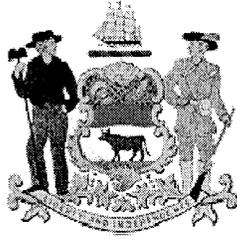


THE STATE OF DELAWARE



DEPARTMENT OF TRANSPORTATION
OFFICE OF TRANSPORTATION SOLUTIONS
PLANS AND SPECIFICATIONS FOR

CONTRACT NUMBER - T201506103

CANAL MAINTENANCE

FEDERAL AID NUMBER - ESTP-2015(17)

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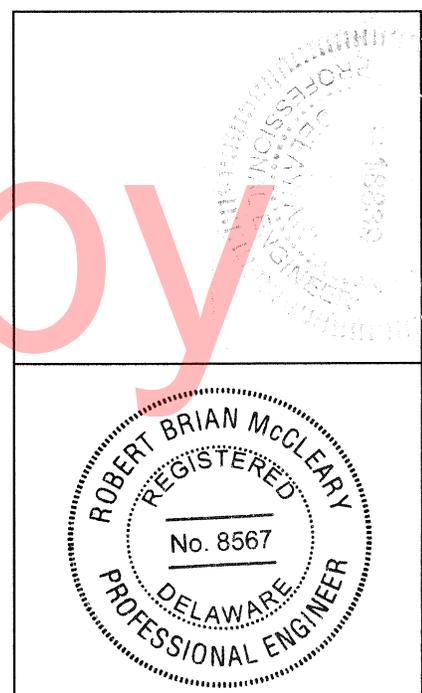
CONTRACT TITLE- PAVEMENT & REHABILITATION - NORTH III, 2015

PLANS PREPARED BY: George L. Cates
DESIGN TECHNICIAN

DATE RECOMMENDED: 5/11/15
[Signature]
GROUP ENGINEER, CONSTRUCTION - NORTH II

DATE RECOMMENDED: 5-8-2015
[Signature]
ASSIST. DIRECTOR, ENGINEERING SUPPORT

DATE RECOMMENDED: 5/11/2015
[Signature]
CHIEF ENGINEER



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DEPARTMENT OF TRANSPORTATION

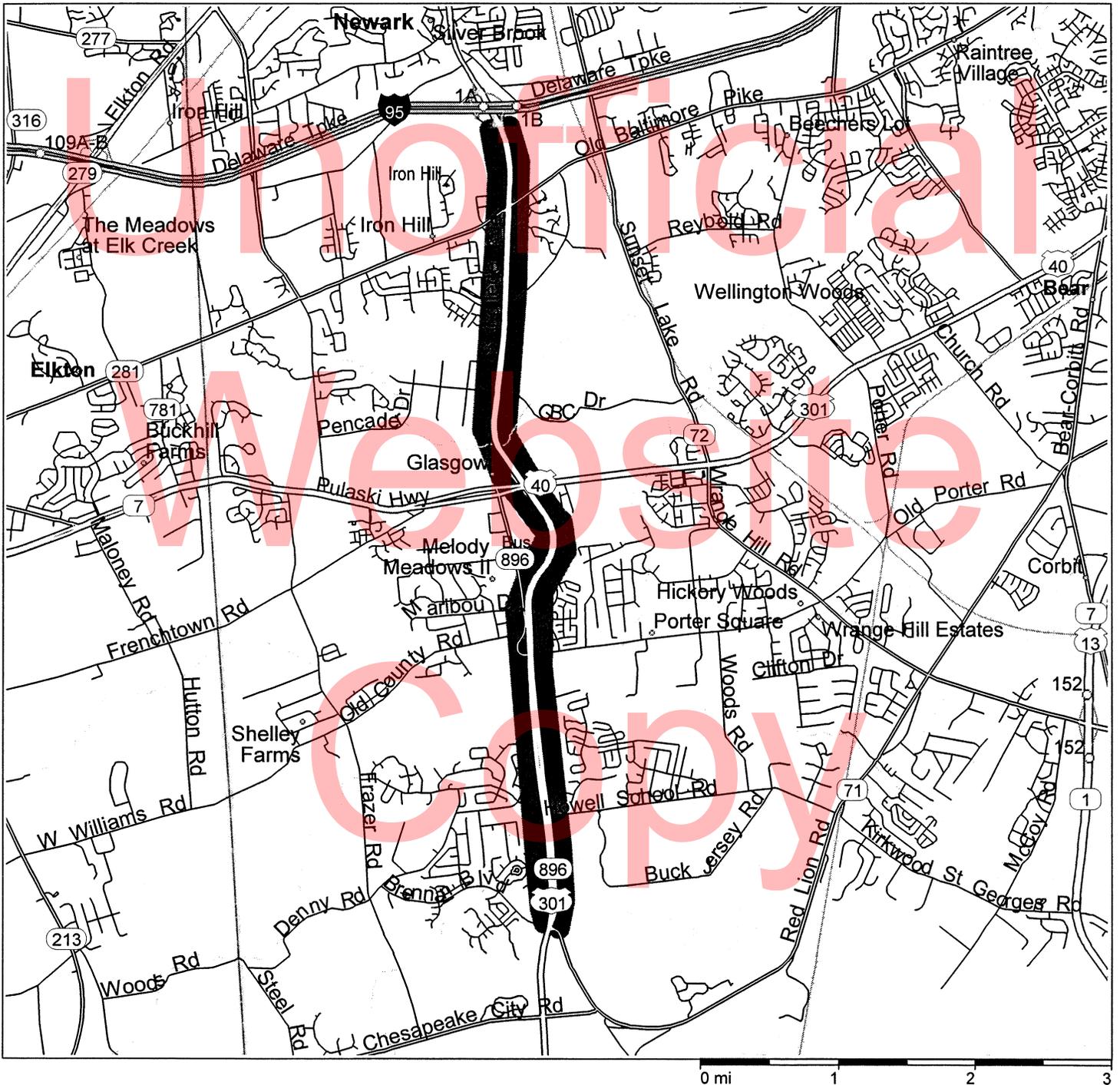
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SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB (387) FROM SUMMIT BRIDGE APPROACHES TO I-95 RAMPS CONCRETE JOINT

LOCATION MAP

Location #1 - Summit Bridge Road - Route 896 NB & SB from Red Lion Road to I-9...



