL SHTS.   | 83                    |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
|   |                       |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |
|   |                       |                          |  |   |          |            |           |            |                  |  |        |                 |  |            |  |  |  |   |           |    |             |    |

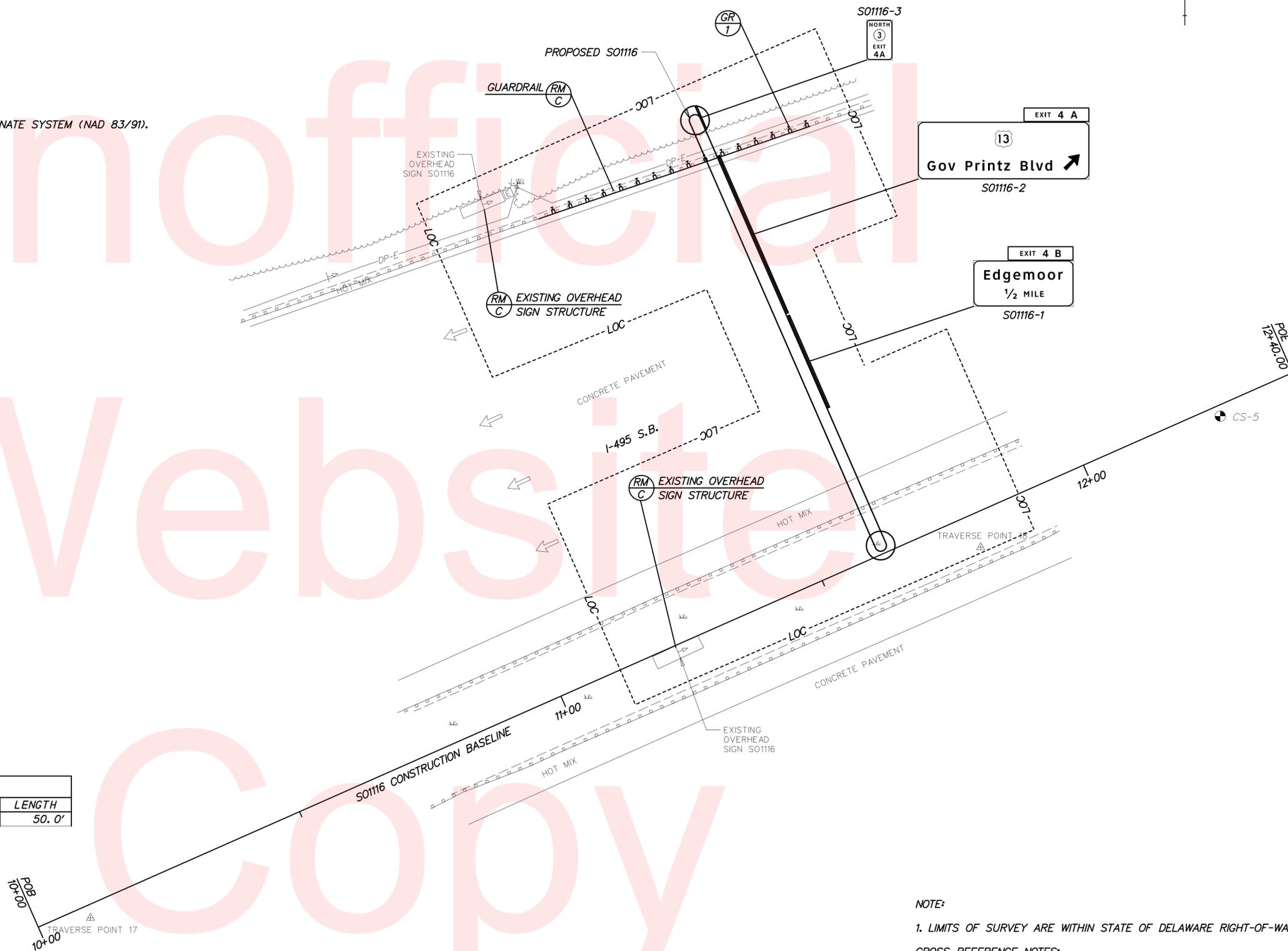
| HORIZONTAL /VERTICAL CONTROL DATA |          |          |             |             |           |
|-----------------------------------|----------|----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET   | NORTHING    | EASTING     | ELEVATION |
| TP-17                             | 10+09.01 | 2.23 RT. | 639095.6378 | 633263.6022 | 42.34     |
| TP-18                             | 11+77.72 | 5.88 RT. | 639160.3785 | 633419.4418 | 39.96     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |  |
|--------------------------------|----------|--------|-------------|-------------|--|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |  |
| POB                            | 10+00.00 | 0.00   | 639094.0390 | 633254.4632 |  |
| POE                            | 12+40.00 | 0.00   | 639190.8961 | 633474.0508 |  |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



| GUARDRAIL SCHEDULE |                                 |            |         |        |
|--------------------|---------------------------------|------------|---------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET  | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 11+30.06   | -77.85' | 50.0'  |

| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-5                 | 12+25.35 | 1.83'  | SEE SOIL BORING LOG |

**NOTE:**  
 1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.  
**CROSS REFERENCE NOTES:**  
 1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

8/28/2015 12:37:09 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Str. I-495 Task2 Amnt. 1673\Drawings\Plans\CP09-SO1116.dgn

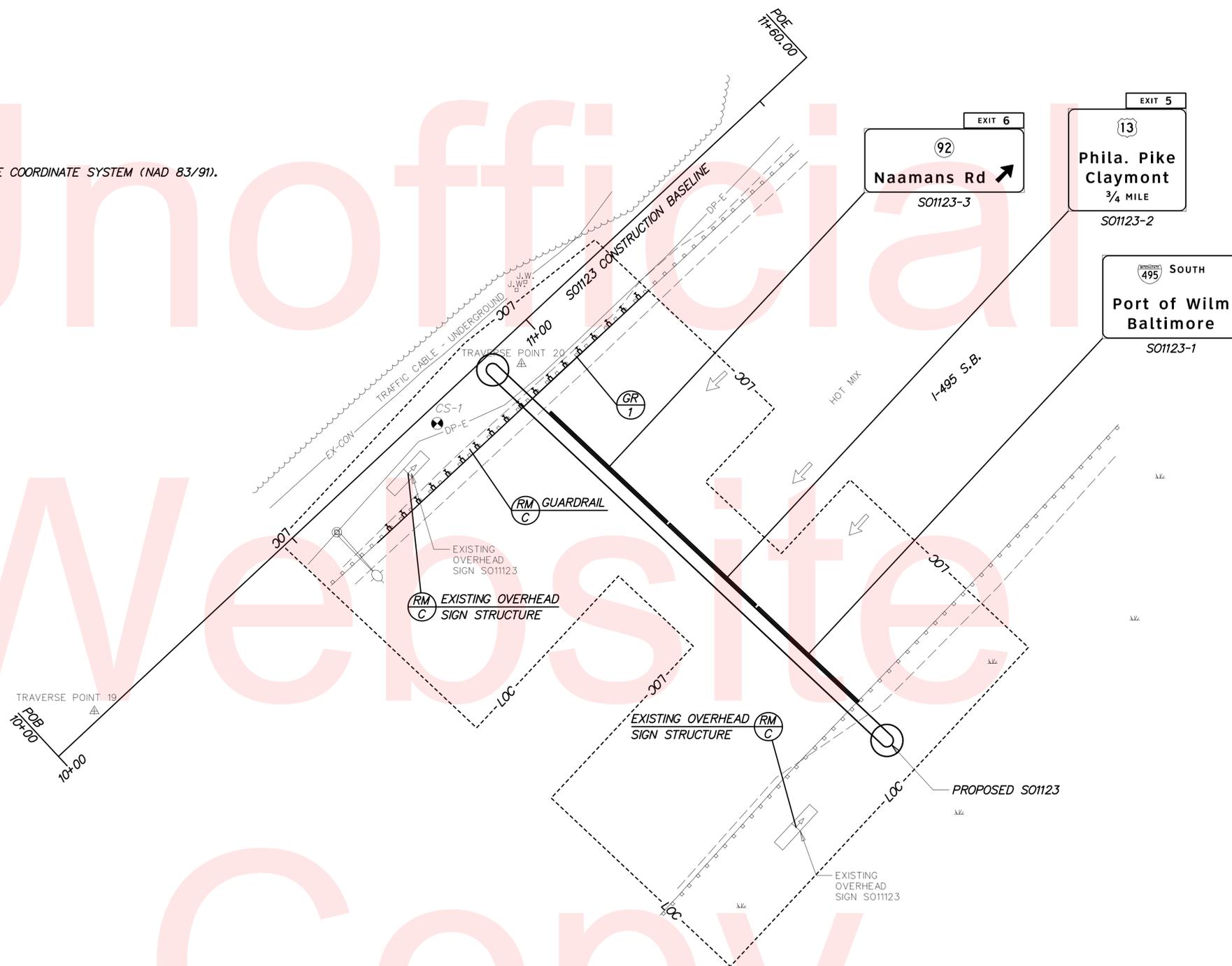
| HORIZONTAL /VERTICAL CONTROL DATA |          |          |             |             |           |
|-----------------------------------|----------|----------|-------------|-------------|-----------|
| POINT                             | STATION  | OFFSET   | NORTHING    | EASTING     | ELEVATION |
| TP-19                             | 10+08.92 | 1.30 LT. | 662000.2841 | 646121.4028 | 82.83     |
| TP-20                             | 10+94.77 | 4.58 RT. | 662054.4875 | 646188.2319 | 84.29     |

| CONSTRUCTION ALIGNMENT CONTROL |          |        |             |             |  |
|--------------------------------|----------|--------|-------------|-------------|--|
| POINT                          | STATION  | OFFSET | NORTHING    | EASTING     |  |
| POB                            | 10+00.00 | 0.00   | 661993.2502 | 646115.7615 |  |
| POE                            | 11+60.00 | 0.00   | 662102.3035 | 646232.8399 |  |

**DATUM REFERENCE:**

HORIZONTAL - THIS LOCATION IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS LOCATION IS REFERENCED TO NAVD 88.



| GUARDRAIL SCHEDULE |                                 |            |        |        |
|--------------------|---------------------------------|------------|--------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | BEGIN STA. | OFFSET | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 10+59.27   | 9.63'  | 50.0'  |

| SOIL BORING SCHEDULE |          |        |                     |
|----------------------|----------|--------|---------------------|
| NO.                  | STATION  | OFFSET | DESCRIPTION         |
| CS-1                 | 10+78.78 | 2.38'  | SEE SOIL BORING LOG |

**NOTE:**  
 1. LIMITS OF SURVEY ARE WITHIN STATE OF DELAWARE RIGHT-OF-WAY.  
**CROSS REFERENCE NOTES:**  
 1. REFER TO SIGN STRUCTURE NOTES FOR SIGN STRUCTURE LOCATION.

8/28/2015 12:37:31 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\CP10-SO1123.dgn

|  |                       |               |   |  |  |  |                                      |
|--|-----------------------|---------------|---|--|--|--|--------------------------------------|
| <b>DELAWARE<br/>DEPARTMENT OF TRANSPORTATION</b> | ADDENDUMS / REVISIONS | SCALE<br>FEET | <b>CANTILEVER AND OVERHEAD SIGN<br/>STRUCTURES, OPEN-END, FY16-18</b> | CONTRACT<br>T201407004<br>COUNTY<br>NEW CASTLE | BRIDGE NO. <b>N/A</b><br>DESIGNED BY: RMB<br>CHECKED BY: DEF | <b>CONSTRUCTION PLAN<br/>SIGN STRUCTURE SO1123</b> | SHEET NO.<br>15<br>TOTAL SHTS.<br>83 |
|  |                       |               |   |  |  |  |                                      |
|  |                       |               |   |  |  |  |                                      |
|  |                       |               |   |  |  |  |                                      |





NOTES:

- COLORS: B = BLACK, G = GREEN, O = ORANGE, W = WHITE, Y = YELLOW
- LETTER LOCATIONS ARE PANEL EDGE TO LOWER LEFT CORNER.
- FOR ARROW DETAILS, SEE FHWA PUBLICATION "STANDARD HIGHWAY SIGNS BOOK".
- SIGNS SHALL BE FABRICATED OF EXTRUDED ALUMINUM CHANNEL PANELS WITH TYPE IX SHEETING PER ITEM 749500.
- ALL COPY SHALL BE CLEARVIEW 5-W FONT, UNLESS NOTED OTHERWISE.
- SIGNS SHOWN ON THIS PLAN SHALL BE PAID FOR UNDER ITEM 749500 "SIGN PANEL".
- THE DESIGN OF SIGNS SHALL FOLLOW THE REQUIREMENTS SET FORTH IN THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE FHWA 2004 EDITION OF "STANDARD HIGHWAY SIGNS" IN CONJUNCTION WITH THE 2012 SUPPLEMENT.

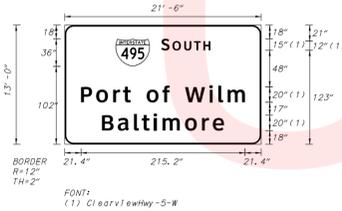
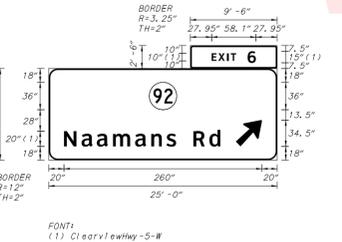
| PANEL DESIGNATION    | QUANTITY | LEGEND   | SIZE  |          |       | COLOR  |                          | BORDER            |        | ARROW | SHIELD | REMARKS |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
|----------------------|----------|----------|---|----------|-------|--------|--------------------------|-------------------|--------|-------|--------|---------|-------|-------|-------|-------|-------|---------------|------|---------------|-------|-------|-------|-------|-------|------|------|------|---|------|------|---------------|------|---|---|------|---------------|------|-------|-------|-------|-------|-------|-------|-------|------|---|---|---|---|------|---------------|---------------|------|------|------|-------|-------|-------|---------------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|-------|---------------|-------|----------|----------|---------------|------|------|------|-----------------------------|-------------------|-------------------|-------|-------|-------|-------|-------|-------|---------------|------|------|------|-------|-------|-------|---------------|-------|-------|------|---|------|------|------|---|---|---|---|------|---------------|---------------|-------|-------|-------|-------|-------|-------|-------|---------|---------|------|---------|---------|----|----|----|-------------------|----|----|-------------------|
|                      |          |          | AREA (S.F.)   | HEIGHT   | WIDTH | LEGEND | BACKGROUND               | WIDTH             | RADIUS |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| S01109-2             | 1        |          | <table border="1"> <thead> <tr> <th>Y SERIES</th> <th>E</th> <th>X</th> <th>I</th> <th>T</th> <th>HT</th> <th>LEN</th> </tr> </thead> <tbody> <tr> <td>10.0</td> <td>28.2</td> <td>36.8</td> <td>48.3</td> <td>53.2</td> <td>10.0</td> <td>10.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td>32.2</td> <td>32.2</td> </tr> <tr> <td>7.5</td> <td>2</td> <td></td> <td></td> <td></td> <td>15.0</td> <td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>75.4</td> <td></td> <td></td> <td></td> <td>10.3</td> <td>10.3</td> </tr> <tr> <td>90.0</td> <td>T</td> <td>e</td> <td>r</td> <td>m</td> <td>i</td> <td>n</td> <td>a</td> <td>l</td> <td>A</td> <td>v</td> <td>e</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>24.0</td> <td>43.5</td> <td>65.0</td> <td>80.0</td> <td>109.9</td> <td>121.7</td> <td>142.0</td> <td>163.2</td> <td>189.6</td> <td>211.6</td> <td>231.2</td> <td>222.0</td> </tr> <tr> <td>53.0</td> <td>P</td> <td>o</td> <td>r</td> <td>t</td> <td>O</td> <td>f</td> <td>W</td> <td>i</td> <td>l</td> <td>m</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.6</td> <td>41.5</td> <td>63.8</td> <td>76.8</td> <td>107.8</td> <td>128.5</td> <td>158.1</td> <td>190.0</td> <td>201.8</td> <td>214.2</td> <td>243.4</td> <td>226.8</td> </tr> <tr> <td>18.0</td> <td>12</td> <td></td> <td></td> <td></td> <td>18.0</td> <td>18.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>95.4</td> <td></td> <td></td> <td></td> <td>25.7</td> <td>25.7</td> </tr> <tr> <td>21.0</td> <td>M</td> <td>I</td> <td>L</td> <td>E</td> <td>12.0</td> <td>12.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>133.1</td> <td>149.0</td> <td>156.1</td> <td>167.0</td> <td>41.5</td> <td>41.5</td> </tr> </tbody> </table>  | Y SERIES | E     | X      | I                        | T                 | HT     | LEN   | 10.0   | 28.2    | 36.8  | 48.3  | 53.2  | 10.0  | 10.0  | Clearview-5-W | 28.2 | 36.8          | 48.2  | 53.2  | 32.2  | 32.2  | 7.5   | 2    |      |      |   | 15.0 | 15.0 | Clearview-5-W | 75.4 |   |   |      | 10.3          | 10.3 | 90.0  | T     | e     | r     | m     | i     | n     | a    | l | A | v | e | 20.0 | Clearview-5-W | 24.0          | 43.5 | 65.0 | 80.0 | 109.9 | 121.7 | 142.0 | 163.2         | 189.6 | 211.6 | 231.2 | 222.0 | 53.0  | P     | o     | r     | t     | O     | f     | W     | i             | l     | m        | 20.0     | Clearview-5-W | 21.6 | 41.5 | 63.8 | 76.8                        | 107.8             | 128.5             | 158.1 | 190.0 | 201.8 | 214.2 | 243.4 | 226.8 | 18.0          | 12   |      |      |       | 18.0  | 18.0  | Clearview-5-W | 95.4  |       |      |   | 25.7 | 25.7 | 21.0 | M | I | L | E | 12.0 | 12.0          | Clearview-5-W | 133.1 | 149.0 | 156.1 | 167.0 | 41.5  | 41.5  | 23.8  | 2' - 6" | 9' - 6" | W    | G       | 2"      | 3" | -- | -- | EXTRUDED ALUMINUM |    |    |                   |
|                      |          |          | Y SERIES  | E        | X     | I      | T                        | HT                | LEN    |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 10.0                 | 28.2     | 36.8     | 48.3  | 53.2     | 10.0  | 10.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 28.2     | 36.8     | 48.2  | 53.2     | 32.2  | 32.2   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 7.5                  | 2        |          |   |          | 15.0  | 15.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 75.4     |          |   |          | 10.3  | 10.3   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 90.0                 | T        | e        | r   | m        | i     | n      | a                        | l                 | A      | v     | e      | 20.0    |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 24.0     | 43.5     | 65.0  | 80.0     | 109.9 | 121.7  | 142.0                    | 163.2             | 189.6  | 211.6 | 231.2  | 222.0   |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 53.0                 | P        | o        | r   | t        | O     | f      | W                        | i                 | l      | m     | 20.0   |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 21.6     | 41.5     | 63.8  | 76.8     | 107.8 | 128.5  | 158.1                    | 190.0             | 201.8  | 214.2 | 243.4  | 226.8   |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 18.0                 | 12       |          |   |          | 18.0  | 18.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 95.4     |          |   |          | 25.7  | 25.7   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 21.0                 | M        | I        | L   | E        | 12.0  | 12.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 133.1    | 149.0    | 156.1   | 167.0    | 41.5  | 41.5   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 360.0                | 16' - 0" | 22' - 6" | W   | G        | 2"    | 12"    | --                       | M1-5<br>36" x 36" |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| S01112-1<br>S01114-1 | 2        |          | <table border="1"> <thead> <tr> <th>Y SERIES</th> <th>N</th> <th>O</th> <th>R</th> <th>T</th> <th>H</th> <th>HT</th> <th>LEN</th> </tr> </thead> <tbody> <tr> <td>154.0</td> <td>118.9</td> <td>135.1</td> <td>150.6</td> <td>162.5</td> <td>174.8</td> <td>15.0</td> <td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>118.9</td> <td>135.1</td> <td>150.6</td> <td>162.5</td> <td>174.8</td> <td>65.1</td> <td>65.1</td> </tr> <tr> <td>87.0</td> <td>C</td> <td>h</td> <td>e</td> <td>s</td> <td>t</td> <td>e</td> <td>r</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>63.0</td> <td>85.4</td> <td>106.0</td> <td>126.0</td> <td>143.2</td> <td>158.2</td> <td>179.8</td> <td>125.9</td> </tr> <tr> <td>50.0</td> <td>P</td> <td>h</td> <td>i</td> <td>l</td> <td>a</td> <td>d</td> <td>e</td> <td>l</td> <td>p</td> <td>h</td> <td>i</td> <td>a</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.9</td> <td>42.6</td> <td>63.7</td> <td>75.5</td> <td>86.6</td> <td>107.1</td> <td>128.4</td> <td>149.9</td> <td>162.3</td> <td>183.6</td> <td>204.7</td> <td>215.3</td> <td>208.3</td> </tr> </tbody> </table>   | Y SERIES | N     | O      | R                        | T                 | H      | HT    | LEN    | 154.0   | 118.9 | 135.1 | 150.6 | 162.5 | 174.8 | 15.0          | 15.0 | Clearview-5-W | 118.9 | 135.1 | 150.6 | 162.5 | 174.8 | 65.1 | 65.1 | 87.0 | C | h    | e    | s             | t    | e | r | 20.0 | Clearview-5-W | 63.0 | 85.4  | 106.0 | 126.0 | 143.2 | 158.2 | 179.8 | 125.9 | 50.0 | P | h | i | l | a    | d             | e             | l    | p    | h    | i     | a     | 20.0  | Clearview-5-W | 21.9  | 42.6  | 63.7  | 75.5  | 86.6  | 107.1 | 128.4 | 149.9 | 162.3 | 183.6 | 204.7 | 215.3 | 208.3         | 325.5 | 15' - 6" | 21' - 0" | W             | G    | 2"   | 12"  | (2)<br>DOWN<br>ARROW<br>22" | M1-1<br>36" x 36" | EXTRUDED ALUMINUM |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Y SERIES             | N        | O        | R   | T        | H     | HT     | LEN                      |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 154.0                | 118.9    | 135.1    | 150.6   | 162.5    | 174.8 | 15.0   | 15.0                     |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 118.9    | 135.1    | 150.6   | 162.5    | 174.8 | 65.1   | 65.1                     |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 87.0                 | C        | h        | e   | s        | t     | e      | r                        | 20.0              |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 63.0     | 85.4     | 106.0   | 126.0    | 143.2 | 158.2  | 179.8                    | 125.9             |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 50.0                 | P        | h        | i   | l        | a     | d      | e                        | l                 | p      | h     | i      | a       | 20.0  |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 21.9     | 42.6     | 63.7  | 75.5     | 86.6  | 107.1  | 128.4                    | 149.9             | 162.3  | 183.6 | 204.7  | 215.3   | 208.3 |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| S01112-2             | 1        |          | <table border="1"> <thead> <tr> <th>Y SERIES</th> <th>E</th> <th>X</th> <th>I</th> <th>T</th> <th>HT</th> <th>LEN</th> </tr> </thead> <tbody> <tr> <td>10.0</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td>10.0</td> <td>10.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td>32.2</td> <td>32.2</td> </tr> <tr> <td>7.5</td> <td>5</td> <td></td> <td></td> <td></td> <td>15.0</td> <td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>75.4</td> <td></td> <td></td> <td></td> <td>10.3</td> <td>10.3</td> </tr> <tr> <td>127.0</td> <td>P</td> <td>h</td> <td>i</td> <td>l</td> <td>a</td> <td>d</td> <td>e</td> <td>l</td> <td>p</td> <td>h</td> <td>i</td> <td>a</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.9</td> <td>42.6</td> <td>63.7</td> <td>75.5</td> <td>86.6</td> <td>107.1</td> <td>128.4</td> <td>149.9</td> <td>162.3</td> <td>183.6</td> <td>204.7</td> <td>215.3</td> <td>208.3</td> </tr> <tr> <td>93.0</td> <td>P</td> <td>i</td> <td>k</td> <td>e</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>94.1</td> <td>114.4</td> <td>126.3</td> <td>144.8</td> <td>65.5</td> <td>65.5</td> </tr> <tr> <td>56.0</td> <td>C</td> <td>l</td> <td>a</td> <td>y</td> <td>m</td> <td>o</td> <td>n</td> <td>t</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>50.3</td> <td>72.6</td> <td>83.6</td> <td>102.6</td> <td>123.5</td> <td>152.9</td> <td>175.2</td> <td>194.7</td> <td>154.2</td> </tr> <tr> <td>13.0</td> <td>E</td> <td>X</td> <td>I</td> <td>T</td> <td>O</td> <td>N</td> <td>L</td> <td>Y</td> <td>12.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>54.5</td> <td>64.8</td> <td>78.5</td> <td>84.5</td> <td>147.5</td> <td>163.0</td> <td>177.6</td> <td>187.0</td> <td>38.6</td> <td>50.0</td> </tr> </tbody> </table> | Y SERIES | E     | X      | I                        | T                 | HT     | LEN   | 10.0   | 28.2    | 36.8  | 48.2  | 53.2  | 10.0  | 10.0  | Clearview-5-W | 28.2 | 36.8          | 48.2  | 53.2  | 32.2  | 32.2  | 7.5   | 5    |      |      |   | 15.0 | 15.0 | Clearview-5-W | 75.4 |   |   |      | 10.3          | 10.3 | 127.0 | P     | h     | i     | l     | a     | d     | e    | l | p | h | i | a    | 20.0          | Clearview-5-W | 21.9 | 42.6 | 63.7 | 75.5  | 86.6  | 107.1 | 128.4         | 149.9 | 162.3 | 183.6 | 204.7 | 215.3 | 208.3 | 93.0  | P     | i     | k     | e     | 20.0  | Clearview-5-W | 94.1  | 114.4    | 126.3    | 144.8         | 65.5 | 65.5 | 56.0 | C                           | l                 | a                 | y     | m     | o     | n     | t     | 20.0  | Clearview-5-W | 50.3 | 72.6 | 83.6 | 102.6 | 123.5 | 152.9 | 175.2         | 194.7 | 154.2 | 13.0 | E | X    | I    | T    | O | N | L | Y | 12.0 | Clearview-5-W | 54.5          | 64.8  | 78.5  | 84.5  | 147.5 | 163.0 | 177.6 | 187.0 | 38.6    | 50.0    | 23.8 | 2' - 6" | 9' - 6" | W  | G  | 2" | 3"                | -- | -- | EXTRUDED ALUMINUM |
| Y SERIES             | E        | X        | I   | T        | HT    | LEN    |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 10.0                 | 28.2     | 36.8     | 48.2  | 53.2     | 10.0  | 10.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 28.2     | 36.8     | 48.2  | 53.2     | 32.2  | 32.2   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 7.5                  | 5        |          |   |          | 15.0  | 15.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 75.4     |          |   |          | 10.3  | 10.3   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 127.0                | P        | h        | i   | l        | a     | d      | e                        | l                 | p      | h     | i      | a       | 20.0  |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 21.9     | 42.6     | 63.7  | 75.5     | 86.6  | 107.1  | 128.4                    | 149.9             | 162.3  | 183.6 | 204.7  | 215.3   | 208.3 |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 93.0                 | P        | i        | k   | e        | 20.0  |        |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 94.1     | 114.4    | 126.3   | 144.8    | 65.5  | 65.5   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 56.0                 | C        | l        | a   | y        | m     | o      | n                        | t                 | 20.0   |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 50.3     | 72.6     | 83.6  | 102.6    | 123.5 | 152.9  | 175.2                    | 194.7             | 154.2  |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 13.0                 | E        | X        | I   | T        | O     | N      | L                        | Y                 | 12.0   |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 54.5     | 64.8     | 78.5  | 84.5     | 147.5 | 163.0  | 177.6                    | 187.0             | 38.6   | 50.0  |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 336.0                | 16' - 0" | 21' - 0" | W   | G        | 2"    | 12"    | --                       | M1-4<br>36" x 36" |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 63.0                 | 3' - 0"  | 21' - 0" | B   | Y        | 2"    | 12"    | DOWN<br>ARROW<br>22"     | --                |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| S01114-2             | 1        |          | <table border="1"> <thead> <tr> <th>Y SERIES</th> <th>E</th> <th>X</th> <th>I</th> <th>T</th> <th>HT</th> <th>LEN</th> </tr> </thead> <tbody> <tr> <td>10.0</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td>10.0</td> <td>10.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td>32.2</td> <td>32.2</td> </tr> <tr> <td>7.5</td> <td>5</td> <td></td> <td></td> <td></td> <td>15.0</td> <td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>75.4</td> <td></td> <td></td> <td></td> <td>10.3</td> <td>10.3</td> </tr> <tr> <td>127.0</td> <td>P</td> <td>h</td> <td>i</td> <td>l</td> <td>a</td> <td>d</td> <td>e</td> <td>l</td> <td>p</td> <td>h</td> <td>i</td> <td>a</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.9</td> <td>42.6</td> <td>63.7</td> <td>75.5</td> <td>86.6</td> <td>107.1</td> <td>128.4</td> <td>149.9</td> <td>162.3</td> <td>183.6</td> <td>204.7</td> <td>215.3</td> <td>208.3</td> </tr> <tr> <td>93.0</td> <td>P</td> <td>i</td> <td>k</td> <td>e</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>94.1</td> <td>114.4</td> <td>126.3</td> <td>144.8</td> <td>65.5</td> <td>65.5</td> </tr> <tr> <td>56.0</td> <td>C</td> <td>l</td> <td>a</td> <td>y</td> <td>m</td> <td>o</td> <td>n</td> <td>t</td> <td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>50.3</td> <td>72.6</td> <td>83.6</td> <td>102.6</td> <td>123.5</td> <td>152.9</td> <td>175.2</td> <td>194.7</td> <td>154.2</td> </tr> <tr> <td>13.0</td> <td>E</td> <td>X</td> <td>I</td> <td>T</td> <td>O</td> <td>N</td> <td>L</td> <td>Y</td> <td>12.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>58.2</td> <td>68.5</td> <td>82.3</td> <td>88.2</td> <td>143.8</td> <td>159.2</td> <td>173.9</td> <td>183.2</td> <td>38.6</td> <td>50.0</td> </tr> </tbody> </table> | Y SERIES | E     | X      | I                        | T                 | HT     | LEN   | 10.0   | 28.2    | 36.8  | 48.2  | 53.2  | 10.0  | 10.0  | Clearview-5-W | 28.2 | 36.8          | 48.2  | 53.2  | 32.2  | 32.2  | 7.5   | 5    |      |      |   | 15.0 | 15.0 | Clearview-5-W | 75.4 |   |   |      | 10.3          | 10.3 | 127.0 | P     | h     | i     | l     | a     | d     | e    | l | p | h | i | a    | 20.0          | Clearview-5-W | 21.9 | 42.6 | 63.7 | 75.5  | 86.6  | 107.1 | 128.4         | 149.9 | 162.3 | 183.6 | 204.7 | 215.3 | 208.3 | 93.0  | P     | i     | k     | e     | 20.0  | Clearview-5-W | 94.1  | 114.4    | 126.3    | 144.8         | 65.5 | 65.5 | 56.0 | C                           | l                 | a                 | y     | m     | o     | n     | t     | 20.0  | Clearview-5-W | 50.3 | 72.6 | 83.6 | 102.6 | 123.5 | 152.9 | 175.2         | 194.7 | 154.2 | 13.0 | E | X    | I    | T    | O | N | L | Y | 12.0 | Clearview-5-W | 58.2          | 68.5  | 82.3  | 88.2  | 143.8 | 159.2 | 173.9 | 183.2 | 38.6    | 50.0    | 23.8 | 2' - 6" | 9' - 6" | W  | G  | 2" | 3"                | -- | -- | EXTRUDED ALUMINUM |
| Y SERIES             | E        | X        | I   | T        | HT    | LEN    |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 10.0                 | 28.2     | 36.8     | 48.2  | 53.2     | 10.0  | 10.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 28.2     | 36.8     | 48.2  | 53.2     | 32.2  | 32.2   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 7.5                  | 5        |          |   |          | 15.0  | 15.0   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 75.4     |          |   |          | 10.3  | 10.3   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 127.0                | P        | h        | i   | l        | a     | d      | e                        | l                 | p      | h     | i      | a       | 20.0  |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 21.9     | 42.6     | 63.7  | 75.5     | 86.6  | 107.1  | 128.4                    | 149.9             | 162.3  | 183.6 | 204.7  | 215.3   | 208.3 |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 93.0                 | P        | i        | k   | e        | 20.0  |        |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 94.1     | 114.4    | 126.3   | 144.8    | 65.5  | 65.5   |                          |                   |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 56.0                 | C        | l        | a   | y        | m     | o      | n                        | t                 | 20.0   |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 50.3     | 72.6     | 83.6  | 102.6    | 123.5 | 152.9  | 175.2                    | 194.7             | 154.2  |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 13.0                 | E        | X        | I   | T        | O     | N      | L                        | Y                 | 12.0   |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| Clearview-5-W        | 58.2     | 68.5     | 82.3  | 88.2     | 143.8 | 159.2  | 173.9                    | 183.2             | 38.6   | 50.0  |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 336.0                | 16' - 0" | 21' - 0" | W   | G        | 2"    | 12"    | --                       | M1-4<br>36" x 36" |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |
| 63.0                 | 3' - 0"  | 21' - 0" | B   | Y        | 2"    | 12"    | TYPE A<br>@ 45°<br>24.8" | --                |        |       |        |         |       |       |       |       |       |               |      |               |       |       |       |       |       |      |      |      |   |      |      |               |      |   |   |      |               |      |       |       |       |       |       |       |       |      |   |   |   |   |      |               |               |      |      |      |       |       |       |               |       |       |       |       |       |       |       |       |       |       |       |       |               |       |          |          |               |      |      |      |                             |                   |                   |       |       |       |       |       |       |               |      |      |      |       |       |       |               |       |       |      |   |      |      |      |   |   |   |   |      |               |               |       |       |       |       |       |       |       |         |         |      |         |         |    |    |    |                   |    |    |                   |

8/28/2015 12:38:23 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc L-495 Task2\_Amtt 1673\Drawings\Plans\S501-Sign Details.dgn



NOTES:

1. COLORS: B = BLACK, G = GREEN, O = ORANGE, W = WHITE, Y = YELLOW
2. LETTER LOCATIONS ARE PANEL EDGE TO LOWER LEFT CORNER.
3. FOR ARROW DETAILS, SEE FHWA PUBLICATION "STANDARD HIGHWAY SIGNS BOOK".
4. SIGNS SHALL BE FABRICATED OF EXTRUDED ALUMINUM CHANNEL PANELS WITH TYPE IX SHEETING PER ITEM 749500.
5. ALL COPY SHALL BE CLEARVIEW 5-W FONT, UNLESS NOTED OTHERWISE.
6. SIGNS SHOWN ON THIS PLAN SHALL BE PAID FOR UNDER ITEM 749500 "SIGN PANEL".
7. THE DESIGN OF SIGNS SHALL FOLLOW THE REQUIREMENTS SET FORTH IN THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE FHWA 2004 EDITION OF "STANDARD HIGHWAY SIGNS" IN CONJUNCTION WITH THE 2012 SUPPLEMENT.

| PANEL DESIGNATION | QUANTITY          | LEGEND   | SIZE        |        |                 | COLOR   |            | BORDER |        | ARROW | SHIELD | REMARKS |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
|-------------------|-------------------|--|-------------|--------|-----------------|---------|------------|--------|--------|-------|--------|---------|----------|--|--------|-------|-----------|--|--|--|--|--|--|--|--|--|--|--|----------|---------------|-------|-------|-------|-------|-------|--|--|--|--|--|--|--|------|------|---------------|--|--|--|--|---------|--|--|--|--|--|--|------|---------------|------|------|------|------|--|-------|-------|-------|-------|-------|-------|--|-------|------|-------------------|--|--|--|--|--|--|--|--|--|--|--|------|---------------|------|------|------|-------|-------|-------|-------|-------|-------|-------|--|--|-------|--------|-------------|---|---|-----|----|------|-----|-------|-------|------|------|-----------|--------|---------------|------|------|------|-------|--------|-------------------|-------------------|-------|----|-----|-----------------------|-------------------|-------------------|------|---------|--|--|--|--|--|--|--|--|--|--|--|------|---------------|----|--|--|--|--|--|--|--|--|--|--|--|------|------|---------|--|--|--|--|--|--|--|--|--|--|--|------|---------------|-------|-------|-------|-------|--|--|--|--|--|--|--|--|------|--------|-----|---|---|-----|----|------|-----|------|-------|------|------|------|-------|-------|---|---|----|----|----|----|-------------------|
|                   |                   |  | AREA (S.F.) | HEIGHT | WIDTH           | LEGEND  | BACKGROUND | WIDTH  | RADIUS |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| S01123-1          | 1                 |  <table border="1"> <thead> <tr> <th colspan="2">Y SERIES</th> <th colspan="10">LETTER SPACINGS</th> <th>HT LEN</th> </tr> </thead> <tbody> <tr> <td>123.0</td> <td>S O U T H</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>15.012.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>126.7</td> <td>141.4</td> <td>156.9</td> <td>169.8</td> <td>182.1</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>64.5</td> </tr> <tr> <td>55.0</td> <td>P o r t</td> <td></td><td></td><td></td><td></td> <td>o f</td> <td></td><td></td><td></td><td></td><td></td><td></td><td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.4</td> <td>41.3</td> <td>63.6</td> <td>76.6</td> <td></td> <td>107.6</td> <td>128.3</td> <td>157.9</td> <td>189.8</td> <td>201.6</td> <td>214.0</td> <td></td> <td>215.2</td> </tr> <tr> <td>18.0</td> <td>B a l t i m o r e</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>48.8</td> <td>69.6</td> <td>90.9</td> <td>101.2</td> <td>116.7</td> <td>128.5</td> <td>158.0</td> <td>180.2</td> <td>194.4</td> <td></td><td></td><td></td><td>160.3</td> </tr> </tbody> </table> <table border="1"> <thead> <tr> <th>SYMBOL</th> <th>ROT</th> <th>X</th> <th>Y</th> <th>WID</th> <th>HT</th> </tr> </thead> <tbody> <tr> <td>M1-1</td> <td>0.0</td> <td>66.8</td> <td>102.0</td> <td>45.0</td> <td>36.0</td> </tr> </tbody> </table>   | Y SERIES    |        | LETTER SPACINGS |         |            |        |        |       |        |         |          |  | HT LEN | 123.0 | S O U T H |  |  |  |  |  |  |  |  |  |  |  | 15.012.0 | Clearview-5-W | 126.7 | 141.4 | 156.9 | 169.8 | 182.1 |  |  |  |  |  |  |  | 64.5 | 55.0 | P o r t       |  |  |  |  | o f     |  |  |  |  |  |  | 20.0 | Clearview-5-W | 21.4 | 41.3 | 63.6 | 76.6 |  | 107.6 | 128.3 | 157.9 | 189.8 | 201.6 | 214.0 |  | 215.2 | 18.0 | B a l t i m o r e |  |  |  |  |  |  |  |  |  |  |  | 20.0 | Clearview-5-W | 48.8 | 69.6 | 90.9 | 101.2 | 116.7 | 128.5 | 158.0 | 180.2 | 194.4 |       |  |  | 160.3 | SYMBOL | ROT         | X | Y | WID | HT | M1-1 | 0.0 | 66.8  | 102.0 | 45.0 | 36.0 | 279.5     | 13'-0" | 21'-6"        | W    | G    | 2"   | 12"   | --     | M1-1<br>45" x 36" | EXTRUDED ALUMINUM |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Y SERIES          |                   | LETTER SPACINGS  |             |        |                 |         |            |        |        |       |        | HT LEN  |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 123.0             | S O U T H         |  |             |        |                 |         |            |        |        |       |        |         | 15.012.0 |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 126.7             | 141.4  | 156.9       | 169.8  | 182.1           |         |            |        |        |       |        |         | 64.5     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 55.0              | P o r t           |  |             |        |                 | o f     |            |        |        |       |        |         | 20.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 21.4              | 41.3   | 63.6        | 76.6   |                 | 107.6   | 128.3      | 157.9  | 189.8  | 201.6 | 214.0  |         | 215.2    |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 18.0              | B a l t i m o r e |  |             |        |                 |         |            |        |        |       |        |         | 20.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 48.8              | 69.6   | 90.9        | 101.2  | 116.7           | 128.5   | 158.0      | 180.2  | 194.4  |       |        |         | 160.3    |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| SYMBOL            | ROT               | X  | Y           | WID    | HT              |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| M1-1              | 0.0               | 66.8   | 102.0       | 45.0   | 36.0            |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| S01123-2          | 1                 |  <table border="1"> <thead> <tr> <th colspan="2">Y SERIES</th> <th colspan="10">LETTER SPACINGS</th> <th>HT LEN</th> </tr> </thead> <tbody> <tr> <td>10.0</td> <td>E X I T</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>10.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>28.2</td> <td>36.8</td> <td>48.2</td> <td>53.2</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>32.2</td> </tr> <tr> <td>7.5</td> <td>P h i l a .</td> <td></td><td></td><td></td><td></td> <td>P i k e</td> <td></td><td></td><td></td><td></td><td></td><td></td><td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>75.4</td> <td></td><td></td><td></td><td></td> <td>134.7</td> <td>155.0</td> <td>166.8</td> <td>185.4</td> <td></td><td></td><td></td><td>10.3</td> </tr> <tr> <td>90.0</td> <td>C l a y m o n t</td> <td></td><td></td><td></td><td></td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>21.9</td> <td>42.6</td> <td>63.7</td> <td>75.5</td> <td>86.6</td> <td>106.9</td> <td>134.7</td> <td>155.0</td> <td>166.8</td> <td>185.4</td> <td></td><td></td><td>178.3</td> </tr> <tr> <td>53.0</td> <td>3/4 M I L E</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>33.9</td> <td>56.2</td> <td>67.3</td> <td>86.2</td> <td>107.0</td> <td>136.5</td> <td>158.8</td> <td>178.2</td> <td></td><td></td><td></td><td></td><td>154.2</td> </tr> <tr> <td>18.0</td> <td>M I L E</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>18.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>34</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>25.6</td> </tr> <tr> <td>21.0</td> <td>M I L E</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>12.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>109.0</td> <td>124.9</td> <td>132.0</td> <td>142.9</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>41.5</td> </tr> </tbody> </table> <table border="1"> <thead> <tr> <th>SYMBOL</th> <th>ROT</th> <th>X</th> <th>Y</th> <th>WID</th> <th>HT</th> </tr> </thead> <tbody> <tr> <td>M1-4</td> <td>0.0</td> <td>93.0</td> <td>138.0</td> <td>36.0</td> <td>36.0</td> </tr> </tbody> </table> | Y SERIES    |        | LETTER SPACINGS |         |            |        |        |       |        |         |          |  | HT LEN | 10.0  | E X I T   |  |  |  |  |  |  |  |  |  |  |  | 10.0     | Clearview-5-W | 28.2  | 36.8  | 48.2  | 53.2  |       |  |  |  |  |  |  |  | 32.2 | 7.5  | P h i l a .   |  |  |  |  | P i k e |  |  |  |  |  |  | 15.0 | Clearview-5-W | 75.4 |      |      |      |  | 134.7 | 155.0 | 166.8 | 185.4 |       |       |  | 10.3  | 90.0 | C l a y m o n t   |  |  |  |  |  |  |  |  |  |  |  | 20.0 | Clearview-5-W | 21.9 | 42.6 | 63.7 | 75.5  | 86.6  | 106.9 | 134.7 | 155.0 | 166.8 | 185.4 |  |  | 178.3 | 53.0   | 3/4 M I L E |   |   |     |    |      |     |       |       |      |      |           | 20.0   | Clearview-5-W | 33.9 | 56.2 | 67.3 | 86.2  | 107.0  | 136.5             | 158.8             | 178.2 |    |     |                       |                   | 154.2             | 18.0 | M I L E |  |  |  |  |  |  |  |  |  |  |  | 18.0 | Clearview-5-W | 34 |  |  |  |  |  |  |  |  |  |  |  | 25.6 | 21.0 | M I L E |  |  |  |  |  |  |  |  |  |  |  | 12.0 | Clearview-5-W | 109.0 | 124.9 | 132.0 | 142.9 |  |  |  |  |  |  |  |  | 41.5 | SYMBOL | ROT | X | Y | WID | HT | M1-4 | 0.0 | 93.0 | 138.0 | 36.0 | 36.0 | 23.8 | 2'-6" | 9'-6" | W | G | 2" | 3" | -- | -- | EXTRUDED ALUMINUM |
| Y SERIES          |                   | LETTER SPACINGS  |             |        |                 |         |            |        |        |       |        | HT LEN  |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 10.0              | E X I T           |  |             |        |                 |         |            |        |        |       |        |         | 10.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 28.2              | 36.8   | 48.2        | 53.2   |                 |         |            |        |        |       |        |         | 32.2     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 7.5               | P h i l a .       |  |             |        |                 | P i k e |            |        |        |       |        |         | 15.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 75.4              |  |             |        |                 | 134.7   | 155.0      | 166.8  | 185.4  |       |        |         | 10.3     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 90.0              | C l a y m o n t   |  |             |        |                 |         |            |        |        |       |        |         | 20.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 21.9              | 42.6   | 63.7        | 75.5   | 86.6            | 106.9   | 134.7      | 155.0  | 166.8  | 185.4 |        |         | 178.3    |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 53.0              | 3/4 M I L E       |  |             |        |                 |         |            |        |        |       |        |         | 20.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 33.9              | 56.2   | 67.3        | 86.2   | 107.0           | 136.5   | 158.8      | 178.2  |        |       |        |         | 154.2    |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 18.0              | M I L E           |  |             |        |                 |         |            |        |        |       |        |         | 18.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 34                |  |             |        |                 |         |            |        |        |       |        |         | 25.6     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 21.0              | M I L E           |  |             |        |                 |         |            |        |        |       |        |         | 12.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 109.0             | 124.9  | 132.0       | 142.9  |                 |         |            |        |        |       |        |         | 41.5     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| SYMBOL            | ROT               | X  | Y           | WID    | HT              |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| M1-4              | 0.0               | 93.0   | 138.0       | 36.0   | 36.0            |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| S01123-3          | 1                 |  <table border="1"> <thead> <tr> <th colspan="2">Y SERIES</th> <th colspan="10">LETTER SPACINGS</th> <th>HT LEN</th> </tr> </thead> <tbody> <tr> <td>10.0</td> <td>E X I T</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>10.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>28.0</td> <td>36.6</td> <td>48.0</td> <td>52.9</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>32.2</td> </tr> <tr> <td>7.5</td> <td>N a a m a n s</td> <td></td><td></td><td></td><td></td> <td>R d</td> <td></td><td></td><td></td><td></td><td></td><td></td><td>15.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>75.2</td> <td></td><td></td><td></td><td></td> <td>189.9</td> <td>210.9</td> <td></td><td></td><td></td><td></td><td></td><td>10.9</td> </tr> <tr> <td>18.0</td> <td>25'-0"</td> <td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td>20.0</td> </tr> <tr> <td>Clearview-5-W</td> <td>20.0</td> <td>43.1</td> <td>63.1</td> <td>84.3</td> <td>113.4</td> <td>134.6</td> <td>154.5</td> <td>189.9</td> <td>210.9</td> <td></td><td></td><td></td><td>205.4</td> </tr> </tbody> </table> <table border="1"> <thead> <tr> <th>SYMBOL</th> <th>ROT</th> <th>X</th> <th>Y</th> <th>WID</th> <th>HT</th> </tr> </thead> <tbody> <tr> <td>M1-5</td> <td>0.0</td> <td>132.0</td> <td>66.0</td> <td>36.0</td> <td>36.0</td> </tr> <tr> <td>AR-Type A</td> <td>315.0</td> <td>245.5</td> <td>18.0</td> <td>27.8</td> <td>43.8</td> </tr> </tbody> </table>  | Y SERIES    |        | LETTER SPACINGS |         |            |        |        |       |        |         |          |  | HT LEN | 10.0  | E X I T   |  |  |  |  |  |  |  |  |  |  |  | 10.0     | Clearview-5-W | 28.0  | 36.6  | 48.0  | 52.9  |       |  |  |  |  |  |  |  | 32.2 | 7.5  | N a a m a n s |  |  |  |  | R d     |  |  |  |  |  |  | 15.0 | Clearview-5-W | 75.2 |      |      |      |  | 189.9 | 210.9 |       |       |       |       |  | 10.9  | 18.0 | 25'-0"            |  |  |  |  |  |  |  |  |  |  |  | 20.0 | Clearview-5-W | 20.0 | 43.1 | 63.1 | 84.3  | 113.4 | 134.6 | 154.5 | 189.9 | 210.9 |       |  |  | 205.4 | SYMBOL | ROT         | X | Y | WID | HT | M1-5 | 0.0 | 132.0 | 66.0  | 36.0 | 36.0 | AR-Type A | 315.0  | 245.5         | 18.0 | 27.8 | 43.8 | 250.0 | 10'-0" | 25'-0"            | W                 | G     | 2" | 12" | TYPE A @ 45°<br>34.5" | M1-5<br>36" x 36" | EXTRUDED ALUMINUM |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Y SERIES          |                   | LETTER SPACINGS  |             |        |                 |         |            |        |        |       |        | HT LEN  |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 10.0              | E X I T           |  |             |        |                 |         |            |        |        |       |        |         | 10.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 28.0              | 36.6   | 48.0        | 52.9   |                 |         |            |        |        |       |        |         | 32.2     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 7.5               | N a a m a n s     |  |             |        |                 | R d     |            |        |        |       |        |         | 15.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 75.2              |  |             |        |                 | 189.9   | 210.9      |        |        |       |        |         | 10.9     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| 18.0              | 25'-0"            |  |             |        |                 |         |            |        |        |       |        |         | 20.0     |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| Clearview-5-W     | 20.0              | 43.1   | 63.1        | 84.3   | 113.4           | 134.6   | 154.5      | 189.9  | 210.9  |       |        |         | 205.4    |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| SYMBOL            | ROT               | X  | Y           | WID    | HT              |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| M1-5              | 0.0               | 132.0  | 66.0        | 36.0   | 36.0            |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |
| AR-Type A         | 315.0             | 245.5  | 18.0        | 27.8   | 43.8            |         |            |        |        |       |        |         |          |  |        |       |           |  |  |  |  |  |  |  |  |  |  |  |          |               |       |       |       |       |       |  |  |  |  |  |  |  |      |      |               |  |  |  |  |         |  |  |  |  |  |  |      |               |      |      |      |      |  |       |       |       |       |       |       |  |       |      |                   |  |  |  |  |  |  |  |  |  |  |  |      |               |      |      |      |       |       |       |       |       |       |       |  |  |       |        |             |   |   |     |    |      |     |       |       |      |      |           |        |               |      |      |      |       |        |                   |                   |       |    |     |                       |                   |                   |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |    |  |  |  |  |  |  |  |  |  |  |  |      |      |         |  |  |  |  |  |  |  |  |  |  |  |      |               |       |       |       |       |  |  |  |  |  |  |  |  |      |        |     |   |   |     |    |      |     |      |       |      |      |      |       |       |   |   |    |    |    |    |                   |

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## SIGN STRUCTURE NOTES

### DESIGN SPECIFICATIONS

1. AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS", 2013, 6TH EDITION.
2. AASHTO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES", 2002, 17TH EDITION.
3. DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AUGUST 2001, INCLUDING ALL SUPPLEMENTALS AND THE CONTRACT SPECIAL PROVISIONS.
4. AWS D1.1 STRUCTURAL WELDING CODE.

