

THE STATE OF DELAWARE



DEPARTMENT OF TRANSPORTATION  
OFFICE OF TRANSPORTATION SOLUTIONS  
CONSTRUCTION PLANS AND SPECIFICATIONS FOR

PAVEMENT & REHABILITATION – KENT II (MILFORD AREA), 2015

CONTRACT NUMBER - T201506202.01

PRIMAVERA ID – 15-06202

FUNDING - 100% STATE

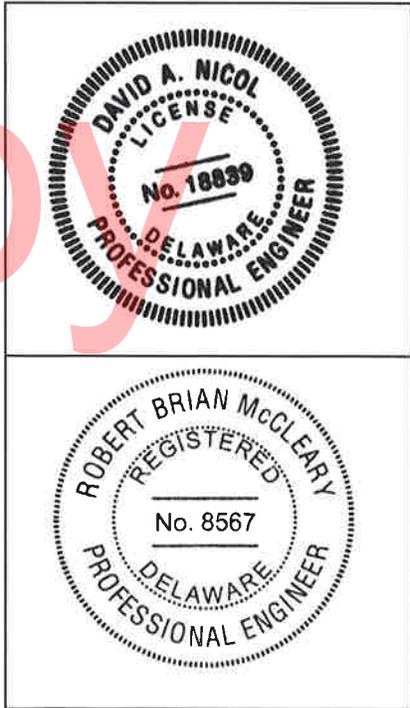
CENTRAL DISTRICT MAINTENANCE

PLANS PREPARED BY: Paul Farkas  
DESIGN TECHNICIAN

DATE RECOMMENDED: 6/5/2015  
Brian L Sabus  
GROUP ENGINEER, CONSTRUCTION

DATE RECOMMENDED: 6-5-15  
David A. Nicol  
ASSIST. DIRECTOR, ENGINEERING SUPPORT

DATE RECOMMENDED: 6/8/15  
Robert Brian McCleary  
CHIEF ENGINEER



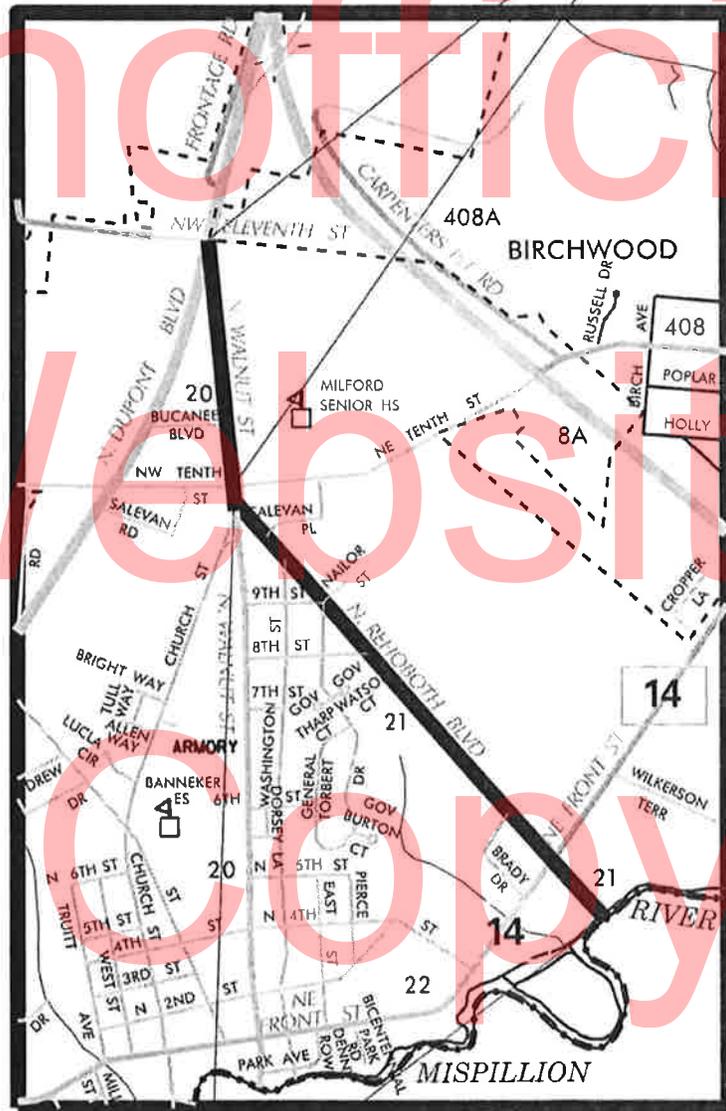
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LOCATION MAP

**Location 1**  
North Walnut St (Milford)  
from US113 to Rehoboth Blvd.



**Location 2**  
Rehoboth Blvd (Milford)  
from North Walnut St. to Mispillion River Bridge.

LOCATION MAP



Location 3

Milford-Harrington Hwy from US113 to 500' West of Blairs Pond Rd.

## PAVEMENT &amp; REHABILITATION, KENT II, (MILFORD AREA) 2015

## GENERAL CONTRACT PLAN NOTES

REVISED 5/15

GENERAL

1. CONSTRUCTION OF THIS CONTRACT SHALL CONFORM TO DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED AUGUST 2001 AS AMENDED BY THE SUPPLEMENTAL SPECIFICATIONS, THE SPECIAL PROVISIONS, THE MOST RECENT STANDARD CONSTRUCTION DETAILS, DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THESE PLANS.
2. THE PURPOSE OF THIS CONTRACT IS TO MAKE GENERAL IMPROVEMENTS TO ROADS AND/OR STREETS WITHIN THE DISTRICT HEREIN SPECIFIED. DEPENDING ON FUND AVAILABILITY, THE DEPARTMENT RESERVES THE RIGHT TO ADD OR DELETE LOCATIONS AND/OR QUANTITIES FOR THIS CONTRACT. SUCH LOCATION OR QUANTITY ADDITIONS OR DELETIONS SHALL NOT BE CAUSE FOR AN INCREASE OR DECREASE IN ANY CONTRACT UNIT BID PRICES. NO ITEM PRICES ARE TO BE RENEGOTIATED DUE TO EITHER AN INCREASE OR DECREASE IN QUANTITY USAGE RESULTING FROM SAID ADDITIONS OR DELETIONS.
3. THE CONTRACTOR SHALL PROVIDE ALL RESIDENTS AND BUSINESSES WHO LIVE ADJACENT TO THE WORK ZONE A MINIMUM 48 HR. PRIOR WRITTEN NOTICE FOR THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES, CONTRACTOR NAME & ADDRESS, AND DELDOT CONTACT NUMBERS. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND/OR PROPERTY OWNER IN ADVANCE OF THE CLOSURE.
4. THE CONTRACTOR SHALL SUBMIT THE REQUIRED COPIES OF A DETAILED PROGRESS SCHEDULE (PSC) AS OUTLINED IN STANDARD SPECIFICATION 108.04 PRIOR TO OR AT THE PRECONSTRUCTION MEETING. DETAILS SHALL INCLUDE A DESCRIPTION OF EACH WORK ACTIVITY, THE PLANNED DAYS OF WORK, MULTIPLE CREWS OR SHIFTS, AND SCHEDULED WORKING HOURS. DURING THE CONTRACT, THE CONTRACTOR SHALL SUBMIT TWO-WEEK (OR AS REQUIRED) "LOOK AHEAD" SCHEDULES TO THE AREA ENGINEER/MANAGER NO LATER THAN NOON OF EACH THURSDAY. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.
- 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE REQUIRED CLEARANCES AND INSURANCES FOR EACH RAILWAY COMPANY INVOLVED IF THE RAILROAD PROPERTY AND/OR FACILITIES ARE LOCATED WITHIN OR NEAR THE PROPOSED LIMITS OF WORK AND/OR IF THE PROPOSED WORK ENCROACHES UPON THE RAILROAD'S RIGHT-OF-WAY, IN ACCORDANCE WITH DELDOT MAINTENANCE OF RAILROAD TRAFFIC ITEM NO. 763502. THE CONTRACTOR MUST CONTACT DELDOT'S RAILROAD SECTION MANAGER AT (302) 760-2183 PRIOR TO ANY WORK STARTED TO COORDINATE THE EXECUTION OF THE APPROPRIATE AGREEMENTS AND AUTHORIZATIONS REQUIRED FROM ANY RAILWAY COMPANY INVOLVED, INCLUDING COORDINATION FOR RAILROAD FLAGGING, IF NECESSARY.**

