

THE STATE OF DELAWARE



DEPARTMENT OF TRANSPORTATION
OFFICE OF TRANSPORTATION SOLUTIONS
CONSTRUCTION PLANS AND SPECIFICATIONS FOR

PAVEMENT & REHABILITATION – SUSSEX I, (LINCOLN AREA), 2015

CONTRACT NUMBER - T201506301.01

PRIMAVERA ID – 15-06301

FUNDING - 100% STATE

SOUTH DISTRICT MAINTENANCE

PLANS PREPARED BY: Paul Farkas
DESIGN TECHNICIAN

DATE RECOMMENDED: 4/14/2015

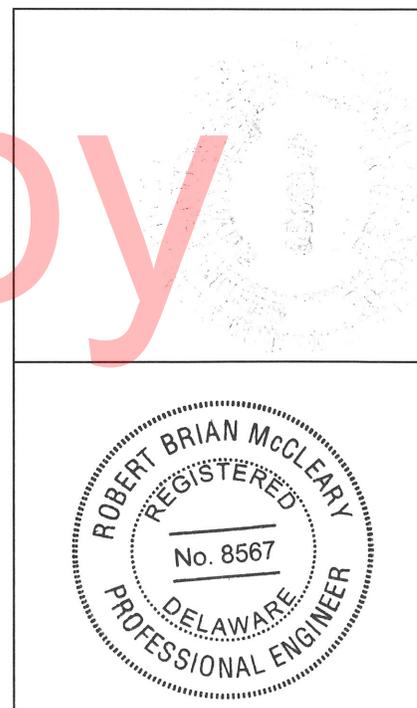
Bradford L. Salms
GROUP ENGINEER, CONSTRUCTION