### DESIGN LOADS

1. THE DESIGN WIND SPEED IS 90 MPH (3-SECOND GUST WIND SPEED), BASED ON A 50-YEAR MEAN RECURRENCE INTERVAL.
2. THE DESIGN ICE LOAD IS 3 PSF.
3. THE DESIGN SIGN PANEL WEIGHT IS 3 PSF.
4. THE DESIGN SIGN PANEL AREA INCLUDES AN ADDITIONAL 15% INCREASE IN AREA OVER THE ACTUAL SIGN PANEL AREA EXCEPT FOR SC1221.
5. FATIGUE DESIGN FOR CANTILEVER SIGN STRUCTURES IS BASED ON FATIGUE CATEGORY I FOR GALLOPING, NATURAL WINDS GUSTS, AND TRUCK-INDUCED GUSTS.

### MATERIALS

1. ALL STRUCTURAL PIPE FOR MASTS AND MAST ARMS SHALL CONFORM TO ASTM A53, GRADE B.
2. ALL STEEL PLATES, SHAPES, AND BACKING RINGS SHALL CONFORM TO AASHTO M270, GRADE 36 OR BETTER.
3. ALL STEEL MEMBERS GREATER THAN OR EQUAL TO 1/2" THICK, SHALL MEET THE CHARPY V-NOTCH REQUIREMENTS FOR ZONE 2, NON-FRACTURE CRITICAL.
4. THE STRUCTURE SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M111.
5. ALL CONNECTION BOLTS (INCLUDING SPLICE BOLTS AND ATTACHMENT BOLTS) SHALL CONFORM TO ASTM A325. WASHERS SHALL CONFORM TO ASTM F436, AND NUTS SHALL CONFORM TO ASTM A563, GRADE DH, OR ASTM A194, GRADE 2H.
6. U-BOLTS SHALL CONFORM TO ASTM A449.
7. ANCHOR BOLTS SHALL CONFORM TO AASHTO M314, GRADE 55. ANCHOR NUTS SHALL CONFORM TO ASTM A563, GRADE DH, OR ASTM A194, GRADE 2H. WASHERS SHALL CONFORM TO ASTM F436.
8. ALL HARDWARE SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M232.
9. PORTLAND CEMENT CONCRETE FOR DRILLED SHAFTS AND SIDEWALKS SHALL BE DELDOT CLASS B, (f'c = 3,000 PSI). PORTLAND CEMENT CONCRETE FOR PEDESTALS SHALL BE DELDOT CLASS A, (f'c = 4,500 PSI).
10. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.
11. REINFORCING STEEL SHALL CONFORM TO AASHTO M31, GRADE 60 AND BE UNCOATED UNLESS OTHERWISE NOTED.
12. ALL REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" UNLESS OTHERWISE NOTED.

### GENERAL

1. FORM MASTS FOR SIGN STRUCTURES TO THE RADII SHOWN ON THE PLANS IN ACCORDANCE WITH THE TUBE AND PIPE ASSOCIATION INTERNATIONAL RECOMMENDED STANDARDS FOR INDUCTION BENDING OF PIPE AND TUBE (TPA-IBS-98).
2. STEEL TEMPLATES SHALL BE USED TO SET ANCHOR BOLTS PLUMB WHEN POURING THE FOUNDATION.
3. BASE PLATES SHALL BE IN FULL CONTACT WITH ALL FLAT WASHERS.
4. ALL ANCHOR BOLTS SHALL BE TIGHTENED USING TURN OF NUT METHOD (1/6 TURN AFTER SNUG TIGHT).
5. THREADS OF ANCHOR BOLTS SHALL BE BURRED OFF AT FACE OF NUT AFTER COLUMN IS INSTALLED.
6. LOCK WASHERS WITH FLAT WASHERS SHALL ONLY BE USED FOR "U" BOLT CONNECTIONS, AND NUT SHALL BE TURNED UNTIL THE LOCK WASHER IS FLAT.
7. MAST AND MAST ARM O.D. DIMENSIONS ARE ACTUAL.
8. FIELD VERIFY ALL DIMENSIONS BEFORE ORDERING ANY MATERIALS.
9. FABRICATE ALL SIGN STRUCTURES INTO THE LARGEST PRACTICAL SECTIONS PRIOR TO GALVANIZING. SUBMIT SPLICE LOCATIONS TO THE ENGINEER FOR APPROVAL. DO NOT COMMENCE FABRICATION UNTIL SUCH SPLICE LOCATIONS ARE APPROVED.
10. DO NOT USE GROUT BETWEEN BASE PLATE AND CONCRETE PEDESTAL.
11. SLOPE TOP OF PEDESTAL 4% FROM CENTER TO NEAR EDGES FOR DRAINAGE.
12. PROVIDE DOUBLE NUTS AND WASHERS FOR EACH ANCHOR BOLT.
13. PERMANENT CAMBER EQUAL TO L/1000 HAS BEEN PROVIDED IN ADDITION TO THE DEAD LOAD CAMBER, WHERE L IS EQUAL TO THE SIGN STRUCTURE SPAN LENGTH.

## SIGN STRUCTURE LOCATIONS

| SIGN STRUCTURE NO. | STRUCTURE TYPE | BASELINE        | STATION "A" | OFFSET "A" | STATION "B" | OFFSET "B" |
|--------------------|----------------|-----------------|-------------|------------|-------------|------------|
| SC1103             | CANTILEVER     | SC1103 & S01107 | 12+14.29    | 70.45' RT. | -           | -          |
| SC1104             | CANTILEVER     | S01104          | 11+23.69    | 65.10' LT. | -           | -          |
| SC1206             | CANTILEVER     | SC1206          | 1+21.59     | 21.74 RT.  | -           | -          |
| SC1221             | CANTILEVER     | SC1221          | 1+47.36     | 18.87 RT.  | -           | -          |
| SC1223             | CANTILEVER     | SC1223          | 0+79.55     | 18.73 LT.  | -           | -          |
| S01107             | OVERHEAD       | SC1103 & S01107 | 12+15.43    | 84.20' LT. | 12+14.29    | 0.24' RT.  |
| S01109             | OVERHEAD       | S01109          | 360+17.37   | 57.50' LT. | 360+17.37   | 55.50' RT. |
| S01112             | OVERHEAD       | S01112          | 11+24.99    | 9.68' RT.  | 11+24.91    | 82.93' RT. |
| S01114             | OVERHEAD       | S01114          | 10+26.45    | 13.27' RT. | 10+22.66    | 90.69' RT. |
| S01116             | OVERHEAD       | S01116          | 11+61.84    | 82.55' LT. | 11+62.08    | 1.47' LT.  |
| S01123             | OVERHEAD       | S01123          | 10+90.83    | 2.18' RT.  | 10+96.57    | 86.76' RT. |

ADDENDUMS / REVISIONS

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

CONTRACT

T201407004

COUNTY

NEW CASTLE

BRIDGE NO.

NA

DESIGNED BY: PM

CHECKED BY: DEF

SIGN STRUCTURE NOTES

SHEET NO.

21

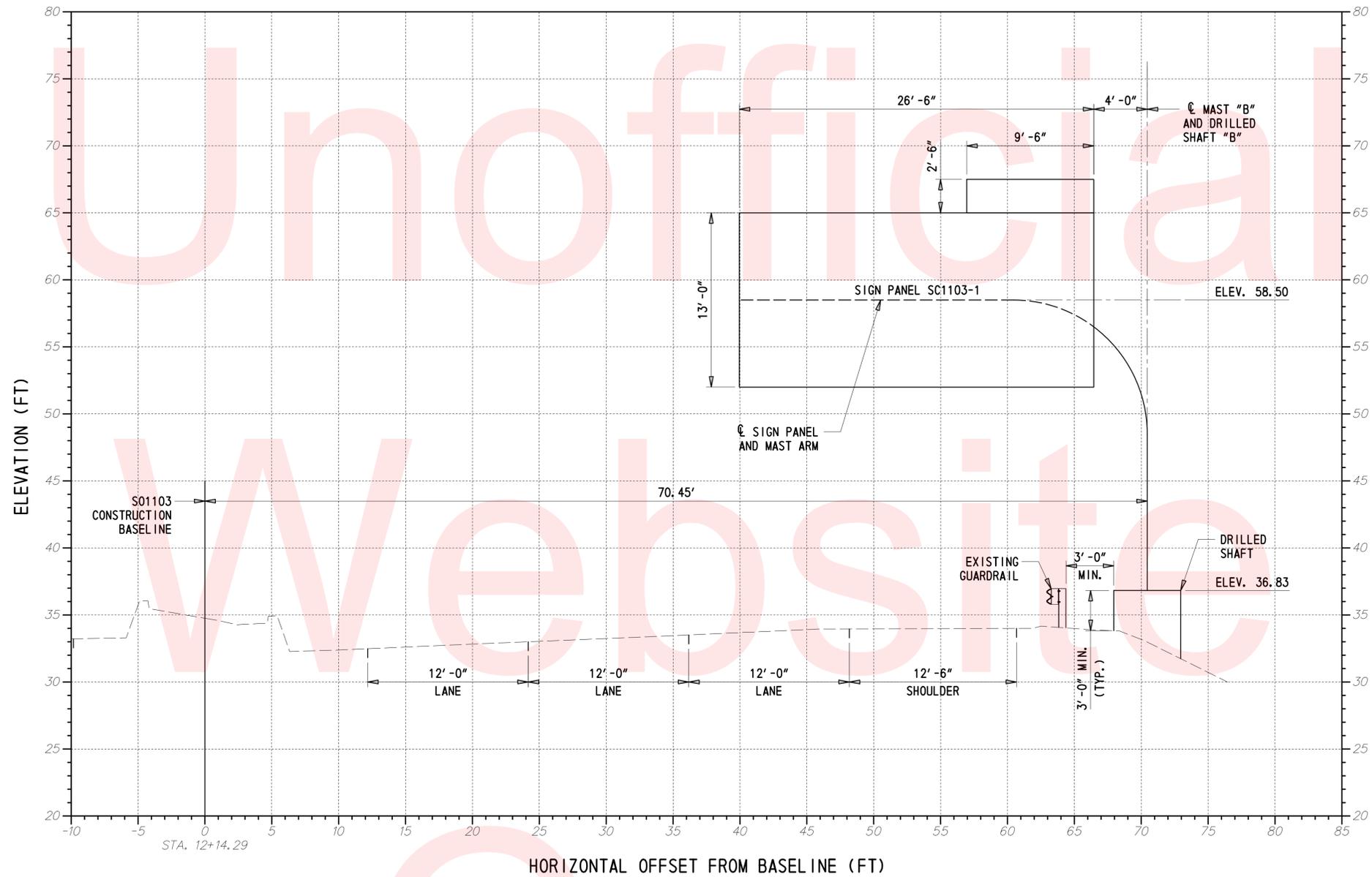
TOTAL SHTS.

83



DELAWARE  
DEPARTMENT OF TRANSPORTATION

8/28/2015 12:39:13 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc L-495\_Task2\_Amnt 1673\Drawings\Plans\X504-SC1103-SC1107.dgn

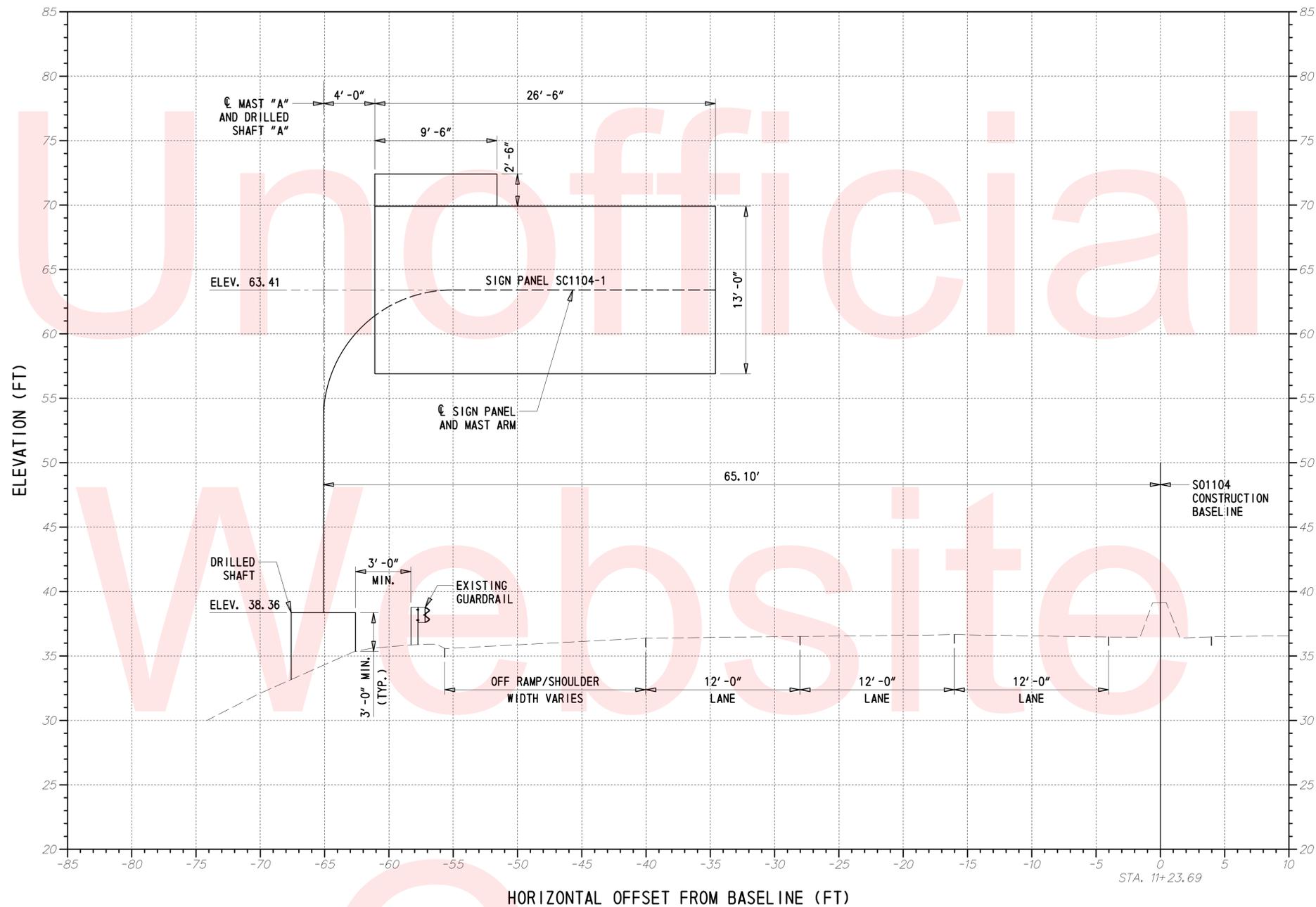


**PROPOSED SIGN STRUCTURE SC1103**

STA. 12+14.29  
S01107 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

**NOTE:**

1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.



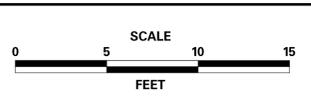
**PROPOSED SIGN STRUCTURE SC1104**

STA. 11+23.69  
S01108 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

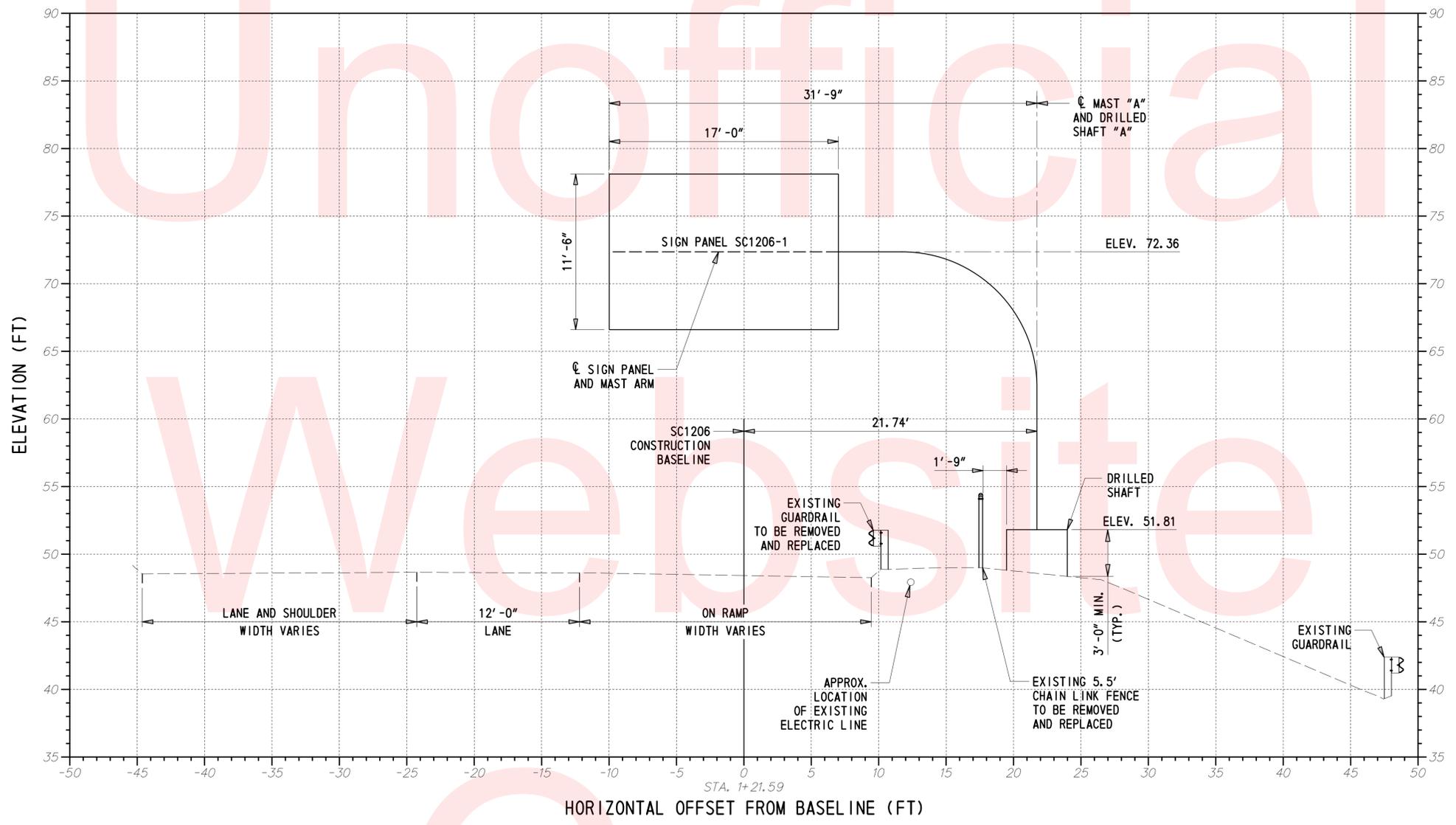
**NOTE:**  
1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:39:33 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Struct - I-495\_Task2\_Amnt.1673\Drawings\Plans\X505-SC1104.dgn

| ADDENDUMS / REVISIONS |
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|                        |                                     |
|------------------------|-------------------------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A                   |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS<br>CHECKED BY: DEF |

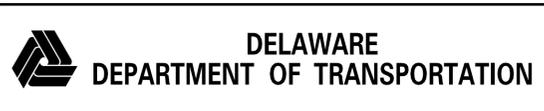


**PROPOSED SIGN STRUCTURE SC1206**

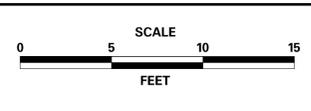
STA. 1+21.59  
 SC1206 CONSTRUCTION BASELINE  
 (LOOKING STATIONS AHEAD)

**NOTE:**  
 1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:39:53 AM J:\425009\03 - Critical Cantilever\_Sgn\_Task\_3\_Ann1\_1673\Drawings\PLANS\X01-SC1206.dgn



| ADDENDUMS / REVISIONS |  |
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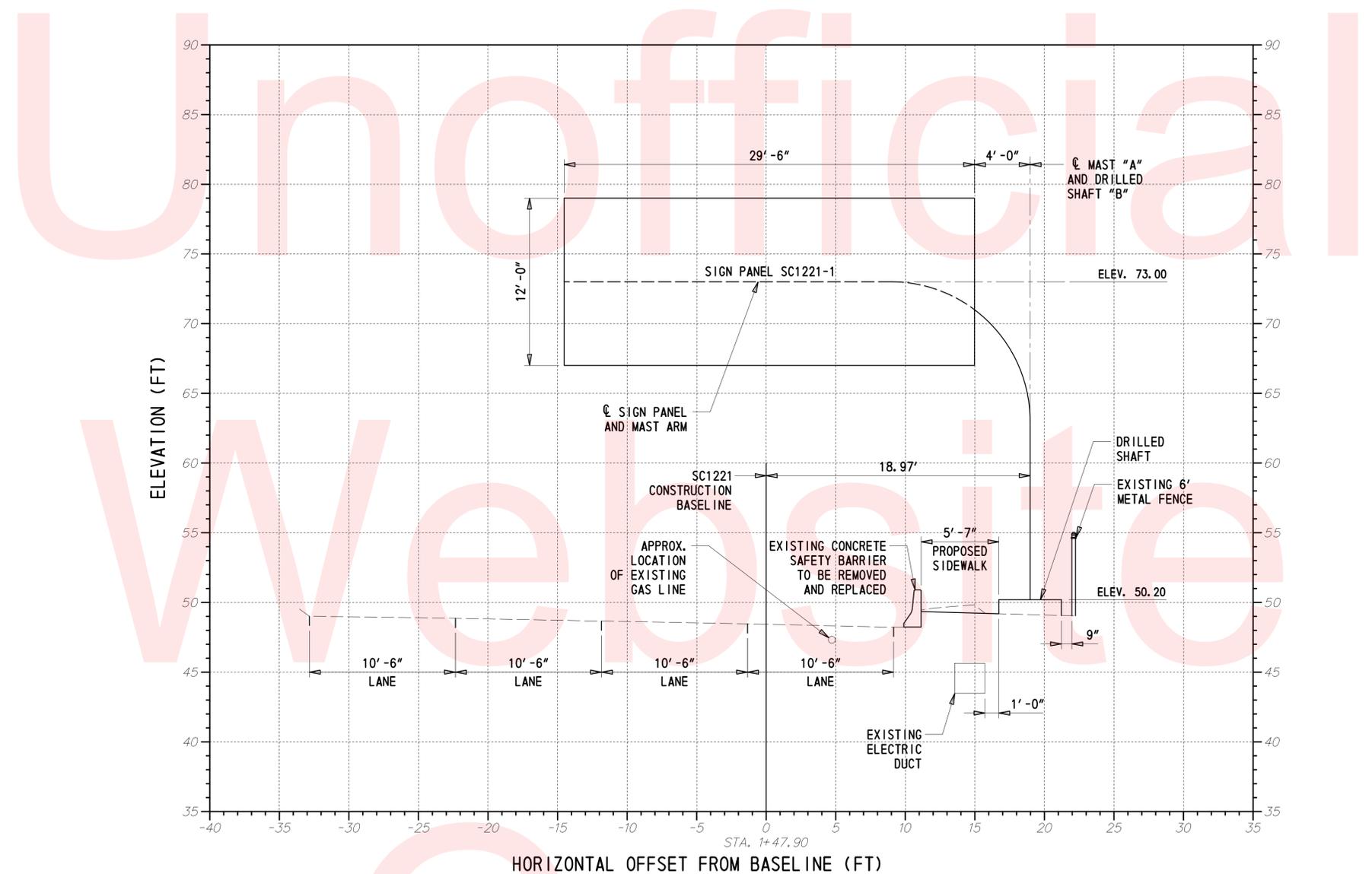


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                   |
|------------------------|-------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS  |
|                        | CHECKED BY: DEF   |

**CROSS SECTION SIGN STRUCTURE SC1206**

|                   |
|-------------------|
| SHEET NO.<br>24   |
| TOTAL SHTS.<br>83 |

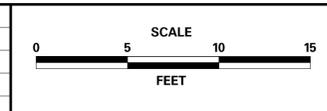


**PROPOSED SIGN STRUCTURE SC1221**  
 STA. 1+47.90  
 SC1221 CONSTRUCTION BASELINE  
 (LOOKING STATIONS AHEAD)

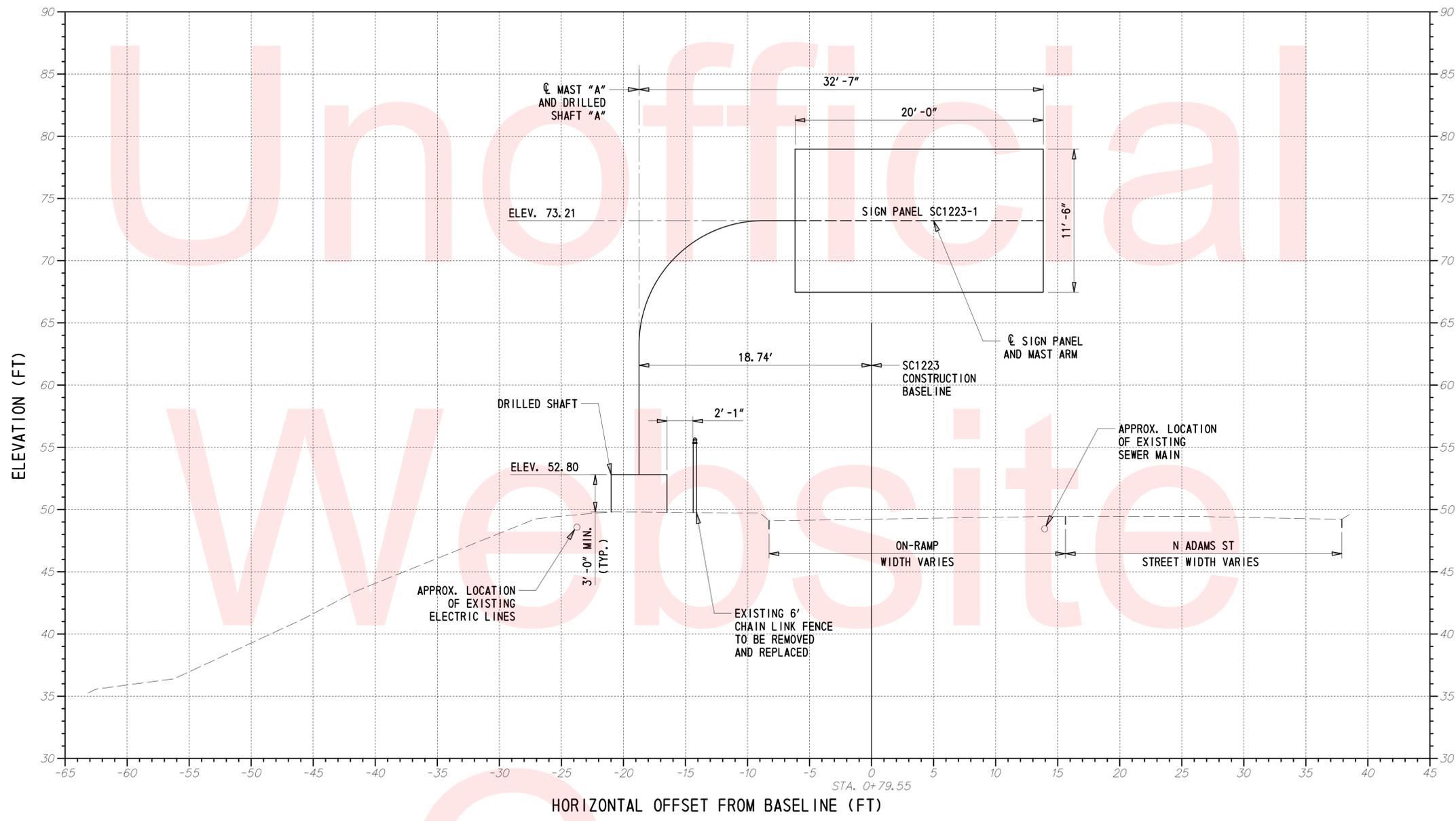
**NOTE:**  
 1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:40:16 AM  
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| ADDENDUMS / REVISIONS |  |
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|            |              |            |
|------------|--------------|------------|
| CONTRACT   | BRIDGE NO.   | <b>N/A</b> |
| T201407004 | DESIGNED BY: | JWS        |
| COUNTY     | CHECKED BY:  | DEF        |
| NEW CASTLE |              |            |



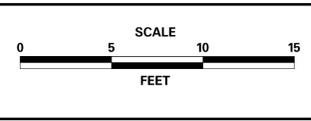
**PROPOSED SIGN STRUCTURE SC1223**

STA. 0+79.55  
SC1223 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

**NOTE:**  
1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

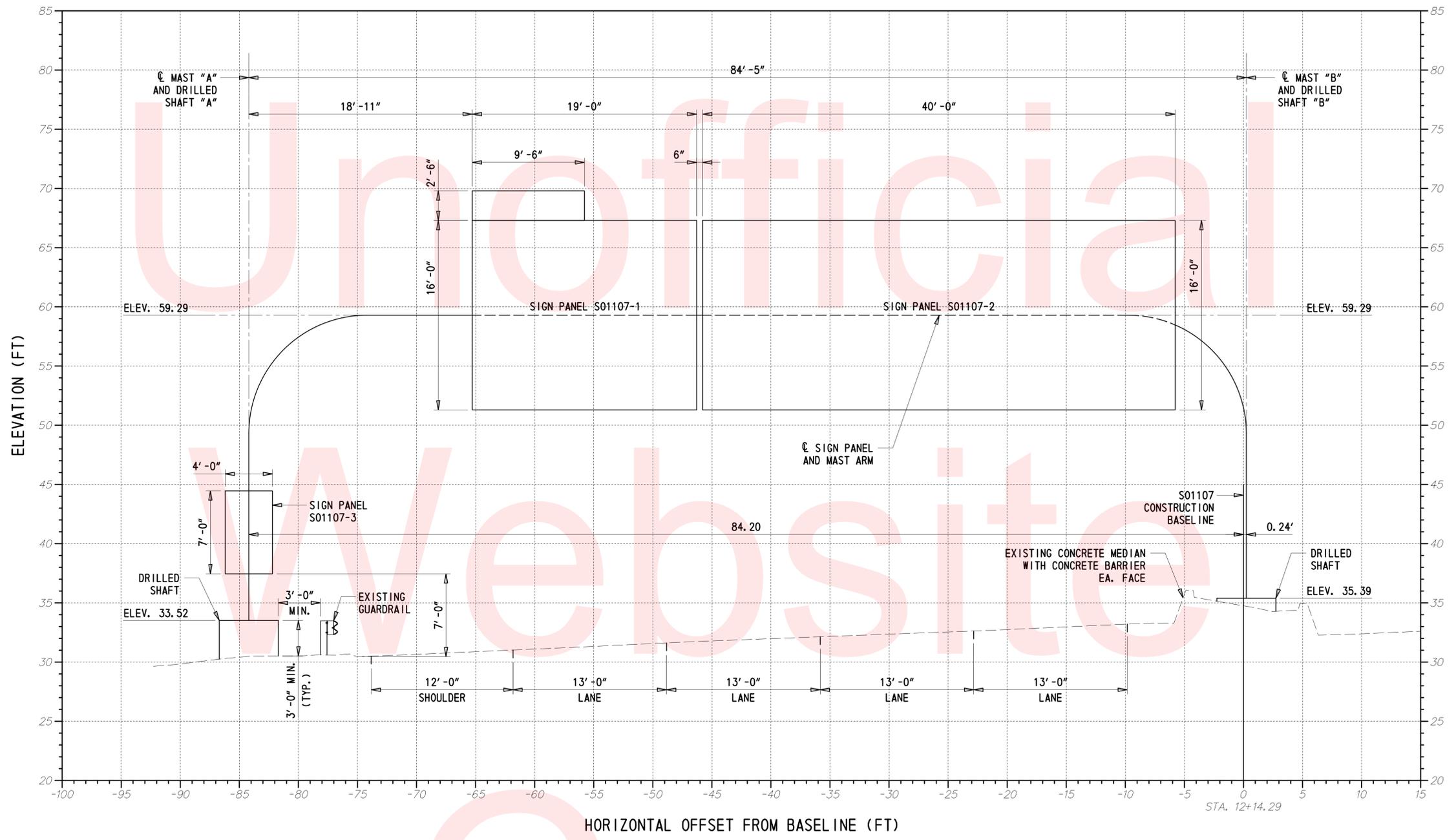
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| ADDENDUMS / REVISIONS |  |
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|                        |                          |
|------------------------|--------------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br><b>N/A</b> |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS         |
|                        | CHECKED BY: DEF          |

|  |                   |
|--|-------------------|
| <b>CROSS SECTION SIGN STRUCTURE SC1223</b> | SHEET NO.<br>26   |
|  | TOTAL SHTS.<br>83 |

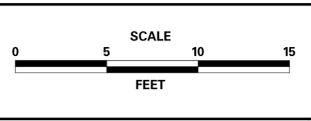


**PROPOSED SIGN STRUCTURE S01107**  
 STA. 12+14.29  
 S01107 CONSTRUCTION BASELINE  
 (LOOKING STATIONS AHEAD)

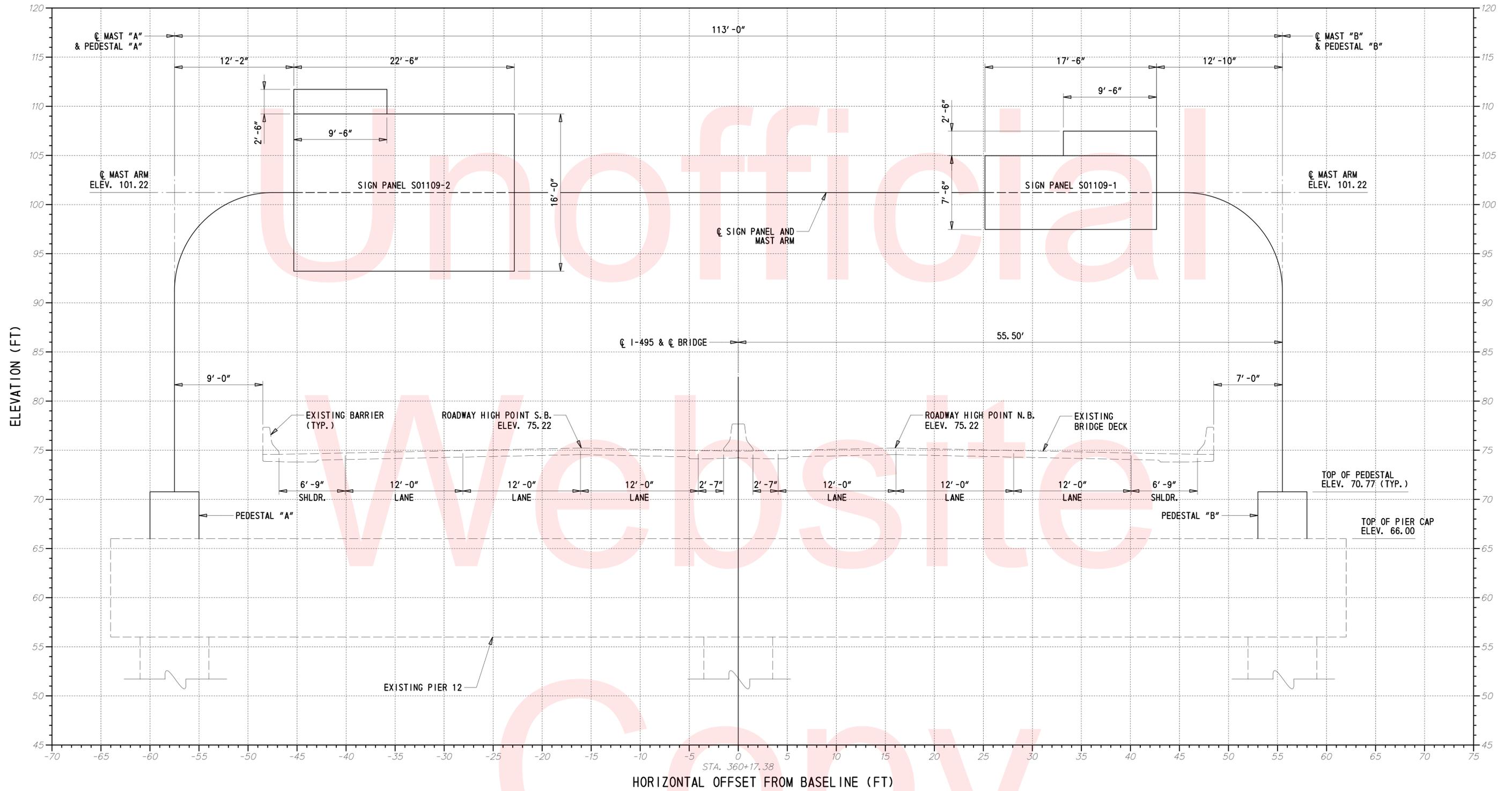
**NOTE:**  
 1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:40:56 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Str. I-495 Task2 Amnt. 1673\Drawings\Plans\X504-S01103-S01107.dgn

| ADDENDUMS / REVISIONS |  |
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|                        |                                     |
|------------------------|-------------------------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A                   |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS<br>CHECKED BY: DEF |



**PROPOSED SIGN STRUCTURE S01109**

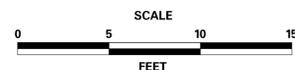
STA. 360+17.38  
S01109 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

- NOTE:**
- CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.
  - BRIDGE SUPERSTRUCTURE NOT SHOWN FOR CLARITY.

8/28/2015 12:41:14 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc I-495 Task2 Amnt. 1673\Drawings\Plans\X506-S01109.dgn



ADDENDUMS / REVISIONS

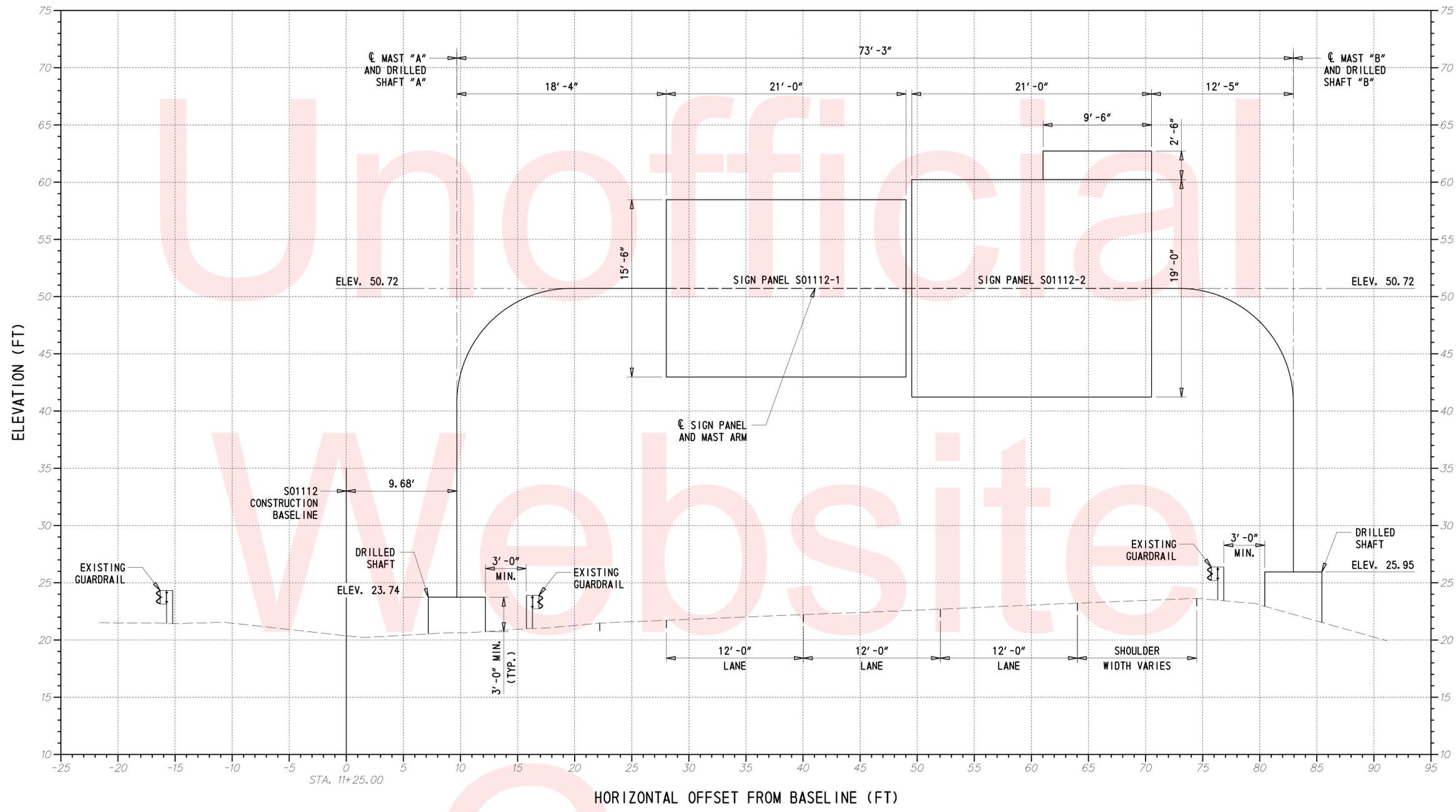


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |              |           |
|------------|--------------|-----------|
| CONTRACT   | BRIDGE NO.   | <b>NA</b> |
| T201407004 | DESIGNED BY: | JLS       |
| COUNTY     | CHECKED BY:  | DEF       |
| NEW CASTLE |              |           |

**CROSS SECTION SIGN STRUCTURE S01109**

|             |    |
|-------------|----|
| SHEET NO.   | 28 |
| TOTAL SHTS. | 83 |

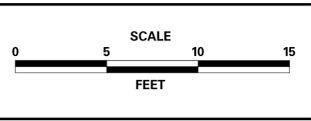


**PROPOSED SIGN STRUCTURE S01112**  
 STA. 11+25.00  
 S01112 CONSTRUCTION BASELINE  
 (LOOKING STATIONS AHEAD)

**NOTE:**  
 1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:44:32 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc L-495 Task2 Amnt 1673\Drawings\Plans\X507-S01112.dgn

| ADDENDUMS / REVISIONS |  |
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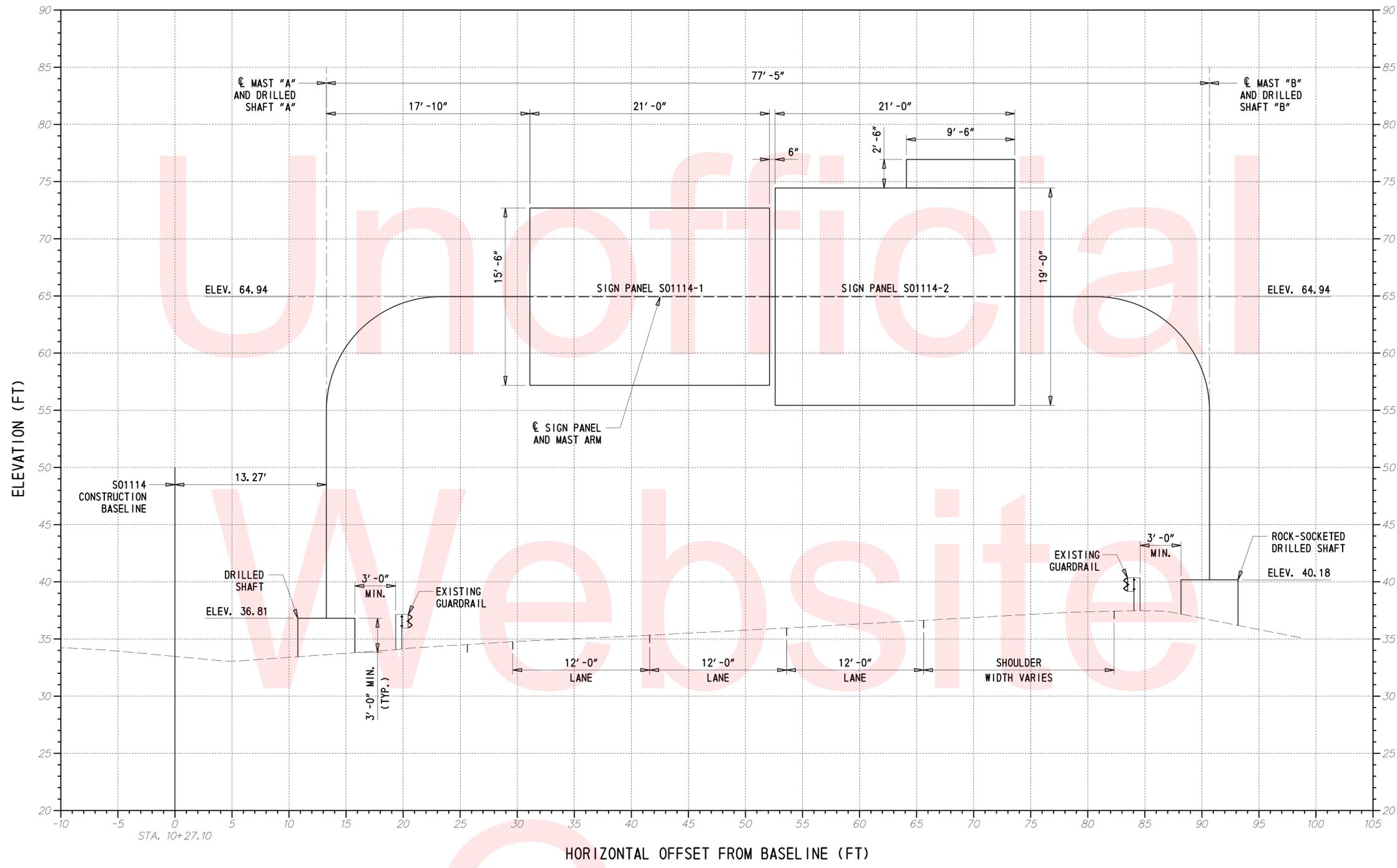


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                                     |
|------------------------|-------------------------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A                   |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS<br>CHECKED BY: DEF |

**CROSS SECTION SIGN STRUCTURE S01112**

|                   |
|-------------------|
| SHEET NO.<br>29   |
| TOTAL SHTS.<br>83 |

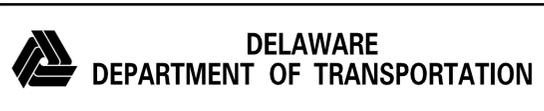


**PROPOSED SIGN STRUCTURE S01114**

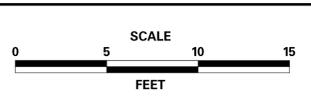
STA. 10+27.10  
S01114 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

**NOTE:**  
1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:41:49 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc L-495\_Task2\_Amnt 1673\Drawings\Plans\X509-S01114.dgn



| ADDENDUMS / REVISIONS |  |
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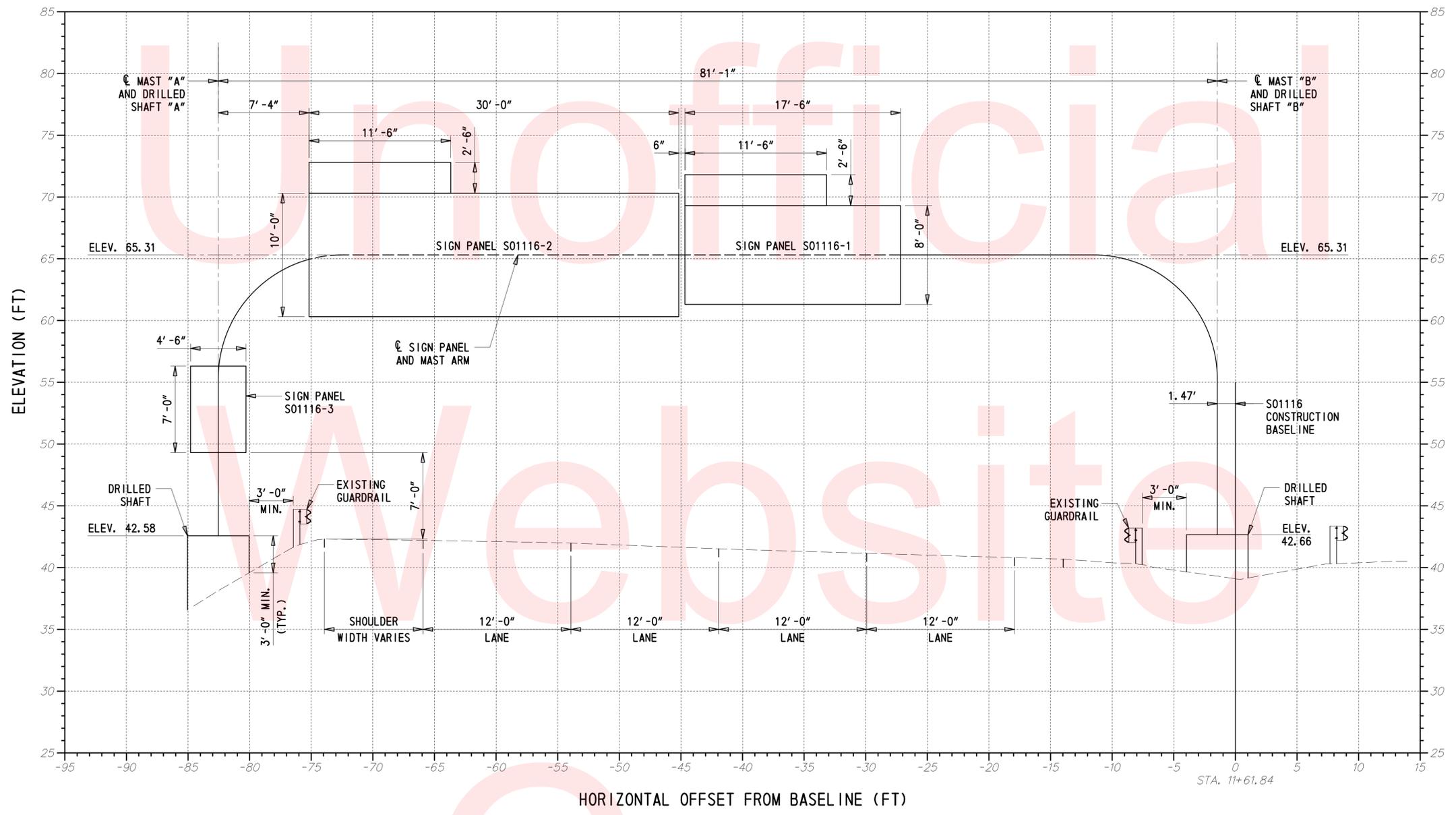