MAINTENANCE OF TRAFFIC

6. PERMANENT ADVANCE WARNING SIGNS WITH THE LEGENDS "ROAD WORK 1500 FT," "ROAD WORK 1000 FT" AND "ROAD WORK 500 FT" SHALL BE INSTALLED IN ADVANCE OF THE WORK AREA IN BOTH DIRECTIONS. A "END ROAD WORK" SIGN SHALL BE LOCATED 500 FEET AFTER THE PROJECT LIMITS. A "ROAD WORK AHEAD" SIGN SHALL BE PLACED AT A DISTANCE NOT LESS THAN 500 FEET BEFORE

## PAVEMENT &amp; REHABILITATION, KENT II, (MILFORD AREA) 2015

## GENERAL CONTRACT PLAN NOTES

**THE WORK AREA. ALL PERMANENT ADVANCE WARNING SIGNS SHALL BE GROUND-MOUNTED ON TWO NCHRP-40 OR MASH APPROVED BREAKAWAY POSTS AND SHALL BE MOUNTED IN COMPLIANCE WITH THE DE-MUTCD. PERMANENT ADVANCE WARNING SHALL BE MOUNTED AT A HEIGHT OF 7 FEET, MEASURED FROM THE PAVEMENT GRADE TO THE BOTTOM OF THE SIGN. THE USE OF SKID-MOUNTED SIGN SUPPORTS IS NOT ALLOWED UNLESS THE CONTRACTOR CAN DEMONSTRATE THAT A UTILITY CONFLICT EXISTS AND/ OR CONCRETE MEDIANS PREVENT THE INSTALLATION OF THE PERMANENT SIGNS IN THE APPROPRIATE LOCATION WHICH SHALL BE VERIFIED BY THE ENGINEER.**

7. THE CONTRACTOR SHALL BE REQUIRED TO SHOW PROOF THAT HE HAS SUFFICIENT APPROVED STRIPING MATERIALS ON HAND TO ENSURE STRIPING IS COMPLETED PRIOR TO FULL OPENING ROADWAY TO TRAFFIC.

8. AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY STRIPING SHALL BE UTILIZED AT LOCATIONS THAT REQUIRE PERMANENT STRIPING, SHALL BE INSTALLED IN ACCORDANCE WITH THE TEMPORARY STRIPING POLICY. TEMPORARY PAVEMENT STRIPING MUST MATCH PERMANENT PAVEMENT STRIPING IN ALL REGARDS. TEMPORARY PAVEMENT MARKINGS SHALL BE PAID AT THE APPLICABLE CONTRACT UNIT PRICE. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE TEMPORARY MARKINGS IN GOOD CONDITION SUCH THAT THE PAVEMENT IS PROPERLY DELINEATED AT ALL TIMES. ANY REFRESHING OF THE TEMPORARY MARKINGS WILL BE AT THE CONTRACTOR'S EXPENSE.

9. ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.

10. THE CONTRACTOR SHALL MAINTAIN ACCESS TO AND ALONG PEDESTRIAN FACILITIES AT ALL TIMES DURING CONSTRUCTION. EARTH, STONE AND GRAVEL SURFACES ARE NOT ACCEPTABLE FOR PROVIDING PEDESTRIAN ACCESS. WHERE PEDESTRIAN ROUTES ARE CLOSED OR BLOCKED, ALTERNATE PEDESTRIAN ACCESS SHALL BE PROVIDED USING TA-28 AND/OR TA-29 OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES OR TO THE SATISFACTION OF THE ENGINEER. PEDESTRIAN DETOUR ROUTES OR ALTERNATE PEDESTRIAN FACILITIES SHALL CONSIST OF ADA COMPLIANT FACILITIES TO THE LEVEL OF WHICH EXISTS ON THE PEDESTRIAN ROUTE. THE COST FOR ANY PROVISIONS SHALL BE INCIDENTAL TO ITEM 763643 - MAINTENANCE OF TRAFFIC, ALL INCLUSIVE.

PEDESTRIAN MAINTENANCE OF TRAFFIC: THIS WORK SHALL CONSIST OF PROVIDING AND MAINTAINING AN ACCESSIBLE PEDESTRIAN ROUTE THROUGHOUT THE PROJECT'S LIMITS IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130.

THE CONTRACTOR SHALL BE REQUIRED TO REVIEW EACH CURB RAMP LOCATION AND SUBMIT THE APPROPRIATE MAINTENANCE OF TRAFFIC DETAIL AND DEVICES TO THE ENGINEER FOR EACH LOCATION AT LEAST 2 WEEKS BEFORE CONSTRUCTION FOR REVIEW, COMMENT, AND APPROVAL.

THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:

- ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPLICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.
- MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES AT ALL TIMES.
- PROVIDE PEDESTRIAN ACCESS THROUGH OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER

## PAVEMENT &amp; REHABILITATION, KENT II, (MILFORD AREA) 2015

## GENERAL CONTRACT PLAN NOTES

FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE CURRENT CONDITIONS.

- WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.
- SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.

A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTIRE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS OR VERTICAL ELEVATION CHANGES GREATER THAN ¼ IN. IN GRADE OR TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIER TO WHEELCHAIR USE.

11. AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR SHALL BE REQUIRED FOR ALL LOCATIONS SPECIFIED IN THE CONTRACT PLANS. A COPY OF THE CERTIFICATION CARD FOR THE ATSSA TRAFFIC CONTROL SUPERVISOR SHALL BE SUBMITTED AT THE PRECONSTRUCTION MEETING. THE PAPER CERTIFICATE IS NOT ACCEPTABLE.

PAVEMENT MARKINGS

12. AT THE PRECONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT DETAILED DRAWINGS (INCLUDING BUT NOT LIMITED TO EXISTING STRIPING LENGTHS, LANE AND SHOULDER WIDTHS, TURN LANE LENGTHS, LOCATIONS OF STOP BARS, TURN ARROWS, CROSSWALKS AND RAILROAD CROSSINGS) THAT DEPICT THE EXISTING PAVEMENT MARKINGS FOR EACH PROJECT LOCATION. THESE DRAWINGS WILL BE REVIEWED BY THE DEPARTMENT'S TRAFFIC SECTION TO DETERMINE IF ANY CHANGES TO THE FINAL PAVEMENT MARKINGS ARE REQUIRED. FINAL PAVEMENT MARKINGS SHALL CONFORM TO ALL EXISTING PATTERNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE DEPARTMENT WILL PROVIDE STRIPING LAYOUT AT LOCATIONS WHERE NO PAVEMENT MARKINGS PREVIOUSLY EXISTED. IF DEPARTMENT FORCES PROVIDE STRIPING LAYOUT ON LOCATIONS WHERE MARKINGS PREVIOUSLY EXISTED, THEN THE CONTRACTOR SHALL REIMBURSE THE DEPARTMENT FOR THE LAYOUT COSTS.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, WHITE EDGE LINES SHALL WRAP AROUND THE RADIUS OF ALL SIDE STREETS AND MAJOR COMMERCIAL ENTRANCES TO A TANGENT POINT. YELLOW CENTERLINES SHALL BE CONTINUOUS AROUND MEDIAN ISLANDS. ALL DOUBLE YELLOW CENTERLINES SHALL BE PLACED IN A 5-6-5 CONFIGURATION (2 EACH, 5" YELLOW STRIPES WITH A 6" GAP BETWEEN).