DATE RECOMMENDED: 4/15/2015

David A. Nici
ASSIST. DIRECTOR, ENGINEERING SUPPORT

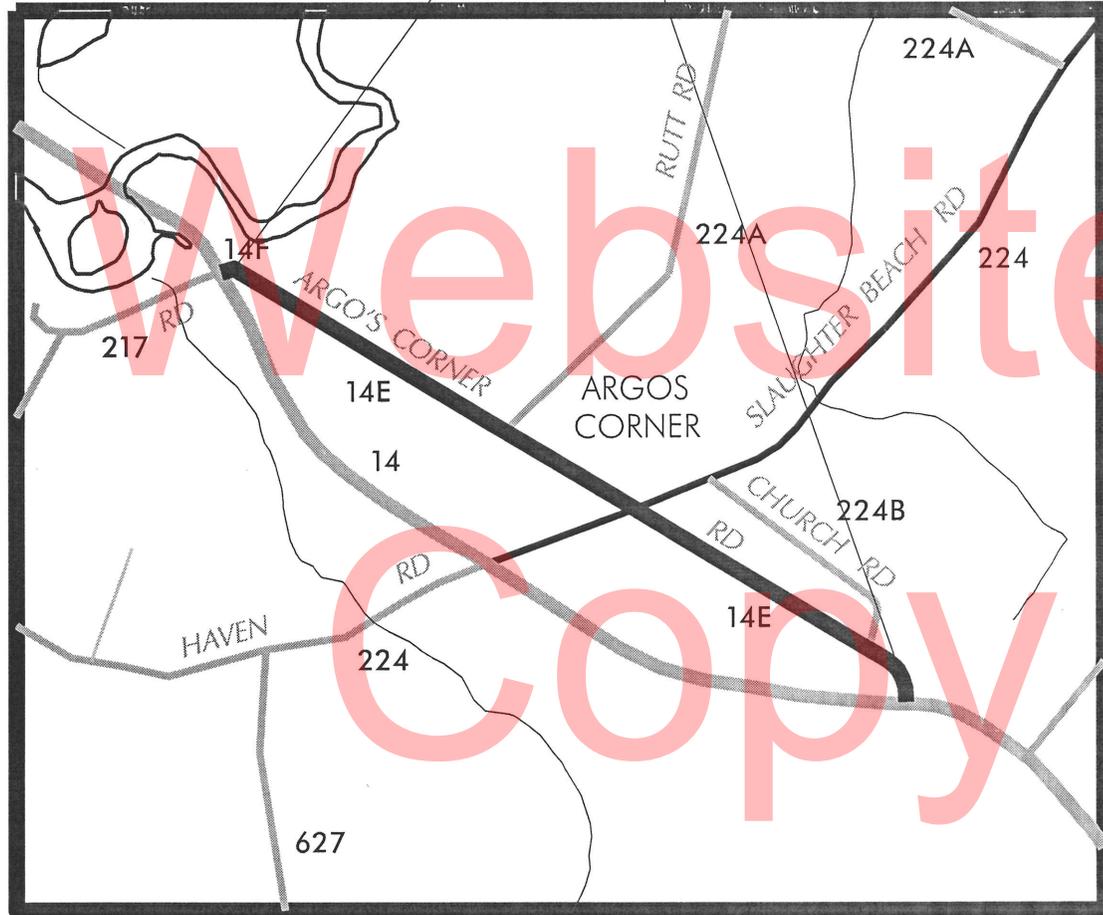
DATE RECOMMENDED: 4/17/2015

Robert Brian McCleary
CHIEF ENGINEER



LOCATION MAP