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                   |
|------------------------|-------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JLS  |
|                        | CHECKED BY: DEF   |

**CROSS SECTION SIGN STRUCTURE S01114**

|                   |
|-------------------|
| SHEET NO.<br>30   |
| TOTAL SHTS.<br>83 |



HORIZONTAL OFFSET FROM BASELINE (FT)

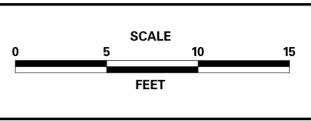
**PROPOSED SIGN STRUCTURE S01116**

STA. 11+61.84  
S01116 CONSTRUCTION BASELINE  
(LOOKING STATIONS AHEAD)

**NOTE:**  
1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:42:13 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc L-495 Task2\_Amnt 1673\Drawings\Plans\XS10-S01116.dgn

| ADDENDUMS / REVISIONS |  |
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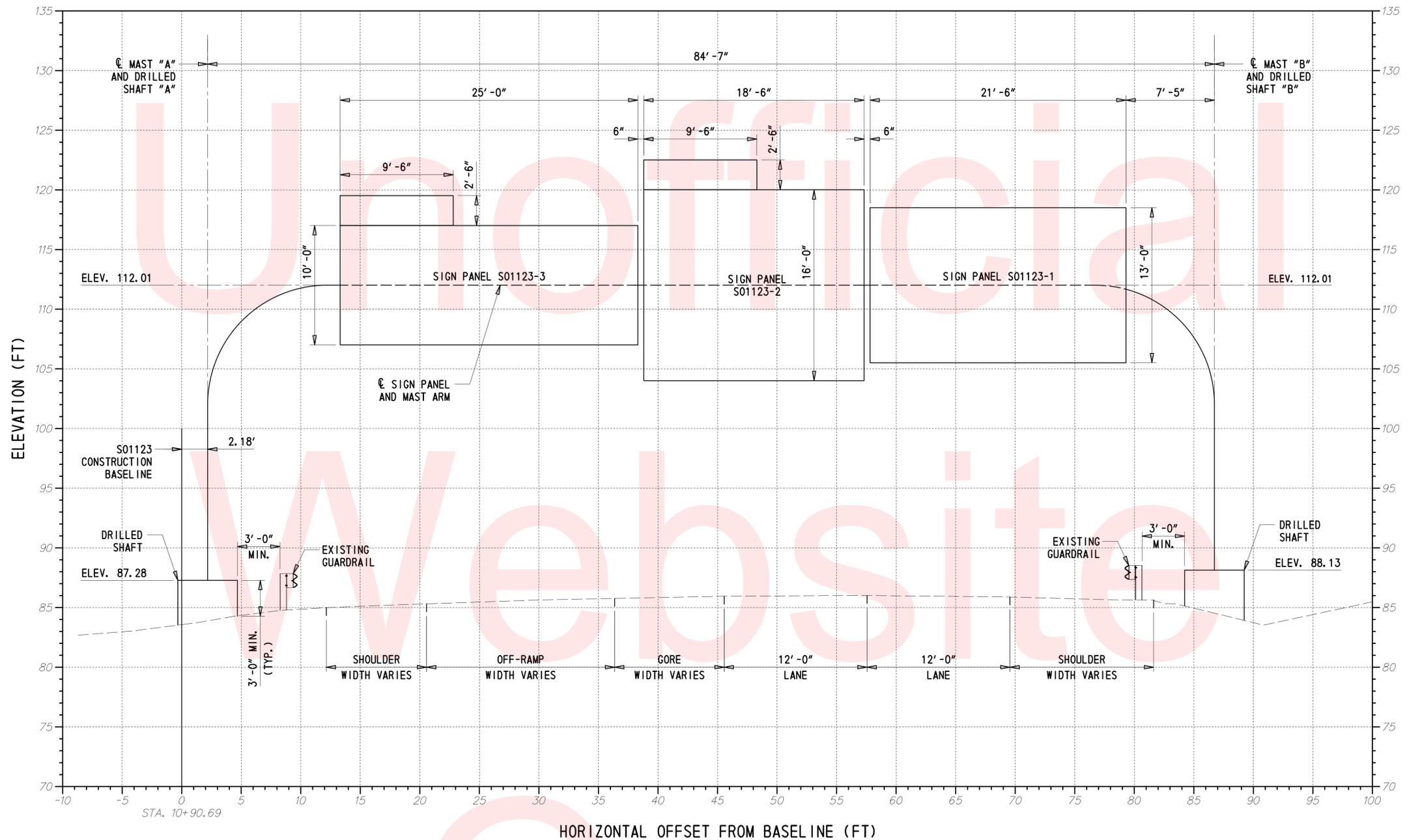


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: JWS |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

**CROSS SECTION**  
**SIGN STRUCTURE S01116**

|             |    |
|-------------|----|
| SHEET NO.   | 31 |
| TOTAL SHTS. | 83 |



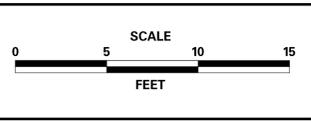
**PROPOSED SIGN STRUCTURE S01123**

STA. 10+90.69  
 S01123 CONSTRUCTION BASELINE  
 (LOOKING STATIONS AHEAD)

**NOTE:**  
 1. CROSS SECTIONS ARE TAKEN NORMAL TO ROADWAY.

8/28/2015 12:42:31 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Str. I-495 Task2\_Amnl\_1673\Drawings\Plans\XStn\S01123.dgn

| ADDENDUMS / REVISIONS |  |
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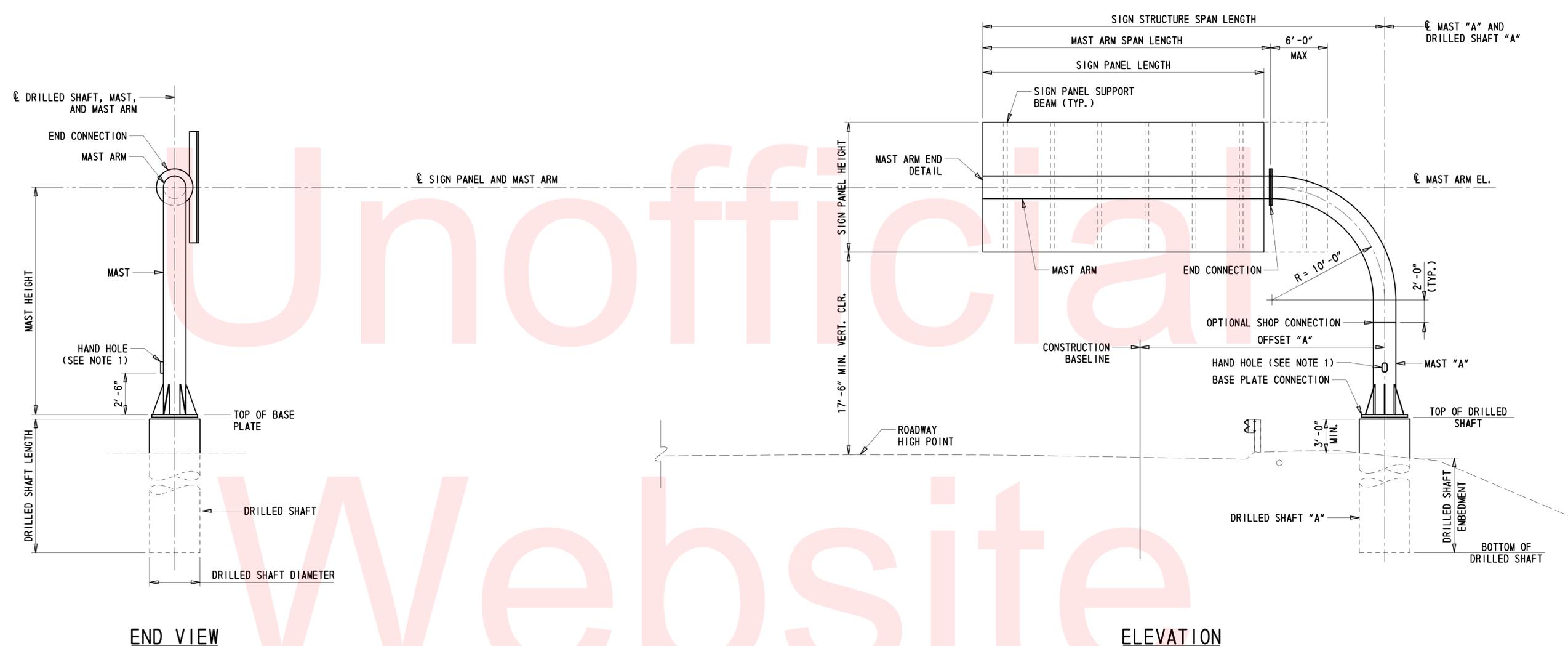


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                                     |
|------------------------|-------------------------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>N/A                   |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: JWS<br>CHECKED BY: DEF |

**CROSS SECTION SIGN STRUCTURE S01123**

|                   |
|-------------------|
| SHEET NO.<br>32   |
| TOTAL SHTS.<br>83 |



END VIEW

ELEVATION

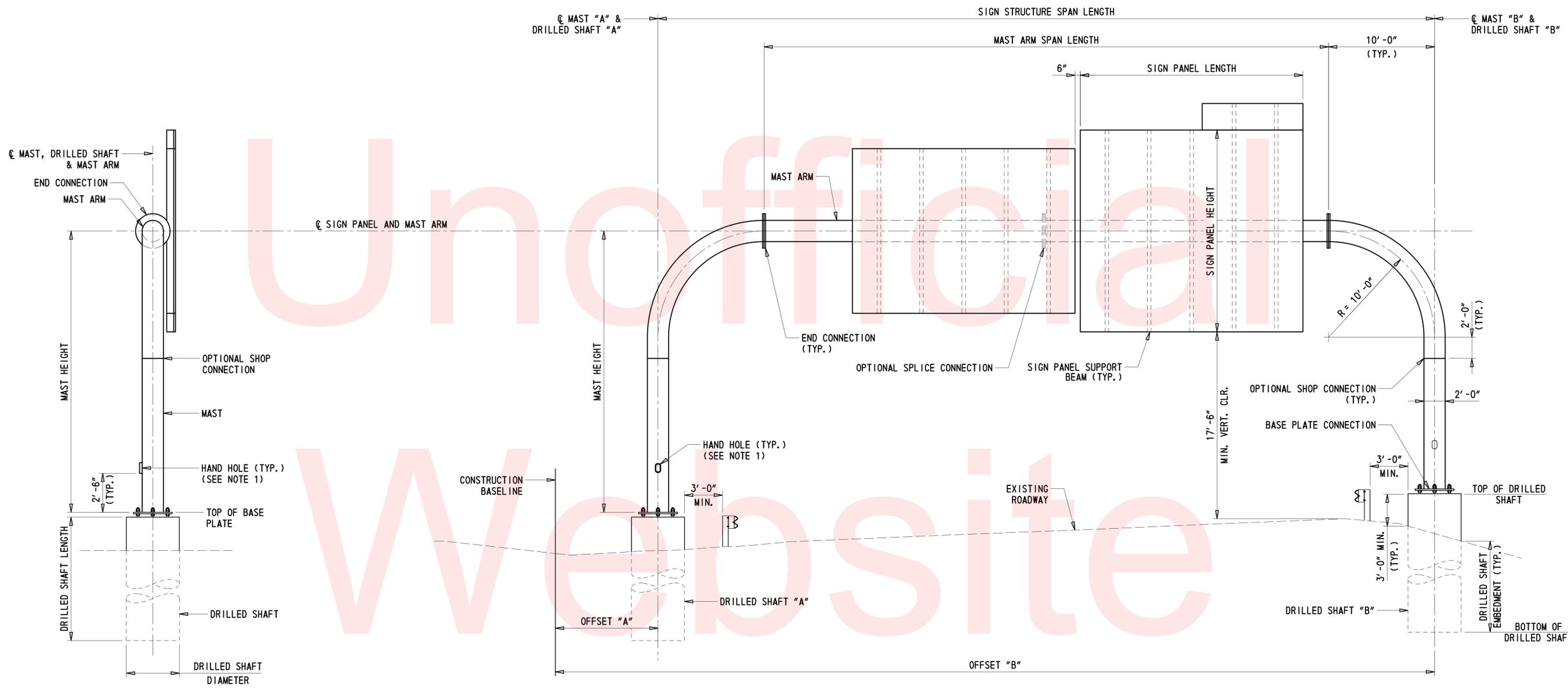
CAMBER DIAGRAM

MAST ARM CAMBER MUST INCLUDE BOTH MAST ARM AND MAST DEFLECTION. BACK RAKING OF MAST IS NOT PERMITTED.

| SIGN STRUCTURE GEOMETRY |                             |                             |                            |                      |                 |                 |                 |
|-------------------------|-----------------------------|-----------------------------|----------------------------|----------------------|-----------------|-----------------|-----------------|
| SIGN STRUCTURE NO.      | ACTUAL SIGN PANEL AREA (SF) | DESIGN SIGN PANEL AREA (SF) | SIGN STRUCTURE SPAN LENGTH | MAST ARM SPAN LENGTH | CL MAST ARM EL. | MAST "A" HEIGHT | MAST ARM CAMBER |
| SC1103                  | 368.3                       | 423.5                       | 30' - 6"                   | 20' - 6"             | 58.50           | 21' - 3"        | 2.40"           |
| SC1104                  | 368.3                       | 423.5                       | 30' - 6"                   | 20' - 6"             | 63.41           | 24' - 7"        | 2.64"           |
| SC1206                  | 195.5                       | 224.8                       | 31' - 9"                   | 21' - 9"             | 72.36           | 20' - 3"        | 3.73"           |
| SC1221                  | 354.0                       | 354.0                       | 33' - 6"                   | 23' - 6"             | 73.00           | 22' - 7"        | 2.80"           |
| SC1223                  | 230.0                       | 264.5                       | 32' - 7"                   | 22' - 7"             | 73.21           | 20' - 2"        | 2.58"           |

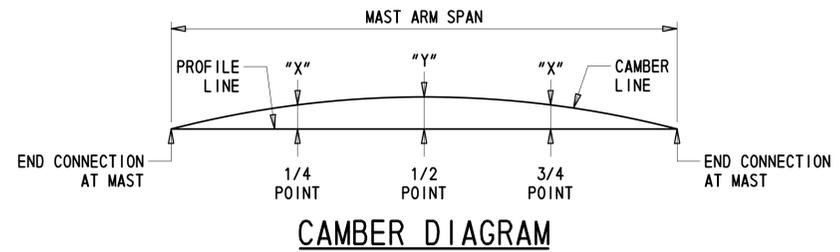
- NOTE:
- LOCATE HAND HOLE ON THE DOWN-TRAFFIC SIDE OF THE MAST, WITH A DIRECTION THAT IS 90 DEGREES TO THE PLANE CONTAINING THE MAST ARM.
- CROSS-REFERENCE NOTES:
- FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
  - FOR SIGN STRUCTURE LOCATIONS, SEE SHEET NO. 21.
  - FOR SIGN STRUCTURE CROSS SECTIONS, SEE SHEETS NO. 22-32.
  - FOR SIGN PANEL LAYOUTS, SEE SHEET NO. 16-20.
  - FOR FOUNDATION DETAILS, SEE SHEET NO. 38-39.
  - FOR BASE PLATE CONNECTION DETAILS, SEE SHEET NO. 36.
  - FOR END CONNECTION DETAILS, SEE SHEET NO. 36.
  - FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET NO. 37.
  - FOR OPTIONAL SHOP CONNECTION DETAILS, SEE SHEET NO. 36.
  - FOR MAST ARM END DETAILS, SEE SHEET NO. 36.
  - FOR HAND HOLE DETAIL, SEE SHEET NO. 37.

8/28/2015 12:42:50 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc L-495 Task2 Amnt 1673\Drawings\Plans\PE01-SC1103-1223.dgn



END VIEW

ELEVATION  
(LOOKING STATIONS AHEAD)



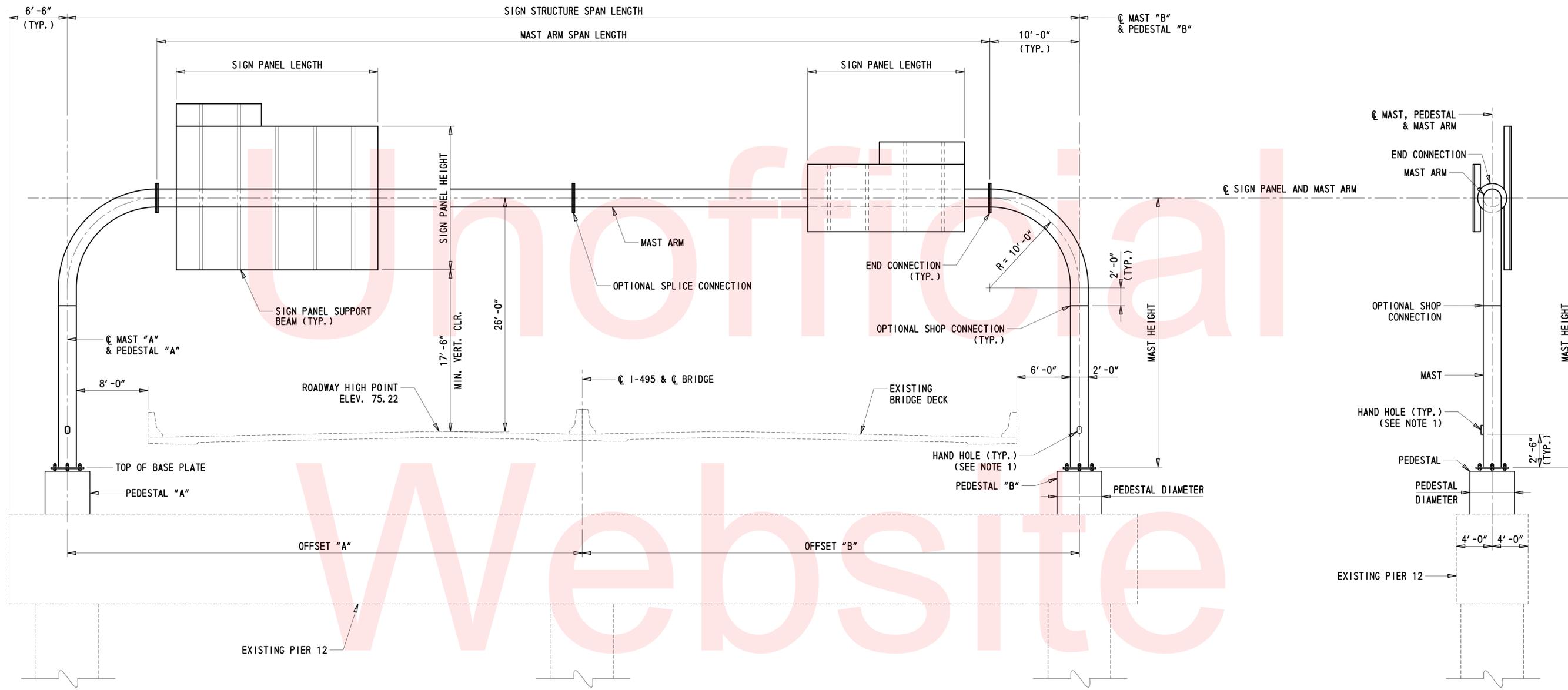
CAMBER DIAGRAM

| SIGN STRUCTURE GEOMETRY |                             |                             |                            |                      |                  |                 |                 |            |            |
|-------------------------|-----------------------------|-----------------------------|----------------------------|----------------------|------------------|-----------------|-----------------|------------|------------|
| SIGN STRUCTURE NO.      | ACTUAL SIGN PANEL AREA (SF) | DESIGN SIGN PANEL AREA (SF) | SIGN STRUCTURE SPAN LENGTH | MAST ARM SPAN LENGTH | CL. MAST ARM EL. | MAST "A" HEIGHT | MAST "B" HEIGHT | CAMBER "X" | CAMBER "Y" |
| S01107                  | 995.8                       | 1112.9                      | 84'-5"                     | 64'-5"               | 59.29            | 25'-4"          | 23'-5"          | 1.56"      | 2.09"      |
| S01112                  | 748.3                       | 860.5                       | 73'-3"                     | 53'-3"               | 50.72            | 26'-6"          | 24'-4"          | 1.11"      | 1.48"      |
| S01114                  | 748.3                       | 860.5                       | 77'-5"                     | 57'-5"               | 64.94            | 27'-8"          | 24'-4"          | 1.24"      | 1.66"      |
| S01116                  | 529.1                       | 572.1                       | 81'-1"                     | 61'-1"               | 65.31            | 22'-3"          | 22'-2"          | 1.29"      | 1.72"      |
| S01123                  | 873.0                       | 1004.0                      | 84'-7"                     | 64'-7"               | 112.01           | 24'-3"          | 23'-5"          | 1.54"      | 2.05"      |

NOTE:  
1. LOCATE HAND HOLE ON THE DOWN-TRAFFIC SIDE OF EACH MAST, WITH A DIRECTION THAT IS 90 DEGREES TO THE PLANE CONTAINING THE MAST ARM.

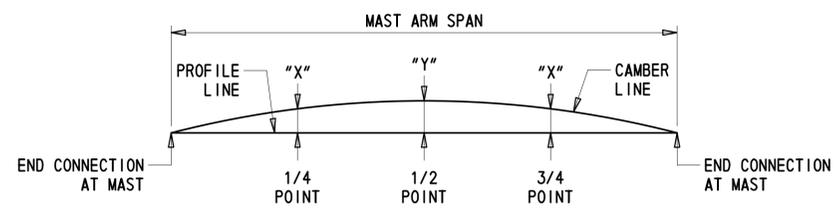
CROSS-REFERENCE NOTES:  
1. FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.  
2. FOR SIGN STRUCTURE LOCATIONS, SEE SHEET NO. 21.  
3. FOR SIGN STRUCTURE CROSS SECTIONS, SEE SHEETS NO. 22-32.  
4. FOR SIGN PANEL LAYOUTS, SEE SHEET NO. 16-20.  
5. FOR FOUNDATION DETAILS, SEE SHEET NO. 38-39.  
6. FOR BASE PLATE CONNECTION DETAILS, SEE SHEET NO. 36.  
7. FOR END CONNECTION DETAILS, SEE SHEET NO. 36.  
8. FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET NO. 37.  
9. FOR OPTIONAL SHOP CONNECTION DETAILS, SEE SHEET NO. 36.  
10. FOR MAST ARM END DETAILS, SEE SHEET NO. 36.  
11. FOR HAND HOLE DETAIL, SEE SHEET NO. 37.

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**ELEVATION**  
(LOOKING STATIONS AHEAD)

**END VIEW**



**CAMBER DIAGRAM**

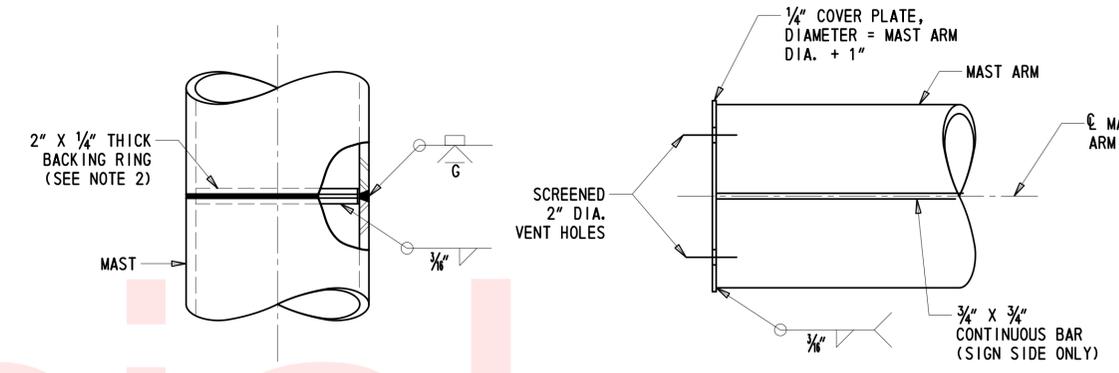
| SIGN STRUCTURE GEOMETRY |                             |                             |                            |                      |                |                 |                 |            |            |
|-------------------------|-----------------------------|-----------------------------|----------------------------|----------------------|----------------|-----------------|-----------------|------------|------------|
| SIGN STRUCTURE NO.      | ACTUAL SIGN PANEL AREA (SF) | DESIGN SIGN PANEL AREA (SF) | SIGN STRUCTURE SPAN LENGTH | MAST ARM SPAN LENGTH | ☉ MAST ARM EL. | MAST "A" HEIGHT | MAST "B" HEIGHT | CAMBER "X" | CAMBER "Y" |
| S01109                  | 538.9                       | 619.7                       | 113'-0"                    | 93'-0"               | 101.22         | 30'-0"          | 30'-0"          | 3.14"      | 4.19"      |

- NOTE:**
1. LOCATE HAND HOLE ON THE DOWN-TRAFFIC SIDE OF EACH MAST, WITH A DIRECTION THAT IS 90 DEGREES TO THE PLANE CONTAINING THE MAST ARM.
  2. BRIDGE SUPERSTRUCTURE NOT SHOWN FOR CLARITY.

- CROSS-REFERENCE NOTES:**
1. FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
  2. FOR SIGN STRUCTURE LOCATIONS, SEE SHEET NO. 21.
  3. FOR SIGN STRUCTURE CROSS SECTIONS, SEE SHEETS NO. 22-32.
  4. FOR SIGN PANEL LAYOUTS, SEE SHEET NO. 16-20.
  5. FOR FOUNDATION DETAILS, SEE SHEET NO. 38-39.
  6. FOR BASE PLATE CONNECTION DETAILS, SEE SHEET NO. 36.
  7. FOR END CONNECTION DETAILS, SEE SHEET NO. 36.
  8. FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET NO. 37.
  9. FOR OPTIONAL SHOP CONNECTION DETAILS, SEE SHEET NO. 36.
  10. FOR MAST ARM END DETAILS, SEE SHEET NO. 36.
  11. FOR HAND HOLE DETAIL, SEE SHEET NO. 37.

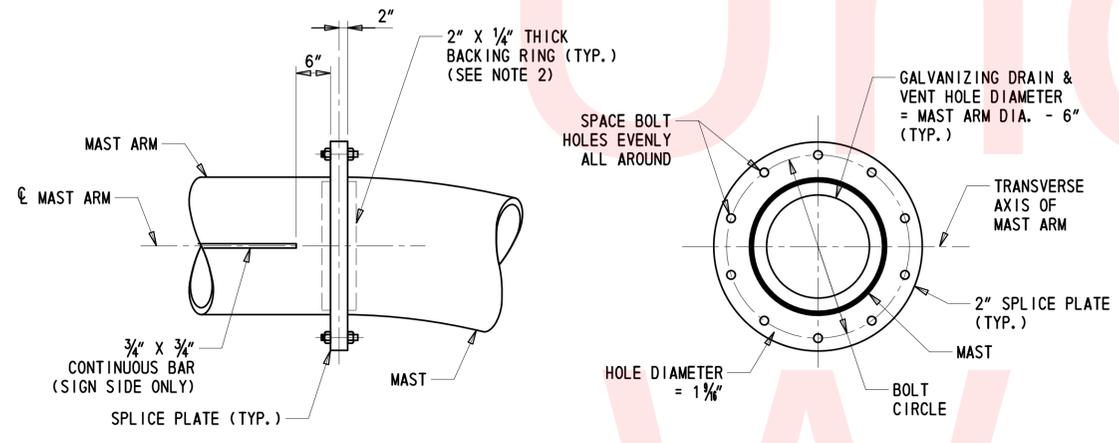
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| SIGN STRUCTURE MEMBER DIMENSIONS |                   |           |              |           |                  |           |            |                   |                    |           |                  |            |                   |
|----------------------------------|-------------------|-----------|--------------|-----------|------------------|-----------|------------|-------------------|--------------------|-----------|------------------|------------|-------------------|
| SIGN STRUCTURE NO.               | MAST AND MAST ARM |           | ANCHOR BOLTS |           |                  |           | BASE PLATE |                   | H. S. SPLICE BOLTS |           | SPLICE PLATE     |            |                   |
|                                  | OUTSIDE DIA.      | THICKNESS | NUMBER       | BOLT DIA. | BOLT CIRCLE DIA. | EMBEDMENT | PLATE DIA. | NO. OF STIFFENERS | NUMBER             | BOLT DIA. | BOLT CIRCLE DIA. | PLATE DIA. | NO. OF STIFFENERS |
| SC1103                           | 24"               | 0.625"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| SC1104                           | 24"               | 0.625"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| SC1206                           | 24"               | 0.625"    | 12           | 2"        | 36"              | 48"       | 44"        | 6                 | 12                 | 1 1/2"    | 32"              | 38"        | -                 |
| SC1221                           | 24"               | 0.625"    | 12           | 2"        | 36"              | 48"       | 44"        | 6                 | 10                 | 1 1/2"    | 32"              | 38"        | -                 |
| SC1223                           | 24"               | 0.500"    | 12           | 2"        | 36"              | 48"       | 44"        | 6                 | 10                 | 1 1/2"    | 32"              | 38"        | -                 |
| S01107                           | 24"               | 0.625"    | 12           | 2"        | 36"              | 48"       | 44"        | 6                 | 10                 | 1 1/2"    | 32"              | 38"        | -                 |
| S01109                           | 24"               | 0.625"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| S01112                           | 24"               | 0.625"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| S01114                           | 24"               | 0.500"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| S01116                           | 24"               | 0.500"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |
| S01123                           | 24"               | 0.625"    | 8            | 2"        | 36"              | 48"       | 44"        | -                 | 8                  | 1 1/2"    | 32"              | 38"        | -                 |



OPTIONAL SHOP CONNECTION DETAIL

MAST ARM END DETAIL

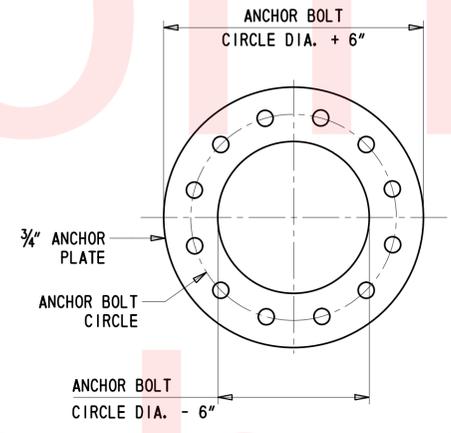


ELEVATION

SECTION

END CONNECTION DETAILS

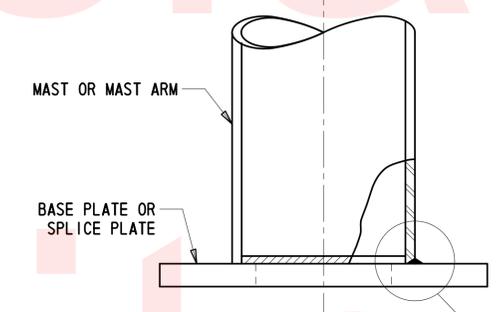
(10 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)



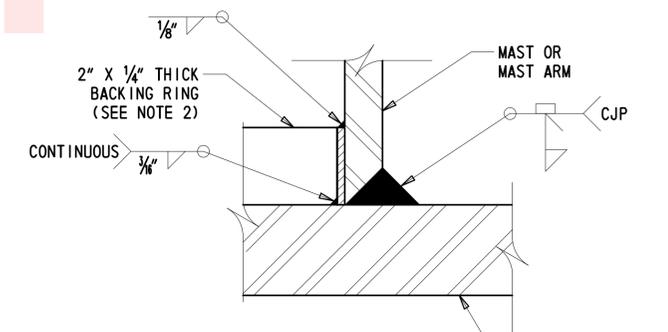
PLAN

ANCHOR PLATE DETAIL

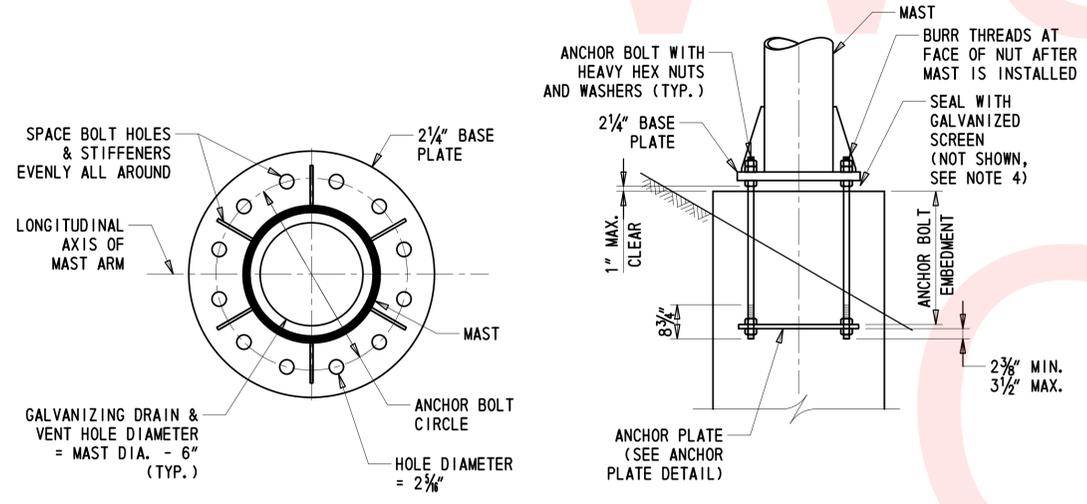
(12 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)



PIPE TO PLATE CONNECTION DETAIL



WELD DETAIL

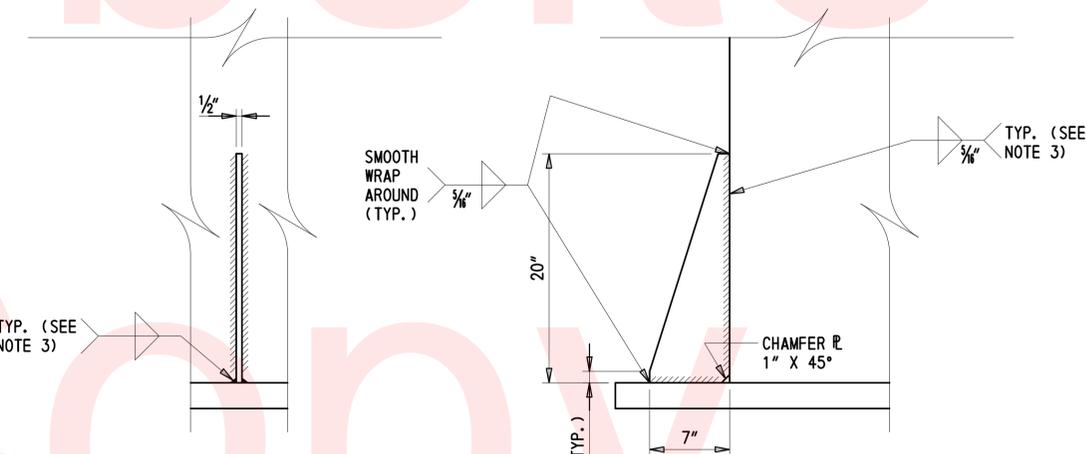


PLAN

ELEVATION

BASE PLATE CONNECTION DETAILS

(12 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)



SECTION

ELEVATION

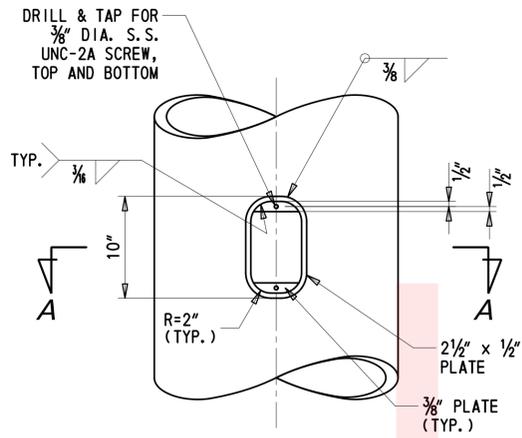
STIFFENER DETAILS

(BASE CONNECTION)

- NOTES:
- PROVIDE STIFFENERS AS INDICATED IN SIGN STRUCTURE MEMBER DIMENSIONS TABLE.
  - TOP WELD OF THE BACKING RING SHALL BE MADE AFTER ULTRASONIC INSPECTION OF THE GROOVE WELD. BACKING RING MUST BE FABRICATED AS A CONTINUOUS RING.
  - TERMINATE WELDS 1/4" SHORT OF STIFFENER CHAMFER.
  - SEAL BASE PLATE TO FOUNDATION GAP WITH GALVANIZED STEEL SCREEN, 1/2" BY 1/2" MESH AND 0.063" DIAMETER WIRES. SCREEN IS TO PREVENT ENTRY OF RODENTS WHILE PERMITTING DRAINAGE. SCREEN IS TO BE REMOVABLE AND ATTACHED TO THE BASE PLATE WITH STAINLESS STEEL HARDWARE.

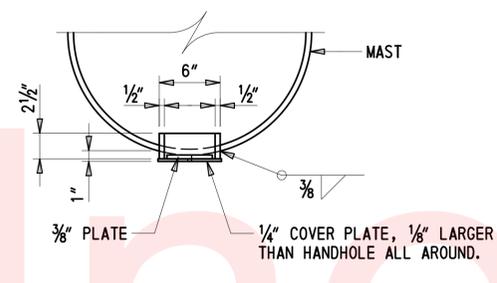
- CROSS-REFERENCE NOTES:
- FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
  - FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET NO. 37.
  - FOR FOUNDATION DETAILS, SEE SHEET NO. 38-39.

8/28/2015 12:43:54 AM T:\Dover\Projects\2014\42-5009.02 - Overhead Sign Strc L-495 Task2 Amnt 1673\Drawings\Plans\DT02-S01107-23.dgn

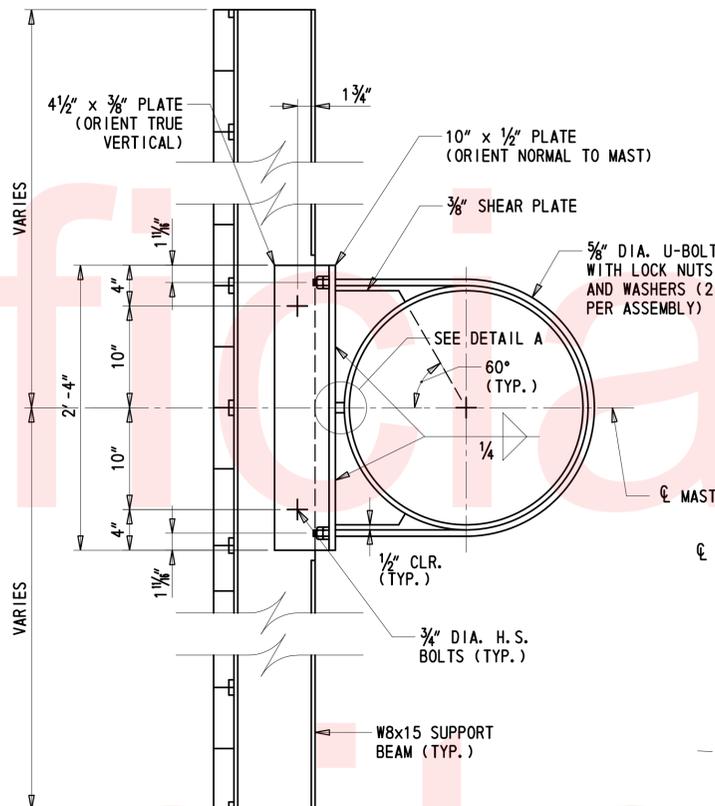


ELEVATION

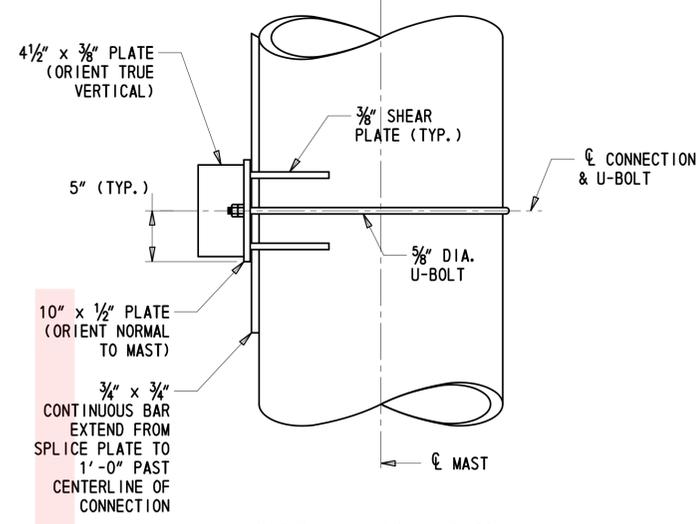
REINFORCED HANDHOLE



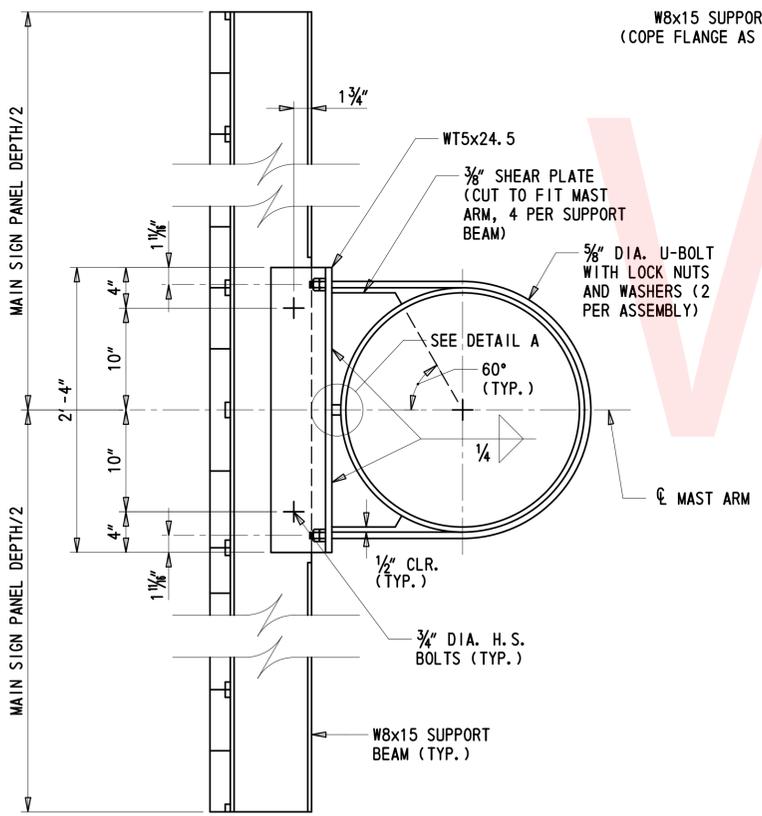
SECTION A-A



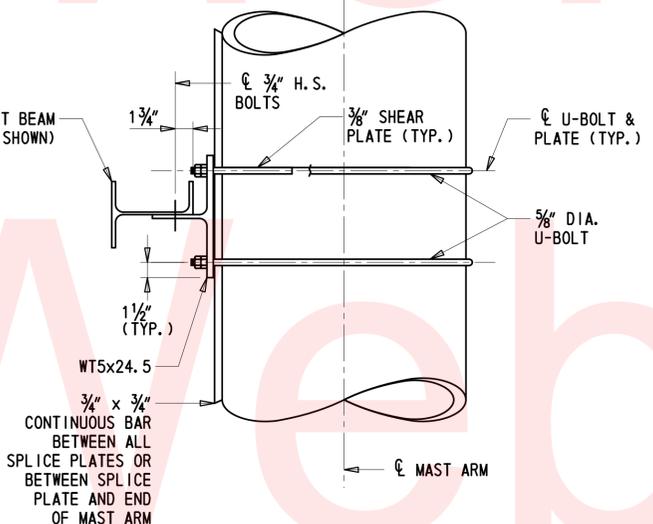
SIGN PANEL SECTION



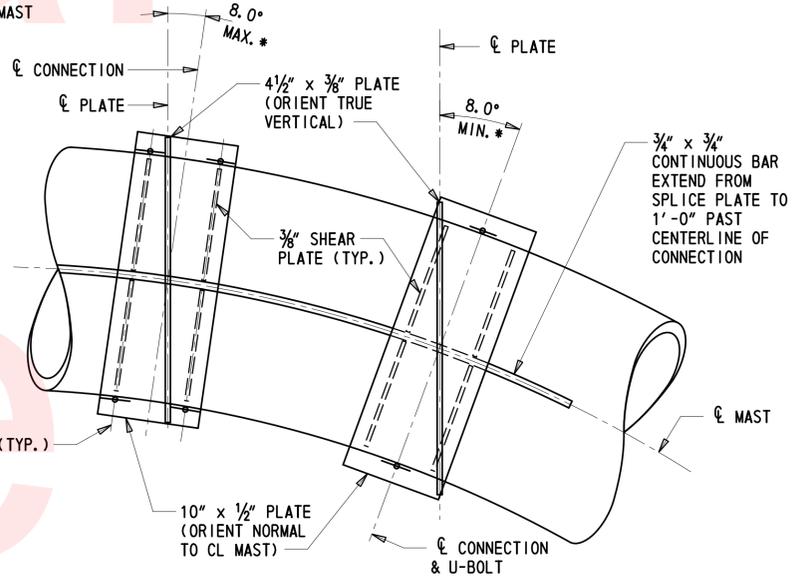
SINGLE U-BOLT PLAN



SIGN PANEL SECTION



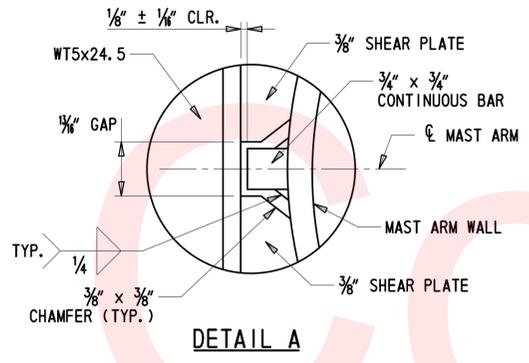
DOUBLE U-BOLT PLAN



DOUBLE U-BOLT

SINGLE U-BOLT

\*NOTE: WHEN ANGLE BETWEEN TRUE VERTICAL AND  $\phi$  CONNECTION IS LESS THAN 8.0°, USE DOUBLE U-BOLT DETAIL. WHEN ANGLE BETWEEN TRUE VERTICAL AND  $\phi$  CONNECTION IS GREATER THAN 8.0°, USE SINGLE U-BOLT DETAIL.



DETAIL A

SUPPORT BEAM TO MAST ARM CONNECTION DETAILS

SUPPORT BEAM TO CURVED MAST CONNECTION DETAILS  
(SUPPORT BEAM NOT SHOWN IN PLAN AND ELEVATION VIEWS FOR CLARITY)

- NOTE:
- LOCATE SUPPORT BEAMS TO AVOID END AND SPLICE CONNECTIONS. MAXIMUM SPACING = 5'-0". MAXIMUM DISTANCE TO PANEL EDGE = 2'-6".

- CROSS-REFERENCE NOTES:
- FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
  - FOR SIGN PANEL DETAILS, SEE SHEET NO. 16-20.