13. ANY ERRONEOUS MARKING WILL NOT BE PAID FOR AND SHALL BE CORRECTED IMMEDIATELY AT THE CONTRACTORS EXPENSE. ERRONEOUS MARKINGS OR SHADOWS THAT EXCEED ONE (1) INCH IN WIDTH SHALL BE REMOVED BY EITHER SAND OR WATER BLASTING AS DIRECTED BY THE ENGINEER. NO OTHER REMOVAL METHODS WILL BE ALLOWED. A FLAT BLACK PAINT OR DRIVEWAY SEALER SHALL BE APPLIED IN THE AREA OF THE REMOVED MARKING TO MASK THE REPAIR. ANY DAMAGE TO THE PAVEMENT CAUSED BY REMOVAL OF ERRONEOUS MARKINGS SHALL BE REPAIRED / REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTORS EXPENSE.

14. STOP LINES "BARS" SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.16 OF THE 2011 DELAWARE

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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

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GENERAL CONTRACT PLAN NOTES

MUTCD.

15. ALL CROSSWALKS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.18 OF THE 2011 DELAWARE MUTCD.

TRAFFIC SIGNAL NOTES

16. TRAFFIC DETECTION LOOPS SHALL BE PLACED IN THE FINAL WEARING SURFACE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

17. TRAFFIC DETECTION LOOPS THAT ARE CURRENTLY 5x7' SHALL BE REPLACED WITH LOOPS THAT ARE 6'x6' AT THE SAME LOCATION. STOP BAR DETECTION LOOPS SHALL BE THE SAME SIZE AS CURRENTLY EXISTING, AND SHOULD BE PLACED STARTING TWO FEET BEHIND THE STOP BAR, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF SEPARATE SIGNAL PLANS HAVE BEEN DEVELOPED, THE PLANS SUPERSEDE THIS NOTE.

PROJECT

18. UNLESS SPECIFIED, OVERLAY WIDTHS SHALL MATCH EXISTING WIDTHS.

19. THE COST OF CLIPPING BACK THE EDGES OF THE ROADWAY, SHOULDERS, AND THE FIRST 3' OF UNPAVED DRIVEWAY ENTRANCES, PICKING UP AND DISPOSING OF WASTE AND EXCESS MATERIAL, AND CLEANING THE EXISTING PAVEMENT PRIOR TO OVERLAY SHALL BE INCIDENTAL TO SECTION 401(.). PREPARING UNPAVED DRIVEWAY ENTRANCES BEYOND THE FIRST 3' SHALL BE PAID UNDER THE APPROPRIATE CONTRACT ITEMS: 202000 AND/OR 30200X.

20. BUTT JOINTS SHALL BE PLACED AT ALL INTERSECTING HOT MIX ROADS AND ANY OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. BUTT JOINTS AT DRAINAGE STRUCTURES NOT TO BE RESURFACED SHALL BE AS PER THE INCLUDED PLAN DETAIL OR LOCATION NOTES. BUTT JOINTS CUT PRIOR TO THE DAY OF THE PAVEMENT OVERLAY SHALL BE RAMPED WITH HOT MIX TRM. IN MILLING AREAS, THERE WILL BE NO SEPARATE PAYMENT FOR BUTT JOINTS. AFTER PAVEMENT MILLING, ALL TRANSVERSE VERTICAL DIFFERENCES, RAISED EDGES OF MANHOLES, CATCH BASINS, WATER VALVE BOXES, ETC...SHALL BE RAMPED WITH HOT-MIX TRM AT A 20:1 OR FLATTER SLOPE PRIOR TO OPENING THE ROADWAY TO TRAFFIC. PAVEMENT MILLINGS WILL NOT BE ALLOWED FOR USE AS RAMPING MATERIAL.

21. TAPER MILLING IS INTENDED FOR MILLING IN THE AREA OF FIXED STRUCTURES (I.E. CURBS GUARDRAIL, ETC.) THE MILLED DEPTH AT THE STRUCTURE SHALL BE THE DEPTH OF THE PROPOSED OVERLAY AND 0" AT A DISTANCE OF +/- 6½' FROM THE STRUCTURE. THE COMPUTED DEPTH FOR PAYMENT PURPOSES IS THE AVERAGE OF THE TWO DEPTHS.

22. ALL MILLED MATERIAL SHALL REMAIN THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE STATED.

23. EXCAVATION FOR P.C.C. CURBS, SIDEWALK OR VALLEY GUTTER SHALL BE INCIDENTAL TO THE BID PRICE FOR THESE RESPECTIVE ITEMS. EXCAVATED MATERIAL, NOT NEEDED ON THE PROJECT, SHALL BE REMOVED FROM THE CONTRACT AT THE CONTRACTORS EXPENSE. ITEM 732XXX, TOPSOIL, SHALL BE USED AS BACKFILL MATERIAL FOR CURB AND SIDEWALK AS DIRECTED BY THE ENGINEER. TOPSOIL SHALL BE SCREENED/SIFTED SO AS NO MATERIAL GREATER THAN .5" IS PRESENT IN MATERIAL USED FOR BACKFILL. AT EACH LOCATION, BACKFILLING SHALL BE PERFORMED IMMEDIATELY UPON REMOVAL OF FORMS FOR CURB OR SIDEWALK WORK. COMPLETE RESTORATION, TOPSOIL, SEEDING, AND REMOVAL OF ALL MATERIALS FOR CURB & GUTTER OR SIDEWALK; SEALING OF VALLEY GUTTERS AND PARGING OF CATCH

PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

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GENERAL CONTRACT PLAN NOTES

BASINS, SHALL BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS OF COMPLETION OF THE ITEM OF WORK. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

24. ALL ADJUSTMENTS SHALL BE MADE NO MORE THAN SEVEN (7) CALENDAR DAYS PRIOR TO THE PLACEMENT OF THE FINAL COURSE OF HOT MIX UNLESS OTHERWISE APPROVED BY THE ENGINEER. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

25. ALL PAVING, INCLUDING TURN LANES, SHOULDERS AND INTERSECTIONS, IS TO BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS FROM THE TIME THE AREA WAS MILLED. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

26. THE CONTRACTOR SHALL TAKE CARE IN REMOVING PAVEMENT AROUND UTILITIES, BUTT JOINTS, CURBING, ETC. SO THAT EXISTING PAVEMENT BEYOND THE SPECIFIED DEPTHS IS NOT DAMAGED. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS MAY RESULT IN PLACING LEVELING COURSES AT THE CONTRACTOR'S EXPENSE. THE REMOVAL AND CLEAN UP OF THE HOT MIX RESIDUE WEDGE REMAINING AFTER MILLING OPERATIONS SHALL BE INCIDENTAL TO THE MILLING ITEM. THE REMOVAL OF EXISTING RAISED PAVEMENT MARKERS (RPM'S) SHALL BE INCIDENTAL TO THE APPLICABLE MILLING AND/OR RECLAMATION ITEMS.

27. ALL PORTLAND CEMENT CONCRETE MUST BE RECEIVED FROM AN APPROVED PRODUCTION PLANT UNLESS OTHERWISE APPROVED BY THE ENGINEER. ONLY CALIBRATED VOLUMETRIC CONCRETE TRUCKS WILL BE PERMITTED TO PROVIDE ON-SITE MIXTURES. THIS PERTAINS TO BOTH WORK OCCURRING DURING NORMAL WORKING HOURS AND NIGHTTIME RESTRICTED WORK.

28. DRAINAGE INLETS, WHICH ARE 4' GREATER IN DEPTH AND ARE SLATED FOR REPAIR WILL HAVE STEPS INSTALLED IN ACCORDANCE WITH SECTION 708.07 OF THE STANDARD SPECIFICATIONS. PAYMENT SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR ITEM 710001.