GENERAL CONTRACT PLAN NOTES

REVISED 1/2015

GENERAL

1. CONSTRUCTION OF THIS CONTRACT SHALL CONFORM TO DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED AUGUST 2001 AS AMENDED BY THE SUPPLEMENTAL SPECIFICATIONS, THE SPECIAL PROVISIONS, THE MOST RECENT STANDARD CONSTRUCTION DETAILS, DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THESE PLANS.

2. THE PURPOSE OF THIS CONTRACT IS TO MAKE GENERAL IMPROVEMENTS TO ROADS AND/OR STREETS WITHIN THE DISTRICT HEREIN SPECIFIED. DEPENDING ON FUND AVAILABILITY, THE DEPARTMENT RESERVES THE RIGHT TO ADD OR DELETE LOCATIONS AND/OR QUANTITIES FOR THIS CONTRACT. SUCH LOCATION OR QUANTITY ADDITIONS OR DELETIONS SHALL NOT BE CAUSE FOR AN INCREASE OR DECREASE IN ANY CONTRACT UNIT BID PRICES. NO ITEM PRICES ARE TO BE RENEGOTIATED DUE TO EITHER AN INCREASE OR DECREASE IN QUANTITY USAGE RESULTING FROM SAID ADDITIONS OR DELETIONS.

3. THE CONTRACTOR SHALL PROVIDE ALL RESIDENTS AND BUSINESSES WHO LIVE ADJACENT TO THE WORK ZONE A MINIMUM 48 HR. PRIOR WRITTEN NOTICE FOR THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES, CONTRACTOR NAME & ADDRESS, AND DELDOT CONTACT NUMBERS. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND/OR PROPERTY OWNER IN ADVANCE OF THE CLOSURE.

4. THE CONTRACTOR SHALL SUBMIT THE REQUIRED COPIES OF A DETAILED PROGRESS SCHEDULE (PSC) AS OUTLINED IN STANDARD SPECIFICATION 108.04 PRIOR TO OR AT THE PRECONSTRUCTION MEETING. DETAILS SHALL INCLUDE A DESCRIPTION OF EACH WORK ACTIVITY, THE PLANNED DAYS OF WORK, MULTIPLE CREWS OR SHIFTS, AND SCHEDULED WORKING HOURS. DURING THE CONTRACT, THE CONTRACTOR SHALL SUBMIT TWO-WEEK (OR AS REQUIRED) "LOOK AHEAD" SCHEDULES TO THE AREA ENGINEER/ MANAGER NO LATER THAN NOON OF EACH THURSDAY. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

MAINTENANCE OF TRAFFIC

5. THE CONTRACTOR SHALL BE REQUIRED TO SHOW PROOF THAT HE HAS SUFFICIENT APPROVED STRIPING MATERIALS ON HAND TO ENSURE STRIPING IS COMPLETED PRIOR TO FULL OPENING ROADWAY TO TRAFFIC.

GENERAL CONTRACT PLAN NOTES

6. AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY STRIPING SHALL BE UTILIZED AT LOCATIONS THAT REQUIRE PERMANENT STRIPING. TEMPORARY PAVEMENT STRIPING MUST MATCH PERMANENT PAVEMENT STRIPING IN ALL REGARDS. TEMPORARY PAVEMENT MARKINGS SHALL BE PAID AT THE APPLICABLE CONTRACT UNIT PRICE. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE TEMPORARY MARKINGS IN GOOD CONDITION SUCH THAT THE PAVEMENT IS PROPERLY DELINEATED AT ALL TIMES. ANY REFRESHING OF THE TEMPORARY MARKINGS WILL BE AT THE CONTRACTOR'S EXPENSE.

7. ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.

8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO AND ALONG PEDESTRIAN FACILITIES AT ALL TIMES DURING CONSTRUCTION. EARTH, STONE AND GRAVEL SURFACES ARE NOT ACCEPTABLE FOR PROVIDING PEDESTRIAN ACCESS. WHERE PEDESTRIAN ROUTES ARE CLOSED OR BLOCKED, ALTERNATE PEDESTRIAN ACCESS SHALL BE PROVIDED USING TA-28 AND/OR TA-29 OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES OR TO THE SATISFACTION OF THE ENGINEER. PEDESTRIAN DETOUR ROUTES OR ALTERNATE PEDESTRIAN FACILITIES SHALL CONSIST OF ADA COMPLIANT FACILITIES TO THE LEVEL OF WHICH EXISTS ON THE PEDESTRIAN ROUTE. THE COST FOR ANY PROVISIONS SHALL BE INCIDENTAL TO ITEM 763XXX -MAINTENANCE OF TRAFFIC.

PEDESTRIAN MAINTENANCE OF TRAFFIC: THIS WORK SHALL CONSIST OF PROVIDING AND MAINTAINING AN ACCESSIBLE PEDESTRIAN ROUTE THROUGHOUT THE PROJECT'S LIMITS IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130.

THE CONTRACTOR SHALL BE REQUIRED TO REVIEW EACH CURB RAMP LOCATION AND SUBMIT THE APPROPRIATE MAINTENANCE OF TRAFFIC DETAIL AND DEVICES TO THE ENGINEER FOR EACH LOCATION AT LEAST 2 WEEKS BEFORE CONSTRUCTION FOR REVIEW, COMMENT, AND APPROVAL BY THE DISTRICT TRAFFIC SAFETY OFFICER

THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:

ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPLICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.

MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES AT ALL TIMES.

PROVIDE PEDESTRIAN ACCESS THROUGH OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN

GENERAL CONTRACT PLAN NOTES

THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE CURRENT CONDITIONS.

WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.

TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.

SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7 FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.

A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTIRE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS OR VERTICAL ELEVATION CHANGES GREATER THAN ¼ IN. IN GRADE OR TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIER TO WHEELCHAIR USE.

9. AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR SHALL BE REQUIRED FOR ALL LOCATIONS SPECIFIED IN THE CONTRACT PLANS. A COPY OF THE CERTIFICATION CARD FOR THE ATSSA TRAFFIC CONTROL SUPERVISOR SHALL BE SUBMITTED AT THE PRECONSTRUCTION MEETING. THE PAPER CERTIFICATE IS NOT ACCEPTABLE.

PAVEMENT MARKINGS

10. AT THE PRECONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT DETAILED DRAWINGS (INCLUDING BUT NOT LIMITED TO EXISTING STRIPING LENGTHS, LANE AND SHOULDER WIDTHS, TURN LANE LENGTHS, LOCATIONS OF STOP BARS, TURN ARROWS, CROSSWALKS AND RAILROAD CROSSINGS) THAT DEPICT THE EXISTING PAVEMENT MARKINGS FOR EACH PROJECT LOCATION. THESE DRAWINGS WILL BE REVIEWED BY THE DEPARTMENT'S TRAFFIC SECTION TO DETERMINE IF ANY CHANGES TO THE FINAL PAVEMENT MARKINGS ARE REQUIRED. FINAL PAVEMENT MARKINGS SHALL CONFORM TO ALL EXISTING PATTERNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, WHITE EDGE LINES SHALL WRAP AROUND THE RADIUS OF ALL SIDE STREETS AND MAJOR COMMERCIAL ENTRANCES TO A TANGENT POINT. YELLOW CENTERLINES SHALL BE CONTINUOUS AROUND MEDIAN ISLANDS. ALL DOUBLE YELLOW CENTERLINES SHALL BE PLACED IN A 5-6-5 CONFIGURATION (2 EACH, 5" YELLOW STRIPES WITH A 6" GAP BETWEEN).

11. ANY ERRONEOUS MARKING WILL NOT BE PAID FOR AND SHALL BE CORRECTED IMMEDIATELY AT THE CONTRACTORS EXPENSE. ERRONEOUS MARKINGS OR SHADOWS THAT EXCEED ONE (1) INCH IN WIDTH SHALL BE REMOVED BY EITHER SAND OR WATER BLASTING AS

GENERAL CONTRACT PLAN NOTES

DIRECTED BY THE ENGINEER. NO OTHER REMOVAL METHODS WILL BE ALLOWED. A FLAT BLACK PAINT OR DRIVEWAY SEALER SHALL BE APPLIED IN THE AREA OF THE REMOVED MARKING TO MASK THE REPAIR. ANY DAMAGE TO THE PAVEMENT CAUSED BY REMOVAL OF ERRONEOUS MARKINGS SHALL BE REPAIRED / REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTORS EXPENSE.

12. STOP LINES "BARS" SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.16 OF THE 2011 DELAWARE MUTCD.

13. ALL CROSSWALKS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.18 OF THE 2011 DELAWARE MUTCD.

TRAFFIC SIGNAL NOTES

14. TRAFFIC DETECTION LOOPS SHALL BE PLACED IN THE FINAL WEARING SURFACE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

15. TRAFFIC DETECTION LOOPS THAT ARE CURRENTLY 5'x7' SHALL BE REPLACED WITH LOOPS THAT ARE 6'x6' AT THE SAME LOCATION. STOP BAR DETECTION LOOPS SHALL BE THE SAME SIZE AS CURRENTLY EXISTING, AND SHOULD BE PLACED STARTING TWO FEET BEHIND THE STOP BAR, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF SEPARATE SIGNAL PLANS HAVE BEEN DEVELOPED, THE PLANS SUPERSEDE THIS NOTE.

PROJECT

16. UNLESS SPECIFIED, OVERLAY WIDTHS SHALL MATCH EXISTING WIDTHS.

17. THE COST OF CLIPPING BACK THE EDGES OF THE ROADWAY, SHOULDERS, AND THE FIRST 3' OF UNPAVED DRIVEWAY ENTRANCES, PICKING UP AND DISPOSING OF WASTE AND EXCESS MATERIAL, AND CLEANING THE EXISTING PAVEMENT PRIOR TO OVERLAY SHALL BE INCIDENTAL TO SECTION 401(). PREPARING UNPAVED DRIVEWAY ENTRANCES BEYOND THE FIRST 3' SHALL BE PAID UNDER THE APPROPRIATE CONTRACT ITEMS: 202000 AND/OR 30200X.

18. BUTT JOINTS SHALL BE PLACED AT ALL INTERSECTING HOT MIX ROADS AND ANY OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. BUTT JOINTS AT DRAINAGE STRUCTURES NOT TO BE RESURFACED SHALL BE AS PER THE INCLUDED PLAN DETAIL OR LOCATION NOTES. BUTT JOINTS CUT PRIOR TO THE DAY OF THE PAVEMENT OVERLAY SHALL BE RAMPED WITH HOT MIX TRM. IN MILLING AREAS, THERE WILL BE NO SEPARATE PAYMENT FOR BUTT JOINTS. AFTER PAVEMENT MILLING, ALL TRANSVERSE VERTICAL DIFFERENCES, RAISED EDGES OF MANHOLES, CATCH BASINS, WATER VALVE BOXES, ETC...SHALL BE RAMPED WITH HOT-MIX TRM AT A 20:1 OR FLATTER SLOPE PRIOR TO OPENING THE ROADWAY TO TRAFFIC. PAVEMENT MILLINGS WILL NOT BE ALLOWED FOR USE AS RAMPING MATERIAL.

19. TAPER MILLING IS INTENDED FOR MILLING IN THE AREA OF FIXED STRUCTURES (I.E. CURBS GUARDRAIL, ETC.) THE MILLED DEPTH AT THE STRUCTURE SHALL BE THE DEPTH OF THE

GENERAL CONTRACT PLAN NOTES

PROPOSED OVERLAY AND 0" AT A DISTANCE OF +/- 6½' FROM THE STRUCTURE. THE COMPUTED DEPTH FOR PAYMENT PURPOSES IS THE AVERAGE OF THE TWO DEPTHS.