8/28/2015 12:44:12 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc L-495\_Task2\_Amnt.1673\Drawings\Plans\DT03-SO1107-23.dgn

| ADDENDUMS / REVISIONS |
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NOT TO SCALE

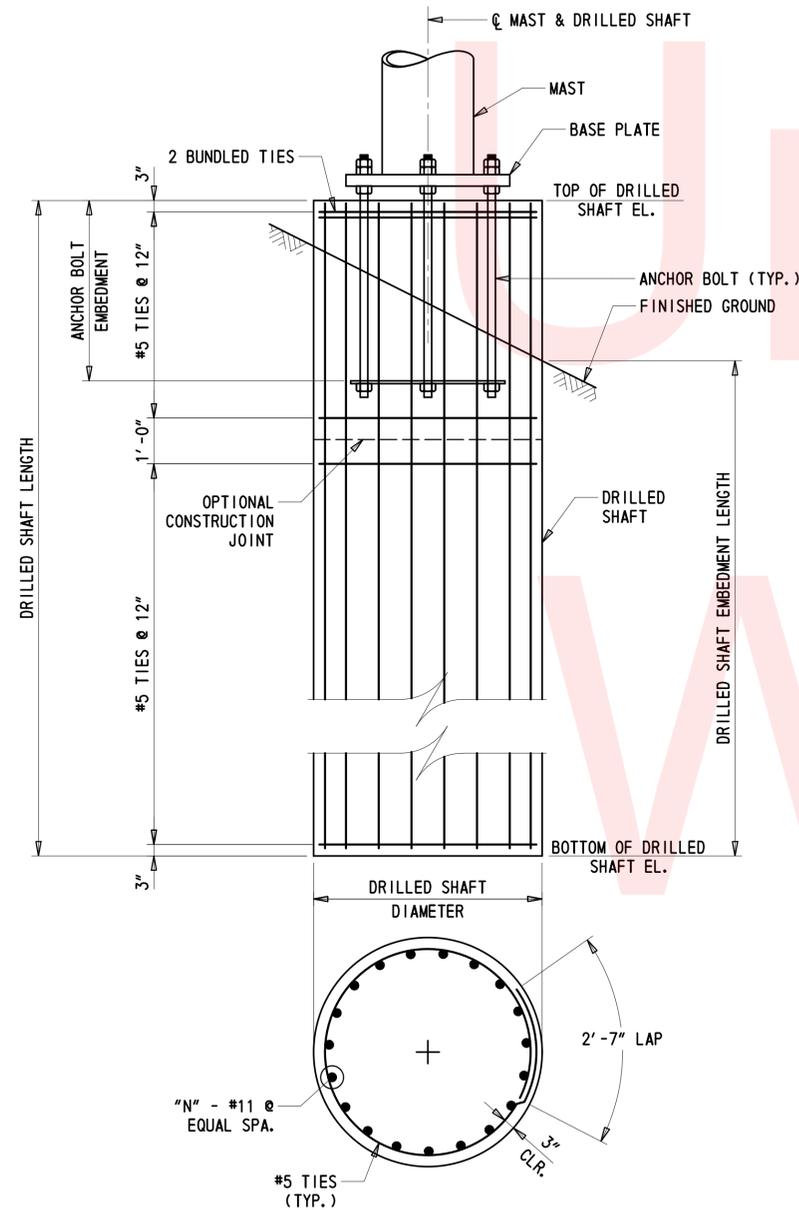
CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|            |                 |     |
|------------|-----------------|-----|
| CONTRACT   | BRIDGE NO.      | N/A |
| T201407004 | DESIGNED BY: PM |     |
| COUNTY     | CHECKED BY: DEF |     |
| NEW CASTLE |                 |     |

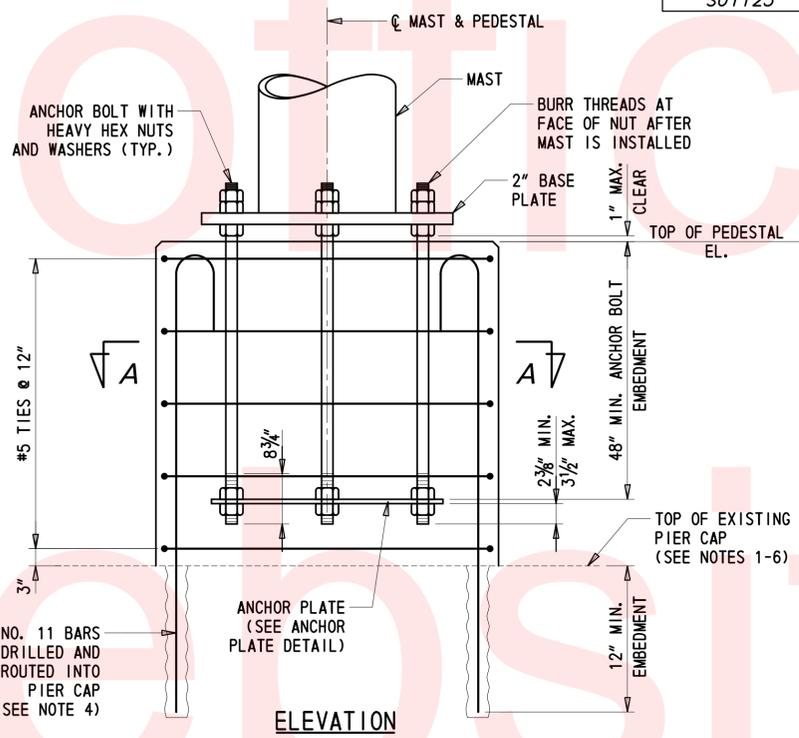
SIGN STRUCTURE DETAILS - 2

|             |    |
|-------------|----|
| SHEET NO.   | 37 |
| TOTAL SHTS. | 83 |

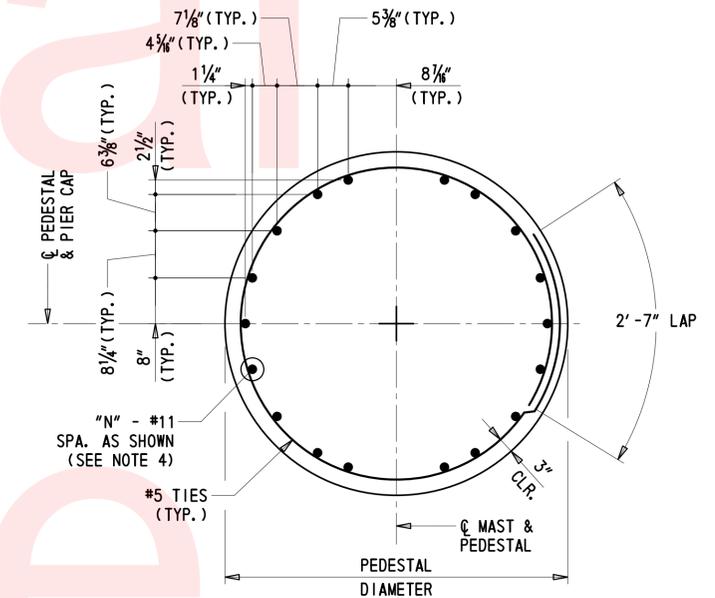
| SIGN STRUCTURE FOUNDATION SUMMARY |                               |         |            |           |                  |     |                               |         |            |           |                  |     |
|-----------------------------------|-------------------------------|---------|------------|-----------|------------------|-----|-------------------------------|---------|------------|-----------|------------------|-----|
| SIGN STRUCTURE NO.                | DRILLED SHAFT OR PEDESTAL "A" |         |            |           |                  |     | DRILLED SHAFT OR PEDESTAL "B" |         |            |           |                  |     |
|                                   | DIAMETER                      | TOP EL. | BOTTOM EL. | LENGTH    | EMBEDMENT LENGTH | "N" | DIAMETER                      | TOP EL. | BOTTOM EL. | LENGTH    | EMBEDMENT LENGTH | "N" |
| SC1103                            | 4'-6"                         | 36.83   | 16.83      | 20'-0"    | 17'-0"           | 19  | -                             | -       | -          | -         | -                | -   |
| SC1104                            | 4'-6"                         | 38.36   | 18.36      | 20'-0"    | 17'-0"           | 19  | -                             | -       | -          | -         | -                | -   |
| SC1206                            | 4'-6"                         | 51.84   | 28.84      | 23'-0"    | 19'-6"           | 18  | -                             | -       | -          | -         | -                | -   |
| SC1221                            | 4'-6"                         | 50.20   | 33.20      | 17'-0"    | 16'-0"           | 18  | -                             | -       | -          | -         | -                | -   |
| SC1223                            | 4'-6"                         | 52.78   | 31.28      | 21'-6"    | 18'-6"           | 15  | -                             | -       | -          | -         | -                | -   |
| S01107                            | 5'-0"                         | 33.52   | 9.52       | 24'-0"    | 21'-0"           | 22  | 5'-0"                         | 35.39   | 9.52       | 24'-0"    | 21'-0"           | 22  |
| S01109                            | 5'-0"                         | 70.77   | 66.00      | 4'-9 1/4" | -                | 18  | 5'-0"                         | 70.77   | 66.00      | 4'-9 1/4" | -                | 18  |
| S01112                            | 4'-6"                         | 23.74   | 4.74       | 19'-0"    | 16'-0"           | 19  | 4'-6"                         | 25.95   | 4.74       | 19'-0"    | 16'-0"           | 19  |
| S01116                            | 4'-6"                         | 42.58   | 24.58      | 18'-0"    | 15'-0"           | 19  | 4'-6"                         | 42.66   | 24.58      | 18'-0"    | 15'-0"           | 19  |
| S01123                            | 4'-6"                         | 87.28   | 65.28      | 22'-0"    | 19'-0"           | 19  | 4'-6"                         | 88.13   | 65.28      | 22'-0"    | 19'-0"           | 19  |



**DRILLED SHAFT FOUNDATION DETAILS**



**ELEVATION**



**SECTION A-A**

**PEDESTAL REINFORCEMENT DETAILS**  
(OVERHEAD SIGN STRUCTURE S01109)

**CROSS-REFERENCE NOTES:**

- FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
- FOR ANCHOR BOLT AND BASE PLATE DETAILS, SEE SHEET NO. 36.

**NOTES:**

- EPOXY ADHESIVE ANCHORING SYSTEM SHALL BE USED TO DEVELOP THE YIELD STRENGTH OF THE DOWEL BARS WITHIN THE SPECIFIED EMBEDMENT. ALL DRILL HOLES SHALL BE PREPARED AND CLEANED OF ANY RESIDUES PRIOR TO INSTALLATION OF EPOXY ADHESIVE ANCHORING SYSTEM FOR THE DOWELS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- THE EPOXY ADHESIVE ANCHORED DOWEL BARS SHALL BE PLACED TO THE DEPTH AS SHOWN IN THE PLANS ADEQUATE TO DEVELOP THE YIELD STRENGTH OF THE DOWELED BARS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- EPOXY ADHESIVE ANCHORING MATERIAL SHALL BE INSERTED TO A DEPTH SUFFICIENT TO ENSURE COMPLETE FILLING OF THE DRILL HOLE AFTER INSERTION OF THE DOWEL BARS IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- CAUTION SHALL BE EXERCISED WHILE DRILLING TO AVOID EXISTING REINFORCING STEEL. THE CONTRACTOR SHALL CHECK AND VERIFY THE DIMENSIONS AND BAR SPACING PRIOR TO DRILLING THE HOLES. THE CONTRACTOR SHALL MAKE NECESSARY ADJUSTMENTS TO THE SPACING SHOWN TO ENSURE PROTECTION OF EXISTING REINFORCING STEEL.

**NOTES (CONT.):**

- REINFORCING STEEL FOR PEDESTAL SHALL BE EPOXY COATED.
- SLOPE TOP OF PEDESTAL 4% FROM CENTER TO NEAR EDGES FOR DRAINAGE.
- TOP OF DRILLED SHAFT SHALL BE 3'-0" MIN. ABOVE FINISHED GROUND.
- ALL DRILLED SHAFT CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE DRILLED SHAFTS SPECIAL PROVISION EXCEPT AS MODIFIED BELOW.
- TEMPORARY STEEL CASING SHALL BE INSTALLED IN DRILLED SHAFT HOLE UNTIL CONCRETE HAS BEEN PLACED.
- NO PERMANENT CASING SHALL BE USED.

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| ADDENDUMS / REVISIONS |
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**NOT TO SCALE**

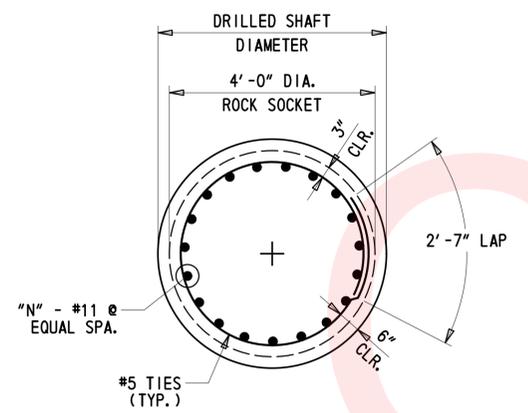
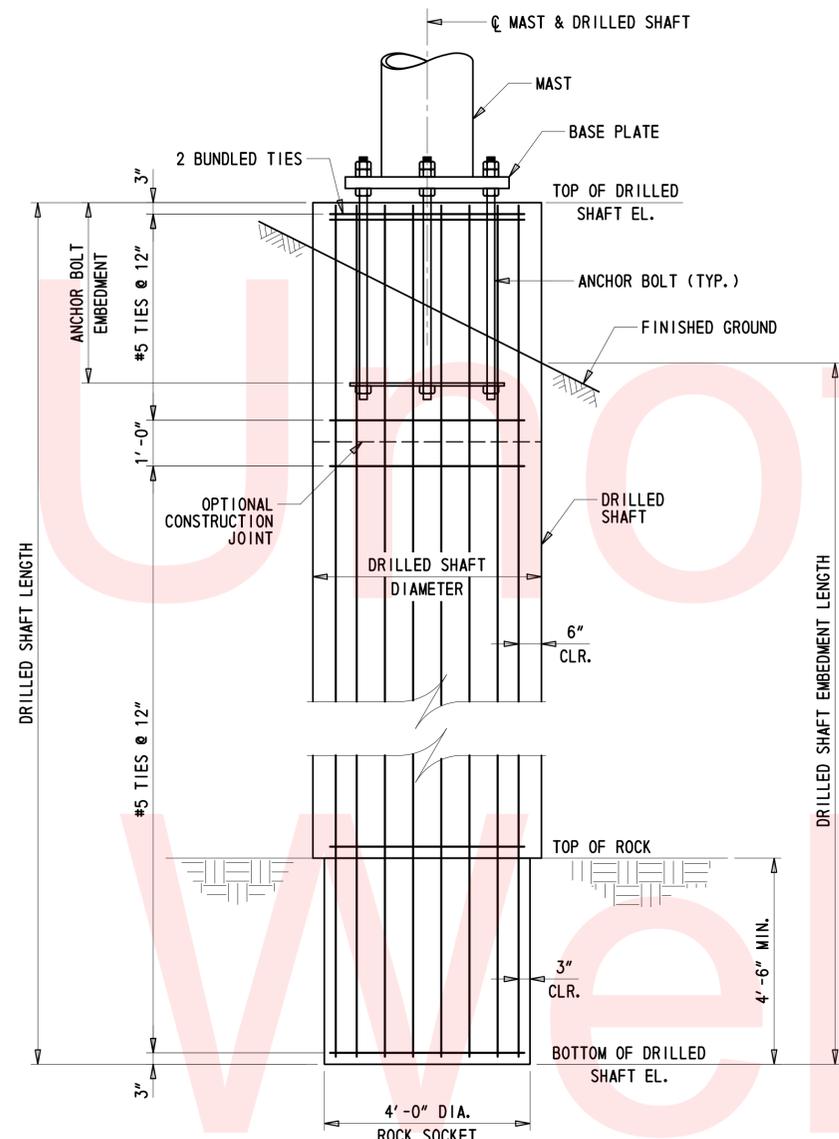
**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                 |     |
|------------|-----------------|-----|
| CONTRACT   | BRIDGE NO.      | N/A |
| T201407004 | DESIGNED BY: PM |     |
| COUNTY     | CHECKED BY: DEF |     |
| NEW CASTLE |                 |     |

**FOUNDATION DETAILS - 1**

|             |    |
|-------------|----|
| SHEET NO.   | 38 |
| TOTAL SHTS. | 83 |

| SIGN STRUCTURE FOUNDATION SUMMARY |                   |         |            |        |                  |     |                                 |         |            |        |                  |     |
|-----------------------------------|-------------------|---------|------------|--------|------------------|-----|---------------------------------|---------|------------|--------|------------------|-----|
| SIGN STRUCTURE NO.                | DRILLED SHAFT "A" |         |            |        |                  |     | ROCK-SOCKETED DRILLED SHAFT "B" |         |            |        |                  |     |
|                                   | DIAMETER          | TOP EL. | BOTTOM EL. | LENGTH | EMBEDMENT LENGTH | "N" | DIAMETER                        | TOP EL. | BOTTOM EL. | LENGTH | EMBEDMENT LENGTH | "N" |
| S01114                            | 4'-6"             | 36.81   | 18.81      | 18'-0" | 15'-0"           | 19  | 4'-6"                           | 40.18   | 22.68      | 17'-6" | 14'-6"           | 19  |



**ROCK-SOCKETED DRILLED SHAFT FOUNDATION DETAILS**  
(OVERHEAD SIGN STRUCTURE S01114)

**CROSS-REFERENCE NOTES:**

- FOR SIGN STRUCTURE NOTES, SEE SHEET NO. 21.
- FOR ANCHOR BOLT AND BASE PLATE DETAILS, SEE SHEET NO. 36.
- FOR DRILLED SHAFT FOUNDATION DETAILS, SEE SHEET 38.

**NOTES:**

- ALL DRILLED SHAFT CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE DRILLED SHAFTS SPECIAL PROVISION EXCEPT AS MODIFIED BELOW.
- THE IDENTIFICATION AND OCCURRENCE OF BEDROCK WILL BE FIELD VERIFIED DURING THE INSTALLATION OF THE DRILLED SHAFTS. THE VERIFICATION PROCESS WILL CONSIST OF:
  - THE TOP OF BEDROCK SHALL BE DEFINED AT THE DEPTH WHERE SPECIAL EXCAVATION STARTS. SPECIAL EXCAVATION IS DEFINED IN THE DRILLED SHAFT SPECIAL PROVISION.
  - IN THE CASE OF TOP-HOLE INSPECTION THE BOTTOM OF SHAFT SHALL BE INSPECTED BY WEIGHTED TAPE USING AT LEAST SIX SEPARATE SOUNDINGS. THE MEASURED THICKNESS OF THE SEDIMENTS SHOULD MEET THE REQUIREMENTS GIVEN IN DRILLED SHAFT SPECIAL PROVISION.
- THE TERMINATION ELEVATIONS OF THE DRILLED SHAFTS WILL BE DETERMINED BY THE ENGINEER BASED ON PROBING RESULTS AND MUST MEET ALL OF THE FOLLOWING CRITERIA:
  - BE AT OR BELOW THE BOTTOM OF DRILLED SHAFT ELEVATION PROVIDED IN THE SIGN STRUCTURE FOUNDATION SUMMARY TABLE; AND
  - HAVE THE MINIMUM EMBEDMENT INTO BEDROCK AS PROVIDED IN THE SIGN STRUCTURE FOUNDATION SUMMARY TABLE & DETAILS.
- TEMPORARY STEEL CASING SHALL BE INSTALLED IN DRILLED SHAFT HOLE UNTIL CONCRETE HAS BEEN PLACED.
- NO PERMANENT CASING SHALL BE USED.
- TOP OF DRILLED SHAFT SHALL BE 3'-0" MIN. ABOVE FINISHED GROUND.

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| ADDENDUMS / REVISIONS |
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**NOT TO SCALE**

**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |            |
|------------|------------------|------------|
| CONTRACT   | BRIDGE NO.       | <b>N/A</b> |
| T201407004 | DESIGNED BY: IZB |            |
| COUNTY     | CHECKED BY: DEF  |            |
| NEW CASTLE |                  |            |

|                               |             |    |
|-------------------------------|-------------|----|
| <b>FOUNDATION DETAILS - 2</b> | SHEET NO.   | 39 |
|                               | TOTAL SHTS. | 83 |

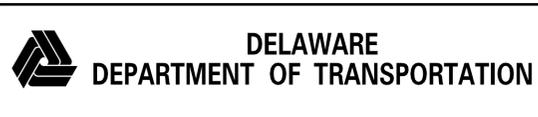
8/28/2015 12:45:14 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc - 495\_Task2\_Amnt\_1673\Drawings\Plans\B00L\_T201407005.dgn

| BORING: CS-1 (S01123) |       | DATE DRILLED: 7/24/14 |  |             |         |
|-----------------------|-------|-----------------------|--|-------------|---------|
| STATION: 10+78.78     |       | ELEVATION: 83.50      |  |             |         |
| OFFSET: 2.38'         |       | NORTHING: 662045.200  |  |             |         |
| EASTING: 646175.028   |       | COMMENTS: N/A         |  |             |         |
| SAMPLE INFORMATION    |       |                       |  |             |         |
| NO.                   | DEPTH | BLOWS /6"             | DESCRIPTION  | CLASS /G.I. | REMARKS |
| 1                     | 0.0   | 8                     | MOIST MEDIUM DENSE GRAY SILTY FINE GRAVEL W/SOME FINE TO COARSE SAND, TRACE OF CLAY.             | A-1-B       |         |
| 2                     | 0.0   | 17                    | WET MEDIUM DENSE GRAY SILTY FINE SAND AND FINE GRAVEL W/SOME COARSE SAND.                        | A-1-B       |         |
| 3                     | 2.0   | 13                    | SATURATED VERY DENSE GRAY SILTY FINE TO COARSE SAND W/TRACE FINE GRAVEL.                         | A-2-4(0)    |         |
| 4                     | 4.0   | 40                    | SATURATED VERY DENSE GRAY SILTY FINE SAND W/SOME COARSE SAND, TRACE OF FINE GRAVEL.              | A-2-4(0)    |         |
| 5                     | 6.0   | 6                     | SATURATED HARD GRAY FINE SANDY SILT W/TRACE COARSE SAND AND FINE GRAVEL.                         | A-4(0)      |         |
| 6                     | 8.0   | 42                    | SATURATED HARD GRAY FINE SANDY SILT W/SOME COARSE SAND, TRACE OF FINE GRAVEL.                    | A-4(0)      |         |
| 7                     | 10.0  | 5                     | SATURATED HARD GRAY FINE SANDY SILT W/SOME COARSE SAND AND CLAY, TRACE OF FINE GRAVEL.           | A-4(0)      |         |
| 8                     | 12.0  | 4                     | SATURATED VERY DENSE GRAY SILTY FINE SAND W/SOME COARSE SAND, TRACE OF FINE GRAVEL.              | A-2-4(0)    |         |
| 9                     | 14.0  | 4                     | SATURATED HARD GRAY FINE SANDY SILT W/SOME COARSE SAND, TRACE OF FINE GRAVEL AND CLAY.           | A-4(0)      |         |
| 10                    | 16.0  | 10                    | SATURATED HARD GRAY FINE SANDY SILT W/SOME COARSE SAND AND FINE GRAVEL, TRACE OF CLAY.           | A-4(0)      |         |
| 11                    | 18.0  | 50                    | SATURATED VERY DENSE GRAY FINE TO COARSE SAND AND FINE GRAVEL W/SOME SILT.                       | A-2-4(0)    |         |
| 12                    | 24.0  | 50                    | NO SIEVE ANALYSIS - INDICATION OF SATURATED VERY DENSE GRAY FINE TO COARSE SAND AND FINE GRAVEL. |             |         |
| 13                    | 29.0  | 50                    | SATURATED VERY DENSE GRAY FINE GRAVEL W/SOME FINE TO COARSE SAND AND SILT.                       | A-1-B       |         |
| R-1                   | 33.0  |                       | GRANITE  |             |         |
| R-2                   | 33.5  |                       | GRANITE  |             |         |
|                       | 38.5  |                       | END BORING   |             |         |
|                       | 43.5  |                       |  |             |         |

| BORING: CS-2A (S01114) |       | DATE DRILLED: 5/22/14 |  |             |         |
|------------------------|-------|-----------------------|--|-------------|---------|
| STATION: 10+45.63      |       | ELEVATION: 33.73      |  |             |         |
| OFFSET: 1.10'          |       | NORTHING: 656627.331  |  |             |         |
| EASTING: 646614.244    |       | COMMENTS: N/A         |  |             |         |
| SAMPLE INFORMATION     |       |                       |  |             |         |
| NO.                    | DEPTH | BLOWS /6"             | DESCRIPTION  | CLASS /G.I. | REMARKS |
| 1                      | 0.0   | 3                     | WET STIFF BROWN COARSE SANDY SILT W/SOME FINE SAND, FINE GRAVEL AND CLAY.                          | A-4(0)      |         |
| 2                      | 0.0   | 13                    | WET VERY DENSE BROWN CLAYEY COARSE SANDY FINE GRAVEL W/SOME FINE SAND AND SILT.                    | A-2-4(0)    |         |
| 3                      | 2.0   | 6                     | WET VERY STIFF BROWN CLAYEY SILT W/SOME FINE SAND AND FINE GRAVEL, TRACE OF COARSE SAND.           | A-4(4)      |         |
| 4                      | 4.0   | 11                    | WET MEDIUM DENSE BROWN CLAYEY COARSE TO FINE SAND W/SOME FINE GRAVEL AND SILT.                     | A-2-4(0)    |         |
| 5                      | 6.0   | 2                     | WET VERY LOOSE BROWN SILTY COARSE TO FINE SAND W/SOME FINE GRAVEL AND CLAY.                        | A-2-4(0)    |         |
| 6                      | 8.0   | 1                     | WET VERY LOOSE BROWN CLAYEY COARSE TO FINE SAND W/SOME FINE GRAVEL AND SILT.                       | A-2-4(0)    |         |
| 7                      | 10.0  | 1                     | NO SIEVE ANALYSIS - INDICATION OF WET DENSE BROWN FINE GRAVEL W/SOME COARSE TO FINE SAND AND SILT. |             |         |
| 8                      | 12.0  | 50                    | WET DENSE BROWN FINE GRAVEL W/SOME COARSE TO FINE SAND AND SILT.                                   | A-1-A       |         |
| C-1                    | 14.0  |                       | BROKEN ROCK 19.5" RECOVERY 0% ROD  |             |         |
| C-2                    | 14.5  |                       | BROKEN ROCK 38" RECOVERY 41% ROD   |             |         |
| C-3                    | 16.5  |                       | BROKEN ROCK 34" RECOVERY RECOVERY 18% ROD  |             |         |
|                        | 20.5  |                       | END BORING   |             |         |
|                        | 25.5  |                       |  |             |         |

| BORING: CS-2 (S01114) |       | DATE DRILLED: 5/29/14 |   |             |         |
|-----------------------|-------|-----------------------|---|-------------|---------|
| STATION: 10+38.40     |       | ELEVATION: 37.45      |   |             |         |
| OFFSET: 89.57'        |       | NORTHING: 656621.385  |   |             |         |
| EASTING: 646702.804   |       | COMMENTS: N/A         |   |             |         |
| SAMPLE INFORMATION    |       |                       |   |             |         |
| NO.                   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1                     | 0.0   | 2                     | MOIST MEDIUM DENSE BROWN SILTY COARSE TO FINE SAND W/SOME FINE GRAVEL.      | A-1-B       |         |
| 2                     | 0.0   | 20                    | MOIST DENSE BROWN COARSE TO FINE SAND AND FINE GRAVEL W/SOME SILT.          | A-1-B       |         |
| 3                     | 2.0   | 14                    | WET VERY STIFF BROWN CLAY W/SOME FINE TO COARSE SAND, FINE GRAVEL AND SILT. | A-6(5)      |         |
| 4                     | 4.0   | 10                    | NO SIEVE ANALYSIS - INDICATION OF WET HARD BROWN SANDY CLAY W/SOME SILT.    | A-6(5)      |         |
| C-1                   | 6.0   |                       | BOULDER - 7" RECOVERY 0% ROD  |             |         |
| C-2                   | 8.0   |                       | WEATHERED ROCK 56" RECOVERY 70.1% ROD                                       |             |         |
| C-3                   | 13.0  |                       | SLIGHTLY WEATHERED ROCK 52" RECOVERY 83% ROD                                |             |         |
|                       | 18.0  |                       | END BORING  |             |         |
|                       | 23.0  |                       |   |             |         |

- NOTES:
- NORTHING AND EASTING COORDINATES ARE REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).
  - ELEVATIONS ARE BASED OFF OF NAVD 88.



| ADDENDUMS / REVISIONS |
|-----------------------|
|                       |
|                       |
|                       |

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: RMB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

|             |    |
|-------------|----|
| SHEET NO.   | 40 |
| TOTAL SHTS. | 83 |

SOIL BORING LOG - 1

| BORING: CS-4 (S01112) |       | DATE DRILLED: 5/20/14 |   |             |         |
|-----------------------|-------|-----------------------|---|-------------|---------|
| STATION: 11+24.64     |       | ELEVATION: 21.48      |   |             |         |
| OFFSET: -13.02'       |       | NORTHING: 645435.607  |   |             |         |
| EASTING: 645645.604   |       | COMMENTS: N/A         |   |             |         |
| SAMPLE INFORMATION    |       |                       |   |             |         |
| NO.                   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1                     | 0.0   | 3<br>10<br>16<br>9    | MOIST VERY STIFF BROWN CLAYEY FINE TO COARSE SANDY SILT W/TRACE FINE GRAVEL.            | A-4(0)      |         |
| 2                     | 0.0   | 12<br>15<br>14        | WET VERY STIFF BROWN SILTY CLAY W/SOME FINE SAND, TRACE OF COARSE SAND AND FINE GRAVEL. | A-6(6)      |         |
| 3                     | 2.0   | 5<br>6<br>8<br>10     | WET STIFF BROWN SILT W/SOME CLAY, TRACE OF FINE TO COARSE SAND AND FINE GRAVEL.         | A-4(4)      |         |
| 4                     | 4.0   | 7<br>8<br>9<br>8      | WET VERY STIFF BROWN CLAYEY SILT W/TRACE FINE TO COARSE SAND.                           | A-4(7)      |         |
| 5                     | 6.0   | 3<br>6<br>7<br>8      | WET STIFF BROWN SILT W/TRACE FINE TO COARSE SAND.                                       | A-4(0)      |         |
| 6                     | 8.0   | 11<br>26<br>14<br>16  | WET HARD BROWN FINE GRAVELLY SILT W/SOME FINE SAND, TRACE OF COARSE SAND.               | A-4(0)      |         |
| 7                     | 10.0  | 3<br>8<br>4<br>7      | WET STIFF BROWN CLAYEY FINE SANDY SILT W/SOME COARSE SAND AND FINE GRAVEL.              | A-4(0)      |         |
| 8                     | 12.0  | 7<br>15<br>14<br>15   | WET MEDIUM DENSE BROWN FINE SAND AND FINE GRAVEL W/SOME COARSE SAND AND SILT.           | A-1-B       |         |
| 9                     | 14.0  | 12<br>32<br>27<br>20  | WET VERY DENSE BROWN SILTY FINE SAND AND FINE GRAVEL W/SOME COARSE SAND.                | A-1-B       |         |
| 10                    | 16.0  | 4<br>5<br>6<br>8      | SATURATED STIFF BROWN FINE SANDY CLAY W/SOME SILT, TRACE OF COARSE SAND.                | A-7-5(15)   |         |
| 11                    | 18.0  | 3<br>6<br>8<br>8      | SATURATED STIFF GREEN FINE SANDY CLAY W/SOME SILT, TRACE OF COARSE SAND.                | A-7-5(5)    |         |
| 12                    | 24.0  | 7<br>12<br>12<br>12   | SATURATED MEDIUM DENSE GREEN CLAYEY FINE SAND W/SOME COARSE SAND AND SILT.              | A-2-7(0)    |         |
| 13                    | 29.0  | 7<br>18<br>50         | SATURATED HARD GREEN CLAYEY FINE SANDY SILT W/TRACE COARSE SAND.                        | A-4(3)      |         |
|                       | 34.0  |                       | END BORING  |             |         |
|                       | 38.0  |                       |   |             |         |

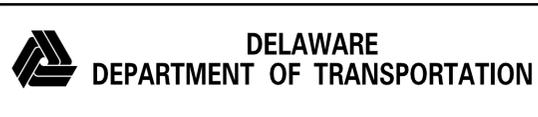
| BORING: CS-5 (S01116) |       | DATE DRILLED: 5/20/14 |   |             |         |
|-----------------------|-------|-----------------------|---|-------------|---------|
| STATION: 12+25.35     |       | ELEVATION:            |   |             |         |
| OFFSET: 1.83'         |       | NORTHING: 639183.31   |   |             |         |
| EASTING: 633461.389   |       | COMMENTS: N/A         |   |             |         |
| SAMPLE INFORMATION    |       |                       |   |             |         |
| NO.                   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1                     | 0.0   | 3<br>7<br>5<br>8      | WET STIFF BROWN SILT W/SOME COARSE TO FINE SAND, FINE GRAVEL, TRACE OF CLAY.                            | A-4(0)      |         |
| 2                     | 0.0   | 7<br>8<br>5           | NO SIEVE ANALYSIS - INDICATION OF WET STIFF BROWN SILT W/SOME COARSE TO FINE SAND AND FINE GRAVEL.      |             |         |
| 3                     | 2.0   | 2<br>3<br>9<br>36     | WET STIFF BROWN CLAYEY FINE GRAVELLY SILT W/SOME FINE TO COARSE SAND.                                   | A-4(1)      |         |
| 4                     | 4.0   | 15<br>20<br>25<br>18  | WET HARD BROWN CLAY W/SOME COARSE TO FINE SAND, FINE GRAVEL AND SILT.                                   | A-6(2)      |         |
| 5                     | 6.0   | 16<br>8.0<br>50       | NO SIEVE ANALYSIS - INDICATION OF WET HARD BROWN CLAY W/SOME COARSE TO FINE SAND, FINE GRAVEL AND SILT. |             |         |
| C-1                   | 8.0   |                       | BOULDERS 6" RECOVERY 0% ROD   |             |         |
| C-2                   | 8.0   |                       | BOULDERS 7" RECOVERY 0% ROD   |             |         |
| C-3                   | 13.0  |                       | ROCK 16" RECOVERY 0% ROD  |             |         |
| C-4                   | 18.0  |                       | ROCK 15" RECOVERY 15% ROD   |             |         |
| C-5                   | 23.0  |                       | NO RECOVERY   |             |         |
|                       | 28.0  |                       | END BORING  |             |         |
|                       | 33.0  |                       |   |             |         |

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**NOTES:**

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- ELEVATIONS ARE BASED OFF OF NAVD 88.

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ADDENDUMS / REVISIONS

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: RMB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

SOIL BORING LOG - 2

|             |    |
|-------------|----|
| SHEET NO.   | 41 |
| TOTAL SHTS. | 83 |

| BORING: CS-7 (SC1104) |       | DATE DRILLED: 7/30/14                                   |  |             |         |
|-----------------------|-------|---|--|-------------|---------|
| STATION: 11+14.30     |       | OFFSET: 49.21'  |  |             |         |
| ELEVATION: 36.28      |       | NORTHING: 626874.092                                    |  |             |         |
| EASTING: 621821.734   |       | COMMENTS: HOLE CALLED PER BILL DETWILER AND DOUG FINNEY |  |             |         |
| SAMPLE INFORMATION    |       |   |  |             |         |
| NO.                   | DEPTH | BLOWS /6"   | DESCRIPTION  | CLASS /G.I. | REMARKS |
| 1                     | 0.0   | 2   | MOIST LOOSE GRAY FINE TO COARSE SANDY FINE GRAVEL W/SOME SILT.           | A-1-B       |         |
|                       |       | 3   |  |             |         |
|                       |       | 4   |  |             |         |
|                       |       | 5   |  |             |         |
|                       | 0.0   | 6   |  |             |         |
| 2                     | 0.0   | 6   | MOIST MEDIUM DENSE GRAY COARSE TO FINE SAND AND FINE GRAVEL W/SOME SILT. | A-1-B       |         |
|                       |       | 8   |  |             |         |
|                       |       | 9   |  |             |         |
|                       | 2.0   | 10  |  |             |         |
| 3                     | 2.0   | 6   | MOIST MEDIUM DENSE GRAY SILTY FINE TO COARSE SAND W/SOME FINE GRAVEL.    | A-2-4(0)    |         |
|                       |       | 7   |  |             |         |
|                       |       | 4   |  |             |         |
|                       | 4.0   | 7   |  |             |         |
| 4                     | 4.0   | 8   | MOIST STIFF GRAY COARSE TO FINE SANDY SILT W/TRACE FINE GRAVEL AND CLAY. | A-4(0)      |         |
|                       |       | 6   |  |             |         |
|                       |       | 5   |  |             |         |
|                       | 6.0   | 7   |  |             |         |
| 5                     | 6.0   | 7   | MOIST VERY DENSE GRAY CLAYEY FINE GRAVEL W/SOME FINE TO COARSE SAND.     | A-2-4(0)    |         |
|                       |       | 50  |  |             |         |
|                       | 8.0   | 50  |  |             |         |
| 6                     | 8.0   | 50  | NO RECOVERY  |             |         |
| 7                     | 10.0  |   | BOULDER  |             |         |
|                       | 12.0  |   | BOULDER  |             |         |
| 8                     | 12.0  |   | BOULDER  |             |         |
|                       | 16.0  |   | BOULDER  |             |         |
| 9                     | 16.0  |   | BOULDER  |             |         |
|                       | 18.0  |   | BOULDER  |             |         |
| 10                    | 18.0  |   | BOULDER  |             |         |
|                       | 24.0  |   | BOULDER  |             |         |
| 11                    | 24.0  |   | BOULDER  |             |         |
|                       | 29.0  |   | BOULDER  |             |         |
| 12                    | 29.0  |   | BOULDER  |             |         |
|                       | 34.0  |   | BOULDER  |             |         |
| 13                    | 34.0  |   | BOULDER  |             |         |
|                       | 39.0  |   | END BORING   |             |         |
|                       | 41.0  |   |  |             |         |

| BORING: CS-8 (SC1103 & S01107) |       | DATE DRILLED: 9/18/14         |  |             |         |
|--------------------------------|-------|-------------------------------|--|-------------|---------|
| STATION: 11+88.74              |       | OFFSET: 40.66'                |  |             |         |
| ELEVATION: 33.90               |       | NORTHING: 625718.668          |  |             |         |
| EASTING: 620286.231            |       | COMMENTS: OBSTRUCTION @ 18.0' |  |             |         |
| SAMPLE INFORMATION             |       |                               |  |             |         |
| NO.                            | DEPTH | BLOWS /6"                     | DESCRIPTION  | CLASS /G.I. | REMARKS |
| 1                              | 0.0   | 3                             | MOIST LOOSE BROWN SILTY COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND CLAY.         | A-2-4(0)    |         |
|                                |       | 4                             |  |             |         |
|                                |       | 6                             |  |             |         |
|                                |       | 5                             |  |             |         |
|                                | 0.0   | 5                             |  |             |         |
| 2                              | 0.0   | 7                             | MOIST MEDIUM DENSE BROWN SILTY FINE TO COARSE SAND AND FINE GRAVEL.                    | A-1-B       |         |
|                                |       | 9                             |  |             |         |
|                                |       | 10                            |  |             |         |
|                                | 2.0   | 9                             |  |             |         |
| 3                              | 2.0   | 6                             | MOIST MEDIUM DENSE BROWN CLAYEY COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT. | A-2-4(0)    |         |
|                                |       | 8                             |  |             |         |
|                                |       | 8                             |  |             |         |
|                                | 4.0   | 8                             |  |             |         |
| 4                              | 4.0   | 10                            | MOIST MEDIUM DENSE BROWN CLAYEY COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT. | A-2-4(0)    |         |
|                                |       | 10                            |  |             |         |
|                                |       | 10                            |  |             |         |
|                                | 6.0   | 8                             |  |             |         |
| 5                              | 6.0   | 9                             | MOIST HARD BROWN CLAYEY FINE GRAVELLY SILT W/SOME COARSE TO FINE SAND.                 | A-4(0)      |         |
|                                |       | 16                            |  |             |         |
|                                |       | 16                            |  |             |         |
|                                | 8.0   | 15                            |  |             |         |
| 6                              | 8.0   | 15                            | WET HARD BROWN CLAYEY FINE GRAVELLY SILT W/SOME FINE TO COARSE SAND.                   | A-4(2)      |         |
|                                |       | 15                            |  |             |         |
|                                |       | 15                            |  |             |         |
|                                | 10.0  | 15                            |  |             |         |
| 7                              | 10.0  |                               | WET BROWN FINE GRAVELLY CLAY W/SOME COARSE TO FINE SAND AND SILT.                      | A-6(1)      |         |
|                                | 12.0  |                               |  |             |         |
| 8                              | 12.0  | 7                             | WET VERY STIFF BROWN CLAYEY COARSE SANDY FINE GRAVELLY SILT W/SOME FINE SAND.          | A-4(0)      |         |
|                                |       | 11                            |  |             |         |
|                                |       | 13                            |  |             |         |
|                                | 14.0  | 9                             |  |             |         |
| 9                              | 14.0  | 11                            | WET VERY STIFF BROWN FINE GRAVELLY CLAY W/SOME COARSE TO FINE SAND AND SILT.           | A-6(2)      |         |
|                                |       | 13                            |  |             |         |
|                                |       | 13                            |  |             |         |
|                                | 16.0  | 14                            |  |             |         |
|                                | 16.0  |                               | END BORING   |             |         |
|                                | 18.0  |                               |  |             |         |

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- NOTES:
- NORTHING AND EASTING COORDINATES ARE REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).
  - ELEVATIONS ARE BASED OFF OF NAVD 88.

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|  |                       |  |              |  |                  |             |     |                     |           |
|--|-----------------------|--|--------------|--|------------------|-------------|-----|---------------------|-----------|
|  <b>DELAWARE DEPARTMENT OF TRANSPORTATION</b> | ADDENDUMS / REVISIONS |  | NOT TO SCALE | CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18 | CONTRACT         | BRIDGE NO.  | N/A | SOIL BORING LOG - 3 | SHEET NO. |
|  | T201407004            |  |              |  | DESIGNED BY: RMB | 42          |     |                     |           |
|  | NEW CASTLE            |  |              |  | CHECKED BY: DEF  | TOTAL SHTS. |     |                     |           |
|  |                       |  |              |  |                  |             |     | 83                  |           |

| BORING: DA-1 (SC1223)   |       | DATE DRILLED: 7/28/14 |   |             |         |
|---|-------|-----------------------|---|-------------|---------|
| STATION: 0+95.22  |       | ELEVATION: 49.82      |   |             |         |
| OFFSET: 18.31 LT.   |       | NORTHING: 4998.9425   |   |             |         |
| EASTING: 4957.3929  |       | EASTING: 4957.3929    |   |             |         |
| COMMENTS: ROCK CORE FROM 33.5' - 38.5' (100% RECOVERY, 60% ROD), 38.5' - 43.5' (100% RECOVERY, 42% ROD) |       |                       |   |             |         |
| SAMPLE INFORMATION  |       |                       |   |             |         |
| NO.   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1   | 0.0   |                       | MOIST BROWN SILTY COARSE SAND AND FINE GRAVEL W/SOME FINE SAND.                                 | A-1-B       |         |
| 2   | 0.0   |                       | MOIST BROWN CLAYEY COARSE SANDY FINE GRAVEL W/SOME FINE SAND AND SILT.                          | A-1-B       |         |
| 3   | 2.0   | 8                     | MOIST MEDIUM DENSE BROWN COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT.                 | A-1-B       |         |
| 4   | 4.0   | 8                     | NO SIEVE ANALYSIS - INDICATION OF MOIST MEDIUM DENSE BROWN COARSE TO FINE SAND AND FINE GRAVEL. |             |         |
| 5   | 6.0   | 7                     | MOIST MEDIUM DENSE BROWN COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT.                 | A-1-B       |         |
| 6   | 8.0   | 10                    | MOIST MEDIUM DENSE BROWN COARSE SAND AND FINE GRAVEL W/SOME FINE SAND, TRACE OF SILT.           | A-1-B       |         |
| 7   | 10.0  | 8                     | MOIST MEDIUM DENSE BROWN COARSE SANDY FINE GRAVEL W/SOME SILT AND CLAY, TRACE OF FINE SAND.     | A-1-A       |         |
| 8   | 12.0  | 6                     | MOIST VERY DENSE BROWN COARSE SANDY FINE GRAVEL W/SOME FINE SAND AND SILT, TRACE OF CLAY.       | A-1-B       |         |
| 9   | 14.0  | 50                    | NO SIEVE ANALYSIS - INDICATION OF MOIST DENSE BROWN SILTY FINE TO COARSE SAND.                  |             |         |
| 10  | 16.0  | 7                     | SATURATED VERY STIFF BROWN SILTY CLAY W/SOME COARSE SAND, TRACE OF FINE SAND.                   | A-7-5(22)   |         |
| 11  | 18.0  | 5                     | SATURATED VERY STIFF BROWN FINE SANDY CLAY W/SOME SILT AND COARSE SAND, TRACE OF FINE GRAVEL.   | A-7-5(11)   |         |
| 12  | 24.0  | 5                     | SATURATED MEDIUM DENSE BROWN SILTY COARSE TO FINE SAND.   | A-2-4(0)    |         |
| 13  | 29.0  | 50                    | NO SIEVE ANALYSIS - INDICATION OF SATURATED DENSE BROWN SILTY COARSE TO FINE SAND.              |             |         |
|   | 34.0  |                       | END BORING  |             |         |
|   | 36.0  |                       |   |             |         |

| BORING: DA-2 (SC1206)                         |       | DATE DRILLED: 9/10/14 |   |             |         |
|---|-------|-----------------------|---|-------------|---------|
| STATION: 1+44.29                              |       | ELEVATION: 49.43      |   |             |         |
| OFFSET: 15.75 RT.                             |       | NORTHING: 4962.2700   |   |             |         |
| EASTING: 5058.4231                            |       | EASTING: 5058.4231    |   |             |         |
| COMMENTS: POSSIBLE OLD ROAD/HOT-MIX @ 18'-20' |       |                       |   |             |         |
| SAMPLE INFORMATION                            |       |                       |   |             |         |
| NO.   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1   | 0.0   | 3                     | MOIST MEDIUM DENSE BROWN SILTY FINE GRAVEL W/SOME COARSE TO FINE SAND.                | A-2-4(0)    |         |
| 2   | 0.0   | 16                    | MOIST HARD BROWN CLAYEY SILT W/TRACE COARSE TO FINE SAND AND FINE GRAVEL.             | A-4(6)      |         |
| 3   | 2.0   | 6                     | MOIST STIFF BROWN SILTY CLAY W/SOME FINE TO COARSE SAND, TRACE OF SILT.               | A-6(9)      |         |
| 4   | 4.0   | 11                    | MOIST STIFF BROWN CLAYEY SILT W/SOME FINE GRAVEL AND COARSE SAND, TRACE OF FINE SAND. | A-4(3)      |         |
| 5   | 6.0   | 7                     | MOIST MEDIUM DENSE BROWN COARSE SANDY FINE GRAVEL W/SOME FINE SAND AND SILT.          | A-1-A       |         |
| 6   | 8.0   | 7                     | WET MEDIUM DENSE BROWN CLAYEY COARSE TO FINE SAND W/SOME SILT AND FINE GRAVEL.        | A-2-4(0)    |         |
| 7   | 10.0  | 6                     | WET MEDIUM DENSE BROWN SILTY FINE TO COARSE SAND W/SOME FINE GRAVEL.                  | A-2-4(0)    |         |
| 8   | 12.0  | 5                     | WET MEDIUM DENSE BROWN SILTY FINE TO COARSE SAND W/SOME FINE GRAVEL.                  | A-2-4(0)    |         |
| 9   | 14.0  | 12                    | WET MEDIUM DENSE BROWN COARSE SAND AND FINE GRAVEL W/SOME FINE SAND, TRACE OF SILT.   | A-1-B       |         |
| 10  | 16.0  | 6                     | WET MEDIUM DENSE BROWN COARSE TO FINE SAND W/SOME FINE GRAVEL AND SILT.               | A-1-B       |         |
| 11  | 18.0  | 13                    | SATURATED STIFF BROWN COARSE TO FINE SANDY SILT W/SOME CLAY.                          | A-5(1)      |         |
| R-1   | 24.0  |                       | RECOVERY - 90% ROD - 47%  |             |         |
| R-2   | 26.0  |                       | RECOVERY - 56% ROD - 51%  |             |         |
|   | 31.0  |                       | END BORING  |             |         |
|   | 35.5  |                       |   |             |         |

Waterloo Site

Copy

**NOTES:**

1. NORTHING AND EASTING COORDINATES ARE BASED OFF AN ASSUMED HORIZONTAL DATUM.
2. ELEVATIONS ARE BASED OFF AN ASSUMED VERTICAL DATUM.

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ADDENDUMS / REVISIONS

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: RMB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

SOIL BORING LOG - 4

|             |    |
|-------------|----|
| SHEET NO.   | 43 |
| TOTAL SHTS. | 83 |

| BORING: DA-4 (SC1221)                                 |       | DATE DRILLED: 9/15/14 |   |             |         |
|---|-------|-----------------------|---|-------------|---------|
| STATION: 1+49.96                                      |       | OFFSET: 1.23 LT.      |   |             |         |
|   |       | ELEVATION: 48.46      |   |             |         |
|   |       | NORTHING: 5336.7139   |   |             |         |
|   |       | EASTING: 4715.9269    |   |             |         |
| COMMENTS: AUGER REFUSAL @ 42.5'. LOST PLUG DOWN HOLE. |       |                       |   |             |         |
| SAMPLE INFORMATION                                    |       |                       |   |             |         |
| NO.   | DEPTH | BLOWS /6"             | DESCRIPTION   | CLASS /G.I. | REMARKS |
| 1   | 0.0   | 5                     | MOIST MEDIUM DENSE GRAY SILTY FINE GRAVEL W/SOME COARSE TO FINE SAND.                                   | A-1-B       |         |
|   | 1.0   | 7                     |   |             |         |
| 2   | 1.0   | 9                     | NO SIEVE ANALYSIS - INDICATION OF MOIST MEDIUM DENSE GRAY SILTY FINE GRAVEL W/SOME COARSE TO FINE SAND. |             |         |
|   |       | 8                     |   |             |         |
| 3   | 2.0   | 7                     | WET VERY STIFF BROWN CLAYEY SILT W/SOME FINE GRAVEL, TRACE OF FINE TO COARSE SAND.                      | A-4(4)      |         |
|   |       | 10                    |   |             |         |
| 4   | 4.0   | 8                     | WET VERY STIFF BROWN SILTY CLAY W/SOME FINE GRAVEL, TRACE OF COARSE TO FINE SAND.                       | A-6(9)      |         |
|   |       | 10                    |   |             |         |
| 5   | 6.0   | 8                     | WET MEDIUM DENSE BROWN COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT.                           | A-1-B       |         |
|   |       | 10                    |   |             |         |
| 6   | 8.0   | 8                     | WET MEDIUM DENSE BROWN FINE SANDY FINE GRAVEL W/SOME COARSE SAND AND SILT.                              | A-1-B       |         |
|   |       | 12                    |   |             |         |
| 7   | 10.0  | 12                    | WET DENSE BROWN COARSE SANDY FINE GRAVEL W/SOME FINE SAND AND SILT.                                     | A-1-B       |         |
|   |       | 21                    |   |             |         |
| 8   | 12.0  | 17                    | WET DENSE BROWN SILTY COARSE TO FINE SAND AND FINE GRAVEL W/SOME CLAY.                                  | A-2-4(0)    |         |
|   |       | 21                    |   |             |         |
| 9   | 14.0  | 8                     | WET VERY STIFF BROWN FINE SANDY SILT W/TRACE COARSE SAND AND FINE GRAVEL.                               | A-4(0)      |         |
|   |       | 11                    |   |             |         |
| 10  | 16.0  | 21                    | NO SIEVE ANALYSIS - INDICATION OF WET HARD BROWN FINE SANDY SILT W/TRACE COARSE SAND AND FINE GRAVEL.   |             |         |
|   |       | 24                    |   |             |         |
| 11  | 18.0  | 18                    | WET HARD BROWN CLAYEY COARSE SANDY SILT W/SOME FINE SAND AND FINE GRAVEL.                               | A-4(1)      |         |
|   |       | 20                    |   |             |         |
| 12  | 24.0  | 10                    | WET MEDIUM DENSE BROWN CLAYEY COARSE SAND AND FINE GRAVEL W/SOME FINE SAND AND SILT.                    | A-2-4(0)    |         |
|   |       | 12                    |   |             |         |
| 13  | 29.0  | 12                    | NO SIEVE ANALYSIS - INDICATION OF WET MEDIUM DENSE BROWN SILTY COARSE TO FINE SAND W/SOME FINE GRAVEL.  |             |         |
|   |       | 13                    |   |             |         |
| 14  | 34.0  | 8                     | WET MEDIUM DENSE BROWN SILTY COARSE TO FINE SAND W/SOME FINE GRAVEL.                                    | A-2-4(0)    |         |
|   |       | 9                     |   |             |         |
|   | 39.0  | 16                    | END BORING  |             |         |
|   | 41.0  |                       |   |             |         |

Unofficial Website Copy

**NOTES:**

1. NORTHING AND EASTING COORDINATES ARE BASED OFF AN ASSUMED HORIZONTAL DATUM.
2. ELEVATIONS ARE BASED OFF AN ASSUMED VERTICAL DATUM.