29. THE PORTLAND CEMENT CONCRETE USED WITHIN ITEM 701002 - ADJUST AND REPAIR EXISTING MAN HOLES AND ITEM 750000 - ADJUST WATER VALVE BOX SHALL CONFORM TO THE REQUIREMENTS OF CLASS A CONCRETE AS STATED WITHIN SECTION 503.02 OF THE DEPARTMENT'S STANDARD SPECIFICATIONS.

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DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT NUMBER- T201506202

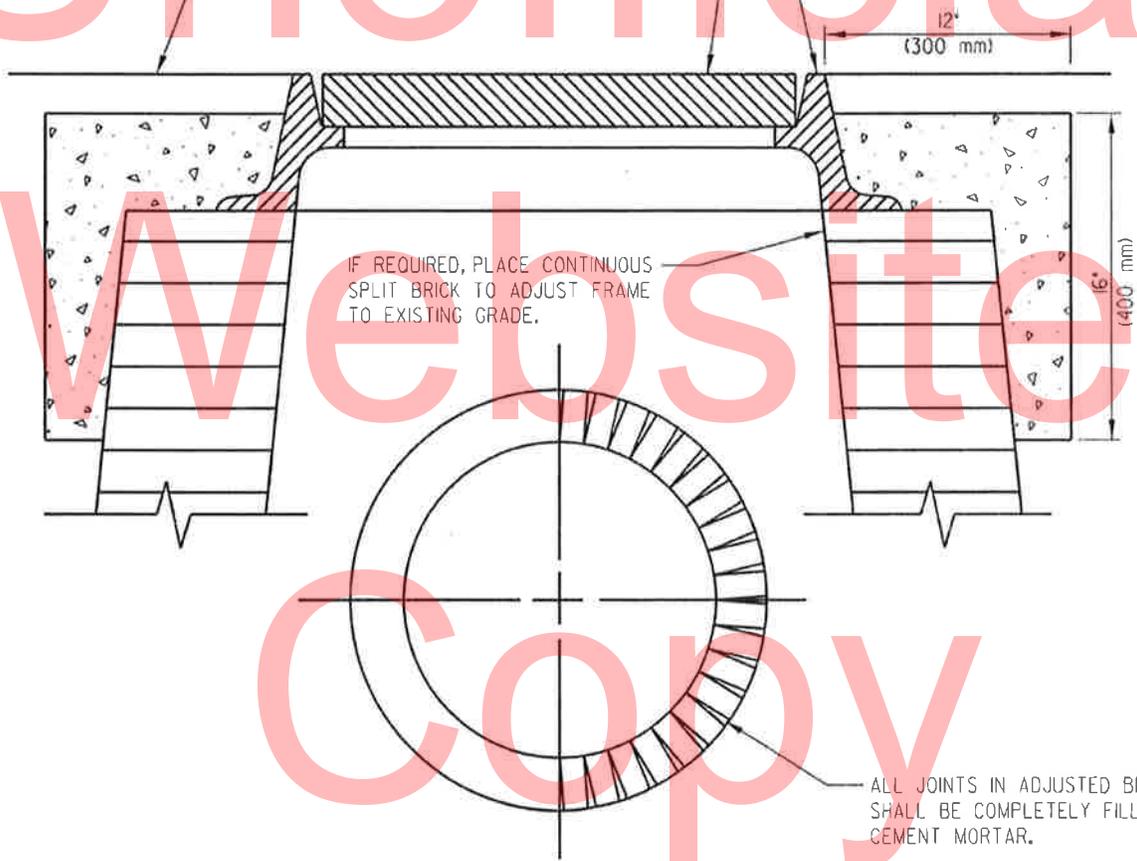
PAGE 9 OF 26

CENTRAL MAINTENANCE MILFORD AREA

DETAILS SHEET

REMOVE AND DISPOSE OF EXISTING FRAME AND COVER AS DIRECTED BY OWNER IF NOT UTILIZED IN THE ADJUSTMENT. PLACE NEW OR EXISTING FRAME AND COVER (AS REQUIRED), TO CORRECT ELEVATION AND RECOLLAR. NEW FRAME AND COVER (IF REQUIRED) TO BE PROVIDED BY OWNER.

REMOVE BIT. ASPHALT MIX AND CONCRETE COLLAR FROM AROUND MANHOLE.



IF REQUIRED, PLACE CONTINUOUS SPLIT BRICK TO ADJUST FRAME TO EXISTING GRADE.

ALL JOINTS IN ADJUSTED BRICKWORK SHALL BE COMPLETELY FILLED WITH CEMENT MORTAR.

ADJUSTING AND REPAIRING MANHOLE

N.T.S.

NOTES:

1. MANHOLES IN CONCRETE ROADWAYS SHALL BE SAWCUT FULL DEPTH TO THE SPECIFIED DIMENSION PRIOR TO ANY ADJUSTMENT BEING MADE. THE SAWCUTTING SHALL BE INCIDENTAL TO ITEM 710002.
2. ANY ADJUSTMENTS AND/OR RELOCATIONS OF MUNICIPALLY OWNED SEWER OR WATER FACILITIES SHALL BE DONE IN ACCORDANCE TO THE RESPECTIVE AGENCIES STANDARD SPECIFICATIONS AS DIRECTED BY THE ENGINEER.

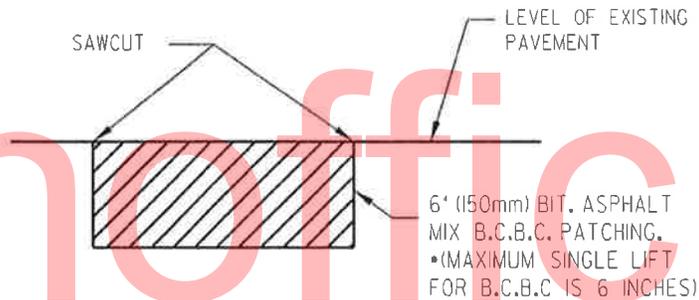
DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT NUMBER- T201506202

PAGE 10 OF 26

CENTRAL MAINTENANCE MILFORD AREA

DETAILS SHEET



BITUMINOUS CONCRETE MIX PATCH DETAIL

N.T.S.

**NOTE:** ALL EDGES TO BE SAWCUT. CUTTING BY USE OF AIR HAMMERS MAY BE SUBSTITUTED FOR SAWCUTTING IF THE AREA IS TO BE OVERLAID UNDER THE SAME CONTRACT. THIS DETAIL SHALL BE USED FOR ALL HOT-MIX PATCHES UNLESS OTHERWISE SHOWN ON THE PLANS.

WHEN PCC PATCHING IS REQUIRED ON A COMPOSITE PAVEMENT (HOT-MIX OVER CONCRETE), THE CONTRACTOR MAY ELECT TO ELIMINATE ANY GRADE DIFFERENCE BETWEEN THE TOP OF THE CONCRETE PATCH AND THE TOP OF THE EXISTING HOT-MIX PAVEMENT BY EITHER PLACING HOT-MIX TRM MATERIAL (PAYMENT UNDER ITEM 402000) OR PLACING CONCRETE FLUSH WITH EXISTING HOT-MIX PAVEMENT (ADDITIONAL CONCRETE PAID AS ITEM 503503). IF CONCRETE IS PLACED FLUSH WITH EXISTING HOT MIX, THE MILLING OF THE CONCRETE WILL BE PAID FOR AS MILLING HOT-MIX.