20. ALL MILLED MATERIAL SHALL REMAIN THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE STATED.

21. EXCAVATION UP TO 1' IN FRONT OF CURB WILL BE INCIDENTAL TO ITEM 701XXX. HOT MIX PLACED IN FRONT OF CURB WILL BE PAID UNDER ITEM 401XXX. USE OF CONCRETE AS FILL IN FRONT OF CURB WILL NO LONGER BE ACCEPTED UNLESS DIRECTED BY THE ENGINEER. AT EACH LOCATION, BACKFILLING SHALL BE PERFORMED IMMEDIATELY UPON REMOVAL OF FORMS FOR CURB OR SIDEWALK. TOPSOIL, SEEDING, SEALING OF VALLEY GUTTERS, REMOVING OF FORMS, AND PARGING OF CATCH BASINS, SHALL BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS OF COMPLETION OF THE ITEM OF WORK. ITEM 732XXX, TOPSOIL, SHALL BE USED AS BACKFILL MATERIAL BEHIND CURB AND AROUND SIDEWALK OR/AS DIRECTED BY THE ENGINEER. TOPSOIL SHALL BE SCREENED/SIFTED SO AS NO MATERIAL GREATER THAN .5" IS PRESENT IN MATERIAL USED FOR BACKFILL. EXCAVATED MATERIAL, NOT NEEDED ON THE PROJECT, SHALL BE REMOVED FROM THE CONTRACT AT THE CONTRACTORS EXPENSE. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

22. ALL ADJUSTMENTS SHALL BE MADE NO MORE THAN SEVEN (7) CALENDAR DAYS PRIOR TO THE PLACEMENT OF THE FINAL COURSE OF HOT MIX UNLESS OTHERWISE APPROVED BY THE ENGINEER. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

23. ALL PAVING, INCLUDING TURN LANES, SHOULDERS AND INTERSECTIONS, IS TO BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS FROM THE TIME THE AREA WAS MILLED. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

24. THE CONTRACTOR SHALL TAKE CARE IN REMOVING PAVEMENT AROUND UTILITIES, BUTT JOINTS, CURBS, ETC. SO THAT EXISTING PAVEMENT BEYOND THE SPECIFIED DEPTHS IS NOT DAMAGED. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS MAY RESULT IN PLACING LEVELING COURSES AT THE CONTRACTOR'S EXPENSE. THE REMOVAL AND CLEAN UP OF THE HOT MIX RESIDUE WEDGE REMAINING AFTER MILLING OPERATIONS SHALL BE INCIDENTAL TO THE MILLING ITEM. THE REMOVAL OF EXISTING RAISED PAVEMENT MARKERS (RPM'S) SHALL BE INCIDENTAL TO THE APPLICABLE MILLING AND/OR RECLAMATION ITEMS.

25. ALL PORTLAND CEMENT CONCRETE MUST BE RECEIVED FROM AN APPROVED PRODUCTION PLANT UNLESS OTHERWISE APPROVED BY THE ENGINEER. ONLY CALIBRATED VOLUMETRIC CONCRETE TRUCKS WILL BE PERMITTED TO PROVIDE ON-SITE MIXTURES. THIS PERTAINS TO BOTH WORK OCCURRING DURING NORMAL WORKING HOURS AND NIGHTTIME RESTRICTED WORK.

26. DRAINAGE INLETS, WHICH ARE 4' GREATER IN DEPTH AND ARE SLATED FOR REPAIR WILL

GENERAL CONTRACT PLAN NOTES

HAVE STEPS INSTALLED IN ACCORDANCE WITH SECTION 708.07 OF THE STANDARD SPECIFICATIONS. PAYMENT SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR ITEM 710001.

27. THE PORTLAND CEMENT CONCRETE USED WITHIN ITEM 710002 - ADJUST AND REPAIR EXISTING MAN HOLES AND ITEM 750000 - ADJUST WATER VALVE BOX SHALL CONFORM TO THE REQUIREMENTS OF CLASS A CONCRETE AS STATED WITHIN SECTION 503.02 OF THE DEPARTMENT'S STANDARD SPECIFICATIONS. ALSO, THE PERIMETER OF THE UTILITY ADJUSTMENT WILL BE SEALED AND WILL BE CONSIDERED INCIDENTAL TO THE APPLICABLE ADJUSTMENT ITEM.

28. WHEN JACKHAMMERING OR THE USE OF A BREAKER IS NECESSARY ON CONTRACTS THAT OCCUR DURING THE NIGHTTIME HOURS THE JACKHAMMERING AND BREAKING MUST CEASE AT 12:00AM REGARDLESS OF ANY NOISE WAIVER THAT MAY EXIST.

29. CURB RAMP AND/OR SIDEWALK CONSTRUCTION (NOT INCLUDING MEDIAN ISLANDS) SHALL BE COMPLETED DURING A SINGLE OPERATION. THE RAMP/SIDEWALK SHALL BE REMOVED AND PLACED BACK THE SAME NIGHT/DAY AND OPEN TO PEDESTRIAN TRAFFIC THE FOLLOWING DAY. THE USE OF A MONOLITHIC POUR SHALL BE PERMITTED WITH A BOND BREAKER TO BE PLACED AT THE BACK OF THE CURB AND EDGE OF SIDEWALK. IF THE CURB RAMP/SIDEWALK CANNOT BE COMPLETED IN A SINGLE OPERATION THE EXCAVATED AREA MUST BE BACKFILLED TO GRADE WITH COMPACTED MILLING MATERIAL ONLY. THIS MATERIAL WILL BE INCIDENTAL TO THE CURB RAMP ITEM. THE DRESSING UP OF THE AREA BEING PAID AS A CURB RAMP WITH TOPSOIL AND SEED WILL BE INCIDENTAL TO THE CURB RAMP ITEMS.

30. ANY MILLING MATERIAL REQUIRED TO MAKE AREAS ADA COMPLIANT IF NOT COMPLETED IN THE SAME SHIFT AT CURB, SIDEWALK, CURB RAMPS, ISLANDS, ETC. WILL BE INCIDENTAL TO THE PAY ITEM ASSOCIATED WITH THE WORK. MILLING MATERIAL IS THE ONLY APPROVED MATERIAL FOR THIS PRACTICE.