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ADDENDUMS / REVISIONS

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

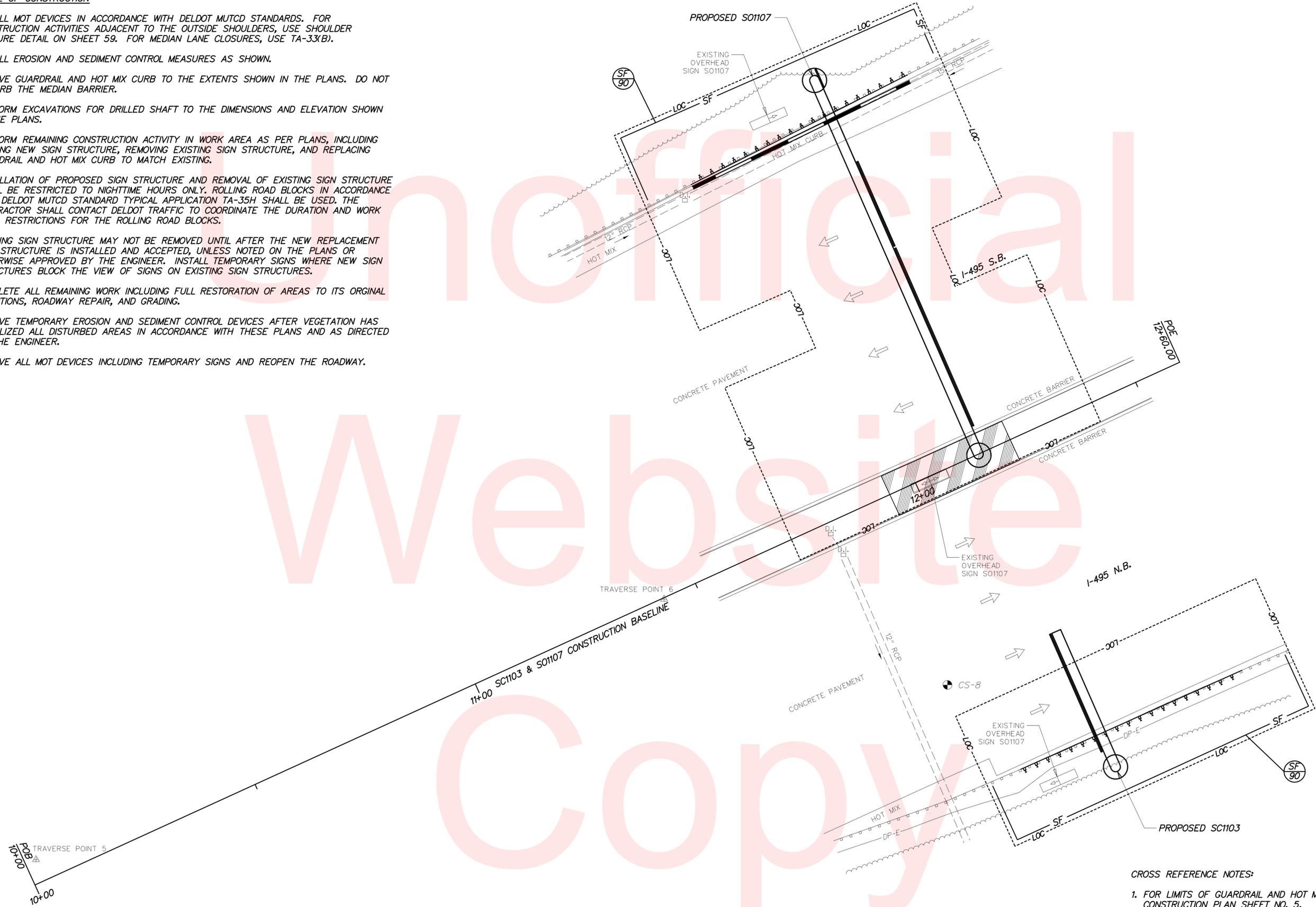
|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: RMB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

SOIL BORING LOG - 5

|             |    |
|-------------|----|
| SHEET NO.   | 44 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

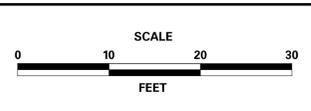
1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59. FOR MEDIAN LANE CLOSURES, USE TA-33(B).
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL AND HOT MIX CURB TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL AND HOT MIX CURB TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



**CROSS REFERENCE NOTES:**  
 1. FOR LIMITS OF GUARDRAIL AND HOT MIX CURB REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 5.

8/28/2015 12:47:32 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\C503-S01107.dgn

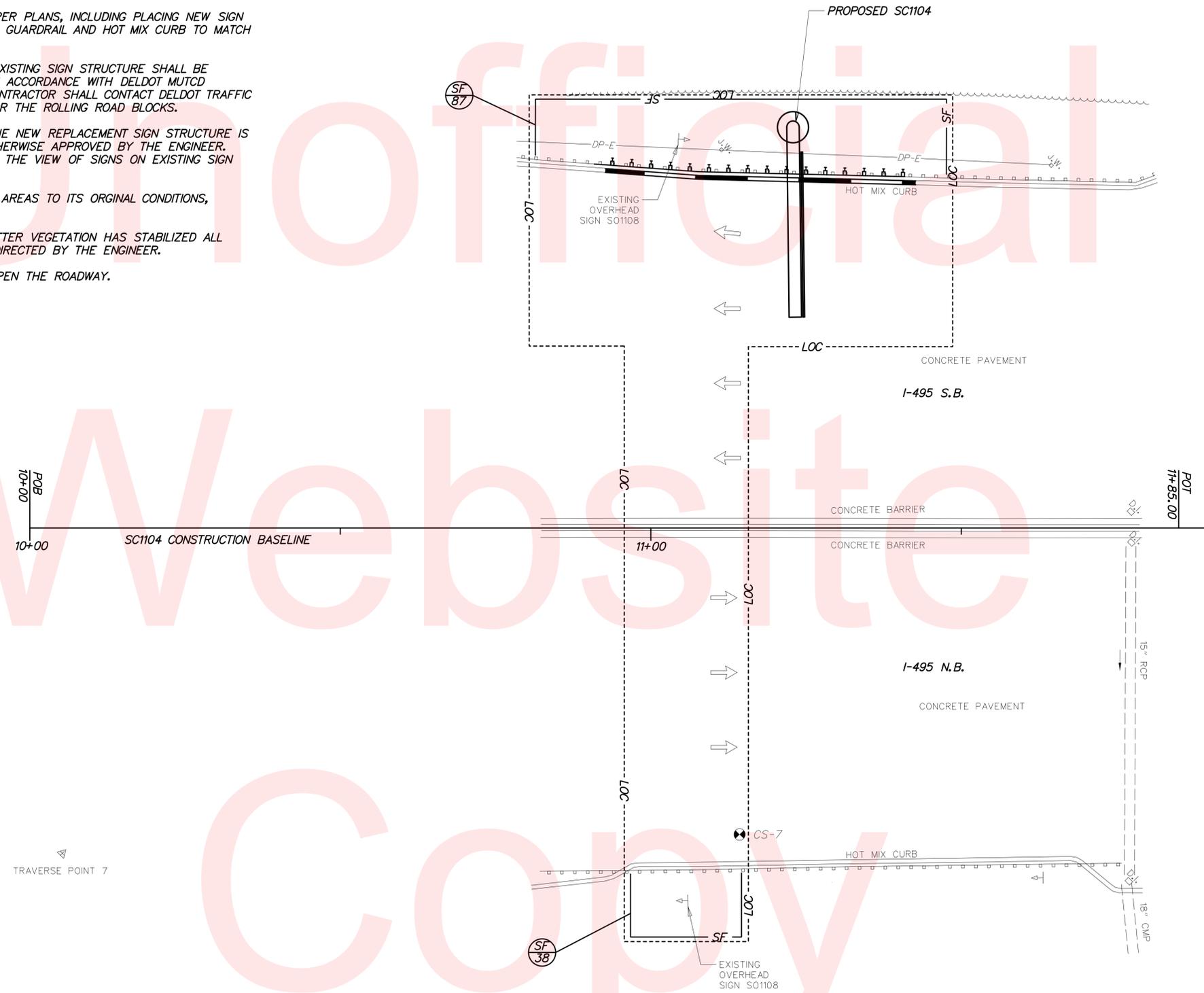
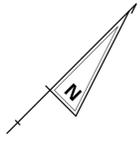
| ADDENDUMS / REVISIONS |  |
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|                       |  |
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|          |            |              |     |
|----------|------------|--------------|-----|
| CONTRACT | T201407004 | BRIDGE NO.   | N/A |
| COUNTY   | NEW CASTLE | DESIGNED BY: | PM  |
|          |            | CHECKED BY:  | DEF |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL AND HOT MIX CURB TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL AND HOT MIX CURB TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.

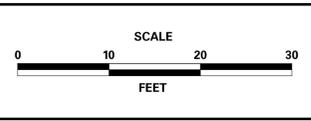


**CROSS REFERENCE NOTES:**  
 1. FOR LIMITS OF GUARDRAIL AND HOT MIX CURB REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 6.

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**DELAWARE DEPARTMENT OF TRANSPORTATION**

| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
|                       |  |



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                 |                 |
|------------------------|-----------------|-----------------|
| CONTRACT<br>T201407004 | BRIDGE NO.<br>  | <b>N/A</b>      |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: PM | CHECKED BY: DEF |

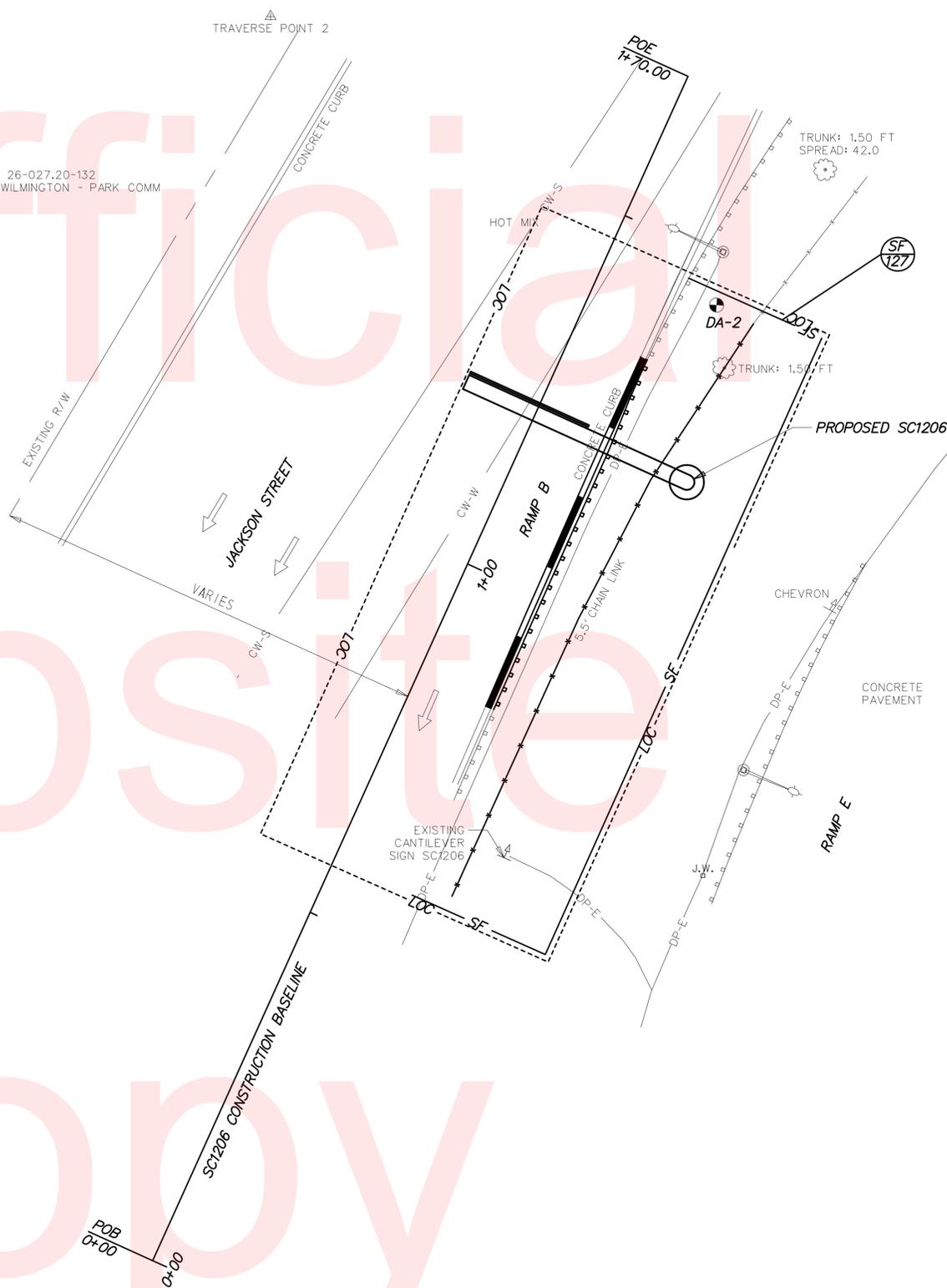
**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURES SC1104 AND S01108**

|                   |
|-------------------|
| SHEET NO.<br>46   |
| TOTAL SHTS.<br>83 |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH TYPICAL APPLICATION 33 (B) OF THE DELAWARE MUTCD FOR THE LEFT LANE ON JACKSON STREET AND IN ACCORDANCE WITH THE DETOUR PLAN.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN. NEAREST TWO DRAINAGE INLETS SOUTH OF THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED WITH ITEM 905005 - INLET SEDIMENT CONTROL, CURB INLET.
3. REMOVE CHAIN LINK FENCE, GUARDRAIL AND CONCRETE CURB TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING CHAIN LINK FENCE, GUARDRAIL AND CONCRETE CURB TO MATCH EXISTING.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER.
7. COMPLETE ALL REMAINING WORK INCLUDING ROADWAY REPAIR AND GRADING.
8. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
9. REMOVE ALL MOT DEVICES AND REOPEN THE ROADWAY.

26-027.20-132  
CITY OF WILMINGTON - PARK COMM

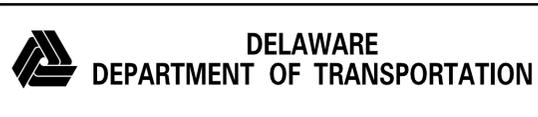


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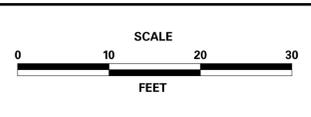
**CROSS-REFERENCE NOTES:**

1. FOR LIMITS OF CHAIN LINK FENCE, GUARDRAIL AND CONCRETE CURB REMOVAL, REFER TO CONSTRUCTION PLAN, SHEET NO. 7.

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| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

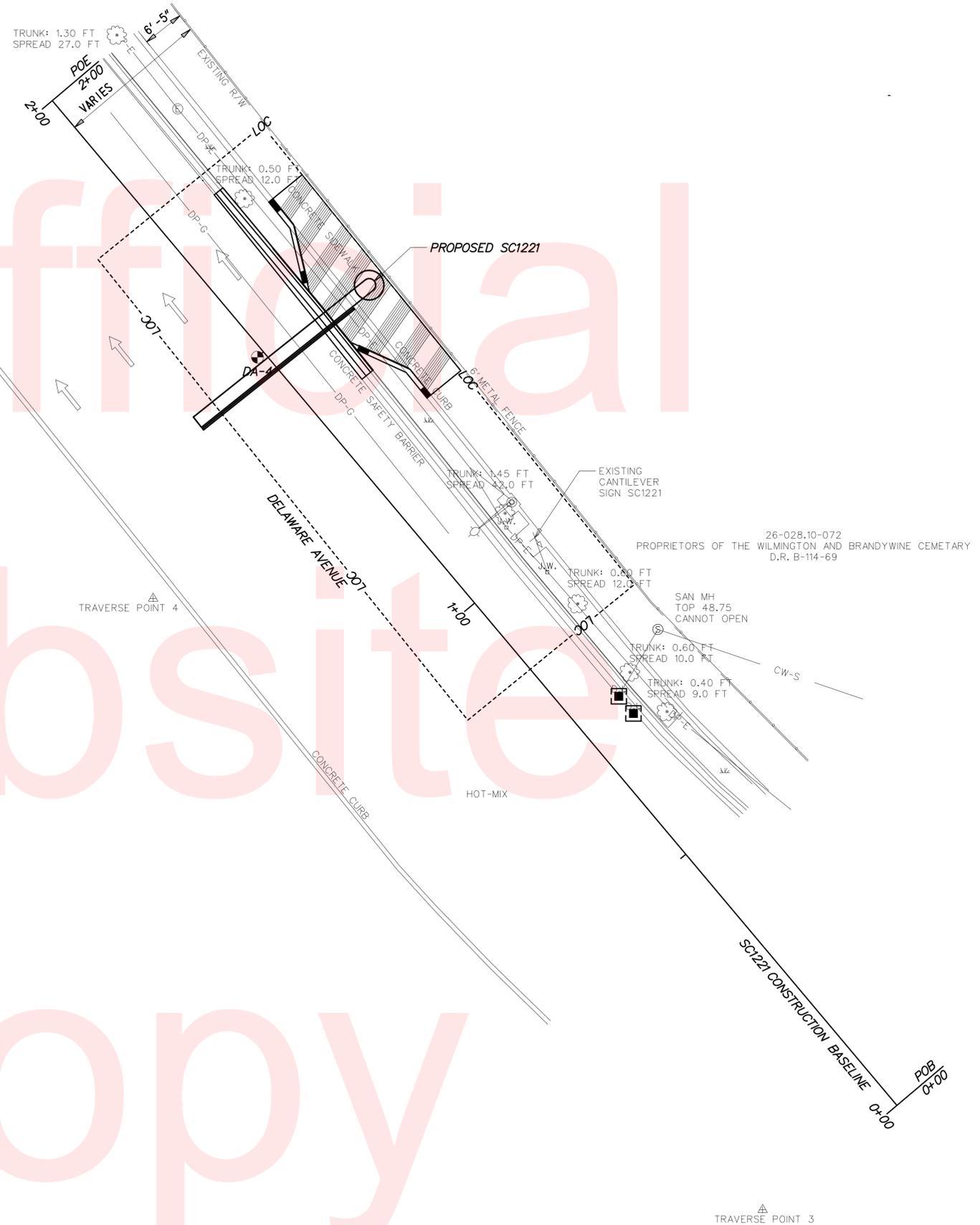
|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: JWS |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN SIGN STRUCTURE SC1206**

|             |    |
|-------------|----|
| SHEET NO.   | 47 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH TYPICAL APPLICATION 37 OF THE DELAWARE MUTCD.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN. NEAREST TWO DRAINAGE INLETS SOUTH OF THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED WITH ITEM 905004 - INLET SEDIMENT CONTROL, DRAINAGE INLET.
3. REMOVE CONCRETE SAFETY BARRIER, CONCRETE CURB, AND SIDEWALK TO THE EXTENTS SHOWN IN THE PLANS. INSTALL 12 FOOT LENGTH OF P.C.C. SAFETY BARRIER WITH TAPERED END AT UPSTREAM END OF EXPOSED BARRIER
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS. CONTRACTOR SHALL REMOVE ALL EXCAVATED MATERIAL AT THE END OF EACH WORK DAY.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING CONCRETE SAFETY BARRIER, CONCRETE CURB, AND SIDEWALK TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME CLOSURE HOURS ONLY.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER.
8. COMPLETE ALL REMAINING WORK INCLUDING ROADWAY REPAIR AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES AND REOPEN THE ROADWAY.



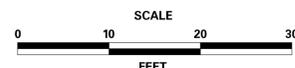
**CROSS-REFERENCE NOTES:**

1. FOR LIMITS OF CONCRETE SAFETY BARRIER, CONCRETE CURB AND SIDEWALK REMOVAL, REFER TO CONSTRUCTION PLAN, SHEET NO. 8.

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ADDENDUMS / REVISIONS



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

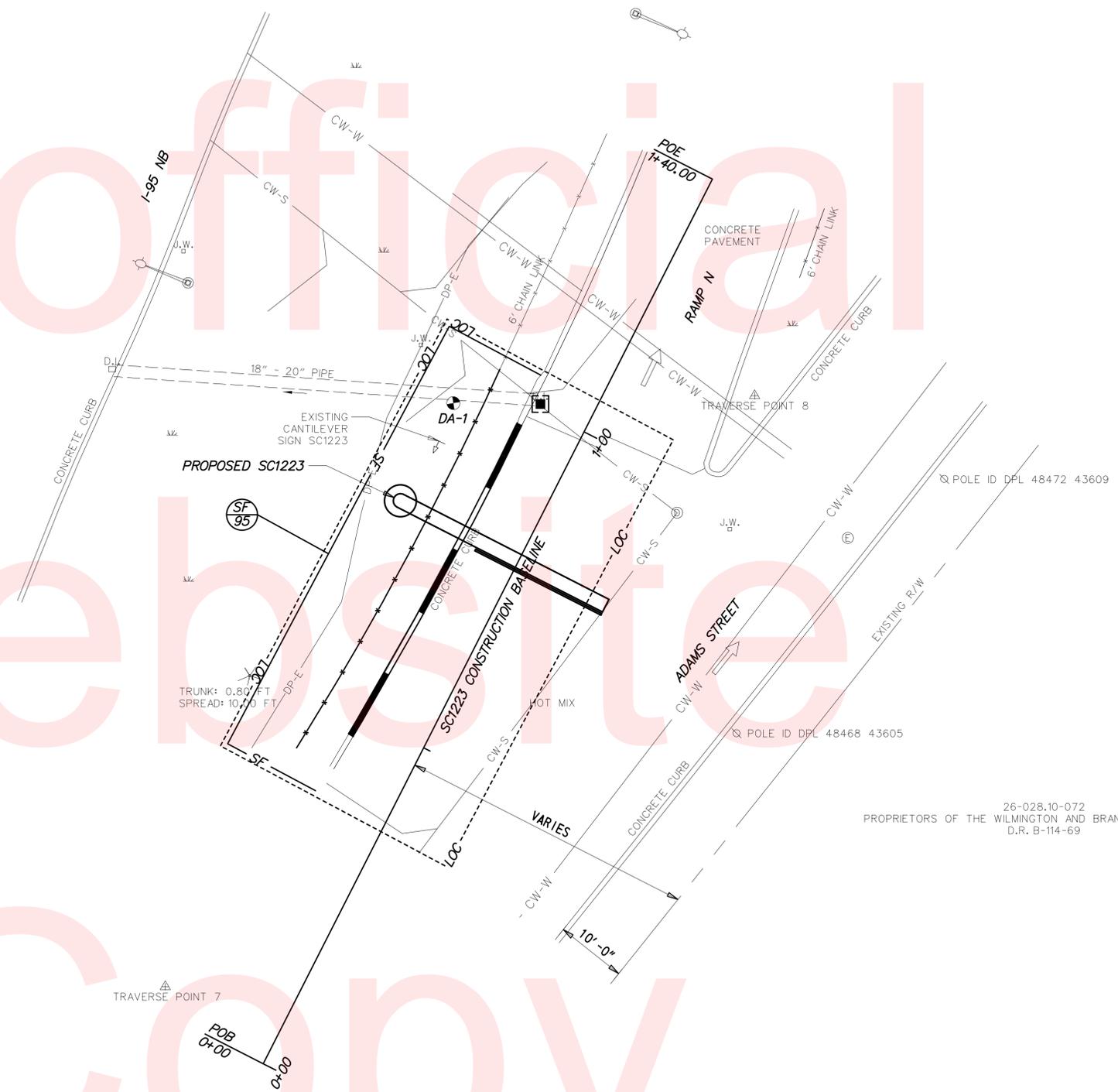
|            |                  |           |
|------------|------------------|-----------|
| CONTRACT   | BRIDGE NO.       | <b>NA</b> |
| T201407004 | DESIGNED BY: JWS |           |
| COUNTY     | CHECKED BY: DEF  |           |
| NEW CASTLE |                  |           |

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN SIGN STRUCTURE SC1221**

|             |    |
|-------------|----|
| SHEET NO.   | 48 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH THE DETOUR PLAN.
2. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN. NEAREST TWO DRAINAGE INLETS NORTH OF THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED WITH ITEM 905005 - INLET SEDIMENT CONTROL, CURB INLET.
3. REMOVE CHAIN LINK FENCE AND CONCRETE CURB TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING CHAIN LINK FENCE AND CONCRETE CURB TO MATCH EXISTING.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER.
7. COMPLETE ALL REMAINING WORK INCLUDING ROADWAY REPAIR AND GRADING.
8. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
9. REMOVE ALL MOT DEVICES AND REOPEN THE ROADWAY.



26-028.10-072  
 PROPRIETORS OF THE WILMINGTON AND BRANDYWINE CEMETARY  
 D.R. B-114-69

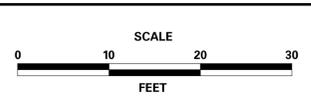
**CROSS-REFERENCE NOTES:**

1. FOR LIMITS OF CHAIN LINK FENCE AND CONCRETE CURB REMOVAL, REFER TO CONSTRUCTION PLAN, SHEET NO. 9.

8/28/2015 12:50:43 AM U:\425009\03 - Critical Cantilever\_Sgn\_Task\_3\_Ann1\_1673\Drawings\Plans\C504-SC1223.dgn

**DELAWARE**  
**DEPARTMENT OF TRANSPORTATION**

| ADDENDUMS / REVISIONS |
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|                       |



**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                  |            |
|------------|------------------|------------|
| CONTRACT   | BRIDGE NO.       | <b>N/A</b> |
| T201407004 | DESIGNED BY: RMB |            |
| COUNTY     | CHECKED BY: DEF  |            |
| NEW CASTLE |                  |            |

**CONSTRUCTION PHASING, M.O.T., AND EROSION CONTROL PLAN SIGN STRUCTURE SC1223**

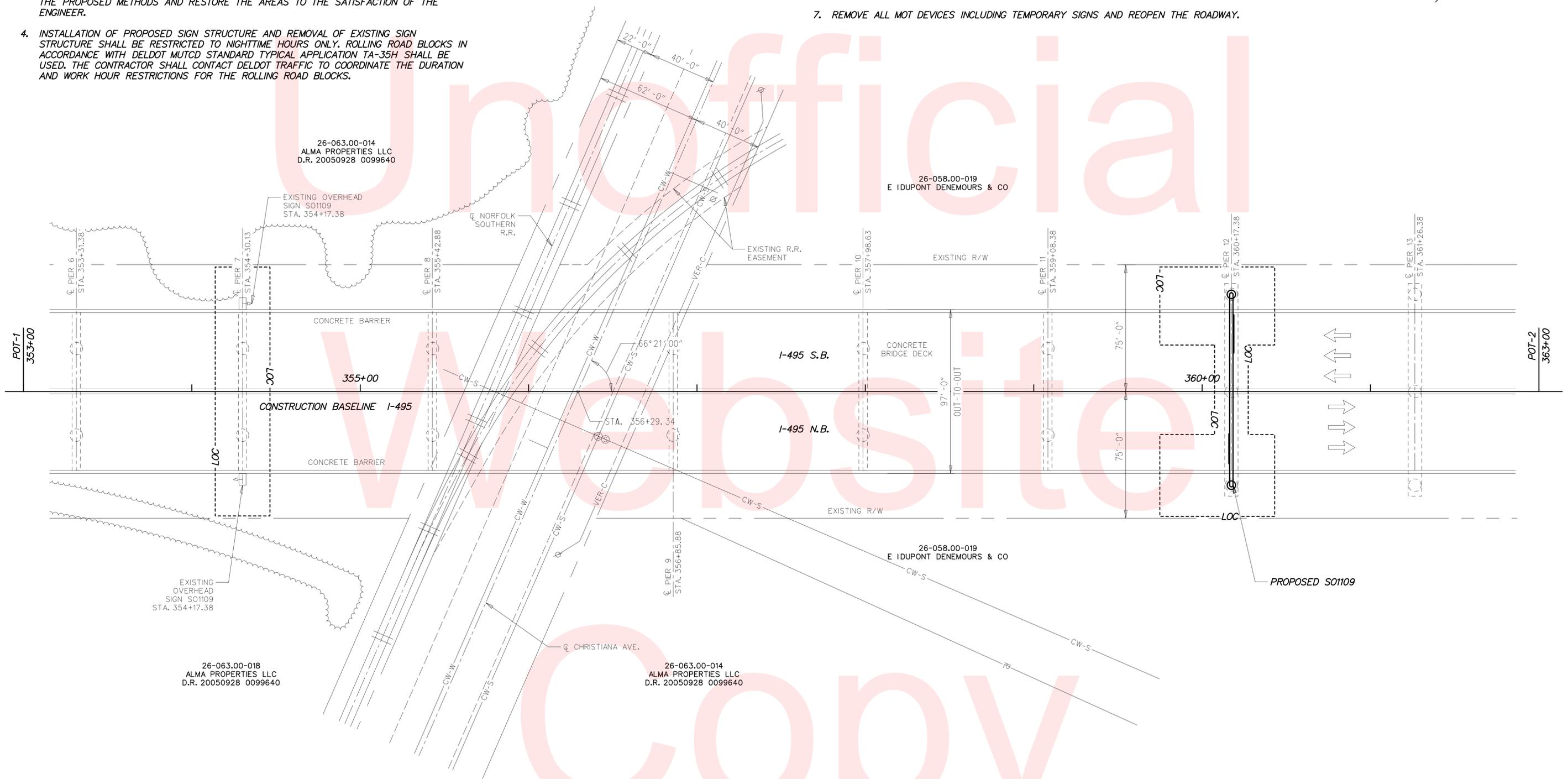
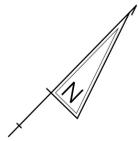
|             |    |
|-------------|----|
| SHEET NO.   | 49 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR LANE CLOSURES, USE TA-33(B).
2. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE AND REMOVING EXISTING SIGN STRUCTURE. CONTRACTOR TO PERFORM ALL CONSTRUCTION ACTIVITIES FROM THE BRIDGE DECK.
3. UPON COMPLETION OF THE WORK THE CONTRACTOR SHALL REMOVE ALL ITEMS USED IN THE PROPOSED METHODS AND RESTORE THE AREAS TO THE SATISFACTION OF THE ENGINEER.
4. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.

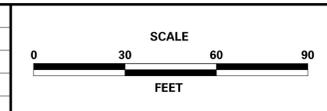
**SEQUENCE OF CONSTRUCTION (CONT.)**

5. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
6. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
7. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



8/28/2015 12:51:31 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\C505-S01109.dgn

| ADDENDUMS / REVISIONS |
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

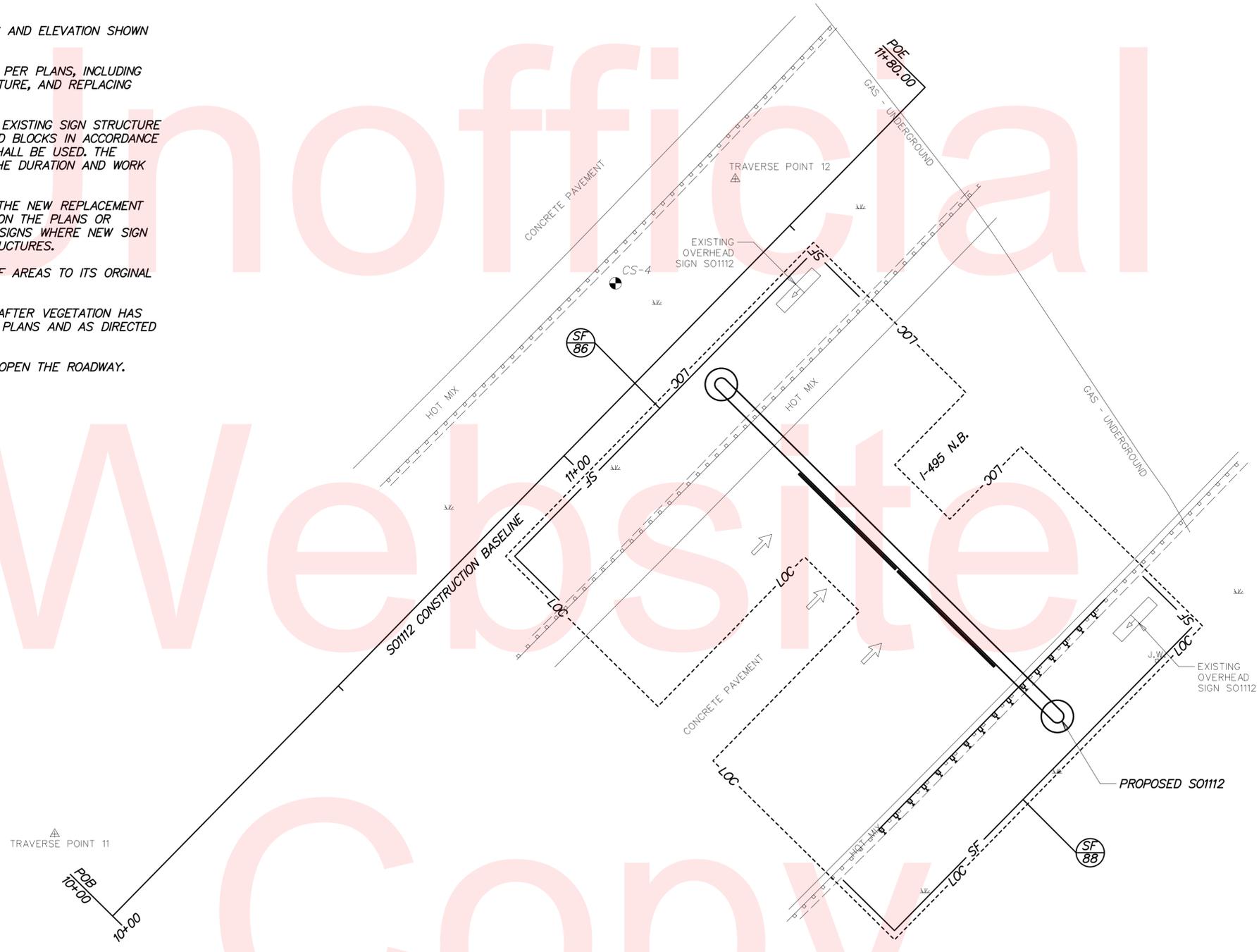
|            |                 |            |
|------------|-----------------|------------|
| CONTRACT   | BRIDGE NO.      | <b>N/A</b> |
| T201407004 | DESIGNED BY: PM |            |
| COUNTY     | CHECKED BY: DEF |            |
| NEW CASTLE |                 |            |

**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE S01109**

|             |    |
|-------------|----|
| SHEET NO.   | 50 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59. FOR CONSTRUCTION ACTIVITIES PERFORMED IN THE MEDIAN OF THE ROADWAY, WORK MUST BE PERFORMED BEHIND THE GUARDRAIL IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-1.
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.

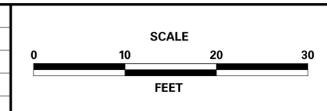


**CROSS REFERENCE NOTES:**  
 1. FOR LIMITS OF GUARDRAIL REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 11.

8/28/2015 12:52:09 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc I-495\_Task2\_Amnt 1673\Drawings\Plans\C506-S0112.dgn

**DELAWARE**  
**DEPARTMENT OF TRANSPORTATION**

| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

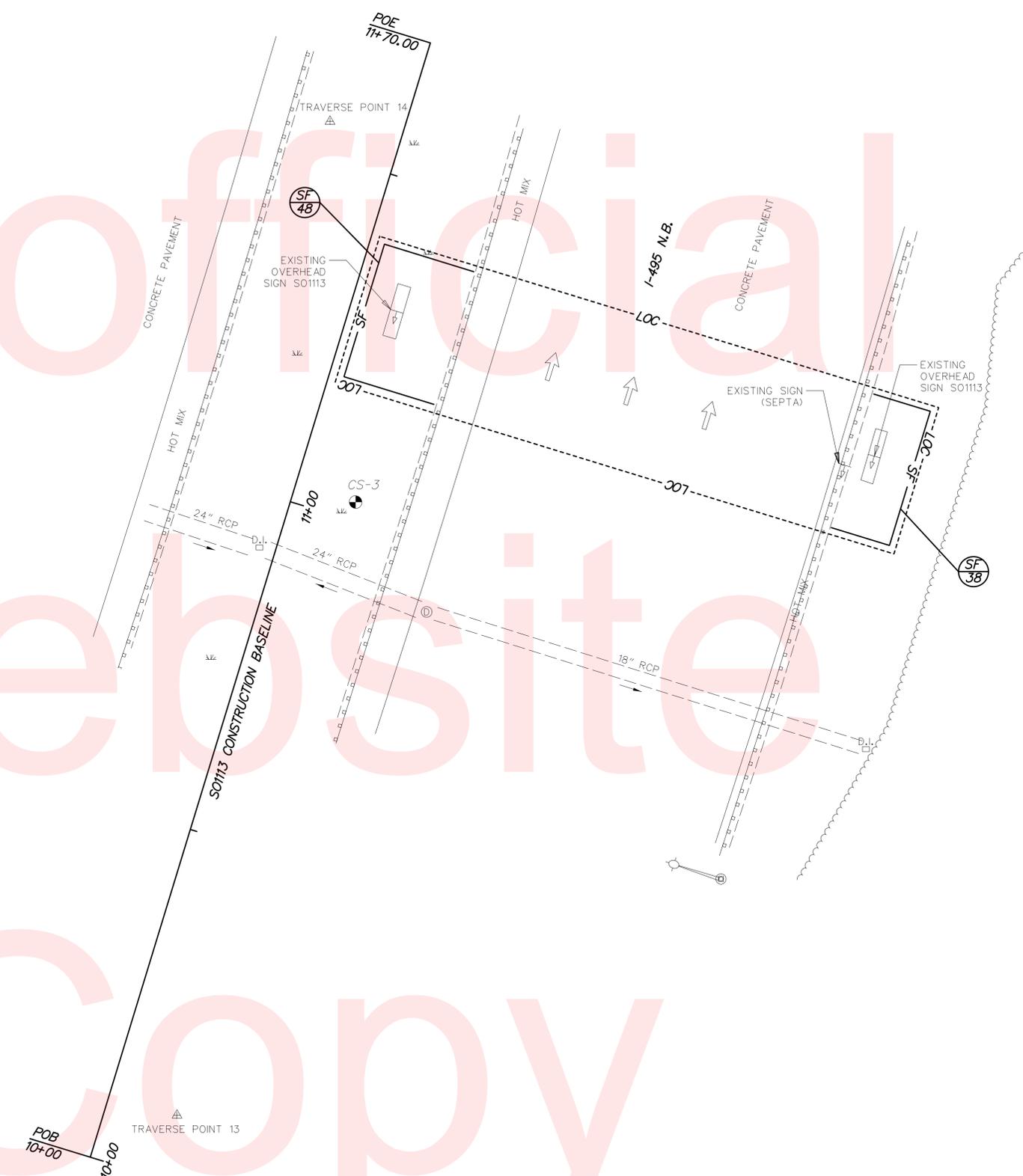
|            |                 |           |
|------------|-----------------|-----------|
| CONTRACT   | BRIDGE NO.      | <b>NA</b> |
| T201407004 | DESIGNED BY: PM |           |
| COUNTY     | CHECKED BY: DEF |           |
| NEW CASTLE |                 |           |

**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE S0112**

|             |    |
|-------------|----|
| SHEET NO.   | 51 |
| TOTAL SHTS. | 83 |

**SEQUENCE OF CONSTRUCTION**

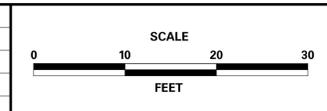
1. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
2. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES PERFORMED IN THE MEDIAN OF THE ROADWAY, WORK MUST BE PERFORMED BEHIND THE GUARDRAIL IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-1.
3. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
4. RELOCATE EXISTING SEPTA SIGN 10' SOUTH ALONG GUARDRAIL PRIOR TO REMOVING EXISTING SIGN STRUCTURE SO1113.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING REMOVING EXISTING SIGN STRUCTURE AND RELOCATING THE EXISTING SEPTA SIGN.
6. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
7. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



Official Website Copy

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| ADDENDUMS / REVISIONS |
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

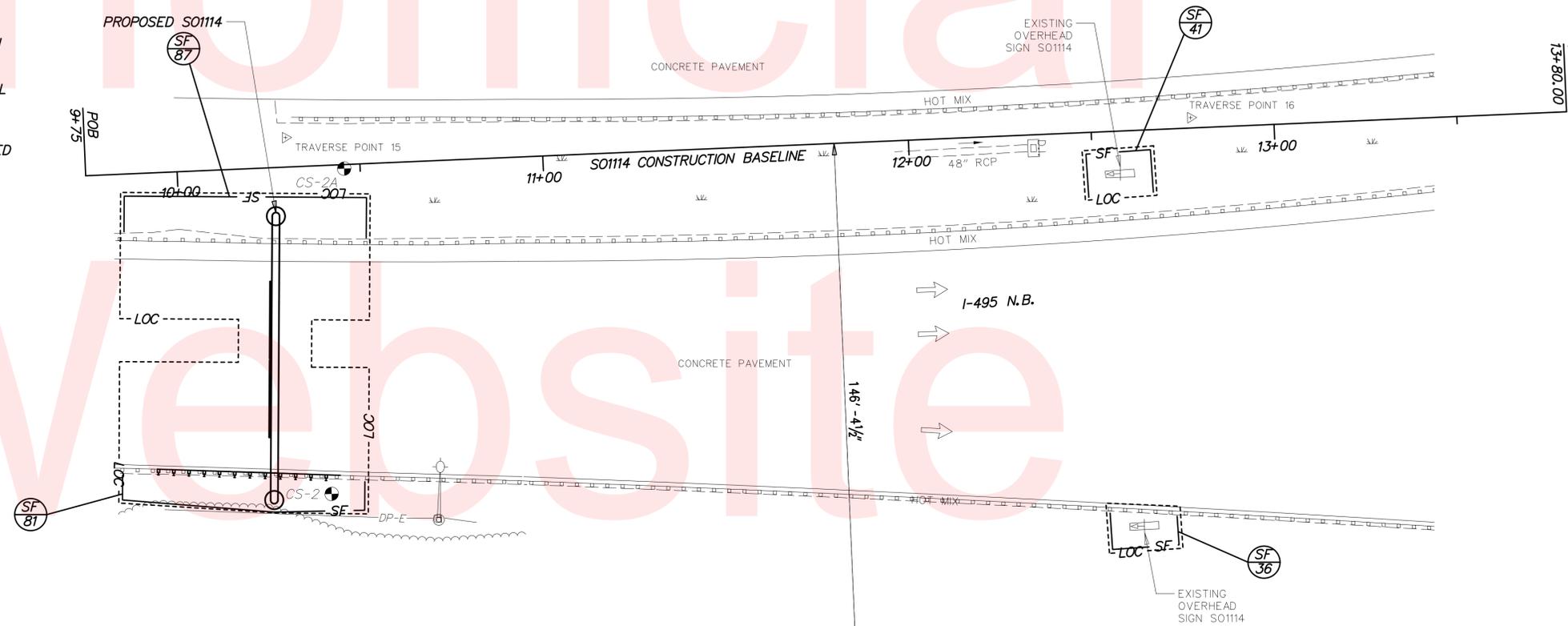
|            |                  |     |
|------------|------------------|-----|
| CONTRACT   | BRIDGE NO.       | N/A |
| T201407004 | DESIGNED BY: IZB |     |
| COUNTY     | CHECKED BY: DEF  |     |
| NEW CASTLE |                  |     |

**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE SO1113**

|             |    |
|-------------|----|
| SHEET NO.   | 52 |
| TOTAL SHTS. | 83 |

SEQUENCE OF CONSTRUCTION

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59. FOR CONSTRUCTION ACTIVITIES PERFORMED IN THE MEDIAN OF THE ROADWAY, WORK MUST BE PERFORMED BEHIND THE GUARDRAIL IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-1.
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



06-072.00-066  
FIRKO HARRY J SR & NANCY L  
D.R. 20040525 0033477

06-072.00-054  
STATE OF DELAWARE  
DIV. OF HISTORICAL & CULTURAL AFFAIRS  
D.R. 073 631

06-072.00-055  
MCNULTY ROBERT WAYNE SR  
D.R. 20140703 0028191

06-072.00-061  
LS HEFFERNAN LIMITED PARTNERSHIP  
D.R. 20110927 0058988

ROBERTS STREET  
40'-0"

**CROSS REFERENCE NOTES:**

1. FOR LIMITS OF GUARDRAIL REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 13.



| ADDENDUMS / REVISIONS |  |
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                 |           |
|------------|-----------------|-----------|
| CONTRACT   | BRIDGE NO.      | <b>NA</b> |
| T201407004 | DESIGNED BY: PM |           |
| COUNTY     | CHECKED BY: DEF |           |
| NEW CASTLE |                 |           |

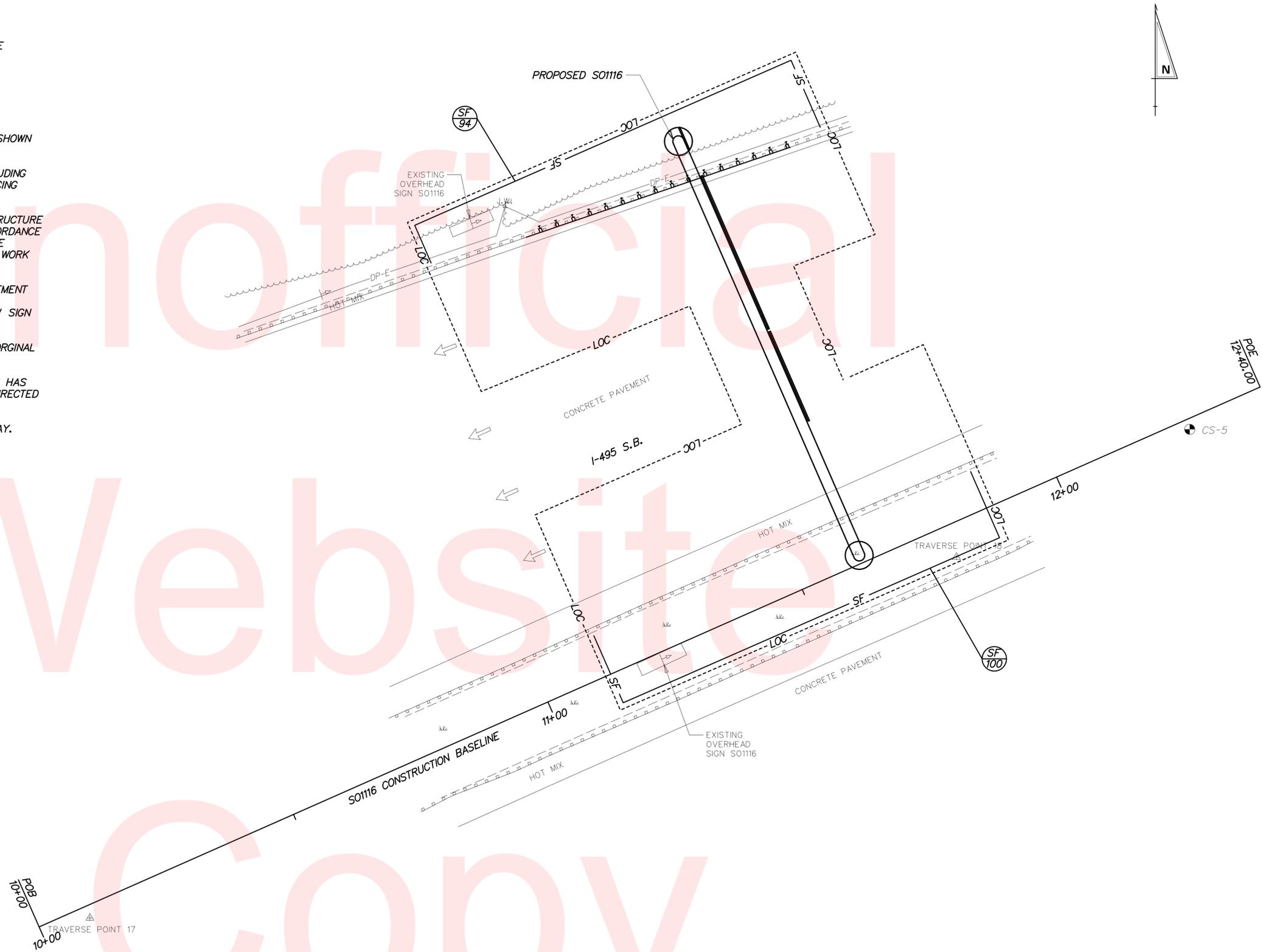
**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE SO1114**

|             |    |
|-------------|----|
| SHEET NO.   | 53 |
| TOTAL SHTS. | 83 |

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**SEQUENCE OF CONSTRUCTION**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59. FOR CONSTRUCTION ACTIVITIES PERFORMED IN THE MEDIAN OF THE ROADWAY, WORK MUST BE PERFORMED BEHIND THE GUARDRAIL IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-1.
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



**CROSS REFERENCE NOTES:**

1. FOR LIMITS OF GUARDRAIL REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 14.

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| ADDENDUMS / REVISIONS |  |
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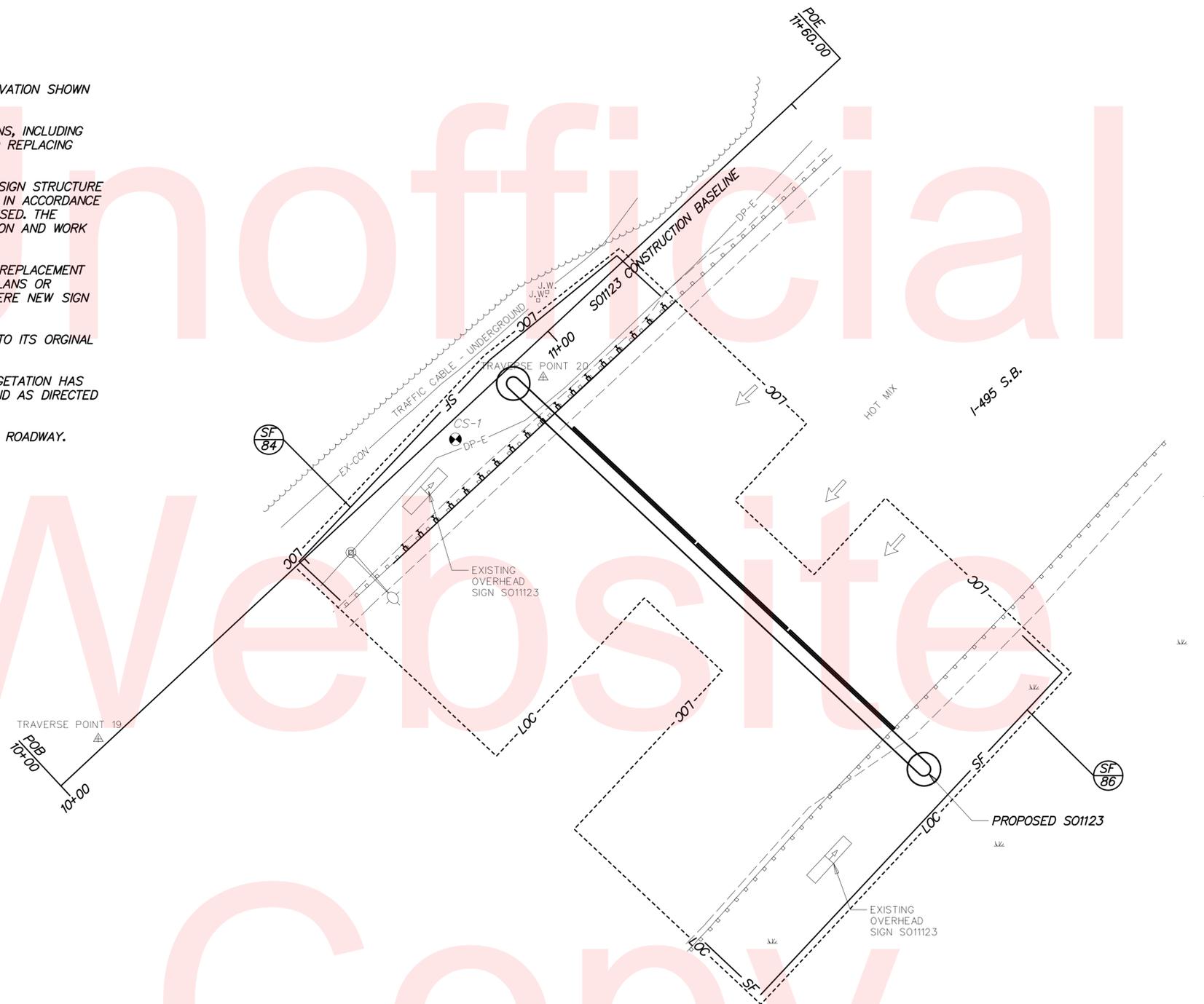
**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                 |            |
|------------|-----------------|------------|
| CONTRACT   | BRIDGE NO.      | <b>N/A</b> |
| T201407004 | DESIGNED BY: PM |            |
| COUNTY     | CHECKED BY: DEF |            |
| NEW CASTLE |                 |            |

|  |             |
|--|-------------|
| <b>CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE SO1116</b> | SHEET NO.   |
|  | 54          |
|  | TOTAL SHTS. |
|  | 83          |

**SEQUENCE OF CONSTRUCTION**

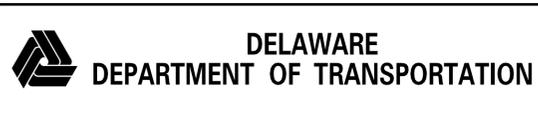
1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARDS. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDERS, USE SHOULDER CLOSURE DETAIL ON SHEET 59. FOR CONSTRUCTION ACTIVITIES PERFORMED IN THE MEDIAN OF THE ROADWAY, WORK MUST BE PERFORMED BEHIND THE GUARDRAIL IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-1.
2. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS SHOWN.
3. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS.
4. PERFORM EXCAVATIONS FOR DRILLED SHAFT TO THE DIMENSIONS AND ELEVATION SHOWN IN THE PLANS.
5. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
6. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
7. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF SIGNS ON EXISTING SIGN STRUCTURES.
8. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREAS TO ITS ORIGINAL CONDITIONS, ROADWAY REPAIR, AND GRADING.
9. REMOVE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER VEGETATION HAS STABILIZED ALL DISTURBED AREAS IN ACCORDANCE WITH THESE PLANS AND AS DIRECTED BY THE ENGINEER.
10. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN THE ROADWAY.