P.C.C. PATCHING NOTES

DELAWARE DEPARTMENT OF TRANSPORTATION

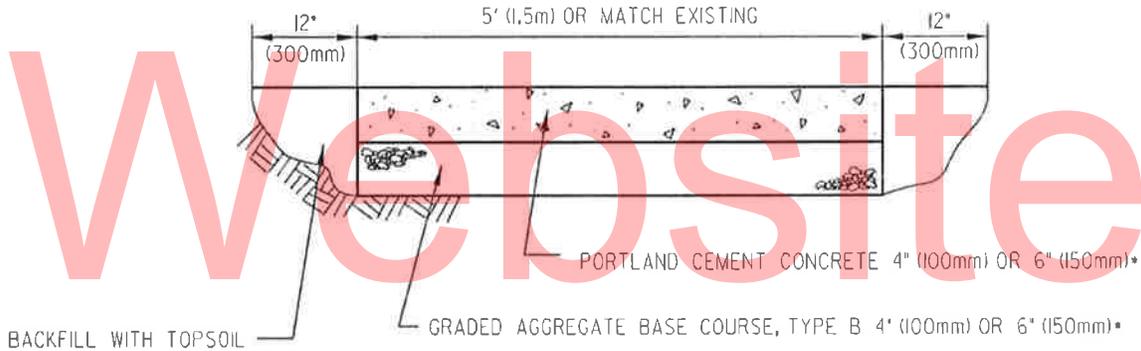
CONTRACT NUMBER- T201506202

PAGE 11 OF 26

CENTRAL MAINTENANCE MILFORD AREA

DETAILS SHEET

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P.C.C. SIDEWALK

N.T.S.

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• NOTE: 6" (150mm) CONCRETE OVER 6" (150mm) GRADED AGGREGATE TO BE USED FOR DRIVEWAY PLACEMENT

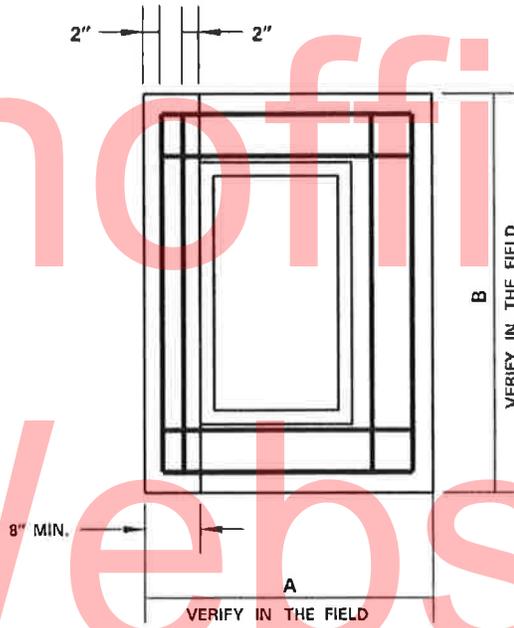
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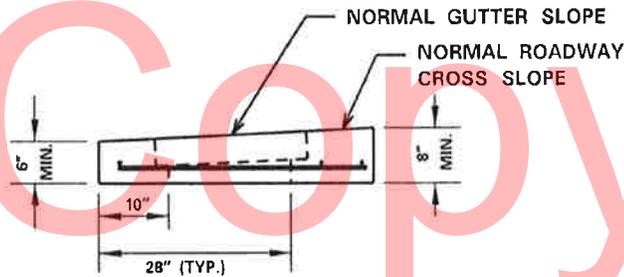
PAGE 12 OF 26

CENTRAL MAINTENANCE MILFORD AREA

DETAILS SHEET



	A	B
PWBD-1	44"	60"
PWBD-2	52"	70"



PRECAST INLET TOP CONVERSION  
WITHOUT CURB OPENING

N.T.S.

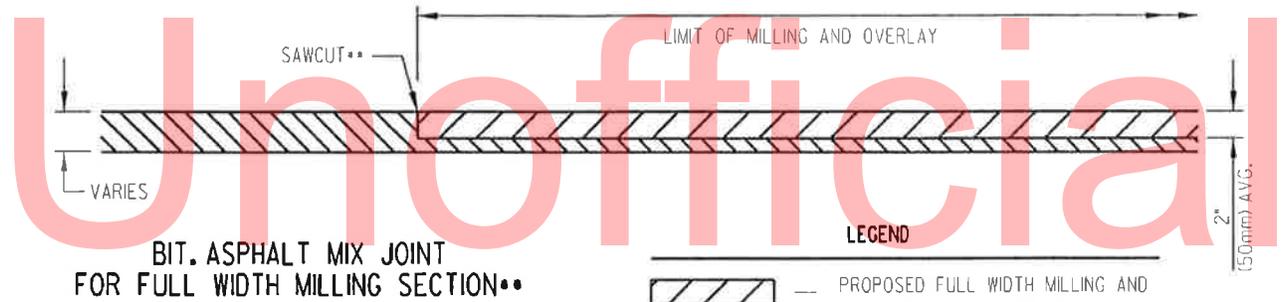
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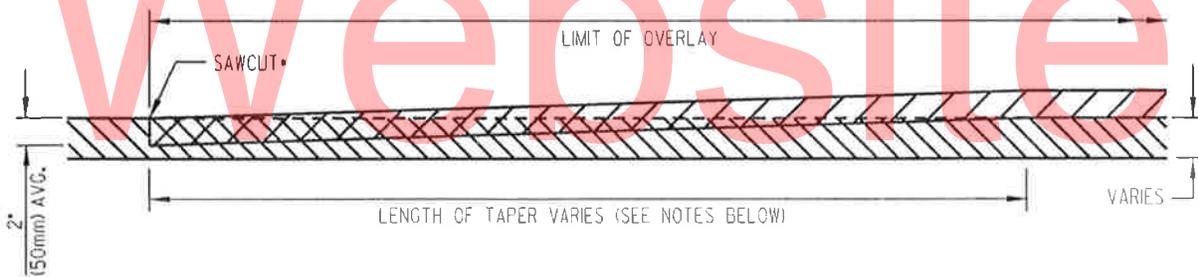
PAGE 13 OF 26

CENTRAL MAINTENANCE MILFORD AREA

DETAILS SHEET



PAYMENT INCIDENTAL TO ITEM 760XXX, MILLING.  
\*\*SAWCUT SHALL BE PAID UNDER ITEM 762001.



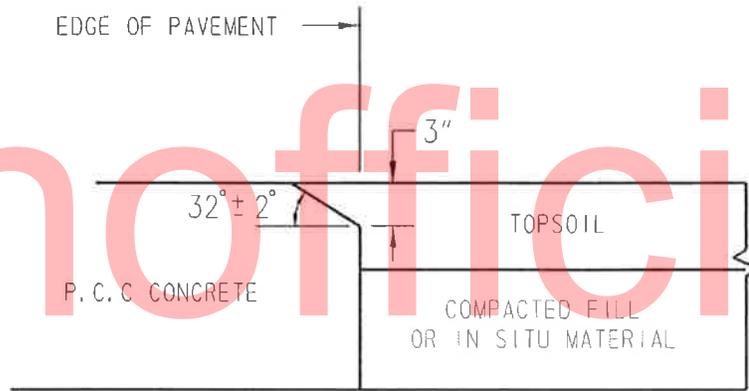
PAYMENT UNDER ITEM 761001.  
\*SAWCUT IS INCIDENTAL TO ITEM 761001.

**NOTES:**

- THE FOLLOWING LENGTHS ARE BASED ON THE POSTED SPEED OF THE ROADWAY:
1. EQUAL TO OR GREATER THAN 55 MPH = 40 FT (9.1m) PER 1" (25mm) OF OVERLAY DEPTH.
  2. LESS THAN 55 MPH = 30 FT (6m) PER 1" (25mm) OF OVERLAY DEPTH.
  3. AT STOP SIGNS = 15 FT (4.6m) PER 1" (25mm) OF OVERLAY DEPTH.