31. WHEN CONTRACT INCLUDES ITEMS 503001 AND 503002 PATCHES WILL BE PRESUMED TO BE STANDARD PATCH SIZES AND LOCATIONS WILL BE DETERMINED AT THE CONSTRUCTION PRE-WALK. THE USE OF STEEL PLATES WILL BE PROHIBITED. ROADWAY MUST BE RETURNED TO UNRESTRICTED USE AT THE END OF EACH WORK DAY. ALL PCC PAVEMENT WILL BE PRESUMED TO BE 9".

32. ITEM 763621 CONSTRUCTION ENGINEERING, REHABILITATION WILL COMPENSATE THE CONTRACTOR FOR THE FOLLOWING SURVEY ACTIVITIES. ANY LAYOUT ASSOCIATED WITH ADA IMPROVEMENTS, PROVIDING GRADES FOR UTILITY ADJUSTMENTS IF NECESSARY, THE LAYOUT OF LANE LINES FOR PAVING, GRADES FOR THE INSTALLATION OF DRAINAGE STRUCTURES OR OTHER ITEMS OF WORK AS DETERMINED BY THE ENGINEER OR PROGRAM MANAGER. THIS ITEM WILL NOT COMPENSATE THE CONTRACTOR FOR THE LAYOUT OF STRIPING AS THIS IS THE RESPONSIBILITY OF THE STRIPING SUBCONTRACTOR. THE HOURLY PAYMENT WILL BE BASED UPON ACTUAL HOURS OF SURVEYING PERFORMED IN THE FIELD. IT WILL NOT INCLUDE TIME SPENT IN VEHICLE OR OFFICE. THE SURVEY CREW WILL BE RESPONSIBLE UPON ARRIVAL AND DEPARTURE ON THE JOBSITE TO CONTACT A MEMBER OF THE INSPECTION STAFF SO THAT

GENERAL CONTRACT PLAN NOTES

HOURS CAN BE TRACKED FOR PAYMENT. THE CONSEQUENCE FOR NOT DOING THIS WILL BE NON-PAYMENT FOR THE SURVEYING SERVICES.

33. UPON RECEIPT OF THE MONTHLY PAY ESTIMATE ANY ISSUES OR DISCREPANCIES SHOULD BE BROUGHT TO THE ATTENTION OF THE ENGINEER/PROGRAM MANAGER IF POSSIBLE BEFORE THE ISSUANCE OF THE NEXT PAY ESTIMATE. TO ALLOW FOR INVOICES FROM SUBCONTRACTORS ETC., A 60 DAY TIME PERIOD TO REPORT DISCREPANCIES WILL BE PERMITTED. ANY DISCREPANCIES BEYOND THE 60 DAY TIME PERIOD WILL NOT BE ENTERTAINED.

34. FOR ITEM 743007 TRAFFIC OFFICERS, REQUIRED INVOICES WILL BE REVIEWED FOR ACCURACY. ANY MAJOR INACCURACIES WILL BE ADJUSTED ACCORDINGLY. FOR EXAMPLE, IF INVOICE SHOWS CHARGED HOURS ON A CERTAIN DATE AND WE HAVE DOCUMENTATION THAT NO WORK OCCURED THAT DATE OR NO OFFICERS WERE PRESENT THE INVOICE WILL BE ADJUSTED ACCORDINGLY.

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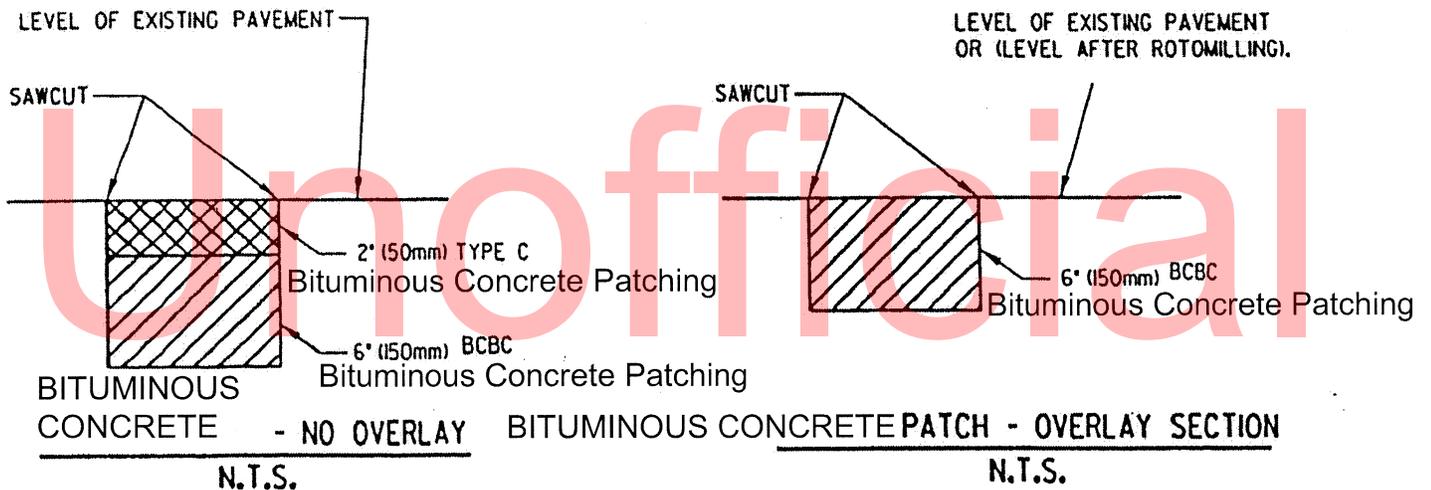
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CANAL

SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB (387) FROM SUMMIT I

DETAILS SHEET



•UNDERCUT EXCAVATION PATCHING TO BE UTILIZED BELOW 8" (200mm) WHEN DIRECTED BY THE ENGINEER.

•UNDERCUT EXCAVATION PATCHING TO BE UTILIZED BELOW 6" (150mm) WHEN DIRECTED BY THE ENGINEER.