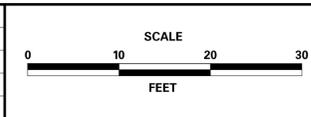
**CROSS REFERENCE NOTES:**

1. FOR LIMITS OF GUARDRAIL REMOVAL, REFER TO CONSTRUCTION PLAN SHEET NO. 15.

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| ADDENDUMS / REVISIONS |  |
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**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |                 |            |
|------------|-----------------|------------|
| CONTRACT   | BRIDGE NO.      | <b>N/A</b> |
| T201407004 | DESIGNED BY: PM |            |
| COUNTY     | CHECKED BY: DEF |            |
| NEW CASTLE |                 |            |

**CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN SIGN STRUCTURE S01123**

|             |    |
|-------------|----|
| SHEET NO.   | 55 |
| TOTAL SHTS. | 83 |

**PORTABLE CHANGEABLE MESSAGE SIGNS**

**PRIOR TO DETOUR**  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

RAMP TO I-95 SB CLOSING

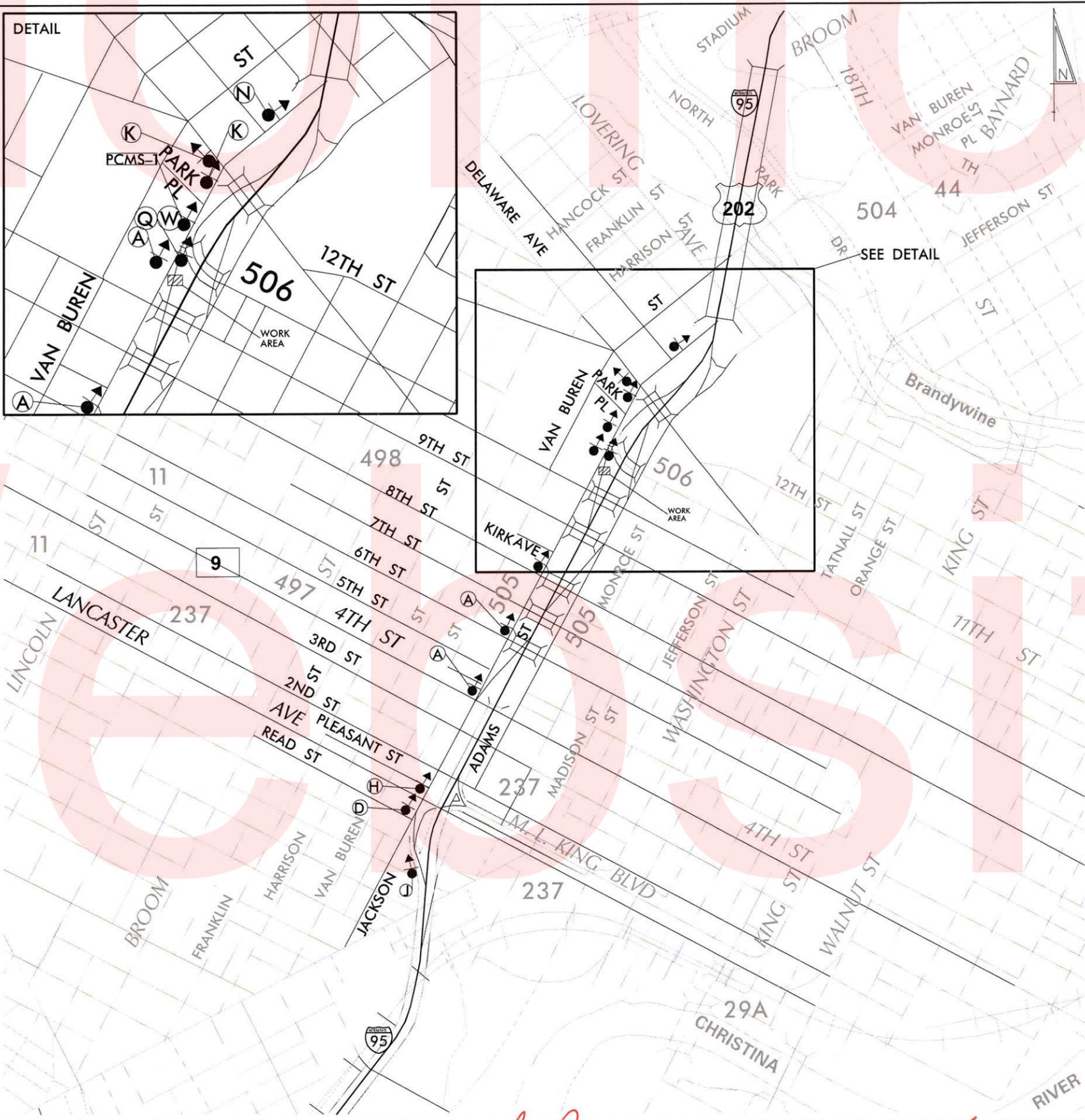
STARTING XX/XX/XX

**DURING DETOUR**  
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF DETOUR)

PCMS-1

RAMP TO I-95 SB CLOSED

FOLLOW DETOUR



**LEGEND**

A: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M6-3

B: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M6-1(L)

C: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M6-1(R)

D: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M6-2(L)

E: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M6-2(R)

F: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M5-1(L)

G: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M5-1(R)

H: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M5-2(L)

I: DETOUR SOUTH I-95 M4-8 M3-3 M1-1 M5-2(R)

J: END DETOUR SOUTH I-95 M4-8 M3-3 M1-1

K: DETOUR AHEAD W20-2

L: DETOUR 1000 FT W20-2

M: DETOUR 500 FT W20-2

N: ROAD CLOSED AHEAD W20-3

O: ROAD CLOSED 1000 FT W20-3

P: ROAD CLOSED 500 FT W20-3

Q: ROAD CLOSED R11-2

R: DETOUR M4-10(L)

S: DETOUR M4-10(R)

T: ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY R11-3a

U: BRIDGE OUT XX MILES AHEAD LOCAL TRAFFIC ONLY R11-3b

V: ROAD CLOSED TO THRU TRAFFIC R11-4

W: BARRICADE

X: BARRICADE

**SPECIAL SIGNS**

48" x 72" sign with 6" C text and 24" I-95 shield.

RAMP TO I-95 SOUTH CLOSED FOLLOW DETOUR

\*BLACK LEGEND ON RETROREFLECTIVE FLUORESCENT ORANGE BACKGROUND

95 SHIELD DESIGN TO BE AS PER DE MUTCD STANDARDS

- GENERAL NOTES**
- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
  - THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS.(AS PER LATEST REVISION)
  - DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
  - SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
  - THE COLORS, DIMENSIONS, AND CHARACTERISTICS OF ALL INTERSTATE, U.S. ROUTE, AND STATE ROUTE SHIELD SIGNS SHALL BE IN ACCORDANCE WITH SECTION 2D.11 OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
  - SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
  - FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
  - SIGNS "N" THROUGH "Q" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE. SHEETING.
  - WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
  - "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
  - BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED Douglas E. Finney DATE: 8/17/2015 RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_ RECOMMENDED Joseph P. ... DATE: 8/18/15 APPROVED CHIEF SAFETY OFFICER ... DATE: 8-19-15 APPROVED TRAFFIC ENGINEER ... DATE: 8/19/15

DELAWARE DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

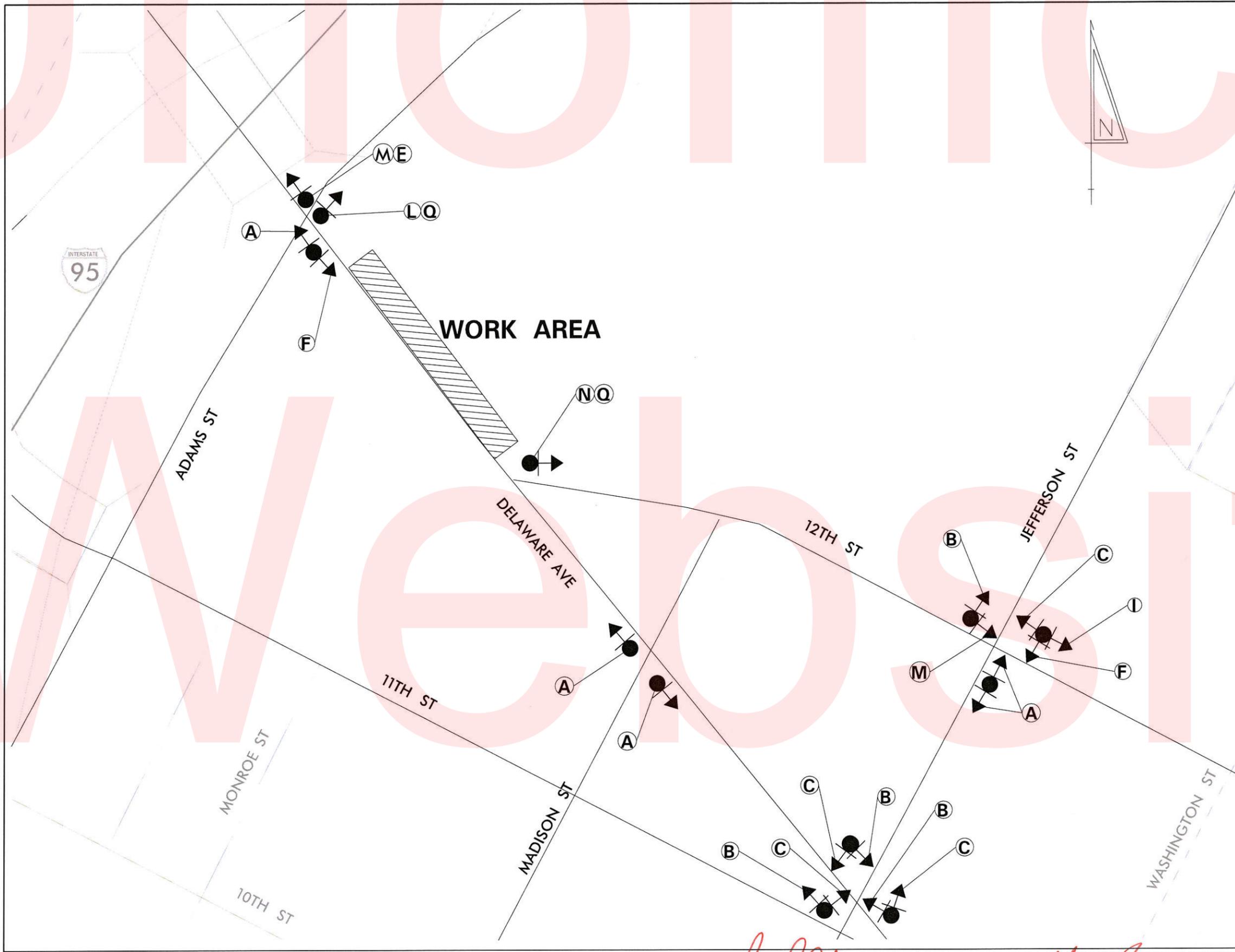
CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

VEHICULAR DETOUR PLAN

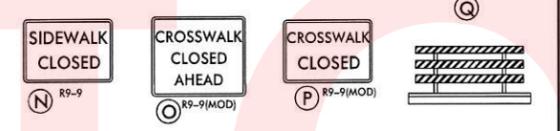
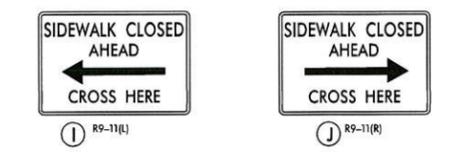
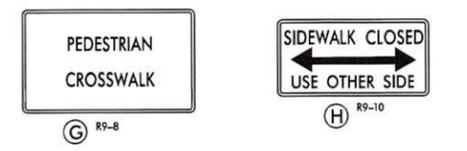
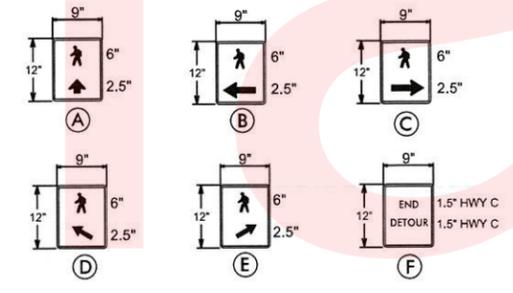
SC 1206

SHEET NO. 56

TOTAL SHTS. 83



**SIGN LEGEND**



**GENERAL NOTES**

1. ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
2. THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS (AS PER LATEST REVISION.)
3. DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
4. SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
5. SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
6. FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
7. WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
8. "O" BARRICADE SHALL COMPLETELY RUN THE FULL WIDTH OF SIDEWALK OR PEDESTRIAN PATH.
9. BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.
10. PEDESTRIAN DETOUR TRAILBLAZERS NOT ATTACHED TO BARRICADES ARE TO EITHER BE GROUND MOUNTED OR ATTACHED TO AN EXISTING SIGN POST AT THE LOCATION SHOWN ON THE PLAN.

RECOMMENDED Stephen P. Finney DATE: 8/20/2015      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED Paul Peitert DATE: 8/20/15      APPROVED CHIEF SAFETY OFFICER John W. Moore DATE: 8-21-15      APPROVED TRAFFIC ENGINEER [Signature] DATE: 8/24/15

**DELAWARE DEPARTMENT OF TRANSPORTATION**

| ADDENDUM / REVISIONS |  |
|----------------------|--|
|                      |  |
|                      |  |
|                      |  |

NOT TO SCALE

**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|            |              |     |
|------------|--------------|-----|
| CONTRACT   | PERMIT NO.   | N/A |
| T201407004 | DESIGNED BY: | JAZ |
| COUNTY     | CHECKED BY:  | JMC |
| NEW CASTLE |              |     |

**PEDESTRIAN DETOUR PLAN**  
SC1221

|             |    |
|-------------|----|
| SHEET NO.   | 57 |
| TOTAL SHTS. | 83 |

SDATES \$TIMES \$FILENAMES

**PORTABLE CHANGEABLE MESSAGE SIGNS**

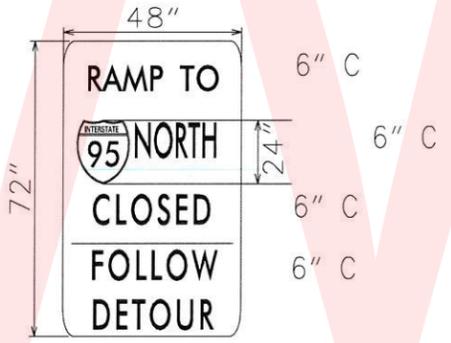
PRIOR TO DETOUR  
(10 DAYS PRIOR TO BEGINNING OF DETOUR)

PCMS-1

RAMP TO  
I-95 NB  
CLOSED

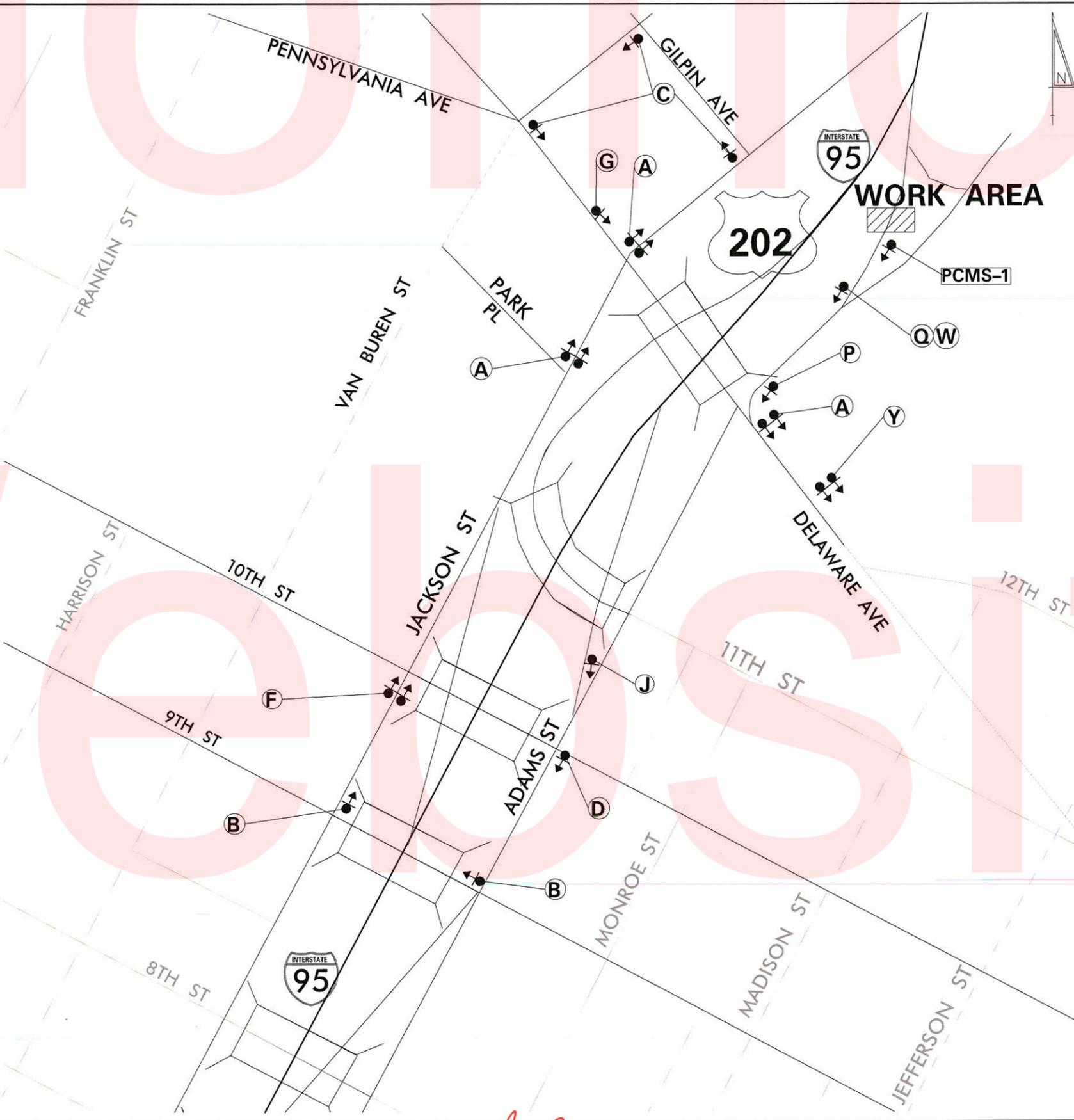
STARTING  
XX/XX/XX

**SPECIAL SIGNS**

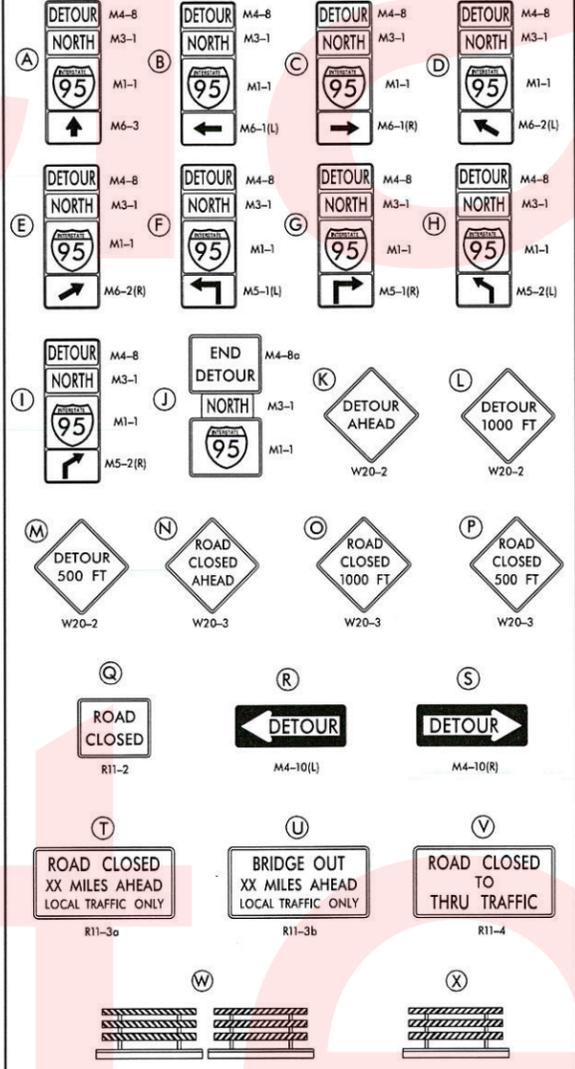


\*BLACK LEGEND ON RETROREFLECTIVE FLUORESCENT ORANGE BACKGROUND

SHIELD DESIGN TO BE AS PER DE MUTCD STANDARDS



**LEGEND**

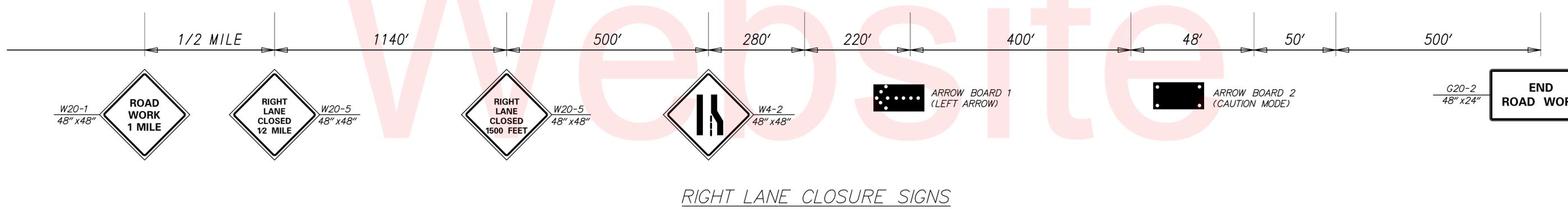
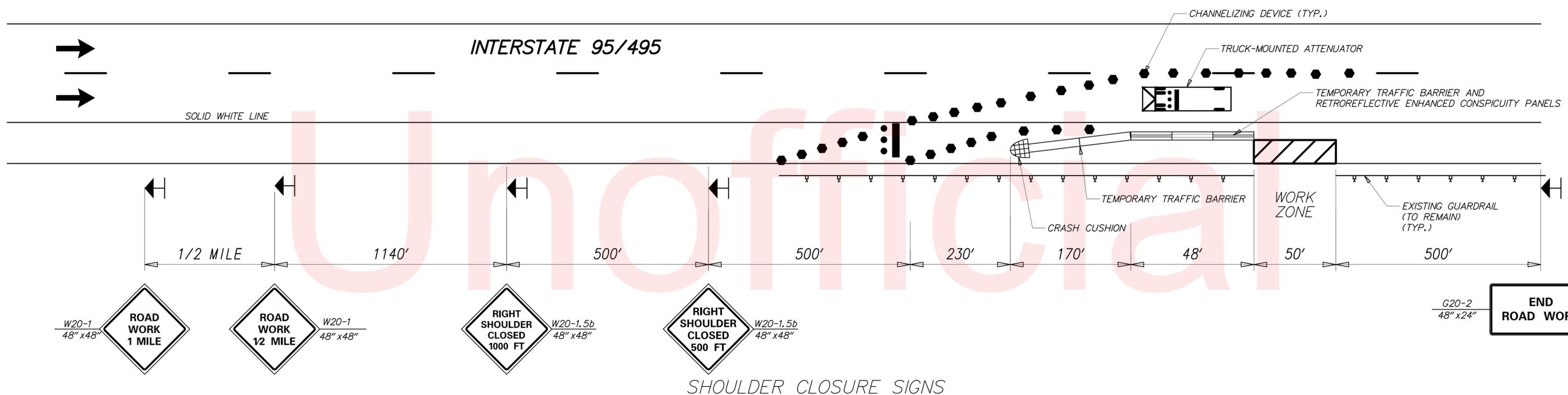


**GENERAL NOTES**

- ALL DETOUR SIGNING, INCLUDING TRAILBLAZERS, ARE TO BE SUPPLIED AND MAINTAINED BY THE GENERAL CONTRACTOR IN COMPLIANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
- THE CONTRACTOR SHALL COMPLY WITH GUIDELINES IN "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD PART 6) FOR BARRICADES AND SIGNS.(AS PER LATEST REVISION)
- DESIGN OF ALL SIGNS SHALL BE IN ACCORDANCE WITH THE FHWA STANDARD HIGHWAY SIGNS BOOK.
- SIZES OF ALL SIGNS SHALL BE IN ACCORDANCE WITH "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.) SIZE OF SIGN SHALL BE BASED ON TYPE OF ROADWAY ON WHICH THE SIGN IS INSTALLED.
- THE COLORS, DIMENSIONS, AND CHARACTERISTICS OF ALL INTERSTATE, U.S. ROUTE, AND STATE ROUTE SHIELD SIGNS SHALL BE IN ACCORDANCE WITH SECTION 2D.11 OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD.)
- SIGNS NO LONGER IN USE SHALL BE COMPLETELY COVERED WITH NO RETROREFLECTIVE MATERIAL SHOWING, OR SHALL BE REMOVED, AS DIRECTED BY THE ENGINEER.
- FIELD CONDITIONS MAY DICTATE CHANGES AT SOME TIME DURING THE LIFE OF THE CONTRACT. IN THE EVENT OF OMISSIONS OR CORRECTIONS, THE SIGNING PROVISIONS OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD) WILL PREVAIL.
- SIGNS "N" THROUGH "Q" AND "T" AND "V", THE WORD "ROAD" SHOULD BE CHANGED TO "RAMP", "RR XING", OR "BRIDGE" WHERE APPLICABLE.
- WARNING SIGNS AND DETOUR TRAILBLAZERS SHALL BE MOUNTED ON BREAKAWAY POSTS AND HAVE RETROREFLECTIVE FLUORESCENT ORANGE SHEETING.
- "W" BARRICADES SHALL COMPLETELY RUN THE FULL WIDTH OF THE ROADWAY.
- BARRICADES SHALL BE A MINIMUM OF 6 FEET WIDE UNLESS DIRECTED BY THE ENGINEER.

RECOMMENDED *Douglas E. Finney* DATE: 8/17/2015      RECOMMENDED \_\_\_\_\_ DATE: \_\_\_\_\_      RECOMMENDED *Joseph P. Del* DATE: 8/18/15      APPROVED CHIEF SAFETY OFFICER *W. J. Masje* DATE: 8-19-15      APPROVED TRAFFIC ENGINEER *Shel* DATE: 8/19/15

|  |                      |  |              |  |            |                 |                       |                  |             |
|--|----------------------|--|--------------|--|------------|-----------------|-----------------------|------------------|-------------|
|  | ADDENDUM / REVISIONS |  | NOT TO SCALE | CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18 | CONTRACT   | PERMIT NO.      | VEHICULAR DETOUR PLAN | SHEET NO.        |             |
|  |                      |  |              |  | T201407004 | N/A             |                       | DESIGNED BY: JAZ | 58          |
|  |                      |  |              |  | NEW CASTLE | CHECKED BY: JMC |                       | SC 1223          | TOTAL SHTS. |

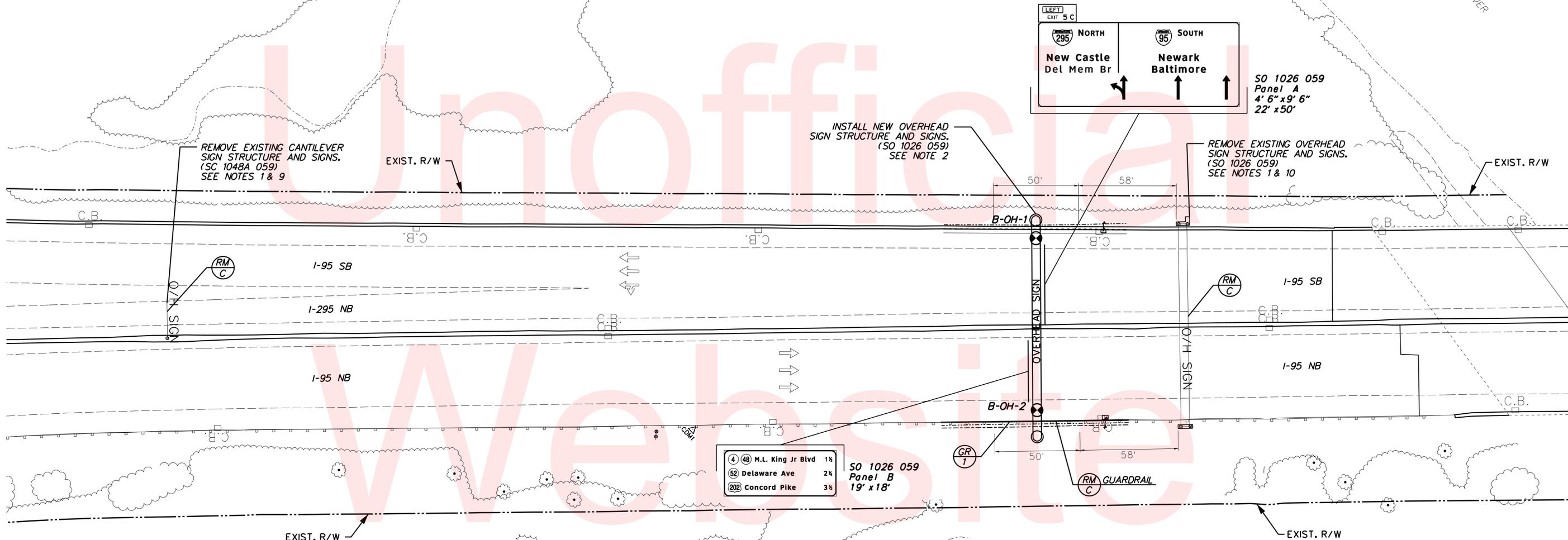
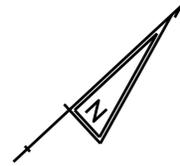


**NOTES:**

1. ALL SIGNING SHALL COMPLY WITH THE "DELAWARE MANUAL ON UNIFORM TRAFFIC DEVICES" (DE MUTCD)
2. ONLY ONE SET OF SIGNS, SHOULDER CLOSURE OR LANE CLOSURE, SHALL BE VISIBLE AT ANY TIME. SIGNS FOR THE CLOSURE TYPE NOT IN USE SHALL BE COMPLETELY COVERED, WITH NO RETROREFLECTIVE MATERIAL SHOWING.
3. WHEN AN ENTRANCE RAMP INTERSECTS THE SIGNED AREA, ADDITIONAL SIGNS OR TRAFFIC CONTROL DEVICES MAY BE WARRANTED ON THE RAMP, AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS WORK WITH OTHER CONTRACTORS IN THE AREA.

8/28/2015 12:55:45 AM T:\Dover\Projects\2014\42-5009.02 - Overhead\_Sign\_Strc\_L-495\_Task2\_Amtt\_1673\Drawings\Plans\DOT1-MPT.dgn

|  |                       |                     |   |            |                  |             |                                |           |
|--|-----------------------|---------------------|---|------------|------------------|-------------|--------------------------------|-----------|
| <b>DELAWARE DEPARTMENT OF TRANSPORTATION</b> | ADDENDUMS / REVISIONS | <b>NOT TO SCALE</b> | <b>CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18</b> | CONTRACT   | BRIDGE NO.       | <b>NA</b>   | <b>SHOULDER CLOSURE DETAIL</b> | SHEET NO. |
|  |                       |                     |   | T201407004 | DESIGNED BY: JAZ | 59          |                                |           |
|  |                       |                     |   | COUNTY     | CHECKED BY: NR   | TOTAL SHTS. |                                |           |
|  |                       |                     |   | NEW CASTLE |                  | 83          |                                |           |



**SEQUENCE OF CONSTRUCTION:**

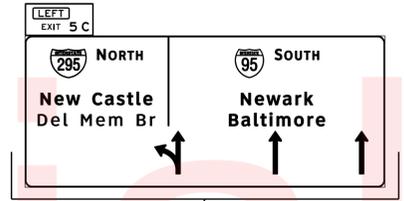
1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDER, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
3. PERFORM EXCAVATIONS FOR DRILLED SHAFT.
4. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
5. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF EXISTING SIGN STRUCTURES.
7. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.

**NOTES:**

9. EXISTING BARRIER MOUNTED CANTILEVER SIGN STRUCTURE (SC 1048A 059) TO BE REMOVED. ANCHOR BOLTS SHALL BE CUT FLUSH WITH BARRIER.
10. EXISTING GROUND MOUNTED OVERHEAD SIGN STRUCTURE (SO 1026 059) TO BE REMOVED. FOUNDATIONS SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
11. SOIL BORING SCHEDULE TO BE COMPLETED BY THE DEPARTMENT CONTRACTOR.

|     |                   |    |
|-----|-------------------|----|
| 4   | M.L. King Jr Blvd | 1% |
| 52  | Delaware Ave      | 2% |
| 202 | Concord Pike      | 3% |

SO 1026 059  
Panel B  
19' x 18'



INSTALL NEW OVERHEAD SIGN STRUCTURE AND SIGNS. (SO 1026 059) SEE NOTE 2

REMOVE EXISTING OVERHEAD SIGN STRUCTURE AND SIGNS. (SO 1026 059) SEE NOTES 1 & 10

REMOVE EXISTING CANTILEVER SIGN STRUCTURE AND SIGNS. (SC 1048A 059) SEE NOTES 1 & 9

| NO. | DESCRIPTION |
|-----|-------------|
| 1   | -           |
| 2   | -           |

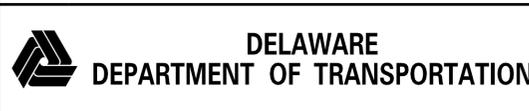
\*SEE NOTE 11

| NO. | ITEM DESCRIPTION / TYPE         | LENGTH |
|-----|---------------------------------|--------|
| 1   | STEEL BEAM GUARDRAIL, TYPE 2-27 | 50.00' |

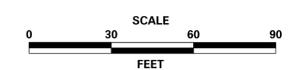
\*25' OFF CENTERLINE OF STRUCTURE

| NO. | ITEM DESCRIPTION / TYPE | LENGTH |
|-----|-------------------------|--------|
| 1   | P. C. C. SAFETY BARRIER | 50.00' |

9/4/2015 2:24:35 PM \\BALSRV01\2012\12079\_DELI610\TASKS\TASK 7 LEFT EXIT SIGN DESIGN SERVICES\CADD\PLANS\CP60-LEFT\_EXIT.DGN



| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |

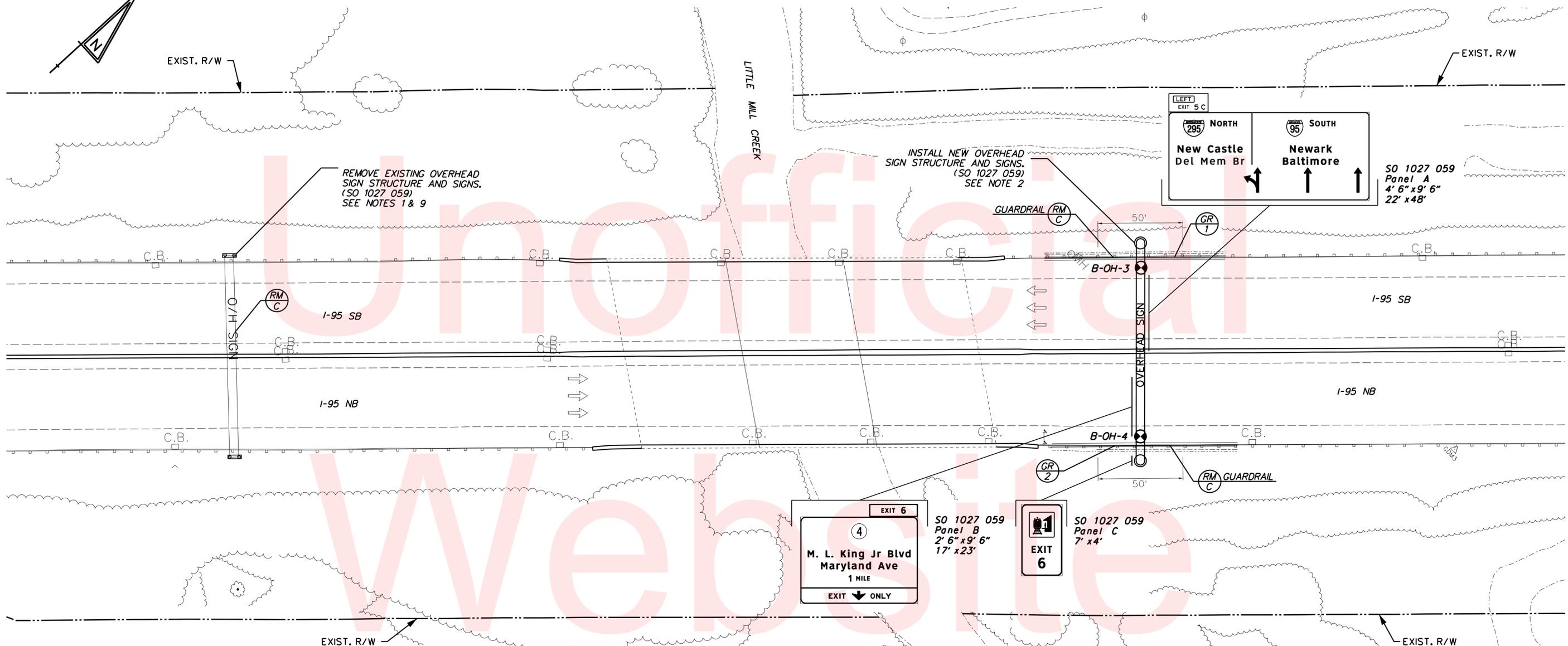
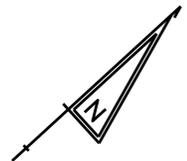


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                   |
|------------------------|-------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.        |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K |
|                        | CHECKED BY: JCR   |

**CONSTRUCTION / SSC / BORING LOCATION PLANS**

|                   |
|-------------------|
| CP-01             |
| SHEET NO.<br>60   |
| TOTAL SHTS.<br>83 |



**SEQUENCE OF CONSTRUCTION:**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDER, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
3. PERFORM EXCAVATIONS FOR DRILLED SHAFT.
4. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
5. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF EXISTING SIGN STRUCTURES.
7. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.

**NOTES:**

9. EXISTING GROUND MOUNTED OVERHEAD SIGN STRUCTURE (SO 1027 059) TO BE REMOVED. FOUNDATIONS SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
10. SOIL BORING SCHEDULE TO BE COMPLETED BY THE DEPARTMENT CONTRACTOR.

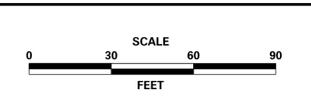
| GUARDRAIL SCHEDULE |                                 |        |
|--------------------|---------------------------------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 50.00' |
| 2                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 50.00' |

\*25' OFF CENTERLINE OF STRUCTURE

| SOIL BORING SCHEDULE |             |
|----------------------|-------------|
| NO.                  | DESCRIPTION |
| 3                    | -           |
| 4                    | -           |

\*SEE NOTE 10

| ADDENDUMS / REVISIONS |
|-----------------------|
|                       |
|                       |
|                       |

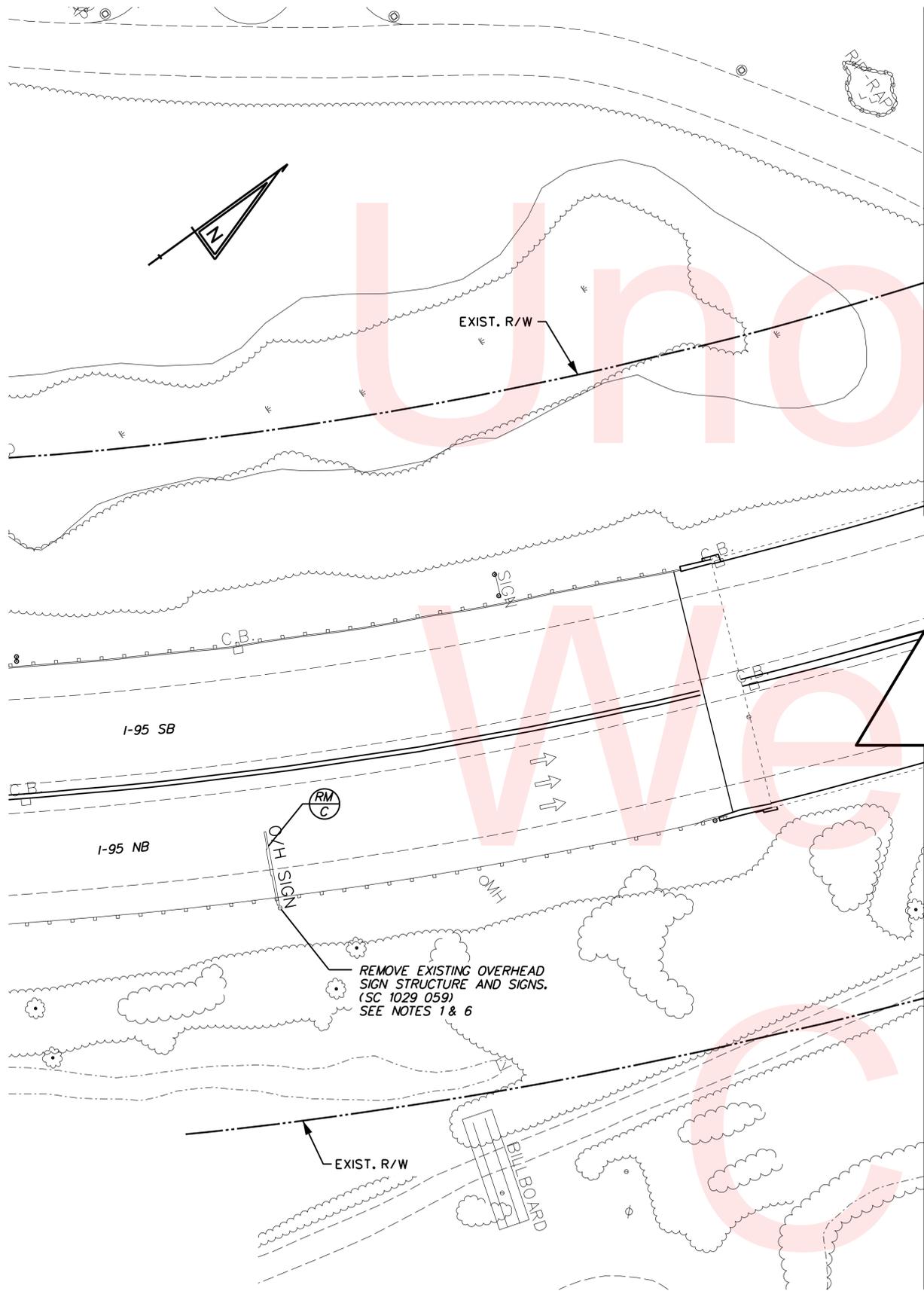


|                        |                   |
|------------------------|-------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.        |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K |
|                        | CHECKED BY: JCR   |

9/4/2015 2:16:42 PM \\BALSRV01\2012\12079\DEL1610\TASKS\TASK 7 LEFT EXIT SIGN DESIGN SERVICES\CADD\PLANS\CP61\LEFT EXIT.DGN

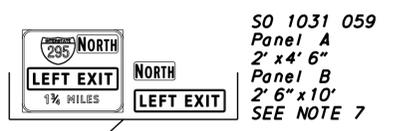


9/4/2015 2:32:27 PM \\BALSRV01\2012\2012\12079\_DE1610\TASKS\TASK\_7\_LEFT\_EXIT\_SIGN\_DESIGN\_SERVICES\CADD\PLANS\CP63\_LEFT\_EXIT.DGN



**SEQUENCE OF CONSTRUCTION:**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING REMOVING EXISTING SIGN STRUCTURE.
3. REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
4. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
5. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.



1750'

I-95 NB

I-95 SB

I-95 NB OFF RAMP

SO 1031 059

BEECH ST

O/H SIGN

REMOVE EXISTING OVERHEAD SIGN STRUCTURE AND SIGNS. (SC 1029 059) SEE NOTES 1 & 6

EXIST. R/W

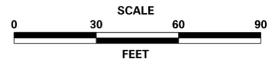
BILLBOARD

**NOTES:**

6. EXISTING GROUND MOUNTED CANTILEVER SIGN STRUCTURE (SC 1029 059) TO BE REMOVED. FOUNDATIONS SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
7. OVERLAY EXISTING SIGN. SEE SHEET 13 FOR OVERLAY INFORMATION.



| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
|                       |  |

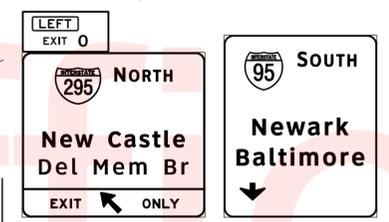
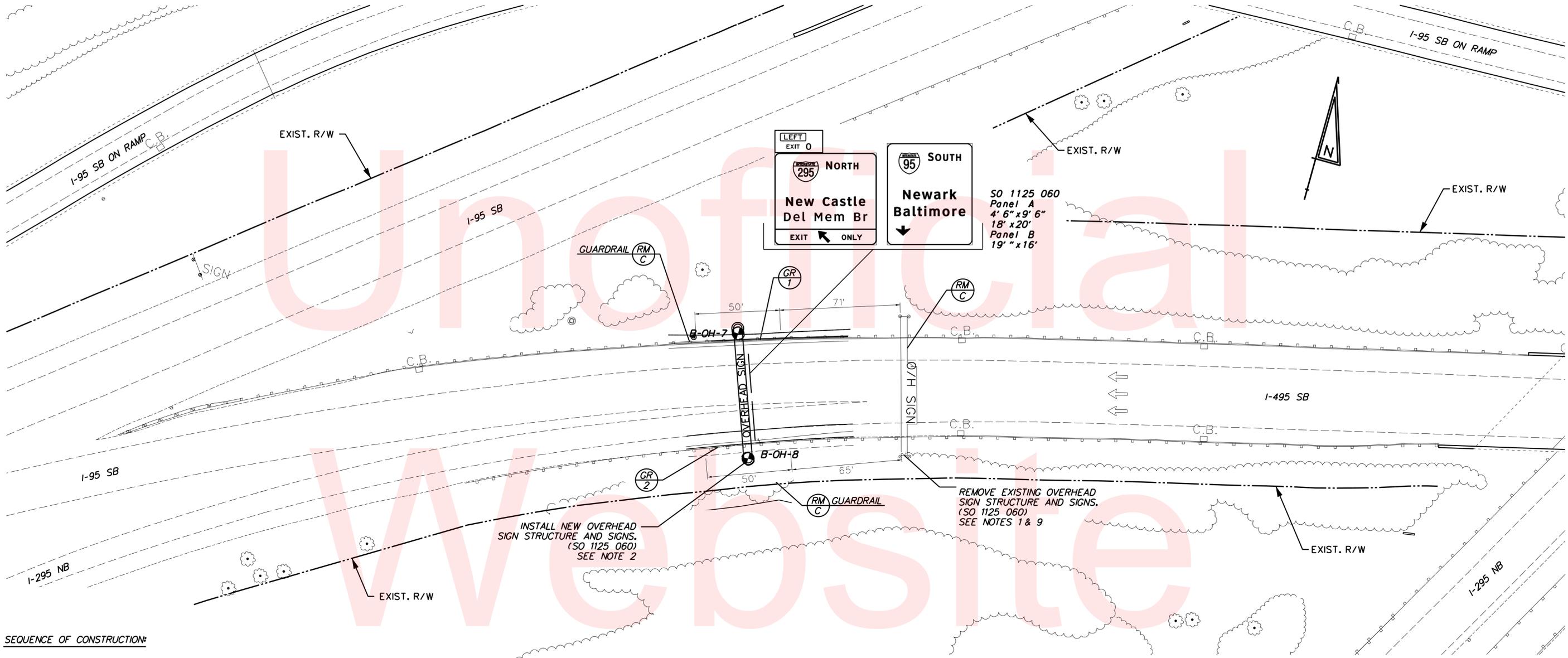


**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                   |
|------------------------|-------------------|
| CONTRACT<br>T201407004 | BRIDGE NO.        |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K |
|                        | CHECKED BY: JCR   |

**CONSTRUCTION / SSC / BORING LOCATION PLANS**

|                   |
|-------------------|
| CP-04             |
| SHEET NO.<br>63   |
| TOTAL SHTS.<br>83 |



**SEQUENCE OF CONSTRUCTION:**

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDER, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
3. PERFORM EXCAVATIONS FOR DRILLED SHAFT.
4. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.
5. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF EXISTING SIGN STRUCTURES.
7. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.