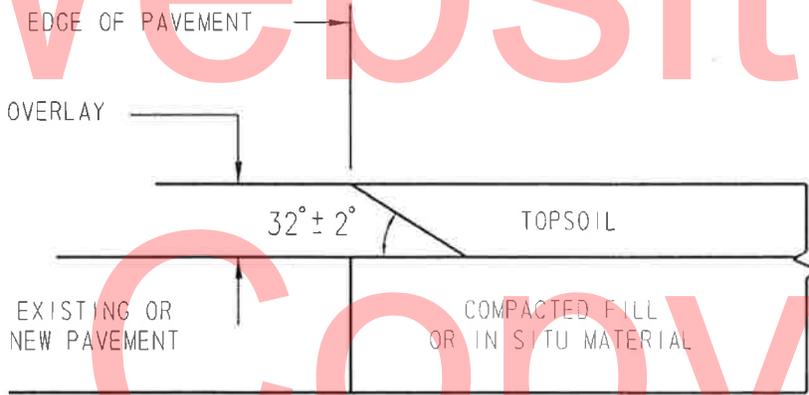
BITUMINOUS ASPHALT MIX JOINT DETAILS

DETAILS SHEET



CONCRETE PAVEMENTS

NOT TO SCALE



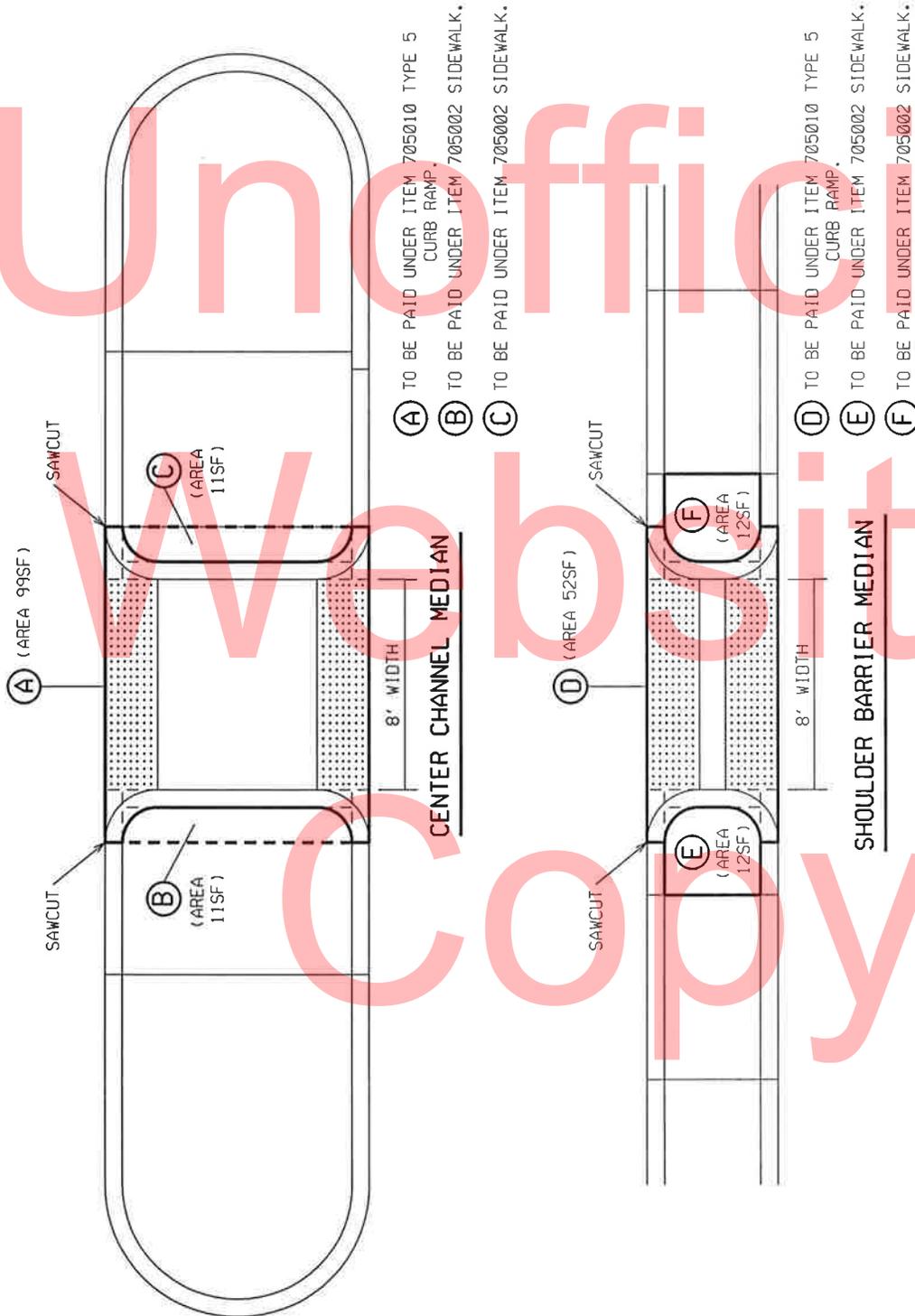
BIT. ASPHALT MIX PAVEMENTS AND OVERLAYS

NOT TO SCALE

SAFETY EDGE DETAIL

NOT TO SCALE

DETAILS SHEET



DETAIL FOR P.C.C. MEDIAN - TYPE 5 CURB RAMPS  
ON REHOBOTH BLVD. ( IN FRONT OF PERDUE PLANT )

N.T.S

CENTRAL  
MAINTENANCE

PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

**LOCATION 1** NORTH WALNUT STREET (K20) - (MILFORD) FROM NORTH REHOBOTH BLVD  
(K21) TO DUPONT BLVD, US113.

LENGTH 630.936 m 2070 L.F. AADT 4790

**GENERAL IMPROVEMENTS**

2" MILLING, BIT. ASPHALT PATCHING, ADA CURB RAMPS AND 2" TYPE C (76-22,C) FULL WIDTH OVERLAY.

**COMMENTS:**

Please see attached sheets for comments

**MATERIAL ESTIMATE FOR LOCATION 1**

302005	GRADED AGGREGATE BASE COURSE, TYPE B	3.00 TON
302007	GRADED AGGREGATE BASE COURSE, TYPE B	2.00 C.Y.
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	21.00 TON
401804	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 70-22 (CARBONATE STONE)	1,281.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	23.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	11.00 TON
406001	BITUMINOUS CONCRETE PATCHING	400.00 SY-IN
705002	P.C.C. SIDEWALK, 6"	120.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	40.00 S.F.
705008	CURB RAMP, TYPE 1	500.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	1.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	1.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	7.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	10.00 EACH
743003	ARROWPANELS, TYPE C	7.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	14.00 EA-DY
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	7.00 EA-DY
743051	FLAGGER, KENT COUNTY, STATE	217.00 HOUR
743063	FLAGGER, KENT COUNTY, STATE, OVERTIME	33.00 HOUR
744544	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	3.00 EACH
745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	40.00 L.F.
746850	POLE BASE, TYPE 4	3.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	780.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	803.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	18,630.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	1,606.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	9,315.00 L.F.



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LOCATION 1 NORTH WALNUT STREET (K20) - (MILFORD) FROM NORTH REHOBOTH BLVD (K21)  
TO DUPONT BLVD, US113.

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COMMENTS:

1. RECOMMENDED M.O.T. TYPICAL APPLICATION, TA-10 (TRAVEL LANES).  
. TYPICAL APPLICATION, TA-11B (TRAVEL LANES).  
. TYPICAL APPLICATION, TA-3 (SHOULDERS).  
. TYPICAL APPLICATION, TA-28 (SIDEWALK).
2. DAYTIME ALLOWABLE LANE CLOSURES: 8:00A - 6:00P.
3. NO WORK SHALL BE PERMITTED WHILE AREA SCHOOLS ARE IN SESSION (MILFORD).
4. ALL VERTICAL DIFFERENCES MUST BE ADDRESSED IN ACCORDANCE WITH THE DE-MUTCD AT THE END OF EACH WORK DAY.
5. GABC ITEM 302005 AND QUANTITY IS FOR UNIMPROVED DRIVEWAYS.
  