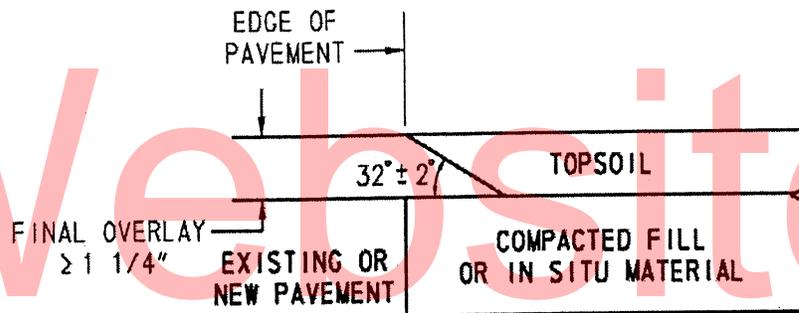
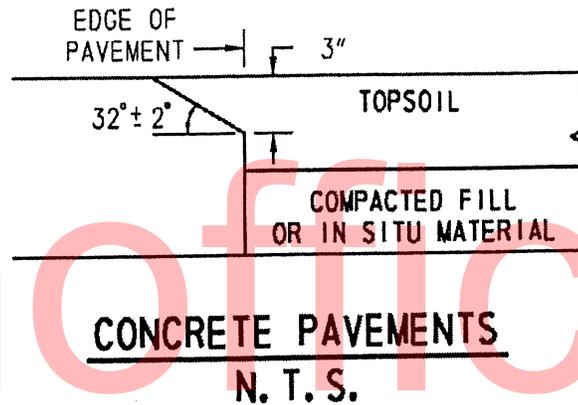
NOTE: PREFERRED EXCAVATION METHOD TO BE BY ROTOMILLING. SAWCUTTING EDGES SHALL BE AS DIRECTED BY ENGINEER AND DEPENDENT ON THE CONDITION LEFT BY THE ROTOMILLING OPERATION. BACKFILL FOR UNDERCUT EXCAVATION SHALL BE SUPERPAVE BCBC PATCH (SEPERATE LIFT). MINIMUM WIDTH OF PATCHING 6' (1.8M).

THIS DETAIL SHALL BE USED FOR ALL HOT MIX PATCHES UNLESS OTHERWISE SHOWN ON THE PLANS. ALL HOT MIX TO BE PLACED USING A PAVER.

WHEN PCC PATCHING IS REQUIRED ON A COMPOSITE PAVEMENT(HOT-MIX OVER CONCRETE), THE CONTRACTOR MAY ELECT TO ELIMINATE ANY GRADE DIFFERENCE BETWEEN THE TOP OF THE CONCRETE PATCH AND THE TOP OF THE EXISTING HOT-MIX PAVEMENT BY EITHER PLACING HOT-MIX TRM MATERIAL(PAYMENT UNDER ITEM 402000) OR PLACING CONCRETE FLUSH WITH EXISTING HOT-MIX PAVEMENT(ADDITIONAL CONCRETE PAID AS ITEM 503503). IF CONCRETE IS PLACED FLUSH WITH EXISTING HOT-MIX, THE ROTOMILLING OF THE CONCRETE WILL BE PAID FOR AS ROTOMILLING HOT-MIX.

P.C.C. PATCHING NOTES

DETAILS SHEET



Prior to placing concrete or bituminous concrete pavement, prepare the shoulder material where the Safety Edge will be placed to provide a foundation that will support its placement.

The Safety Edge is installed during a bituminous concrete resurfacing project using a special, removable wedge shape compaction device attached to and extending below the screed strike-off plate of the paver. This device is used to construct a pre-compacted, long lasting, low angle wedge fillet on the outside edge of the paved mat. The device has a self-adjusting internal spring that allows it to follow the surface independent of other paver components. The device has an angled surface that compacts the asphalt as it enters the device while another fixed-angle surface forms the tapered edge. As the asphalt continues under the wedge-forming surface, the asphalt is smoothed to create a finished surface on the tapered edge. Two wedge shape compaction devices that are commercially available are TransTech's Shoulder Wedge Maker and Advant-Edge Paving Equipment's Advant-Edger. The use of a single plate strike off is not allowed. Compaction of the edge should not be done with the first pass of the roller in order to give the Safety Edge a chance to harden some.

For concrete pavement or overlay, modify the paver screed to create the shoulder wedge as per the Safety Edge cross section.

LOCATION 1 387(SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB) FROM RED LION ROAD -
ROUTE 71 TO I-95 RAMPS CONCRETE JOINT

LENGTH 9981.5904 m 32748 L.F. AADT 38941

GENERAL IMPROVEMENTS

SEE COMMENTS PAGE FOR GENERAL IMPROVEMENTS

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 1

302005	GRADED AGGREGATE BASE COURSE, TYPE B	85.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	160.00 TON
401754	FOG SEAL	750.00 GAL
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	585.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, P 64-22, PATCHING	170.00 TON
401830	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)	6,100.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	20.00 TON
406001	BITUMINOUS CONCRETE PATCHING	3,080.00 SY-IN
501502	DIAMOND GRINDING P.C.C. PAVEMENT	16,738.00 S.Y.
501534	INTERFACE JOINT SEALING REPAIR	36,150.00 L.F.
503001	PATCHING P.C.C. PAVEMENT, 6' TO 15', TYPE A	210.00 S.Y.
503002	PATCHING P.C.C. PAVEMENT GREATER THAN 15' TO 100', TYPE B	240.00 S.Y.
503006	DOWEL BARS	384.00 EACH
503501	CRACK AND JOINT SEALING LESS THAN 3/4" WIDE	262,458.00 L.F.
503502	CRACK AND JOINT SEALING 3/4" TO 1 3/4" WIDE	10,071.00 L.F.
503503	** PATCHING CONCRETE	13,800.00 SY-IN
503517	P.C.C. PATCHING, PARTIAL DEPTH	200.00 SY-IN
701011	PORTLAND CEMENT CONCRETE CURB, TYPE 2	340.50 L.F.
701020	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 1-8	20.00 L.F.
701022	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8	43.00 L.F.
705001	P.C.C. SIDEWALK, 4"	575.00 S.F.
705002	P.C.C. SIDEWALK, 6"	810.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	50.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	700.00 S.F.
705010	CURB RAMP, TYPE 5	165.75 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	1.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	1.00 EACH