INSTALL NEW OVERHEAD SIGN STRUCTURE AND SIGNS. (SO 1125 060) SEE NOTE 2

REMOVE EXISTING OVERHEAD SIGN STRUCTURE AND SIGNS. (SO 1125 060) SEE NOTES 1 & 9

**NOTES:**

9. EXISTING GROUND MOUNTED OVERHEAD SIGN STRUCTURE (SO 1125 060) TO BE REMOVED. FOUNDATIONS SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
10. SOIL BORING SCHEDULE TO BE COMPLETED BY THE DEPARTMENT CONTRACTOR.

| GUARDRAIL SCHEDULE |                                 |        |
|--------------------|---------------------------------|--------|
| NO.                | ITEM DESCRIPTION / TYPE         | LENGTH |
| 1                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 50.00' |
| 2                  | STEEL BEAM GUARDRAIL, TYPE 2-27 | 50.00' |

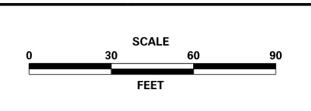
\*25' OFF CENTERLINE OF STRUCTURE

| SOIL BORING SCHEDULE |             |
|----------------------|-------------|
| NO.                  | DESCRIPTION |
| 7                    | -           |
| 8                    | -           |

\*SEE NOTE 10

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| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |



|          |            |              |      |
|----------|------------|--------------|------|
| CONTRACT | T201407004 | BRIDGE NO.   |      |
| COUNTY   | NEW CASTLE | DESIGNED BY: | RK&K |
|          |            | CHECKED BY:  | JCR  |

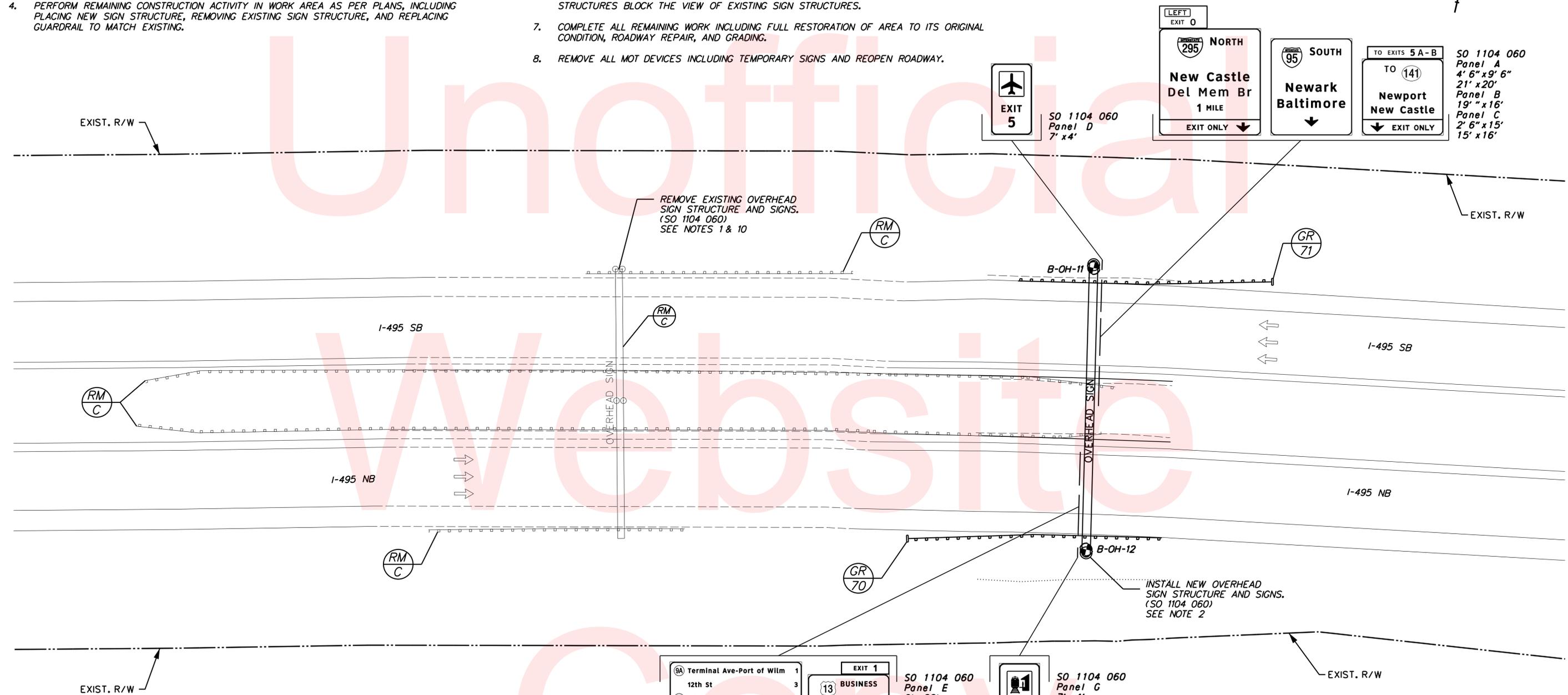


SEQUENCE OF CONSTRUCTION:

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER, FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDER, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
3. PERFORM EXCAVATIONS FOR DRILLED SHAFT.
4. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.

SEQUENCE OF CONSTRUCTION (CONT.):

5. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF EXISTING SIGN STRUCTURES.
7. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.



NOTES:

9. EXISTING GROUND MOUNTED OVERHEAD SIGN STRUCTURE (SO 1104 060) TO BE REMOVED. FOUNDATIONS SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
10. SOIL BORING SCHEDULE TO BE COMPLETED BY THE DEPARTMENT CONTRACTOR.

| SOIL BORING SCHEDULE |             |
|----------------------|-------------|
| NO.                  | DESCRIPTION |
| 11                   | -           |
| 12                   | -           |

\*SEE NOTE 10

| GUARDRAIL SCHEDULE |  |            |         |
|--------------------|--|------------|---------|
| NO.                | ITEM DESCRIPTION / TYPE                                  | BEGIN STA. | LENGTH  |
| 70                 | GUARDRAIL END TREATMENT ATTENUATOR, TYPE 1               | N/A        | 1 EA.   |
|                    | GALVANIZED STEEL BEAM GUARDRAIL, TYPE 2-31 END ANCHORAGE | N/A        | 150.00' |
| 71                 | GUARDRAIL END TREATMENT ATTENUATOR, TYPE 1               | N/A        | 1 EA.   |
|                    | GALVANIZED STEEL BEAM GUARDRAIL, TYPE 2-31 END ANCHORAGE | N/A        | 150.00' |

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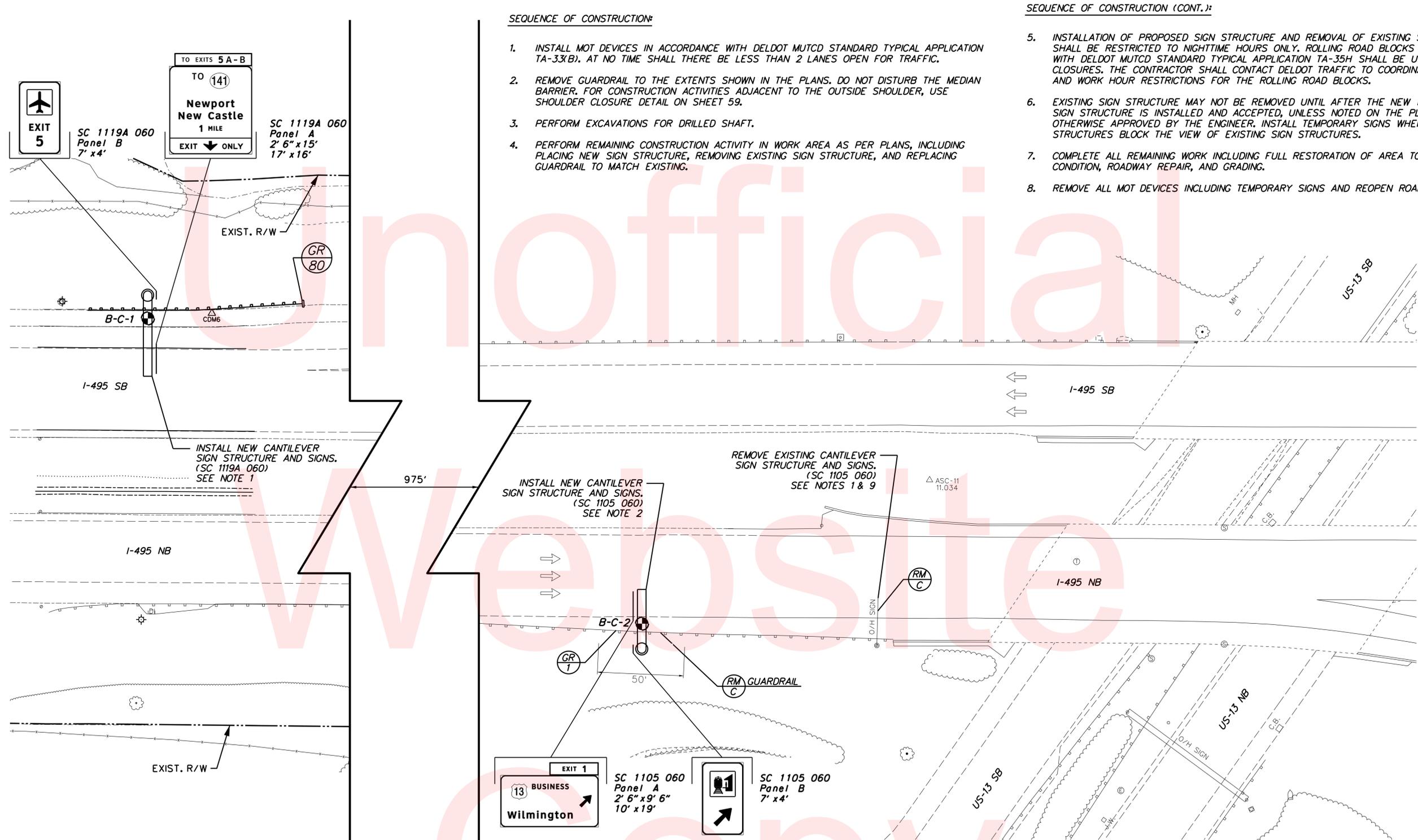
|   |                       |                          |  |                        |                   |  |                   |
|---|-----------------------|--------------------------|--|------------------------|-------------------|--|-------------------|
| <p><b>DELAWARE</b><br/>DEPARTMENT OF TRANSPORTATION</p> | ADDENDUMS / REVISIONS | <p>SCALE</p> <p>FEET</p> | <p><b>CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18</b></p> | CONTRACT<br>T201407004 | BRIDGE NO.        | <p><b>CONSTRUCTION / SSC / BORING LOCATION PLANS</b></p> | SHEET NO.<br>66   |
|   |                       |                          |  | COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K |  | TOTAL SHTS.<br>83 |
|   |                       |                          |  | CHECKED BY: JCR        |                   |  |                   |
|   |                       |                          |  |                        |                   |  |                   |

SEQUENCE OF CONSTRUCTION (CONT.):

5. INSTALLATION OF PROPOSED SIGN STRUCTURE AND REMOVAL OF EXISTING SIGN STRUCTURE SHALL BE RESTRICTED TO NIGHTTIME HOURS ONLY. ROLLING ROAD BLOCKS IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-35H SHALL BE USED FOR SHOULDER CLOSURES. THE CONTRACTOR SHALL CONTACT DELDOT TRAFFIC TO COORDINATE THE DURATION AND WORK HOUR RESTRICTIONS FOR THE ROLLING ROAD BLOCKS.
6. EXISTING SIGN STRUCTURE MAY NOT BE REMOVED UNTIL AFTER THE NEW REPLACEMENT SIGN STRUCTURE IS INSTALLED AND ACCEPTED, UNLESS NOTED ON THE PLANS OR OTHERWISE APPROVED BY THE ENGINEER. INSTALL TEMPORARY SIGNS WHERE NEW SIGN STRUCTURES BLOCK THE VIEW OF EXISTING SIGN STRUCTURES.
7. COMPLETE ALL REMAINING WORK INCLUDING FULL RESTORATION OF AREA TO ITS ORIGINAL CONDITION, ROADWAY REPAIR, AND GRADING.
8. REMOVE ALL MOT DEVICES INCLUDING TEMPORARY SIGNS AND REOPEN ROADWAY.

SEQUENCE OF CONSTRUCTION:

1. INSTALL MOT DEVICES IN ACCORDANCE WITH DELDOT MUTCD STANDARD TYPICAL APPLICATION TA-33(B). AT NO TIME SHALL THERE BE LESS THAN 2 LANES OPEN FOR TRAFFIC.
2. REMOVE GUARDRAIL TO THE EXTENTS SHOWN IN THE PLANS. DO NOT DISTURB THE MEDIAN BARRIER. FOR CONSTRUCTION ACTIVITIES ADJACENT TO THE OUTSIDE SHOULDER, USE SHOULDER CLOSURE DETAIL ON SHEET 59.
3. PERFORM EXCAVATIONS FOR DRILLED SHAFT.
4. PERFORM REMAINING CONSTRUCTION ACTIVITY IN WORK AREA AS PER PLANS, INCLUDING PLACING NEW SIGN STRUCTURE, REMOVING EXISTING SIGN STRUCTURE, AND REPLACING GUARDRAIL TO MATCH EXISTING.



NOTES:

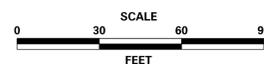
9. EXISTING GROUND MOUNTED CANTILEVER SIGN STRUCTURE (SO 1105 060) TO BE REMOVED. FOUNDATION SHALL BE REMOVED A MINIMUM OF 1' BELOW GRADE.
10. SOIL BORING SCHEDULE TO BE COMPLETED BY THE DEPARTMENT CONTRACTOR.

| GUARDRAIL SCHEDULE |  |             |        |         |
|--------------------|--|-------------|--------|---------|
| NO.                | ITEM DESCRIPTION / TYPE                    | BEG IN STA. | OFFSET | LENGTH  |
| 80                 | GUARDRAIL END TREATMENT ATTENUATOR, TYPE 1 | N/A         | N/A    | 1 EA.   |
|                    | GALVANIZED STEEL BEAM GUARDRAIL, TYPE 2-31 |             | N/A    | 150.00' |
|                    | END ANCHORAGE                              |             | N/A    | 1 EA.   |

| SOIL BORING SCHEDULE |             |
|----------------------|-------------|
| NO.                  | DESCRIPTION |
| 1                    | -           |
| 2                    | -           |

\*SEE NOTE 10

ADDENDUMS / REVISIONS



CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|          |            |              |      |
|----------|------------|--------------|------|
| CONTRACT | T201407004 | BRIDGE NO.   |      |
| COUNTY   | NEW CASTLE | DESIGNED BY: | RK&K |
|          |            | CHECKED BY:  | JCR  |

CONSTRUCTION / SSC / BORING LOCATION PLANS

|             |
|-------------|
| CP-08       |
| SHEET NO.   |
| 67          |
| TOTAL SHTS. |
| 83          |

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| PANEL DESIGNATION | QUANTITY | LEGEND | SIZE    |        | COLOR |        | BORDER  |       | ARROW | SHIELD | REMARKS |                   |
|-------------------|----------|--------|---------|--------|-------|--------|---------|-------|-------|--------|---------|-------------------|
|                   |          |        | AREA    | HEIGHT | WIDTH | LEGEND | BCKGRND | WIDTH |       |        |         | RADIUS            |
| SO 1026059 A      | 1        |        | 142.75  | 4'-6"  | 9'-6" | W      | G       | 2"    | 3"    | --     | --      | EXTRUDED ALUMINUM |
| SO 1027059 A      | 1        |        | 1098.75 | 4'-6"  | 9'-6" | W      | G       | 2"    | 3"    | --     | --      | EXTRUDED ALUMINUM |
| SO 1028059 A      | 1        |        | 1242.75 | 4'-6"  | 9'-6" | W      | G       | 2"    | 3"    | --     | --      | EXTRUDED ALUMINUM |

NOTES: 1. COLORS: B=BLACK, BL=BLUE, BR=BROWN, G=GREEN, W=WHITE/SILVER, Y=YELLOW.  
 2. THE CONTRACTOR WILL FURNISH AND INSTALL ALL SIGNS SHOWN ON THIS SHEET.

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| PANEL DESIGNATION            | QUANTITY | LEGEND | SIZE                 |        |        | COLOR  |          | BORDER |        | ARROW | SHIELD                                    | REMARKS           | PANEL DESIGNATION | QUANTITY | LEGEND               | SIZE                 |        |        | COLOR  |          | BORDER |                        | ARROW | SHIELD          | REMARKS                    |                   |
|------------------------------|----------|--------|----------------------|--------|--------|--------|----------|--------|--------|-------|---|-------------------|-------------------|----------|----------------------|----------------------|--------|--------|--------|----------|--------|------------------------|-------|-----------------|----------------------------|-------------------|
|                              |          |        | AREA                 | HEIGHT | WIDTH  | LEGEND | BACKGRND | WIDTH  | RADIUS |       |   |                   |                   |          |                      | AREA                 | HEIGHT | WIDTH  | LEGEND | BACKGRND | WIDTH  | RADIUS                 |       |                 |                            |                   |
| SO 1026059 B                 | 1        |        | 198 <sup>sq</sup>    | 9'-0"  | 22'-0" | W      | G        | 2"     | 12"    | --    | (3)MI-5<br>24"x24"<br><br>MI-4<br>30"x24" | EXTRUDED ALUMINUM | SO 1028059 B      | 1        |                      | 368.75 <sup>sq</sup> | 2'-6"  | 9'-6"  | W      | G        | 2"     | 3"                     | --    | --              | MI-5<br>36"x36"            | EXTRUDED ALUMINUM |
| SO 1027059 B                 | 1        |        | 414.75 <sup>sq</sup> | 2'-6"  | 9'-6"  | W      | G        | 2"     | 3"     | --    | MI-5<br>36"x36"                           | EXTRUDED ALUMINUM | SO 1031059 A      | 1        |                      | 9 <sup>sq</sup>      | 2'-0"  | 4'-6"  | W      | G        | --     | --                     | --    | --              | OVERLAY:<br>SHEET ALUMINUM |                   |
| SO 1027059 C<br>SO 1028059 C | 2        |        | 28 <sup>sq</sup>     | 7'-0"  | 4'-0"  | W      | G        | 1.25"  | 6"     | --    | I-7<br>30"x30"                            | EXTRUDED ALUMINUM | SO 1031059 B      | 1        |                      | 25 <sup>sq</sup>     | 2'-6"  | 10'-0" | B      | Y        | 1.25"  | 2.25"                  | --    | --              | --                         |                   |
|                              |          |        |                      |        |        |        |          |        |        |       |   | SO 1125060 A      | 1                 |          | 402.75 <sup>sq</sup> | 4'-6"                | 9'-6"  | W      | G      | 2"       | 3"     | --                     | --    | MI-1<br>60"x48" | EXTRUDED ALUMINUM          |                   |
|                              |          |        |                      |        |        |        |          |        |        |       |   |                   |                   |          |                      | 3'-0"                | 20'-0" | B      | Y      | 2"       | 12"    | TYPE A<br>@135°<br>36° | --    |                 |                            |                   |

NOTES: 1. COLORS: B=BLACK, BL=BLUE, BR=BROWN, G=GREEN, W=WHITE/SILVER, Y=YELLOW.  
2. THE CONTRACTOR WILL FURNISH AND INSTALL ALL SIGNS SHOWN ON THIS SHEET.

| PANEL DESIGNATION            | QUANTITY | LEGEND | SIZE   |        | COLOR  |        | BORDER   |       | ARROW | SHIELD         | REMARKS           | PANEL DESIGNATION            | QUANTITY     | LEGEND | SIZE   |        | COLOR  |        | BORDER |          | ARROW | SHIELD | REMARKS         |                   |                   |
|------------------------------|----------|--------|--------|--------|--------|--------|----------|-------|-------|----------------|-------------------|------------------------------|--------------|--------|--------|--------|--------|--------|--------|----------|-------|--------|-----------------|-------------------|-------------------|
|                              |          |        | AREA   | HEIGHT | WIDTH  | LEGEND | BACKGRND | WIDTH |       |                |                   |                              |              |        | RADIUS | AREA   | HEIGHT | WIDTH  | LEGEND | BACKGRND |       |        |                 | WIDTH             | RADIUS            |
| SO I124060 B<br>SO I104060 B | 2        |        | 304.8  | 19'-0" | 16'-0" | W      | G        | 2"    | 12"   | DOWN ARROW 22" | MI-1 48"x48"      | EXTRUDED ALUMINUM            | SO I124060 D | 1      |        | 28.8   | 7'-0"  | 4'-0"  | W      | G        | 1.25" | 6"     | TYPE A @45° 36" | 1-5 30"x30"       | EXTRUDED ALUMINUM |
| SO I124060 A                 | 1        |        | 402.75 | 15'-0" | 20'-0" | W      | G        | 2"    | 12"   | --             | MI-1 60"x48"      | EXTRUDED ALUMINUM            | SO I124060 E | 1      |        | 215.75 | 12'-0" | 16'-0" | W      | G        | 2"    | 12"    | --              | MI-4 36"x36"      | EXTRUDED ALUMINUM |
| SO I124060 C                 | 1        |        | 325.5  | 15'-0" | 16'-0" | W      | G        | 2"    | 12"   | MI-5 45"x36"   | EXTRUDED ALUMINUM | SO I124060 F<br>SO I104060 G | 2            |        | 28.8   | 7'-0"  | 4'-0"  | W      | G      | 1.25"    | 6"    | --     | 1-7 30"x30"     | EXTRUDED ALUMINUM |                   |

NOTES:  
1. COLORS: B=BLACK, BL=BLUE, BR=BROWN, G=GREEN, W=WHITE/SILVER, Y=YELLOW.  
2. THE CONTRACTOR WILL FURNISH AND INSTALL ALL SIGNS SHOWN ON THIS SHEET.

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| PANEL DESIGNATION             | QUANTITY | LEGEND | SIZE   |        | COLOR  |        | BORDER   |       | ARROW | SHIELD | REMARKS     | PANEL DESIGNATION | QUANTITY      | LEGEND | SIZE   |        | COLOR  |        | BORDER |          | ARROW | SHIELD | REMARKS |                 |                   |
|-------------------------------|----------|--------|--------|--------|--------|--------|----------|-------|-------|--------|-------------|-------------------|---------------|--------|--------|--------|--------|--------|--------|----------|-------|--------|---------|-----------------|-------------------|
|                               |          |        | AREA   | HEIGHT | WIDTH  | LEGEND | BACKGRND | WIDTH |       |        |             |                   |               |        | RADIUS | AREA   | HEIGHT | WIDTH  | LEGEND | BACKGRND |       |        |         | WIDTH           | RADIUS            |
| SO 1104060 A                  | 1        |        | 462.75 | 4'-6"  | 9'-6"  | W      | G        | 2"    | 3"    | --     | --          | EXTRUDED ALUMINUM | SO 1104060 E  | 1      |        | 234    | 9'-0"  | 26'-0" | W      | G        | 2"    | 12"    | --      | (2)MI-5 24"x24" | EXTRUDED ALUMINUM |
| SO 1104060 C                  | 1        |        | 293.5  | 2'-6"  | 15'-0" | W      | G        | 2"    | 3"    | --     | --          | EXTRUDED ALUMINUM | SO 1104060 F  | 1      |        | 215.75 | 2'-6"  | 9'-6"  | W      | G        | 2"    | 3"     | --      | --              | EXTRUDED ALUMINUM |
| SO 1104060 D<br>SC 1119A060 B | 2        |        | 28     | 7'-0"  | 4'-0"  | W      | G        | 1.25" | 6"    | --     | I-5 30"x30" | EXTRUDED ALUMINUM | SC 1119A060 A | 1      |        | 309.5  | 2'-6"  | 15'-0" | W      | G        | 2"    | 3"     | --      | --              | EXTRUDED ALUMINUM |
|                               |          |        |        |        |        |        |          |       |       |        |             |                   |               |        |        |        |        |        |        |          |       |        |         |                 |                   |

NOTES: 1. COLORS: B=BLACK, BL=BLUE, BR=BROWN, G=GREEN, W=WHITE/SILVER, Y=YELLOW.  
2. THE CONTRACTOR WILL FURNISH AND INSTALL ALL SIGNS SHOWN ON THIS SHEET.

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| PANEL DESIGNATION | QUANTITY | LEGEND | SIZE                 |        | COLOR  |        | BORDER   |       | ARROW | SHIELD               | REMARKS         | PANEL DESIGNATION | QUANTITY | LEGEND | SIZE   |      | COLOR  |       | BORDER |          | ARROW | SHIELD | REMARKS |
|-------------------|----------|--------|----------------------|--------|--------|--------|----------|-------|-------|----------------------|-----------------|-------------------|----------|--------|--------|------|--------|-------|--------|----------|-------|--------|---------|
|                   |          |        | AREA                 | HEIGHT | WIDTH  | LEGEND | BACKGRND | WIDTH |       |                      |                 |                   |          |        | RADIUS | AREA | HEIGHT | WIDTH | LEGEND | BACKGRND |       |        |         |
| SC 1105060 A      | 1        |        | 213.75 <sup>sq</sup> | 10'-0" | 19'-0" | W      | G        | 2"    | 3"    | --                   | --              | EXTRUDED ALUMINUM |          |        |        |      |        |       |        |          |       |        |         |
| SC 1105060 B      | 1        |        | 28 <sup>sq</sup>     | 7'-0"  | 4'-0"  | W      | G        | 1.25" | 6"    | TYPE A<br>45°<br>26" | 1-7<br>30"x30"  | EXTRUDED ALUMINUM |          |        |        |      |        |       |        |          |       |        |         |
| SO 1125060 B      | 1        |        | 304 <sup>sq</sup>    | 19'-0" | 16'-0" | W      | G        | 2"    | 12"   | DOWN<br>ARROW<br>22" | MI-1<br>48"x48" |                   |          |        |        |      |        |       |        |          |       |        |         |

NOTES: 1. COLORS: B=BLACK, BL=BLUE, BR=BROWN, G=GREEN, W=WHITE/SILVER, Y=YELLOW.  
2. THE CONTRACTOR WILL FURNISH AND INSTALL ALL SIGNS SHOWN ON THIS SHEET.

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# GENERAL NOTES

## DESIGN SPECIFICATIONS:

1. AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS", 2013, 6TH EDITION.
2. AASHTO "LRFD BRIDGE DESIGN SPECIFICATIONS, 6TH EDITION, 2012", INCLUDING ALL REVISIONS.
3. DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AUGUST 2001, INCLUDING ALL SUPPLEMENTALS.
4. AASHTO/AWS D1.5 BRIDGE WELDING CODE AND AWS D1.1 STRUCTURAL WELDING CODE.

## DESIGN LOADS:

1. THE DESIGN WIND SPEED IS 90 MPH (3-SECOND GUST WIND SPEED), BASED ON A 50-YEAR MEAN RECURRENCE INTERVAL.
2. THE DESIGN ICE LOAD IS 3 PSF.
3. THE DESIGN SIGN PANEL AREA INCLUDES AN ADDITIONAL 15% INCREASE IN AREA OVER THE ACTUAL SIGN PANEL AREA.
4. FATIGUE DESIGN FOR OVERHEAD SIGN STRUCTURES IS BASED ON FATIGUE CATEGORY II FOR NATURAL WIND GUSTS AND TRUCK- INDUCED GUSTS.
5. FATIGUE DESIGN FOR CANTILEVER SIGN STRUCTURES IS BASED ON FATIGUE CATEGORY I FOR GALLOPING, NATURAL WINDS GUSTS, AND TRUCK INDUCED GUSTS.

## MATERIALS:

1. SIGN STRUCTURE POSTS AND OVERHEAD MEMBERS SHALL BE ROUND, NON-TAPERED, TUBULAR STEEL PIPE CONFORMING TO THE FOLLOWING REQUIREMENTS:
  - A. THE PIPE SHALL HAVE A MINIMUM YIELD STRENGTH OF 52,000 PSI AND CONFORM TO ONE OF THE FOLLOWING MATERIAL DESIGNATIONS:
    - ASTM A53, GRADE B, TYPE E OR S
    - API 5LX, GRADE X52
    - ASTM A106, GRADE C, TYPE S
    - ASTM A500, GRADE B
  - B. MILL CERTIFICATIONS FOR EACH SIGN STRUCTURE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
  - C. THE PIPE SHALL HAVE A MINIMUM CHARPY V-NOTCH IMPACT TEST RESULT OF 25 FT-LB AT 40°F. CHARPY V-NOTCH SAMPLING AND TESTING PROCEDURES SHALL BE IN ACCORDANCE WITH ASTM A673, FREQUENCY H.
2. ALL STEEL PLATES AND SHAPES SHALL CONFORM TO AASHTO M270, GRADE 36 OR BETTER. ALL STEEL PLATES AND SHAPES SHALL MEET THE CHARPY V-NOTCH REQUIREMENTS FOR ZONE 2, NON-FRACTURE CRITICAL.
3. THE STRUCTURE SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M111.
4. ALL CONNECTION BOLTS SHALL CONFORM TO AASHTO M164. WASHERS SHALL CONFORM TO AASHTO M293, AND NUTS SHALL CONFORM TO AASHTO M291, GRADE DH, OR AASHTO M292, GRADE 2H.
5. ANCHOR BOLTS SHALL CONFORM TO AASHTO M314, GRADE 55. ANCHOR NUTS SHALL CONFORM TO AASHTO M291, GRADE DH, OR AASHTO M292, GRADE 2H. WASHERS SHALL CONFORM TO AASHTO M293.
6. ALL HARDWARE SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M232.
7. PORTLAND CEMENT CONCRETE FOR PEDESTALS AND FOUNDATIONS SHALL BE DELDOT CLASS B ( $f'c = 3,000$  PSI).
8. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.
9. REINFORCING STEEL SHALL CONFORM TO AASHTO M31, GRADE 60.
10. ALL REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" UNLESS OTHERWISE NOTED.

## GENERAL:

1. BECAUSE SURVEY DATA PROVIDING SIGN STRUCTURE LOCATIONS AND ROADWAY CROSS-SLOPES WAS NOT AVAILABLE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE SURVEY INFORMATION NECESSARY TO VERIFY MINIMUM SIGN STRUCTURE HEIGHTS AND SPAN LENGTHS FOR EACH SIGN STRUCTURE. THE CONTRACTOR SHALL SUBMIT ALL SURVEY INFORMATION TO THE DEPARTMENT FOR FINAL DESIGN AND TO VERIFY THE STRUCTURAL ADEQUACY OF ALL MEMBER SIZES AND CONNECTIONS PROVIDED IN THE CONTRACT DRAWINGS.
2. DRILLED SHAFT FOUNDATION SIZES AND REINFORCING DETAILS SHOWN IN THE CONTRACT DRAWINGS ARE FOR ESTIMATION PURPOSES ONLY. THE FINAL DESIGN OF EACH DRILLED SHAFT FOUNDATION WILL BE PERFORMED BY THE ENGINEER ONCE THE DEPARTMENT OBTAINS SOIL BORING INFORMATION.
3. FORM MASTS FOR SIGN STRUCTURES TO THE RADII SHOWN ON THE PLANS IN ACCORDANCE WITH THE TUBE AND PIPE ASSOCIATION INTERNATIONAL RECOMMENDED STANDARDS FOR INDUCTION BENDING OF PIPE AND TUBE (TPA-IBS-98).
4. STEEL TEMPLATES SHALL BE USED TO SET ANCHOR BOLTS PLUMB WHEN POURING THE FOUNDATION.
5. BASE PLATES SHALL BE IN FULL CONTACT WITH ALL FLAT WASHERS.
6. ALL ANCHOR BOLTS SHALL BE TIGHTENED USING TURN OF NUT METHOD (1/6 TURN AFTER SNUG TIGHT).
7. THREADS OF ANCHOR BOLTS SHALL BE BURRED OFF AT FACE OF NUT AFTER COLUMN IS INSTALLED.
8. LOCK WASHERS WITH FLAT WASHERS SHALL ONLY BE USED FOR U-BOLT CONNECTIONS AND NUTS SHALL BE TURNED UNTIL THE LOCK WASHER IS FLAT.
9. COLUMN, CHORD, AND BRACING O.D. DIMENSIONS ARE ACTUAL.
10. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS BEFORE ORDERING ANY MATERIALS.
11. FABRICATE ALL SIGN STRUCTURES INTO THE LARGEST PRACTICAL SECTIONS PRIOR TO GALVANIZING. SUBMIT SPLICE LOCATIONS TO THE ENGINEER FOR APPROVAL. DO NOT COMMENCE FABRICATION UNTIL SUCH SPLICE LOCATIONS ARE APPROVED.
12. DO NOT USE GROUT BETWEEN BASE PLATE AND CONCRETE PEDESTAL.
13. SLOPE TOP OF PEDESTAL 4% FROM CENTER TO NEAR EDGES FOR DRAINAGE.
14. PROVIDE DOUBLE NUTS AND WASHERS FOR EACH ANCHOR BOLT.
15. PERMANENT CAMBER EQUAL TO L/1000 HAS BEEN PROVIDED IN ADDITION TO THE DEAD LOAD CAMBER. CAMBER SHALL BE INCORPORATED DURING FABRICATION.
16. THE COST OF THE ROUND, NON-TAPERED, TUBULAR STEEL PIPE AND STRUCTURAL STEEL SHAPES, PLATES AND BARS, INCLUDING FABRICATION, COATING, ERECTION AND CONNECTIONS SHALL BE PAID FOR UNDER THE PERTINENT "STEEL SIGN STRUCTURES, TUBULAR ARCH, CANTILEVER" OR "STEEL SIGN STRUCTURES, TUBULAR ARCH, OVERHEAD" ITEM.
17. THE COST OF EACH FOUNDATION, INCLUDING EXCAVATION, REINFORCEMENT, CONCRETE AND ANCHORAGE MATERIALS, SHALL BE PAID FOR UNDER THE PERTINENT DRILLED SHAFT FOUNDATION ITEM.
18. THIS SET OF PLANS SHALL BE USED BY THE CONTRACTOR TO BID THE PROJECT.
19. ALL TOWER SUPPORTS SHALL BE LOCATED BEHIND PHYSICAL TRAFFIC BARRIERS, IF PRESENT.
20. INSTALL ACCESS HOLE ON POST OPPOSITE DIRECTION OF TRAFFIC.

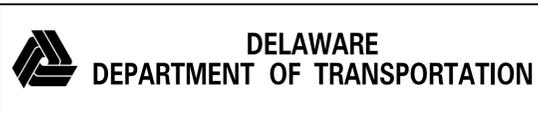
# SHEET INDEX

| SHEET NO. | DESCRIPTION                                      |
|-----------|--|
| SS-01     | SIGN STRUCTURE GENERAL NOTES AND INDEX OF SHEETS |
| SS-02     | OVERHEAD SIGN STRUCTURE ELEVATION AND END VIEWS  |
| SS-03     | CANTILEVER SIGN STRUCTURE ELEVATION AND END VIEW |
| SS-04     | SIGN SUPPORT STRUCTURE CHARTS                    |
| SS-05     | OVERHEAD SIGN STRUCTURE DETAILS                  |
| SS-06     | CANTILEVER SIGN STRUCTURE DETAILS                |
| SS-07     | OVERHEAD/CANTILEVER SIGN STRUCTURE DETAILS       |
| SS-08     | SIGN STRUCTURE FOUNDATION DETAILS                |

| SIGN STRUCTURE LOCATION |                |          |   |               |
|-------------------------|----------------|----------|---|---------------|
| SIGN STRUCTURE NUMBER   | STRUCTURE TYPE | BASELINE | SUPPORT OFFSET FROM EXISTING SIGN STRUCTURE * |               |
|                         |                |          | WEST SUPPORT                                  | EAST SUPPORT  |
| S01026 059              | OVERHEAD       | I-95     | 86' SOUTH                                     | 87' SOUTH     |
| S01027 059              | OVERHEAD       | I-95     | 537' NORTH                                    | 534' NORTH    |
| S01028 059              | OVERHEAD       | I-95     | 564' NORTH                                    | 563' NORTH    |
|                         |                |          | NORTH SUPPORT                                 | SOUTH SUPPORT |
| S01104 060              | OVERHEAD       | I-495    | 281' EAST                                     | 275' EAST     |
| S01124 060              | OVERHEAD       | I-495    | 335' EAST                                     | 346' EAST     |
| S01125 060              | OVERHEAD       | I-495    | 98' WEST                                      | 92' WEST      |
| SC1105 060              | CANTILEVER     | I-495    | -   | 137' WEST     |
| SC1119A-060             | CANTILEVER     | I-495    | NEW STRUCTURE                                 |               |

\* OFFSET DISTANCES ARE MEASURED BETWEEN SUPPORT CENTERS AND REPORTED TO THE NEAREST FOOT. FINAL SIGN STRUCTURE LOCATIONS SHALL BE COORDINATED WITH AND APPROVED BY THE OWNER.

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| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
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|                       |  |
|                       |  |

**NOT TO SCALE**

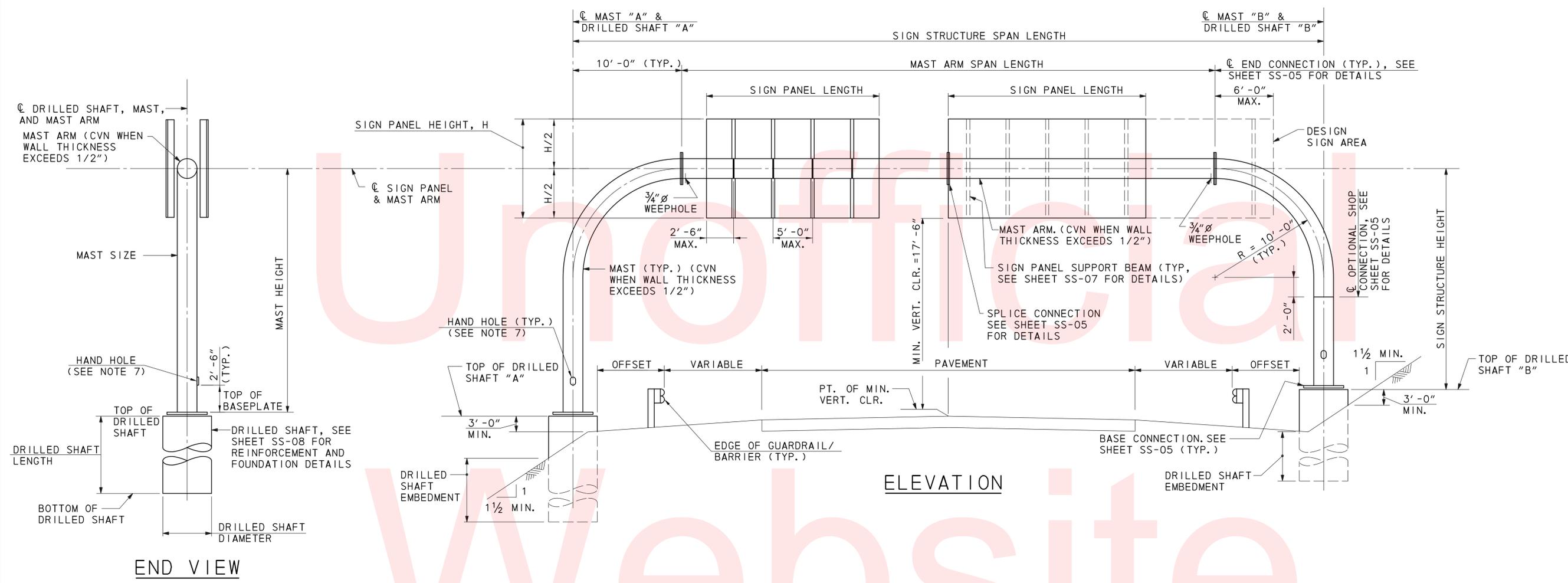
**CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18**

|                        |                   |     |
|------------------------|-------------------|-----|
| CONTRACT<br>T201407004 | BRIDGE NO.        |     |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K | PLO |
|                        | CHECKED BY:       | JW  |

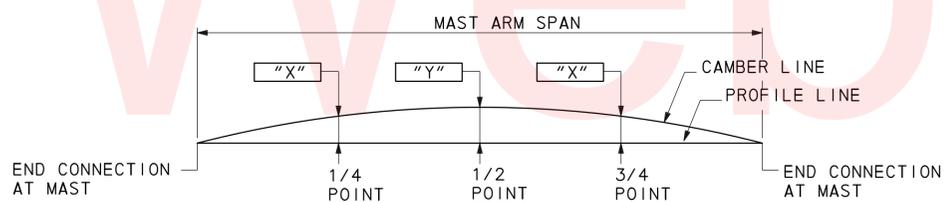
**GENERAL NOTES AND INDEX OF SHEETS**

|                   |
|-------------------|
| SS-01             |
| SHEET NO.<br>73   |
| TOTAL SHTS.<br>83 |

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ELEVATION



CAMBER DIAGRAM

MAXIMUM CAMBER (AT 1/2 POINT) REPRESENTS MAXIMUM DEAD LOAD CAMBER PLUS L/1000 WHERE L = SIGN STRUCTURE SPAN.

| CAMBER                |         |           |           |
|-----------------------|---------|-----------|-----------|
| SIGN STRUCTURE NUMBER | SPAN    | "X" (IN.) | "Y" (IN.) |
| SO 1026 059           | 107'-3" | 2.99      | 3.54      |
| SO 1027 059           | 108'-0" | 3.03      | 3.64      |
| SO 1028 059           | 107'-0" | 3.05      | 3.65      |
| SO 1104 060           | 150'-0" | 5.51      | 7.05      |
| SO 1124 060           | 163'-0" | 7.24      | 9.46      |
| SO 1125 060           | 56'-6"  | 1.31      | 1.41      |

NOTES:

- FOR GENERAL NOTES, SEE SHEET SS-01.
- FOR SIGN PANEL DETAILS, SEE SIGN DETAIL SHEETS.
- GROUT SHALL NOT BE PLACED BETWEEN BASE PLATE AND CONCRETE CAISSON.
- TIGHTEN ANCHOR BOLTS USING TURN-OF-NUT METHOD (ADDITIONAL 1/6 TURN AFTER SNUG TIGHT) ONCE SIGN PANEL IS LEVELED.
- SEAL GAP BETWEEN BASE PLATE AND CONCRETE DRILLED SHAFT WITH GALVANIZED SCREEN, 1 1/4" TO 3/8" OPENING, TO PREVENT ENTRY OF RODENTS. SCREEN IS TO BE REMOVABLE AND ATTACHED TO BASE PLATE WITH STAINLESS STEEL HARDWARE. SCREEN IS TO BE OF SUFFICIENT STIFFNESS TO PREVENT ENTRY BETWEEN SCREEN AND FOUNDATION WHILE PERMITTING DRAINAGE.
- FOR OVERHEAD SIGN SUPPORT STRUCTURE CHART, SEE SHEET SS-04.
- LOCATE HAND HOLE ON THE DOWN-TRAFFIC SIDE OF EACH MAST, WITH A DIRECTION THAT IS 90 DEGREES TO THE PLANE CONTAINING THE MAST ARM. FOR HAND HOLE DETAILS, SEE SHEET SS-07.

| ADDENDUMS / REVISIONS |
|-----------------------|
|                       |
|                       |
|                       |

NOT TO SCALE

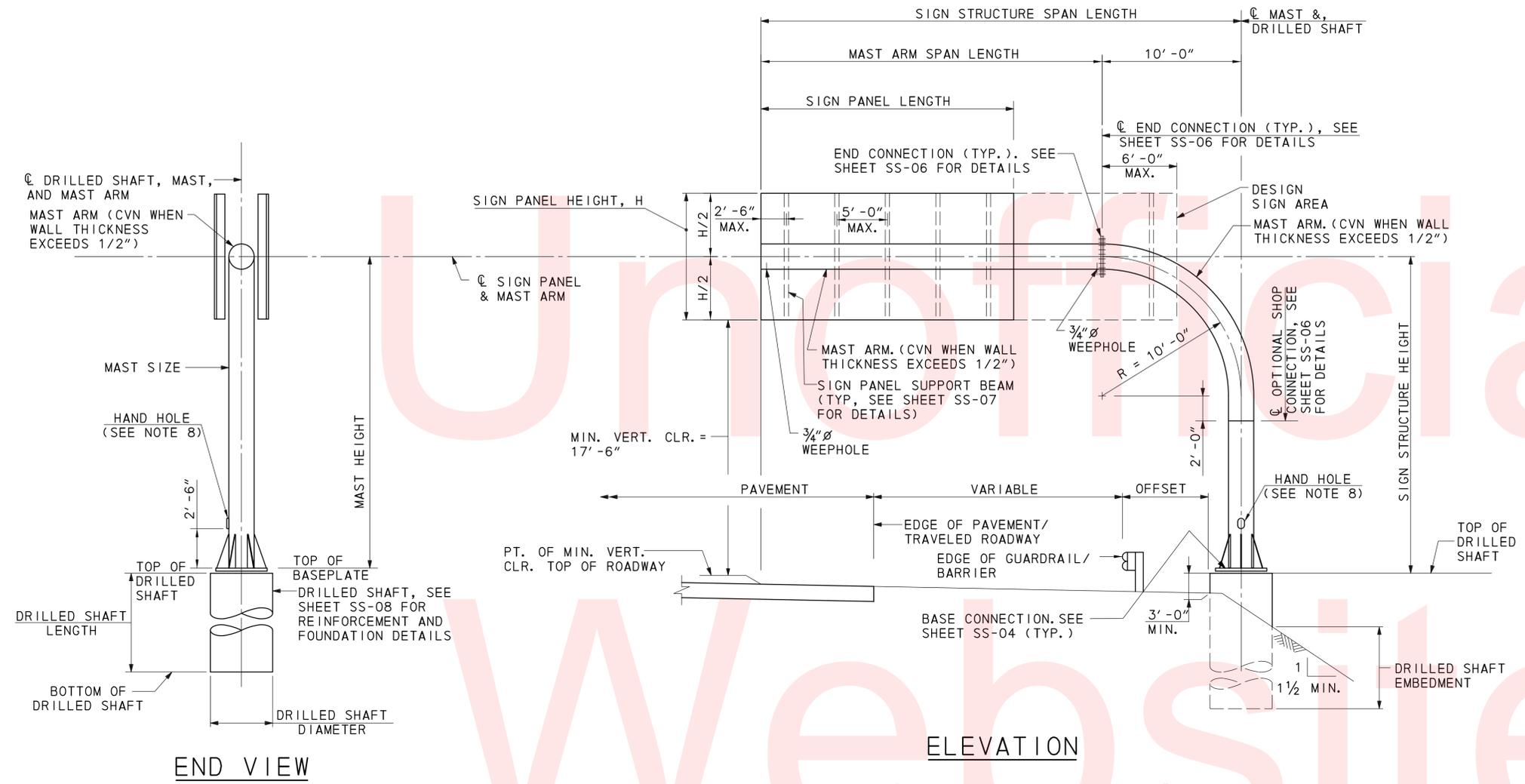
CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|                        |                   |     |
|------------------------|-------------------|-----|
| CONTRACT<br>T201407004 | BRIDGE NO.        |     |
| COUNTY<br>NEW CASTLE   | DESIGNED BY: RK&K | PLD |
|                        | CHECKED BY:       | JW  |

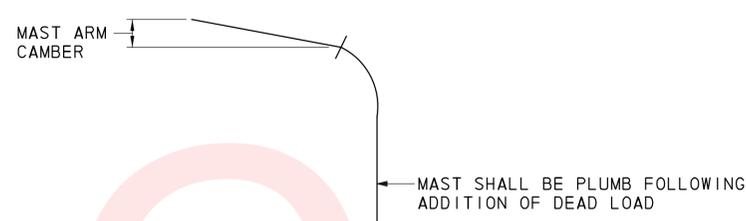
**OVERHEAD SIGN STRUCTURE - ELEVATION AND END VIEWS**

|                   |
|-------------------|
| SS-02             |
| SHEET NO.<br>74   |
| TOTAL SHTS.<br>83 |

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ELEVATION



CAMBER DIAGRAM

MAST ARM CAMBER MUST INCLUDE BOTH MAST ARM AND MAST DEFLECTION  
BACK RAKING OF MAST IS NOT PERMITTED.

| CAMBER                |               |                 |
|-----------------------|---------------|-----------------|
| SIGN STRUCTURE NUMBER | MAST ARM SPAN | MAST ARM CAMBER |
| SC 1105 060           | 24'-6"        | 2.74            |
| SC 1119A 060          | 37'-0"        | 7.39            |

- NOTES:**
- FOR GENERAL NOTES, SEE SHEET SS-01.
  - FOR SIGN PANEL DETAILS, SEE SIGN DETAIL SHEETS.
  - GROUT SHALL NOT BE PLACED BETWEEN BASE PLATE AND CONCRETE CAISSON.
  - TIGHTEN ANCHOR BOLTS USING TURN-OF-NUT METHOD (ADDITIONAL 1/6 TURN AFTER SNUG TIGHT) ONCE SIGN PANEL IS LEVELED.
  - TERMINATE WELDS 1/2" SHORT OF STIFFENER ENDS AND STIFFENER CHAMFER.
  - SEAL GAP BETWEEN BASE PLATE AND CONCRETE DRILLED SHAFT WITH GALVANIZED SCREEN, 1 1/4" TO 3/8" OPENING, TO PREVENT ENTRY OF RODENTS. SCREEN IS TO BE REMOVABLE AND ATTACHED TO BASE PLATE WITH STAINLESS STEEL HARDWARE. SCREEN IS TO BE OF SUFFICIENT STIFFNESS TO PREVENT ENTRY BETWEEN SCREEN AND FOUNDATION WHILE PERMITTING DRAINAGE.
  - FOR CANTILEVER SIGN SUPPORT STRUCTURE CHART, SEE SHEET SS-04.
  - LOCATE HAND HOLE ON THE DOWN-TRAFFIC SIDE OF EACH MAST, WITH A DIRECTION THAT IS 90 DEGREES TO THE PLANE CONTAINING THE MAST ARM.



| ADDENDUMS / REVISIONS |  |
|-----------------------|--|
|                       |  |
|                       |  |
|                       |  |

NOT TO SCALE

CANTILEVER AND OVERHEAD SIGN STRUCTURES, OPEN-END, FY16-18

|            |                   |     |
|------------|-------------------|-----|
| CONTRACT   | BRIDGE NO.        |     |
| T201407004 | DESIGNED BY: RK&K | PLO |
| COUNTY     | CHECKED BY:       | JW  |
| NEW CASTLE |                   |     |