6. CURB RAMPS, TYPE 1 @  
-----  
A. 1. DOVER POOL ENT. (1X)  
. 2. ESPRESSO ENT. (1X)  
. 3. PROFESSIONAL CENTER ENT. (1X)  
. 4. DEL MAR TV (1X)  
B. EXISTING R.O.W FOR THIS LOCATION IS 60' AS PER ARCHIVE PLANS;  
. 1. 70-06-007  
. 2. SKR8546  
. 3. SKR 1157

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**LOCATION 2**                      **NORTH REHOBOTH BLVD (K21) - (MILFORD) FROM MISPILLION RIVER BRIDGE  
TO NORTH WALNUT ST (K20)**

LENGTH      1176.528 m                      3860 L.F.                      AADT      8659

**GENERAL IMPROVEMENTS**

2" MILLING, BIT. ASPHALT PATCHING, ADA CURB RAMPS AND 2" TYPE C (76-22,C) FULL WIDTH OVERLAY.

**COMMENTS:**

Please see attached sheets for comments

**MATERIAL ESTIMATE FOR LOCATION 2**

302007	GRADED AGGREGATE BASE COURSE, TYPE B	33.00 C.Y.
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	99.00 TON
401804	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 70-22 (CARBONATE STONE)	3,156.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	80.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	27.00 TON
406001	BITUMINOUS CONCRETE PATCHING	1,400.00 SY-IN
705002	P.C.C. SIDEWALK, 6"	1,616.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	184.00 S.F.
705008	CURB RAMP, TYPE 1	625.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	630.00 S.F.
705010	CURB RAMP, TYPE 5	439.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	5.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	5.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	16.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	8.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	4.00 EACH
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	14.00 EA-DY
743051	FLAGGER, KENT COUNTY, STATE	329.00 HOUR
743063	FLAGGER, KENT COUNTY, STATE, OVERTIME	49.00 HOUR
744505	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	1.00 EACH
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	700.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	336.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	34,740.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	671.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	17,310.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	30.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	120.00 S.F.

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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

**LOCATION 2**                      **NORTH REHOBOTH BLVD (K21) - (MILFORD) FROM MISPILLION RIVER BRIDGE  
TO NORTH WALNUT ST (K20)**

LENGTH      1176.528 m                      3860 L.F.                      AADT      8659

**GENERAL IMPROVEMENTS**

2" MILLING, BIT. ASPHALT PATCHING, ADA CURB RAMPS AND 2" TYPE C (76-22,C) FULL WIDTH OVERLAY.

**COMMENTS:**

Please see attached sheets for comments

**MATERIAL ESTIMATE FOR LOCATION 2**

750000	ADJUST WATER VALVE BOXES	15.00 EACH
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	177.00 S.Y.
760507	PROFILE MILLING, BITUMINOUS CONCRETE	53,378.00 SY-IN
762001	SAW CUTTING, BITUMINOUS CONCRETE	2,970.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	60.00 L.F.
763621	CONSTRUCTION ENGINEERING, REHABILITATION	20.00 HOUR
908001	TOPSOIL (TON)	42.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	499.00 S.Y.

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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

LOCATION 2 NORTH REHOBOTH BLVD (K21) - (MILFORD) FROM MISPILLION RIVER BRIDGE TO  
NORTH WALNUT ST (K20)

COMMENTS:

1. RECOMMENDED M.O.T. TYPICAL APPLICATION, TA-10 (TRAVEL LANES).  
. TYPICAL APPLICATION, TA-11B (TRAVEL LANES).  
. TYPICAL APPLICATION, TA-3 (SHOULDERS).  
. TYPICAL APPLICATION, TA-28 (SIDEWALK).
2. DAYTIME ALLOWABLE LANE CLOSURES: 8:00A - 6:00P.
3. ALL VERTICAL DIFFERENCES MUST BE ADDRESSED IN ACCORDANCE WITH THE DE-MUTCD AT THE END OF EACH WORK DAY.
4. MILLING AND PAVEMENT NOTES
  - A. SKIP OVER NEWER PAVEMENT LOCATED AT PERDUE PLANT ENTRANCES, DO NOT MILL AND OVERLAY.
  - B. PROJECT LIMIT INCLUDES SOUTHBOUND WALNUT STREET IN FRONT OF KENT & SUSSEX AUTO CARE TO PAVEMENT JOINT.
  - C. TIE BACK INTO SIDE INTERSECTING STREETS TO EXISTING PAVEMENT JOINTS.
5. PWBD CONVERSION LOCATIONS\*
  - A. DRAINAGE INLET CONVERSIONS TO BE PAID UNDER ITEM 716502.
    - . 1. CARWASH ENTRANCE. (2X)
    - . 2. AMERICAN LEGION POST. (1X)
    - . 3. EAST SALEVAN ST. (1X)

\*CONFIRM DIMENSIONS IN THE FIELD.
6. CURB RAMPS AND SIDEWALK REPLACEMENT
  - A. 1. TYPE 5 @ NE 10TH ST. (1X)  
. 2. REPLACE EXISTING SIDEWALK FROM THE FEDERATED AUTO PARTS ENT. EXISTING RAMP TO THE EXISTING RAMP AT NE 10TH ST. TURN LANE. THE TWO RAMPS ARE COMPLIANT.
  - B. 1. TYPE 3 @ ONE STOP SHOP/WALNUT ST. SPUR. (1X)  
. 2. TYPE 1 @ ONE STOP SHOP ENTRANCES. (3X)
  - C. 1. TYPE 1 @ NORTH WASHINGTON ST., NORTH SIDE (1X)  
. 2. TYPE 2 @ NORTH WASHINGTON ST., NORTH SIDE (1X)  
. 3. REPLACE EXISTING SIDEWALK BETWEEN THE ABOVE TWO RAMPS.
  - D. 1. TYPE 5 @ NORTH WASHINGTON ST. NORTH ISLAND. (1X) SOUTH ISLAND IS COMPLIANT.  
. 2. TYPE 3 @ NORTH WASHINGTON ST., SOUTH SIDE SPUR. (1X)  
. 3. TYPE 1 @ NORTH WASHINGTON ST., SOUTH SIDE SPUR. (1X)  
. 4. REPLACE EXISTING SIDEWALK BETWEEN THE ABOVE TWO RAMPS.
  - E. EXISTING R.O.W FOR THIS LOCATION IS 100' AS PER ARCHIVE PLANS;
    - . 1. 90-062-05
    - . 2. 82-035-02
    - . 3. 104

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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

**LOCATION 3** MILFORD-HARRINGTON HWY, DE14 (K36) FROM 500' WEST OF BLAIRS POND RD  
(K444) TO DUPONT BLVD, US113

LENGTH 5169.408 m 16960 L.F. AADT 10706

**GENERAL IMPROVEMENTS**

2" MILLING, REPLACE DRAINAGE INLET TOPS AND 2" TYPE C RUBBERIZED ASPHALT AND FULL WIDTH OVERLAY.

**COMMENTS:**

Please see attached sheets for comments

**MATERIAL ESTIMATE FOR LOCATION 3**

302005	GRADED AGGREGATE BASE COURSE, TYPE B	61.00 TON
302007	GRADED AGGREGATE BASE COURSE, TYPE B	12.00 C.Y.
401582	ASPHALT RUBBER HMA	5,804.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	343.00 TON
401801	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 64-22 (CARBONATE STONE)	4,438.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	195.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	9.00 TON
406001	BITUMINOUS CONCRETE PATCHING	3,400.00 SY-IN
705002	P.C.C. SIDEWALK, 6"	630.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	9.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	9.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	20.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	13.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	9.00 EACH
743003	ARROWPANELS, TYPE C	32.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	14.00 EA-DY
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	32.00 EA-DY
743051	FLAGGER, KENT COUNTY, STATE	970.00 HOUR
743063	FLAGGER, KENT COUNTY, STATE, OVERTIME	146.00 HOUR
746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	800.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLAST IC	534.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	127,200.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	1,067.00 S.F.
748502	RAISED/RECESSED PAVEMENT MARKER	212.00 EACH
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	63,600.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	16.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	15.00 S.F.