LOCATION 1 **387(SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB) FROM RED LION ROAD -
ROUTE 71 TO I-95 RAMPS CONCRETE JOINT**

LENGTH 9981.5904 m 32748 L.F. AADT 38941

GENERAL IMPROVEMENTS

SEE COMMENTS PAGE FOR GENERAL IMPROVEMENTS

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 1

710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	4.00 EACH
743003	ARROWPANELS, TYPE C	270.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	20.00 EA-DY
743005	FURNISH AND MAINTAIN PORTABLE LIGHT ASSEMBLY	720.00 EA-DY
743007	** TRAFFIC OFFICERS	2,000.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	270.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	3,200.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	320.00 HOUR
744530	CONCUIT JUNCTION WELL, TYPE 11, PRECAST CONCRETE/ POLYMER LID-FRAME	14.00 EACH
744531	CONDUIT JUNCTION WELL, TYPE 14, PRECAST CONCRETE/ POLYMER LID-FRAME	2.00 EACH
744541	FURNISH & INSTALL FRAME AND LID FOR JUNCTION WELL, TYPE 11	10.00 EACH
744542	FURNISH & INSTALL FRAME AND LID FOR JUNCTION WELL, TYPE 14	2.00 EACH
744544	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	10.00 EACH
745602	FURNISH & INSTALL UP TO 4" SCHEDULE 80 HDPE CONDUIT (BORE)	630.00 L.F.
745603	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (OPEN CUT)	165.00 L.F.
745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	2,355.00 L.F.
745606	FURNISH & INSTALL UP TO 4" GALVANIZED STEEL CONDUIT (TRENCH)	110.00 L.F.
746614	POLE BASE EXTENSION	640.00 C.F.
746847	POLE BASE, TYPE 3	8.00 EACH
746850	POLE BASE, TYPE 4	7.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	9,880.00 L.F.
747516	CABINET BASE, TYPE P	2.00 EACH
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	5,770.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	100,800.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	11,540.00 S.F.
748033	PERMANENT PAVEMENT STRIPING, ALKYD-THERMOPLASTIC, 5"	250.00 L.F.
748502	RAISED/RECESSED PAVEMENT MARKER	423.00 EACH

LOCATION 1 387(SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB) FROM RED LION ROAD -
ROUTE 71 TO I-95 RAMPS CONCRETE JOINT

LENGTH 9981.5904 m 32748 L.F. AADT 38941

GENERAL IMPROVEMENTS

SEE COMMENTS PAGE FOR GENERAL IMPROVEMENTS

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 1

748513	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, 12"	200.00 L.F.
748529	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, SYMBOL/LEGEND	8,300.00 S.F.
748530	REMOVAL OF PAVEMENT STRIPING	953.00 S.F.
748535	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 4"	11,150.00 L.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	219,940.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	1,980.00 L.F.
748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	5.00 EACH
748557	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 3"	68,910.00 L.F.
748559	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 5"	17,230.00 L.F.
748566	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, 8"	990.00 L.F.
748568	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 9"	1,980.00 L.F.
748569	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 14"	990.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	50.00 EACH
749688	INSTALLATION OF 4" DIAMETER HOLE, LESS THAN OR EQUAL TO 6" DEPTH	10.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	160.00 S.F.
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	300.00 S.Y.
760013	RUMBLE STRIPS, BIKE-FRIENDLY, CONCRETE	19,725.00 L.F.
760016	RUMBLE STRIPS, HOT-MIX	32,200.00 L.F.
760507	PROFILE MILLING, BITUMINOUS CONCRETE	110,500.00 SY-IN
762001	SAW CUTTING, BITUMINOUS CONCRETE	385.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	1,780.00 L.F.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	20.00 HOUR
908001	TOPSOIL (TON)	100.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	250.00 S.Y.

** DENOTES FIXED PRICE ITEM

LOCATION 1 387(SUMMIT BRIDGE ROAD - ROUTE 896 NB & SB) FROM RED LION ROAD - ROUTE 71 TO I-95 RAMP CONCRETE JOINT

COMMENTS:

GENERAL IMPROVEMENTS

BITUMINOUS CONCRETE SECTION NB FROM SUMMIT BRIDGE TO SOUTH OF PORTER ROAD - 2" PROFILE MILL, BITUMINOUS CONCRETE PATCHING, 2" BITUMINOUS CONCRETE, SUPERPAVE TYPE C, PG 70-22

CONCRETE SECTION SB FROM I-95 TO SUMMIT BRIDGE AND NB FROM PORTER ROAD TO I-95 - PCC PATCHING, PARTIAL DEPTH SPALL REPAIRS, DIAMOND GRINDING SB RIGHT LANE FROM ROUTE 40 TO BRENNAN ESTATES, FOGSEAL ALL EXISTING BITUMINOUS CONCRETE SHOULDERS WITHIN THE PCC PAVEMENT SECTIONS

THE BITUMINOUS CONCRETE SECTION NB BETWEEN STATIONS 429+00 TO 445+00 WILL NOT BE INCLUDED AS PART OF THIS CONTRACT; THIS SECTION WILL BE ADDRESSED UNDER CONTRACT T200504110 HSIP NCC, N54, HOWELL SCHOOL ROAD, SR896 TO SR 71 - (SEE PLAN SHEETS). ALSO, THE TWO EXISTING CROSSEVERS/INTERSECTIONS AT HOWELL SCHOOL ROAD AND DENNY'S ROAD WILL NOT BE REHABILITATED.