CANTILEVER SIGN STRUCTURE - ELEVATION AND END VIEW

|             |
|-------------|
| SS-03       |
| SHEET NO.   |
| 75          |
| TOTAL SHTS. |
| 83          |

**OVERHEAD SIGN SUPPORT STRUCTURE CHART**

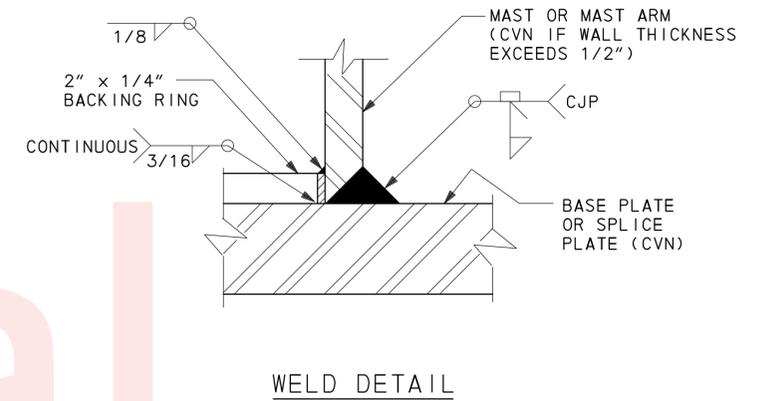
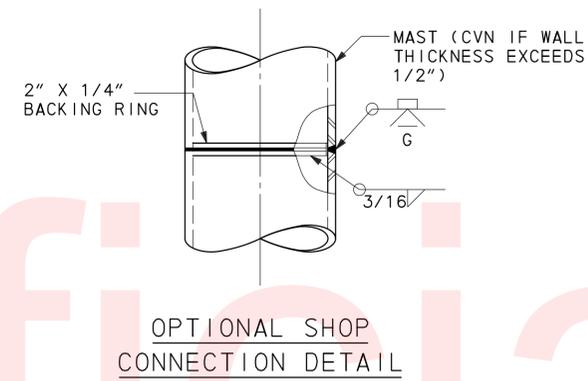
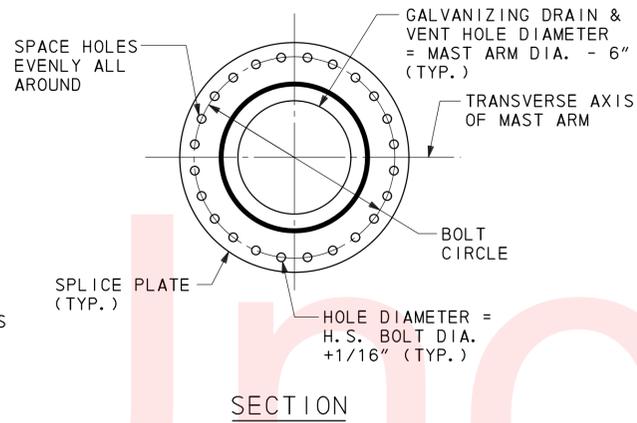
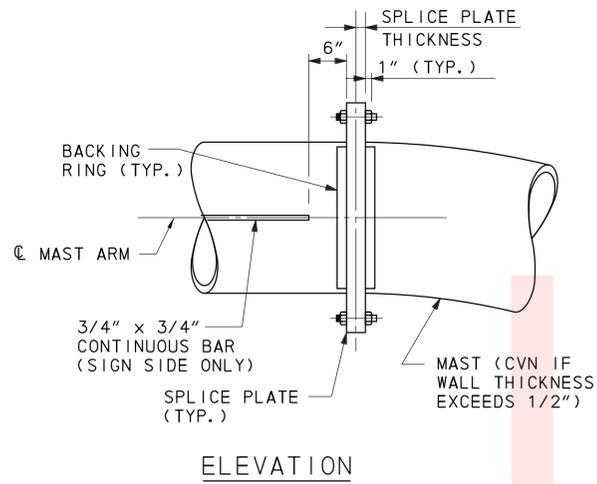
|                    |                             |                        | MAST AND MAST ARM     |                        |                   |                    | ANCHOR BOLTS |                   |                      | BASE PLATE        |                    | H. S. (SPLICE) BOLTS |                   |                      | SPLICE PLATE      |                    |
|--------------------|-----------------------------|------------------------|-----------------------|------------------------|-------------------|--------------------|--------------|-------------------|----------------------|-------------------|--------------------|----------------------|-------------------|----------------------|-------------------|--------------------|
| SIGN STRUCTURE NO. | SIGN STRUCTURE SPAN LENGTH* | DESIGN PANEL AREA (SF) | MAST ARM SPAN LENGTH* | SIGN STRUCTURE HEIGHT* | DIAMETER (INCHES) | THICKNESS (INCHES) | NO. OF BOLTS | DIAMETER (INCHES) | BOLT CIRCLE (INCHES) | DIAMETER (INCHES) | THICKNESS (INCHES) | NO. OF BOLTS         | DIAMETER (INCHES) | BOLT CIRCLE (INCHES) | DIAMETER (INCHES) | THICKNESS (INCHES) |
| SO 1026 059        | 127' -3"                    | 1542                   | 107' -3"              | 28' -6"                | 30                | 0.625              | 16           | 2                 | 38                   | 45                | 3.75               | 16                   | 1.5               | 38                   | 45                | 2.5                |
| SO 1027 059        | 128' -0"                    | 1741                   | 108' -0"              | 28' -6"                | 30                | 0.625              | 16           | 2                 | 38                   | 45                | 3.75               | 16                   | 1.5               | 38                   | 45                | 2.5                |
| SO 1028 059        | 127' -0"                    | 1853                   | 107' -0"              | 30' -0"                | 30                | 0.625              | 16           | 2                 | 38                   | 45                | 3.75               | 16                   | 1.5               | 38                   | 45                | 2.5                |
| SO 1125 060        | 76' -6"                     | 890                    | 56' -6"               | 28' -0"                | 24                | 0.562              | 8            | 2                 | 30                   | 37                | 2                  | 8                    | 1.5               | 30                   | 37                | 2                  |
| SO 1124 060        | 183' -0"                    | 1510                   | 163' -0"              | 28' -0"                | 30                | 0.625              | 16           | 2                 | 38                   | 45                | 3.75               | 16                   | 2                 | 38                   | 45                | 2.5                |
| SO 1104 060        | 170' -0"                    | 2036                   | 150' -0"              | 28' -0"                | 32                | 0.625              | 16           | 2                 | 40                   | 47                | 3.75               | 16                   | 2                 | 40                   | 47                | 2.5                |

**CANTILEVER SIGN SUPPORT STRUCTURE CHART**

|                    |                             |                        | MAST AND MAST ARM     |                        |                   |                    | ANCHOR BOLTS |                   |                      | BASE PLATE        |                    | H. S. (SPLICE) BOLTS |              |                   | SPLICE PLATE         |                   |                    |                   |
|--------------------|-----------------------------|------------------------|-----------------------|------------------------|-------------------|--------------------|--------------|-------------------|----------------------|-------------------|--------------------|----------------------|--------------|-------------------|----------------------|-------------------|--------------------|-------------------|
| SIGN STRUCTURE NO. | SIGN STRUCTURE SPAN LENGTH* | DESIGN PANEL AREA (SF) | MAST ARM SPAN LENGTH* | SIGN STRUCTURE HEIGHT* | DIAMETER (INCHES) | THICKNESS (INCHES) | NO. OF BOLTS | DIAMETER (INCHES) | BOLT CIRCLE (INCHES) | DIAMETER (INCHES) | THICKNESS (INCHES) | NO. OF STIFFENERS    | NO. OF BOLTS | DIAMETER (INCHES) | BOLT CIRCLE (INCHES) | DIAMETER (INCHES) | THICKNESS (INCHES) | NO. OF STIFFENERS |
| SC 1119A 060       | 47' -0"                     | 356                    | 37' -0"               | 28' -0"                | 24                | 0.562              | 12           | 2                 | 30                   | 42                | 2                  | 6                    | 12           | 1.5               | 30                   | 37                | 2                  | -                 |
| SC 1105 060        | 34' -6"                     | 246                    | 24' -6"               | 28' -0"                | 24                | 0.562              | 12           | 2                 | 30                   | 42                | 2                  | 6                    | 12           | 1.5               | 30                   | 37                | 2                  | -                 |

\* NOTE: BECAUSE SURVEY SHOWING ROADWAY HIGH POINTS AND CROSS-SLOPES WAS NOT AVAILABLE, THE DESIGN SIGN STRUCTURE HEIGHT WAS DETERMINED ASSUMING A 3-FOOT VERTICAL TOLERANCE BETWEEN THE TRUE HIGH POINT IN THE FINISHED ROADWAY SURFACE AND PROPOSED TOP OF DRILLED SHAFT FOUNDATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONDUCTING SURVEYS TO VERIFY SPAN LENGTHS AND STRUCTURE HEIGHTS. SURVEY RESULTS SHALL BE SUBMITTED TO THE DEPARTMENT FOR FINAL DESIGN AND VERIFICATION OF ALL INFORMATION PROVIDED IN THE SIGN STRUCTURE CHARTS.

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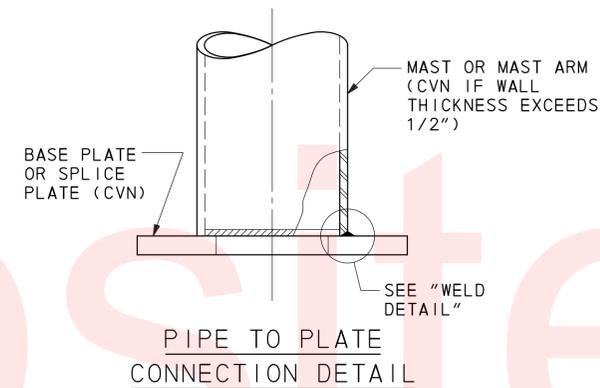
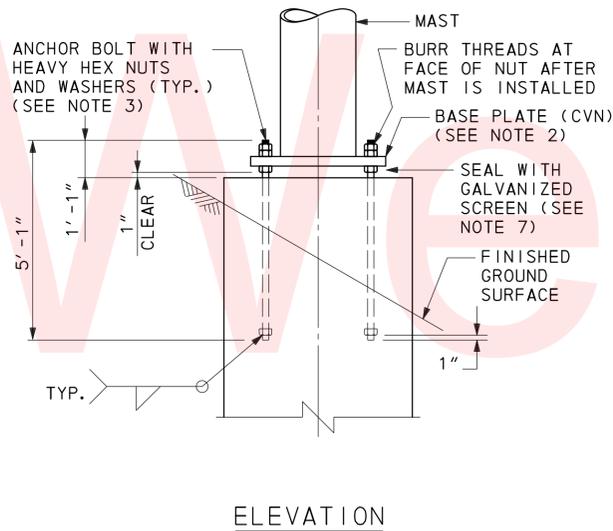
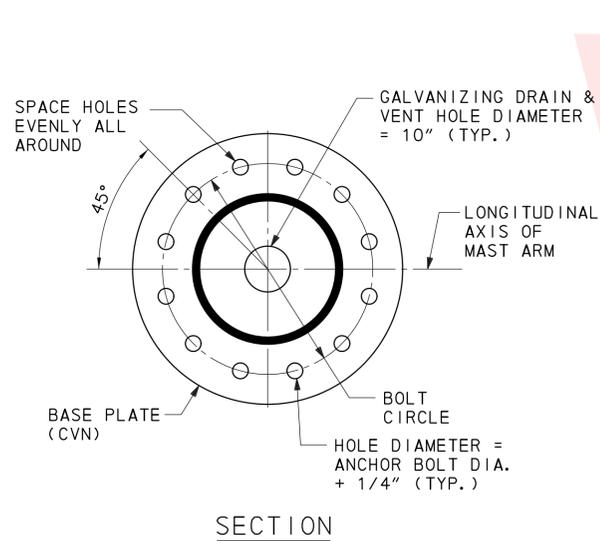


**OVERHEAD STRUCTURE  
END CONNECTION DETAILS**

(MAST ARM SPLICE CONNECTION SIMILAR)  
(24 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)

**WELD DETAIL NOTE:**

BACKING RING MUST BE FITTED/SIZED TO THE PIPE COLUMN AND CONTINUOUSLY FILLET WELDED TO THE BASE PLATE BEFORE THE FULL PENETRATION GROOVE WELD IS MADE. BACKING RING MUST BE FABRICATED AS A CONTINUOUS RING.

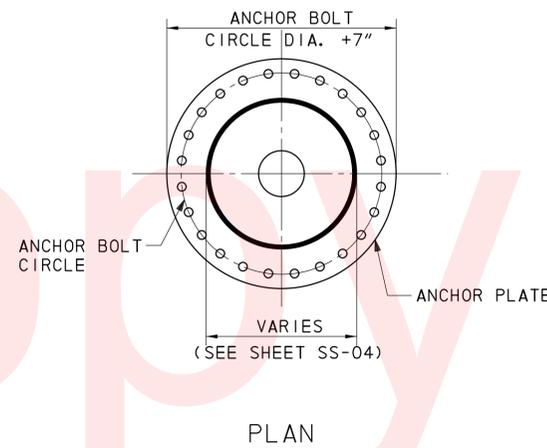


**NOTES:**

1. FOR GENERAL NOTES, SEE SHEET SS-01.
2. GROUT SHALL NOT BE PLACED BETWEEN BASE PLATE AND CONCRETE DRILLED SHAFT.
3. TIGHTEN ANCHOR BOLTS USING TURN-OF-NUT METHOD (ADDITIONAL 1/6 TURN AFTER SNUG TIGHT) ONCE SIGN PANEL IS LEVELED.
4. TERMINATE WELDS 1/2" SHORT OF STIFFENER ENDS AND STIFFENER CHAMFER.
5. FOR DRILLED SHAFT INFORMATION, SEE SHEET SS-08.
6. FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET SS-07.
7. SEAL WITH GALVANIZED SCREEN, 1 1/4" TO 3/8" OPENING, TO PREVENT ENTRY OF RODENTS. SCREEN IS TO BE REMOVABLE AND ATTACHED TO BASE PLATE WITH STAINLESS STEEL HARDWARE. SCREEN IS TO BE OF SUFFICIENT STIFFNESS TO PREVENT ENTRY BETWEEN SCREEN AND FOUNDATION WHILE PERMITTING DRAINAGE.

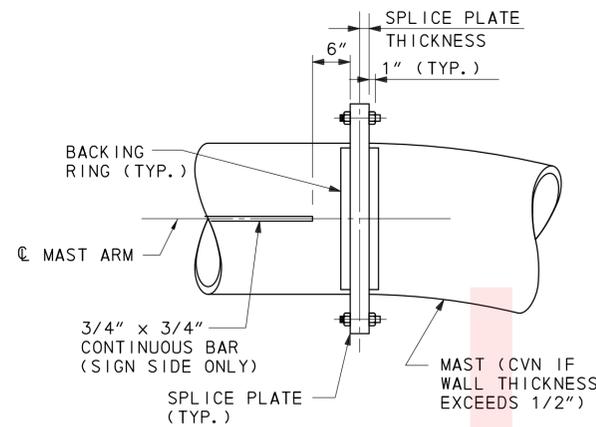
**OVERHEAD STRUCTURE  
BASE CONNECTION DETAILS**

(12 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)

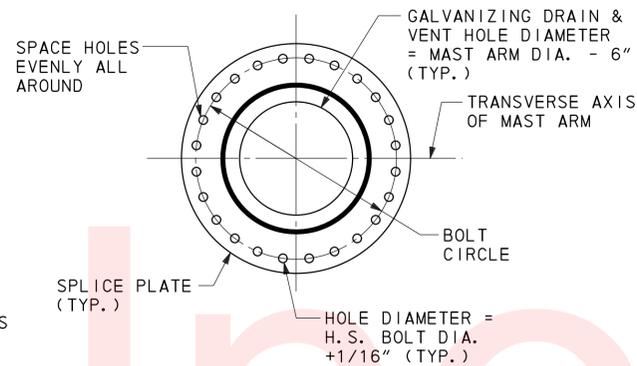


(24 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)

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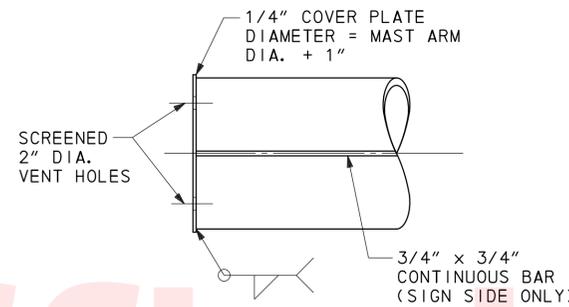
ELEVATION



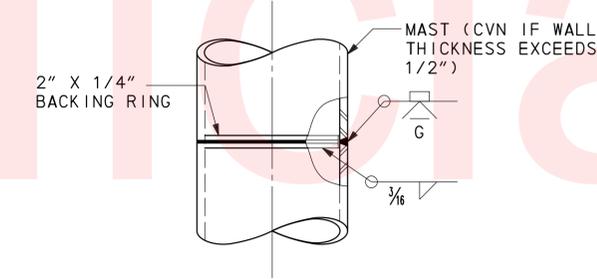
SECTION

**CANTILEVER STRUCTURE  
END CONNECTION DETAILS**

(MAST ARM SPLICE CONNECTION SIMILAR)  
(24 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)



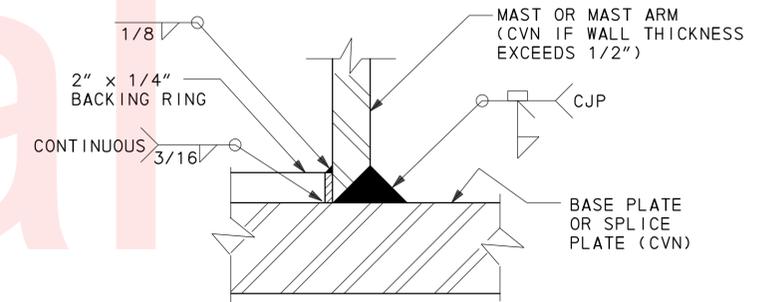
**MAST ARM END DETAIL**  
(CANTILEVER STRUCTURES)



**OPTIONAL SHOP  
CONNECTION DETAIL**

**NOTES:**

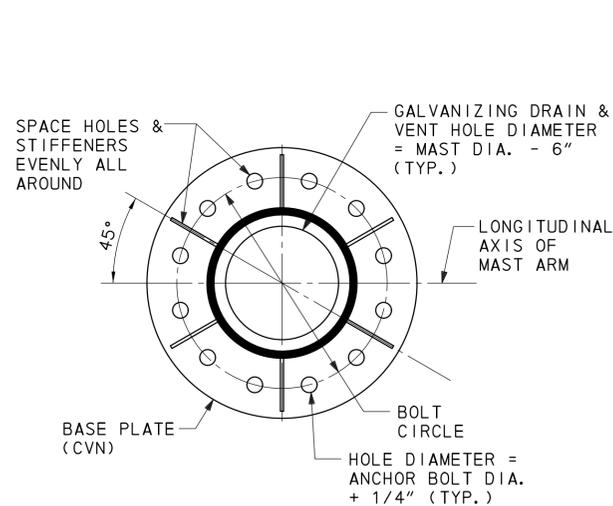
1. FOR GENERAL NOTES, SEE SHEET SS-01.
2. GROUT SHALL NOT BE PLACED BETWEEN BASE PLATE AND CONCRETE DRILLED SHAFT.
3. TIGHTEN ANCHOR BOLTS USING TURN-OF-NUT METHOD (ADDITIONAL 1/6 TURN AFTER SNUG TIGHT) ONCE SIGN PANEL IS LEVELED..
4. TERMINATE WELDS 1/2" SHORT OF STIFFENER ENDS AND STIFFENER CHAMFER.
5. PROVIDE STIFFENERS AS INDICATED IN TABLES.
6. FOR DRILLED SHAFT INFORMATION, SEE SHEET SS-08.
7. FOR SIGN PANEL SUPPORT BEAM DETAILS, SEE SHEET SS-07.
8. SEAL WITH GALVANIZED SCREEN, 1 1/4" TO 3/8" OPENING, TO PREVENT ENTRY OF RODENTS. SCREEN IS TO BE REMOVABLE AND ATTACHED TO BASE PLATE WITH STAINLESS STEEL HARDWARE. SCREEN IS TO BE OF SUFFICIENT STIFFNESS TO PREVENT ENTRY BETWEEN SCREEN AND FOUNDATION WHILE PERMITTING DRAINAGE.



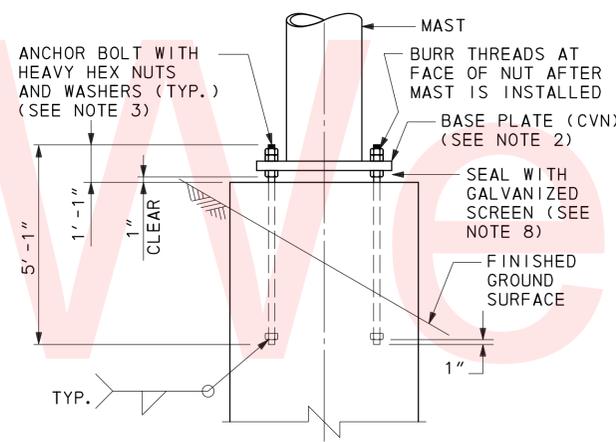
**WELD DETAIL**

**WELD DETAIL NOTE:**

BACKING RING MUST BE FITTED/SIZED TO THE PIPE COLUMN AND CONTINUOUSLY FILLET WELDED TO THE BASE PLATE BEFORE THE FULL PENETRATION GROOVE WELD IS MADE. BACKING RING MUST BE FABRICATED AS A CONTINUOUS RING.



SECTION

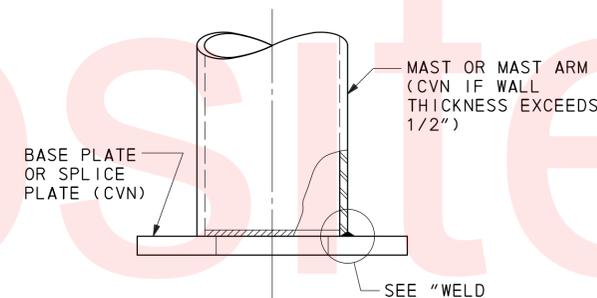


ELEVATION

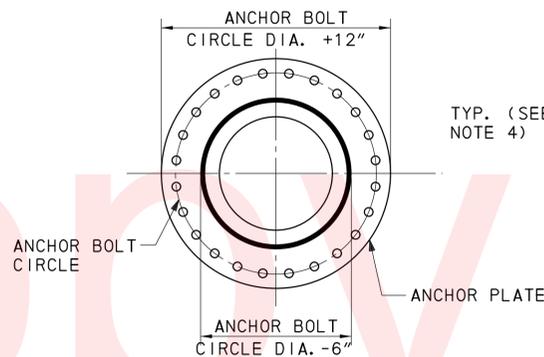
**CANTILEVER STRUCTURE  
BASE CONNECTION DETAILS**

(12 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)

NOTE: STIFFENERS NOT SHOWN FOR CLARITY



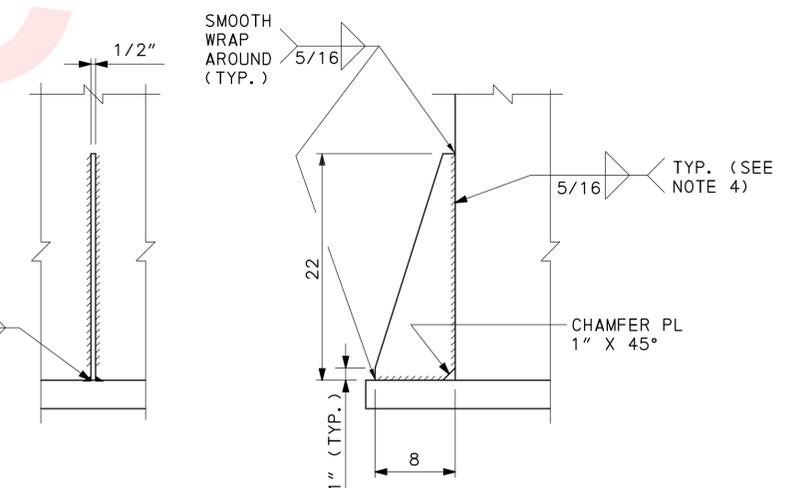
**PIPE TO PLATE  
CONNECTION DETAIL**



PLAN

**ANCHOR PLATE DETAIL**

(24 BOLT CONFIGURATION SHOWN, OTHER CONFIGURATIONS SIMILAR)



SECTION

ELEVATION

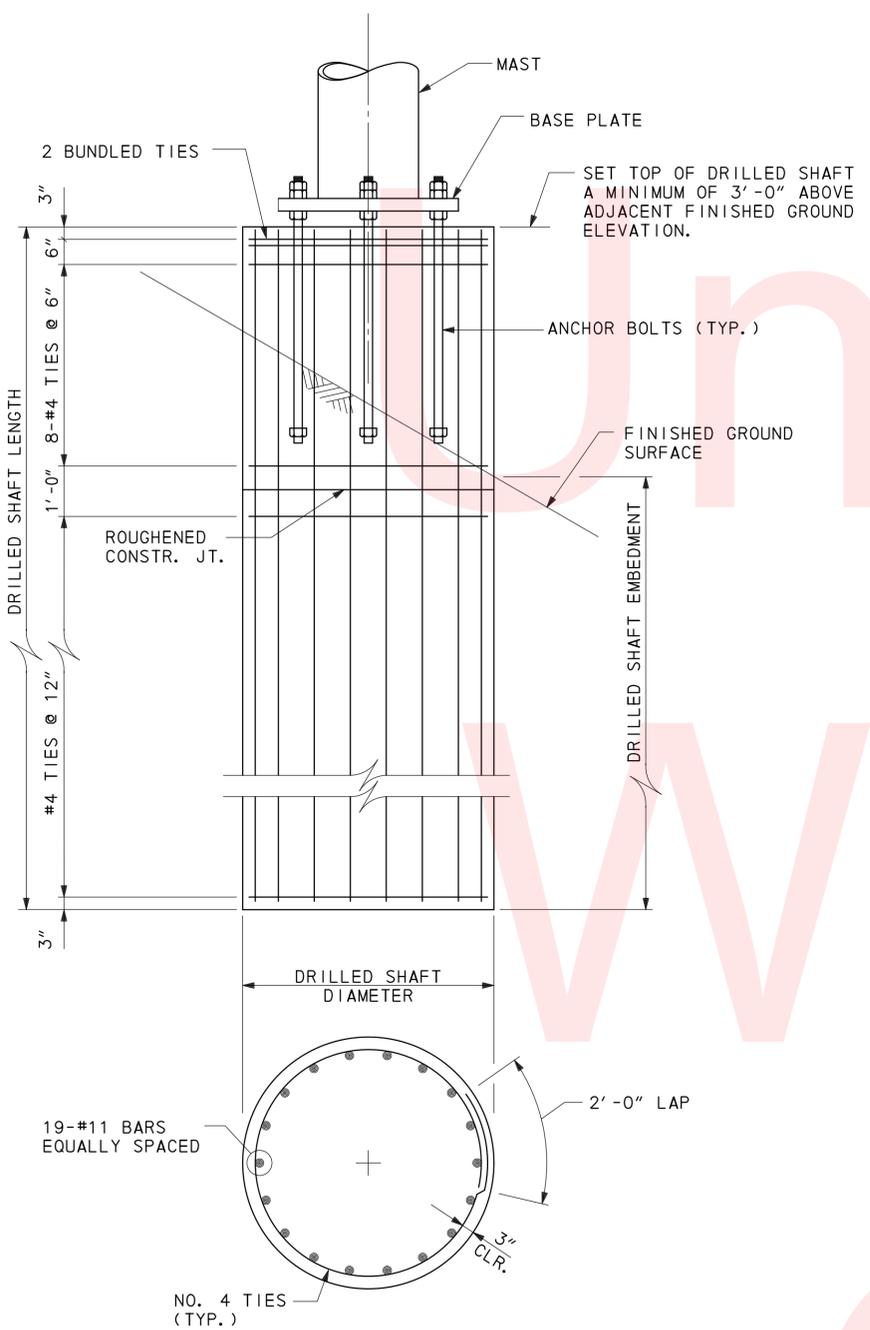
**STIFFENER DETAILS**

(BASE CONNECTION)

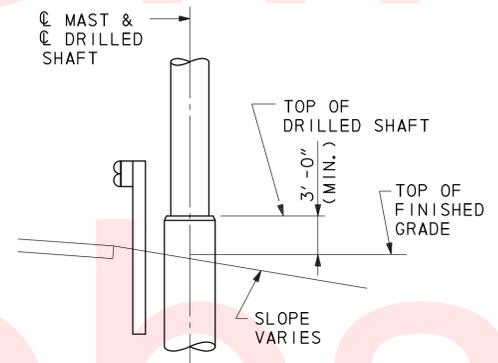
9/4/2015 2:47:39 PM \\BALSRV01\2012\12079\DEL1610\TASKS\TASK 7 LEFT EXIT SIGN DESIGN SERVICES\CADD\PLANS\STRUCTURES\WONDTUBE\_STRUCT\_DETAILS\CP78\_LEFT\_EXIT.DGN



9/4/2015 2:04:05 PM \\BALSRV01\2012\12079\DEL1610\TASKS\TASK 7 LEFT EXIT SIGN DESIGN SERVICES\CADD\PLANS\STRUCTURES\WINDTUBE STRUCT DETAILS\CP80.LEFT.EXIT.DGN



DRILLED SHAFT FOUNDATION DETAILS  
ROADSIDE INSTALLATION

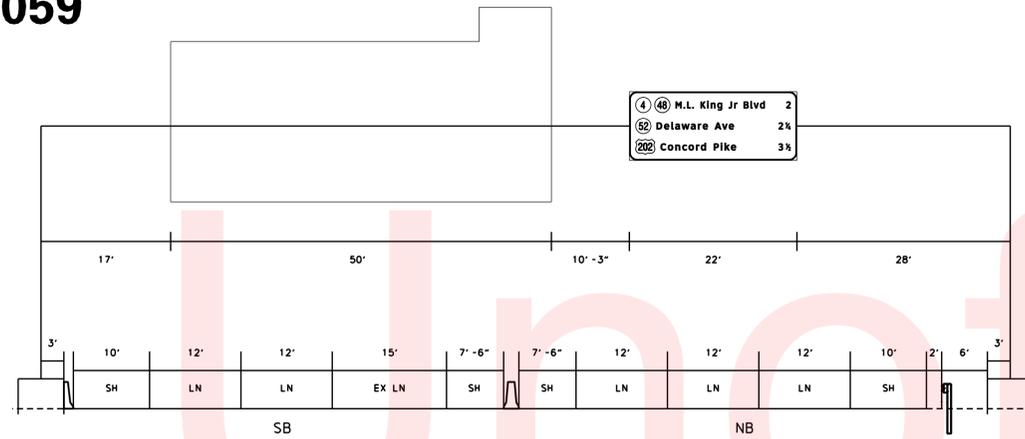


TYPICAL SHOULDER DETAIL

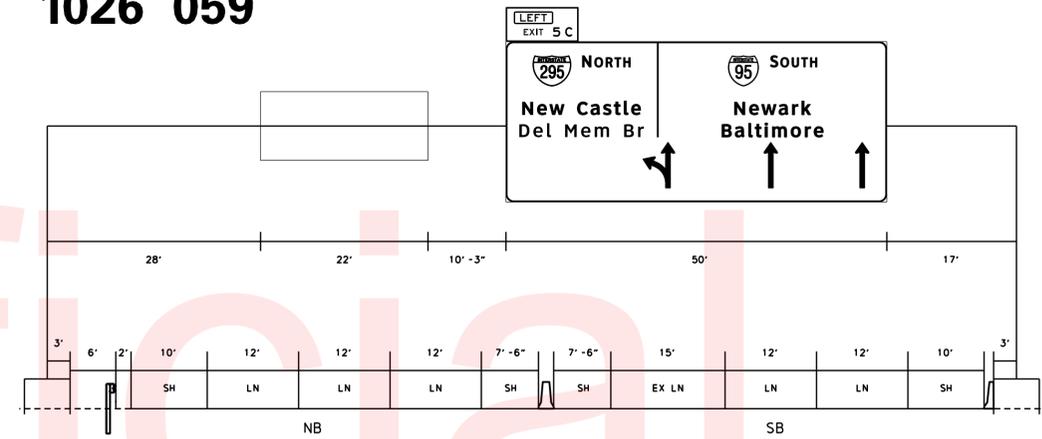
| SIGN STRUCTURE FOUNDATIONS |                             |  |                              |
|----------------------------|-----------------------------|--|------------------------------|
| SIGN STRUCTURE NUMBER      | DRILLED SHAFT DIAMETER (FT) | DRILLED SHAFT LENGTH (EA. FOUNDATION) (FT) | DRILLED SHAFT EMBEDMENT (FT) |
| SO 1026 059                | 5.0                         | 27.0                                       | 21.5                         |
| SO 1027 059                | 5.0                         | 27.0                                       | 21.5                         |
| SO 1028 059                | 5.0                         | 28.0                                       | 22.5                         |
| SO 1104 060                | 5.0                         | 29.0                                       | 23.5                         |
| SO 1124 060                | 5.0                         | 28.0                                       | 22.5                         |
| SO 1125 060                | 5.0                         | 22.0                                       | 16.5                         |
| SC 1105 060                | 5.0                         | 21.0                                       | 15.5                         |
| SC 1119A 060               | 5.0                         | 21.0                                       | 15.5                         |

NOTE: DRILLED SHAFT FOUNDATION SIZES, LENGTHS, AND REINFORCING LAYOUT SHOWN ARE INTENDED FOR ESTIMATION PURPOSES ONLY. THE FINAL DESIGN OF EACH DRILLED SHAFT FOUNDATION WILL BE PERFORMED BY THE ENGINEER ONCE SOIL BORING INFORMATION IS OBTAINED BY THE DEPARTMENT.

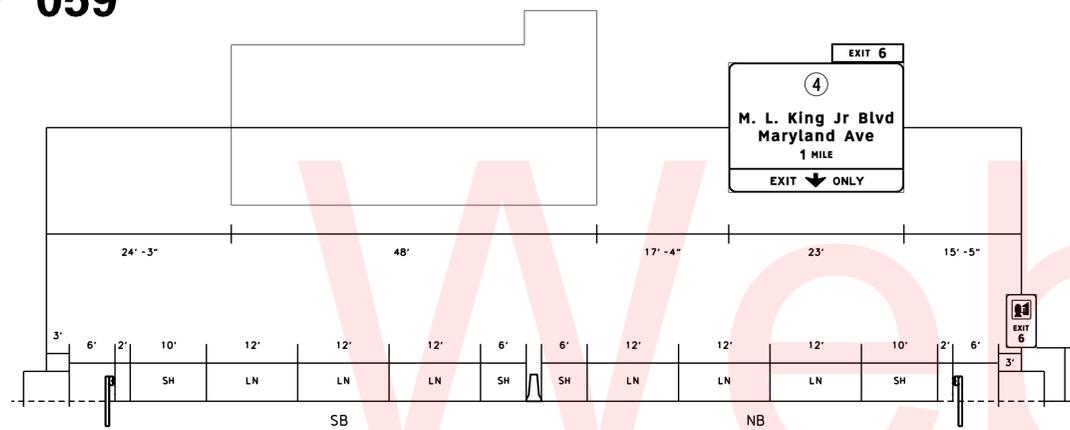
SO 1026 059  
NB



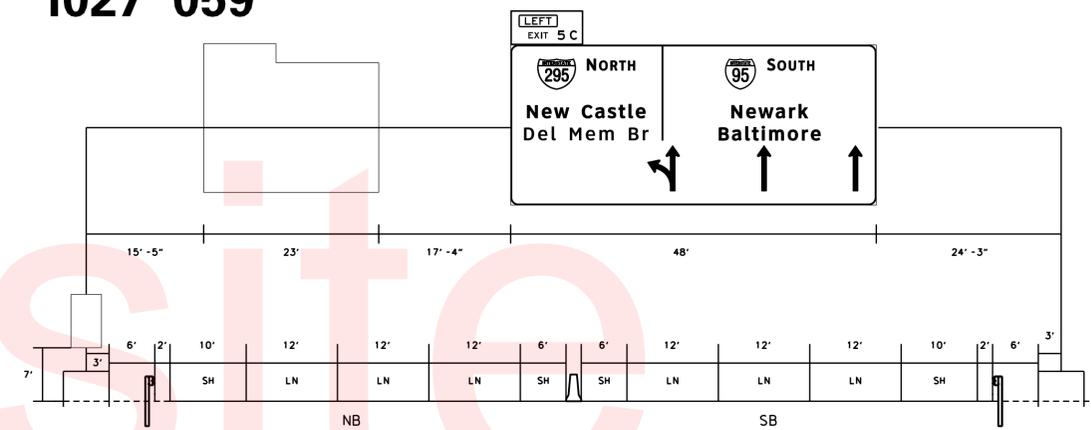
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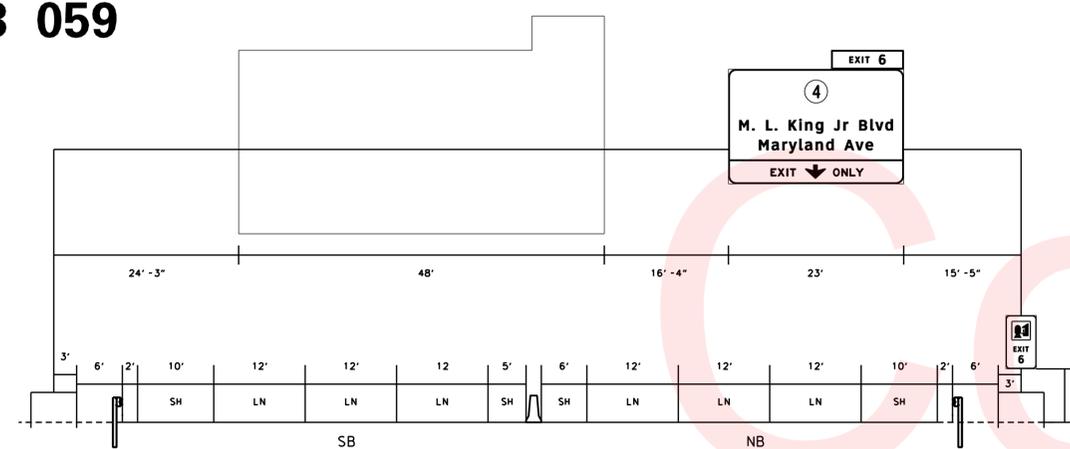
SO 1027 059  
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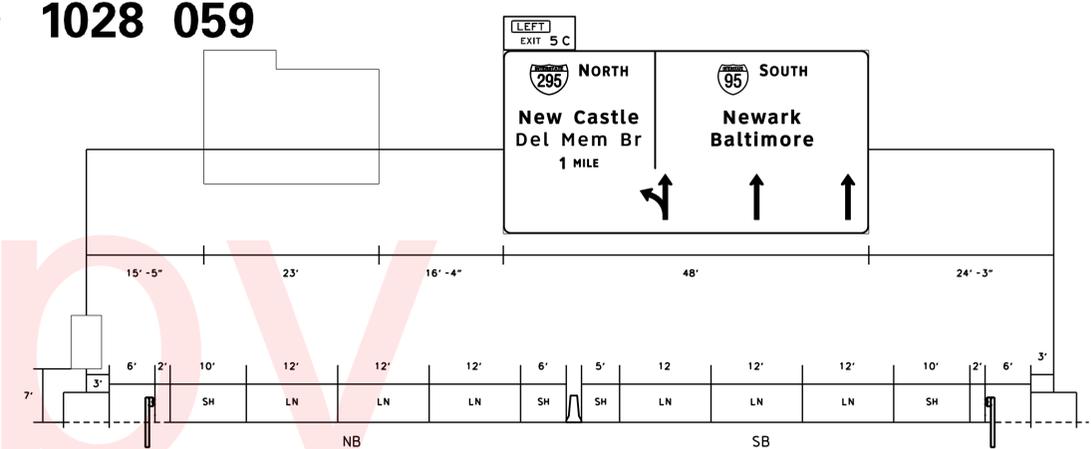
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SO 1028 059  
NB

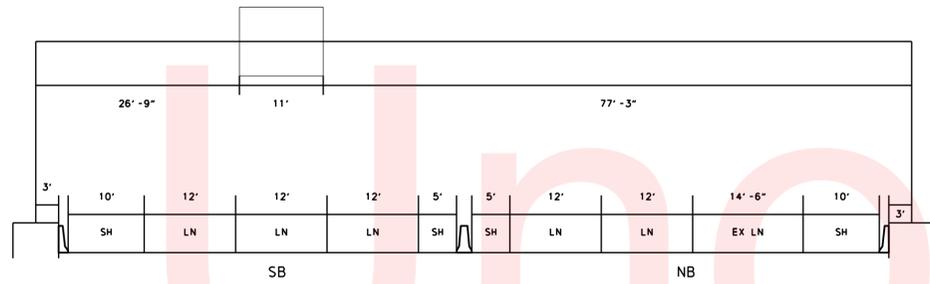


SO 1028 059  
SB

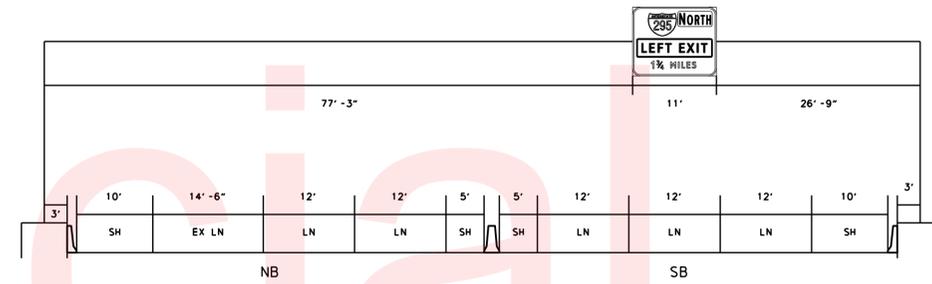


••PROPOSED CROSS SECTION ARE APPROXIMATE. CONTRACTOR SHALL VERIFY ALL LENGTHS PRIOR TO STRUCTURE DESIGN SUBMISSION.

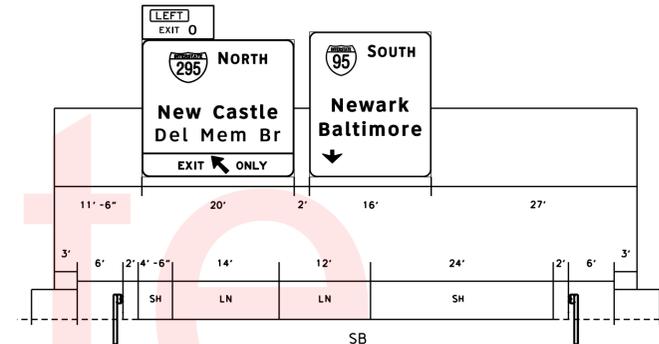
SO 1031 059  
NB



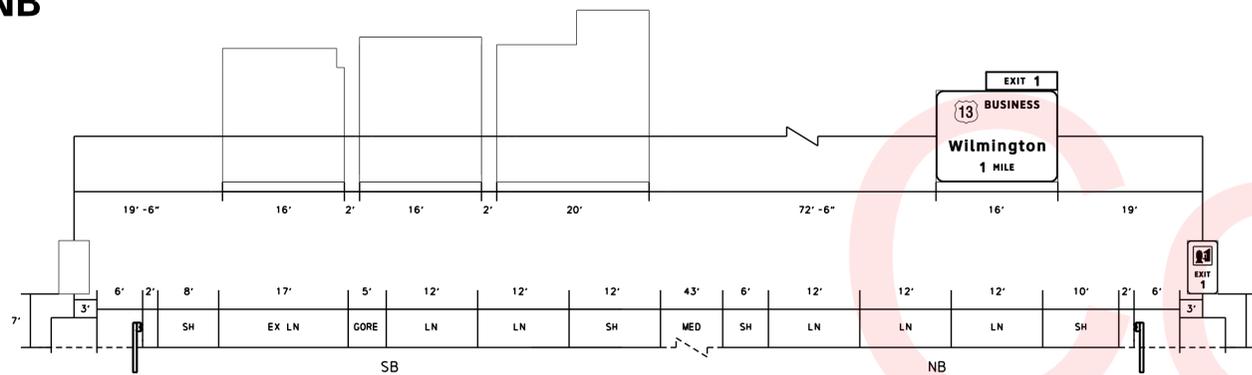
SO 1031 059  
SB



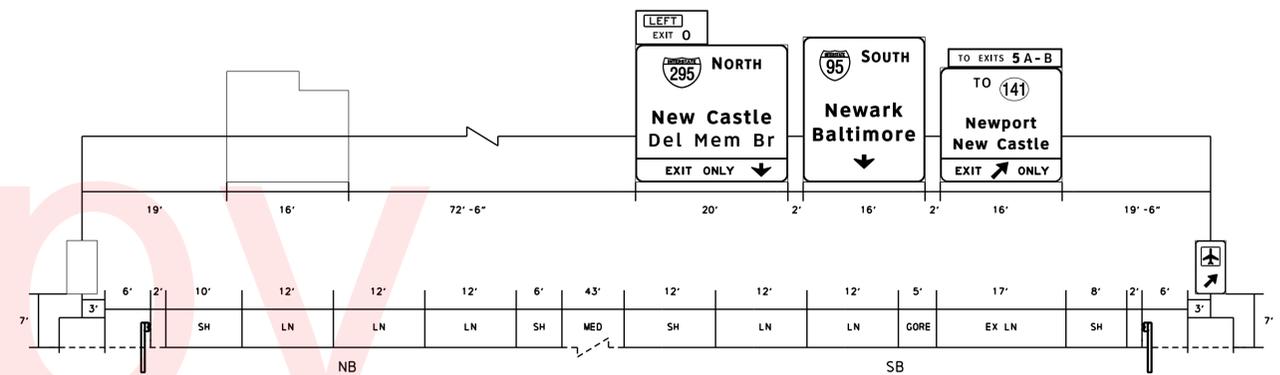
SO 1125 060  
SB



SO 1124 060  
NB

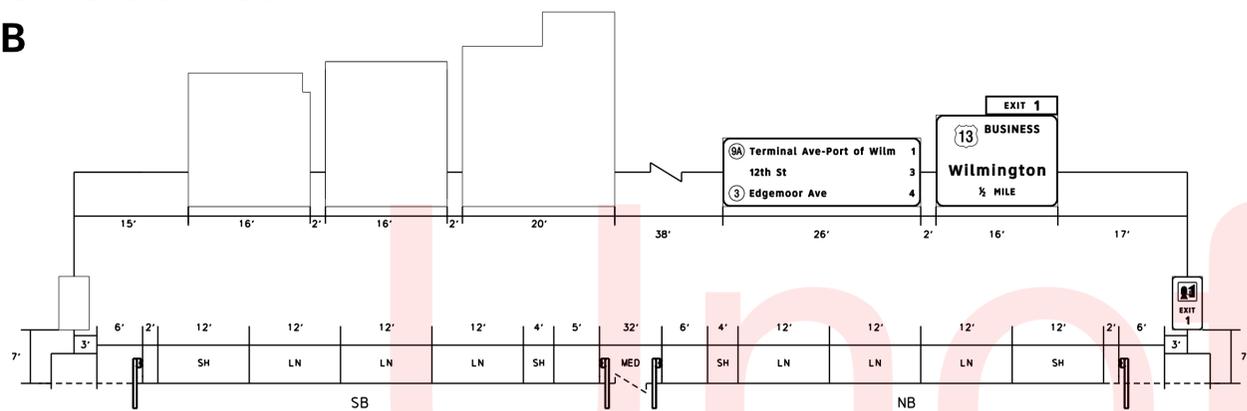


SO 1124 060  
SB

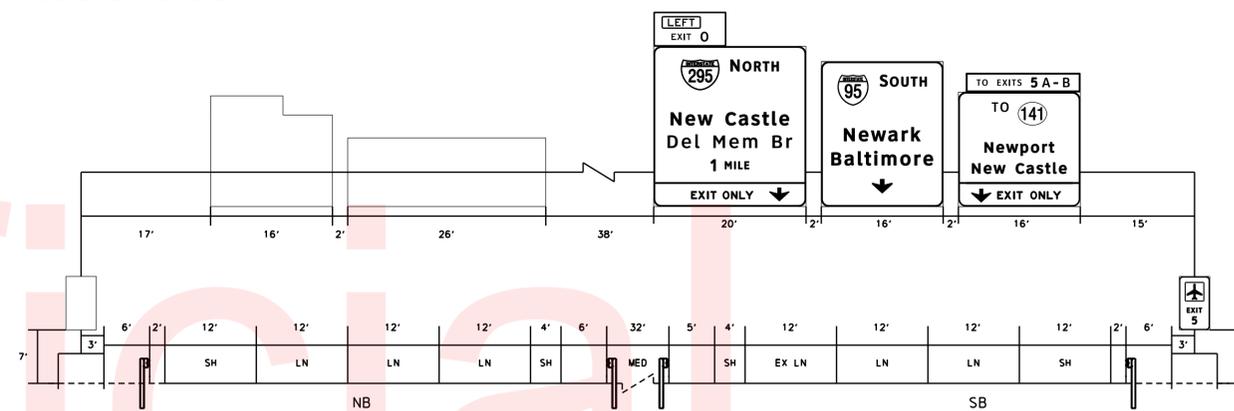


••PROPOSED CROSS SECTION ARE APPROXIMATE. CONTRACTOR SHALL VERIFY ALL LENGTHS PRIOR TO STRUCTURE DESIGN SUBMISSION.

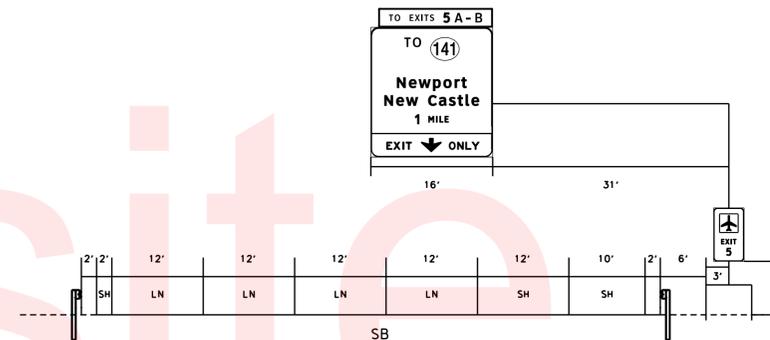
SO 1104 060  
NB



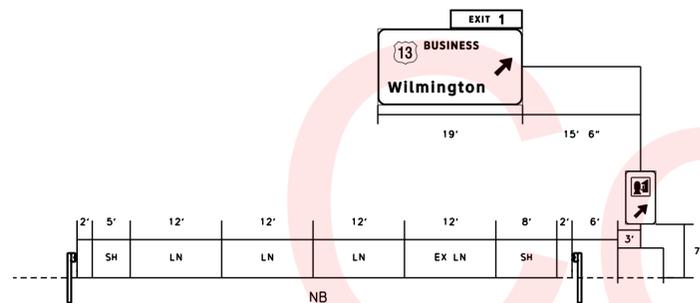
SO 1104 060  
SB



SC 1119A 060  
SB



SC 1105 060  
NB



••PROPOSED CROSS SECTION ARE APPROXIMATE. CONTRACTOR SHALL VERIFY ALL LENGTHS PRIOR TO STRUCTURE DESIGN SUBMISSION.