CENTRAL  
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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

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**LOCATION 3**                      **MILFORD-HARRINGTON HWY, DE14 (K36) FROM 500' WEST OF BLAIRS POND RD  
(K444) TO DUPONT BLVD, US113**

LENGTH      5169.408 m                      16960 L.F.                      AADT    10706

**GENERAL IMPROVEMENTS**

2" MILLING, REPLACE DRAINAGE INLET TOPS AND 2" TYPE C RUBBERIZED ASPHALT AND FULL WIDTH OVERLAY.

**COMMENTS:**

Please see attached sheets for comments

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**MATERIAL ESTIMATE FOR LOCATION 3**

750000	ADJUST WATER VALVE BOXES	9.00 EACH
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	140.00 S.Y.
760502	PAVEMENT MILLING, TAPER CUT	140.00 SY-IN
760507	PROFILE MILLING, BITUMINOUS CONCRETE	170,809.00 SY-IN
761001	BUTT JOINTS, HOT MIX	3,025.00 S.Y.
762001	SAW CUTTING, BITUMINOUS CONCRETE	4,260.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	200.00 L.F.
908001	TOPSOIL (TON)	530.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	5,653.00 S.Y.

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PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015

LOCATION 3 MILFORD-HARRINGTON HWY, DE14 (K36) FROM 500' WEST OF BLAIRS POND RD  
(K444) TO DUPONT BLVD, US113

COMMENTS:

1. RECOMMENDED M.O.T. TYPICAL APPLICATION, TA-11B (TRAVEL LANES).  
 . TYPICAL APPLICATION, TA-33 (TRAVEL LANES).  
 . TYPICAL APPLICATION, TA-3A (SHOULDERS).
2. DAYTIME ALLOWABLE LANE CLOSURES: 8:00A - 6:00P.
3. ALL VERTICAL DIFFERENCES MUST BE ADDRESSED IN ACCORDANCE WITH THE DE-MUTCD AT THE END OF EACH WORK DAY.
4. GABC ITEM 302005 AND QUANTITY IS FOR UNIMPROVED DRIVEWAYS.
5. THE P.C.C. REMOVAL AND 6" SIDEWALK ITEMS ARE FOR CONCRETE DRIVEWAYS.
6. MILLING AND PAVEMENT NOTES  
 -----  
 A. 1. USE RUBBERIZED ASPHALT FOR THE TRAVEL LANES.  
 2. USE 64-22 (C) TYPE C WARM-MIX FOR SHOULDERS AND INTERSECTING ROADS.  
 B. SKIP OVER NEWER PAVEMENT WESTBOUND LANES AT US113 IN FRONT OF CVS. DO NOT MILL AND OVERLAY.  
 C. TIE BACK TO EXISTING PAVEMENT JOINTS FOR INTERSECTING ROADS.
7. PWBD CONVERSION LOCATIONS\*  
 -----  
 A. DRAINAGE INLET CONVERSIONS TO BE PAID UNDER ITEM 716502.  
 . 1. TRACTOR SUPPLY ENT. SOUTH. (1X)  
 . 2. TRACTOR SUPPLY ENT. NORTH. (2X)  
 . 3. FIRE HYDRANT NEAR NAPA. (1X)  
 . 4. MAILBOX #2415 (1X)  
 . 5. MAILBOX #2469 (1X)  
 . 6. BMAILBOX #2533 (1X)  
 . 7. MAILBOX #2645 (1X)  
 . 8. BLAIRS POND RD (1X)  
 \*CONFIRM DIMENSIONS IN THE FIELD.  
 B. THERE MAY BE OTHER PWBD'S LOCATED WITHIN THE DITCH LINE. CONVERT AS NEEDED AND AS DIRECTED BY THE FIELD ENGINEER.  
 C. EXISTING R.O.W FOR THIS LOCATION IS 80' AS PER ARCHIVE PLANS;  
 . 1. 64-01-032

DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT: T201506202

PAGE 25 OF 26

CENTRAL PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015  
 MAINTENANCE

CONTRACT QUANTITIES SUMMARY SHEET

302005	GRADED AGGREGATE BASE COURSE, TYPE B	64.00 TON
302007	GRADED AGGREGATE BASE COURSE, TYPE B	47.00 C.Y.
401582	ASPHALT RUBBER HMA	5,804.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	463.00 TON
401801	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 64-22 (CARBONATE STONE)	4,438.00 TON
401804	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 70-22 (CARBONATE STONE)	4,437.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	298.00 TON
402000	BITUMINOUS CONCRETE AND/OR COLD-LAID BITUMINOUS CONCRETE (TRM)	47.00 TON
406001	BITUMINOUS CONCRETE PATCHING	5,200.00 SY-IN
705002	P.C.C. SIDEWALK, 6"	2,366.00 S.F.
705007	SIDEWALK SURFACE DETECTABLE WARNING SYSTEM	224.00 S.F.
705008	CURB RAMP, TYPE 1	1,125.00 S.F.
705009	CURB RAMP, TYPE 2, 3, AND/OR 4	630.00 S.F.
705010	CURB RAMP, TYPE 5	439.00 S.F.
708060	REPLACE DRAINAGE INLET GRATE(S)	15.00 EACH
708061	REPLACE DRAINAGE INLET FRAME(S)	15.00 EACH
710001	ADJUSTING AND REPAIRING EXISTING DRAINAGE INLET	43.00 EACH
710002	ADJUSTING AND REPAIRING EXISTING MANHOLE	31.00 EACH
716502	CONVERTING EXISTING CATCH BASIN TO DRAINAGE INLET	13.00 EACH
743003	ARROW PANELS, TYPE C	39.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	42.00 EA-DY
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	39.00 EA-DY
743051	FLAGGER, KENT COUNTY, STATE	1,516.00 HOUR
743063	FLAGGER, KENT COUNTY, STATE, OVERTIME	228.00 HOUR
744505	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	1.00 EACH
744544	ADJUST OR REPAIR EXISTING CONDUIT JUNCTION WELL	3.00 EACH
745604	FURNISH & INSTALL UP TO 4" SCHEDULE 80 PVC CONDUIT (TRENCH)	40.00 L.F.
746850	POLE BASE, TYPE 4	3.00 EACH

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DELAWARE DEPARTMENT OF TRANSPORTATION

CONTRACT: T201506202

PAGE 26 OF 26

CENTRAL PAVEMENT & REHABILITATION, KENT II, (MILFORD AREA) 2015  
MAINTENANCE

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CONTRACT QUANTITIES SUMMARY SHEET

746924	FURNISH & INSTALL LOOP WIRE 1-CONDUCTOR #14 AWG ENCASED IN 1/4" FLEXIBLE TUBING IN A LOOP SAWCUT	2,280.00 L.F.
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	1,673.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	180,570.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	3,344.00 S.F.
748502	RAISED/RECESSED PAVEMENT MARKER	212.00 EACH
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	90,225.00 L.F.
749687	INSTALLATION OR REMOVAL OF TRAFFIC SIGN(S) ON SINGLE SIGN POST	76.00 EACH
749690	INSTALLATION OR REMOVAL OF TRAFFIC SIGNS ON MULTIPLE SIGN POSTS	135.00 S.F.
750000	ADJUST WATER VALVE BOXES	37.00 EACH
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	330.00 S.Y.
760502	PAVEMENT MILLING, TAPER CUT	140.00 SY-IN
760507	PROFILE MILLING, BITUMINOUS CONCRETE	245,971.00 SY-IN
761001	BUTT JOINTS, HOT MIX	3,025.00 S.Y.
762001	SAW CUTTING, BITUMINOUS CONCRETE	8,970.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	260.00 L.F.
763000	INITIAL EXPENSE	LUMP SUM
763621	CONSTRUCTION ENGINEERING, REHABILITATION	28.00 HOUR
763643	MAINTENANCE OF TRAFFIC, ALL INCLUSIVE	LUMP SUM
908001	TOPSOIL (TON)	582.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	6,260.00 S.Y.