PLEASE SEE INFORMATION IN REFERENCE TO THE EXISTING PRECAST CONCRETE PAVEMENT SYSTEM

RESTRICTED WORK HOURS: 7:30 PM TO 5:30 AM

RECOMMEND TRAFFIC CONTROL: TA33, TA35

CURB RAMP LOCATIONS:

GLASGOW HIGH SCHOOL
GBC DRIVE

ENTRANCE AND DRIVEWAY:

RED LION ROAD
LABRADOR DRIVE
MANSION HOUSE ROAD
MANSION HOUSE ROAD CROSSOVER

INFORMATION AND DETAILS FOR RAISED/RECESSED PAVEMENT MARKERS CAN BE FOUND ON THE DELDOT WEBSITE (WWW.DELDOT.GOV) BY GOING TO PUBLICATIONS/MUTCD/PART 3 PAVEMENT MARKINGS

INFORMATION AND DETAILS FOR RUMBLE STRIP INSTALLATION CAN BE FOUND ON THE DELDOT WEBSITE (WWW.DELDOT.GOV) BY GOING TO PUBLICATIONS/DELAWARE GUIDANCE MEMORANDUM/1-18.

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PAVEMENT AND REHABILITATION, NORTH III, 2015

CONTRACT QUANTITIES SUMMARY SHEET

302005	GRADED AGGREGATE BASE COURSE, TYPE B	85.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	160.00 TON
401754	FOG SEAL	750.00 GAL
401755	RECYCLED ASPHALT PAVEMENT MILLINGS FOR ROADWAY EDGE	585.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	170.00 TON
401830	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)	6,100.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	20.00 TON
406001	BITUMINOUS CONCRETE PATCHING	3,080.00 SY-IN
501502	DIAMOND GRINDING P.C.C. PAVEMENT	16,738.00 S.Y.
501534	INTERFACE JOINT SEALING REPAIR	36,150.00 L.F.
503001	PATCHING P.C.C. PAVEMENT, 6' TO 15', TYPE A	210.00 S.Y.
503002	PATCHING P.C.C. PAVEMENT GREATER THAN 15' TO 100', TYPE B	240.00 S.Y.
503006	DOWEL BARS	384.00 EACH
503501	CRACK AND JOINT SEALING LESS THAN 3/4" WIDE	262,458.00 L.F.
503502	CRACK AND JOINT SEALING 3/4" TO 1 3/4" WIDE	10,071.00 L.F.
503503	** PATCHING CONCRETE	13,800.00 SY-IN
503517	P.C.C. PATCHING, PARTIAL DEPTH	200.00 SY-IN
701011	PORTLAND CEMENT CONCRETE CURB, TYPE 2	340.50 L.F.
701020	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 1-8	20.00 L.F.
701022	INTEGRAL PORTLAND CEMENT CONCRETE CURB & GUTTER, TYPE 3-8	43.00 L.F.
705001	P.C.C. SIDEWALK, 4"	575.00 S.F.
705002	P.C.C. SIDEWALK, 6"	810.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	50.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	700.00 S.F.
705010	CURB RAMP, TYPE 5	165.75 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	1.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	1.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	4.00 EACH

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CONTRACT: T201506103

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PAVEMENT AND REHABILITATION, NORTH III, 2015

CONTRACT QUANTITIES SUMMARY SHEET

743003	ARROWPANELS, TYPE C	270.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	20.00 EA-DY
743005	FURNISH AND MAINTAIN PORTABLE LIGHT ASSEMBLY	720.00 EA-DY
743007	** TRAFFIC OFFICERS	2,000.00 HOUR
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	270.00 EA-DY
743056	FLAGGER, NEW CASTLE COUNTY, FEDERAL	3,200.00 HOUR
743065	FLAGGER, NEW CASTLE COUNTY, FEDERAL, OVERTIME	320.00 HOUR
744530	CONDUIT JUNCTION WELL, TYPE 11, PRECAST CONCRETE/ POLYMER LID-FRAME	14.00 EACH
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745602	FURNISH & INSTALL UP TO 4" SCHEDULE 80 HDPE CONDUIT (BORE)	630.00 L.F.
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745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	2,355.00 L.F.
745606	FURNISH & INSTALL UP TO 4" GALVANIZED STEEL CONDUIT (TRENCH)	110.00 L.F.
746614	POLE BASE EXTENSION	640.00 C.F.
746847	POLE BASE, TYPE 3	8.00 EACH
746850	POLE BASE, TYPE 4	7.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	9,880.00 L.F.
747516	CABINET BASE, TYPE P	2.00 EACH
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	5,770.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	100,800.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	11,540.00 S.F.
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748502	RAISED/RECESSED PAVEMENT MARKER	423.00 EACH
748513	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, 12"	200.00 L.F.
748529	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, SYMBOL/LEGEND	8,300.00 S.F.

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PAVEMENT AND REHABILITATION, NORTH III, 2015

CONTRACT QUANTITIES SUMMARY SHEET

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748535	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 4"	11,150.00 L.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	219,940.00 L.F.
748549	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 10"	1,980.00 L.F.
748553	PREFORMED RETROREFLECTIVE THERMOPLASTIC PAVEMENT MARKINGS, BIKE SYMBOL	5.00 EACH
748557	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 3"	68,910.00 L.F.
748559	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 5"	17,230.00 L.F.
748566	RETROREFLECTIVE PREFORMED PATTERNED MARKINGS, 8"	990.00 L.F.
748568	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, BLACK, 9"	1,980.00 L.F.
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749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	50.00 EACH
749688	INSTALLATION OF 4" DIAMETER HOLE, LESS THAN OR EQUAL TO 6" DEPTH	10.00 EACH
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760016	RUMBLE STRIPS, HOT-MIX	32,200.00 L.F.
760507	PROFILE MILLING, BITUMINOUS CONCRETE	110,500.00 SY-IN
762001	SAW CUTTING, BITUMINOUS CONCRETE	385.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	1,780.00 L.F.
763000	INITIAL EXPENSE	LUMP SUM
763621	CONSTRUCTION ENGINEERING, REHABILITATION	20.00 HOUR
763643	MAINTENANCE OF TRAFFIC, ALL INCLUSIVE	LUMP SUM
908001	TOPSOIL (TON)	100.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	250.00 S.Y.

** DENOTES FIXED PRICE ITEM