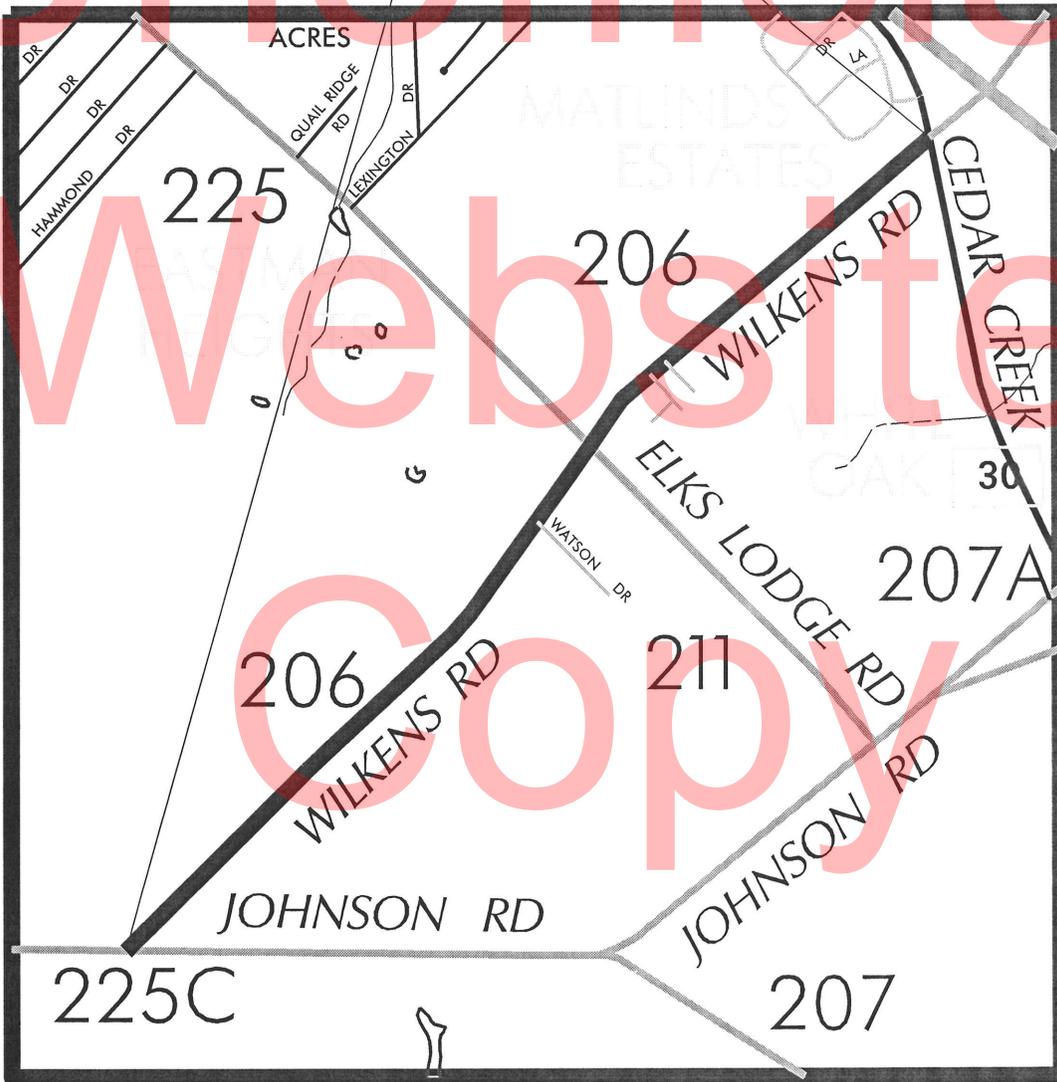
Location 1



Argos Corner Rd from SR1 to SR1

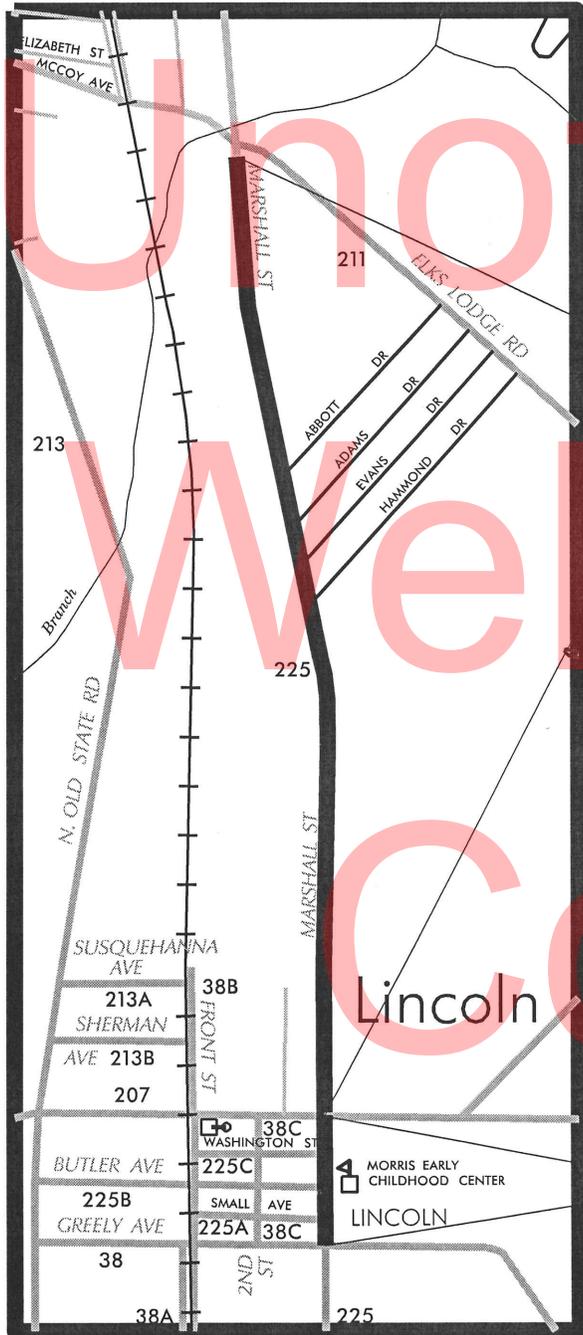
LOCATION MAP

Location 2



Wilkins Rd from Johnson Rd to Cedar Creek Rd

LOCATION MAP

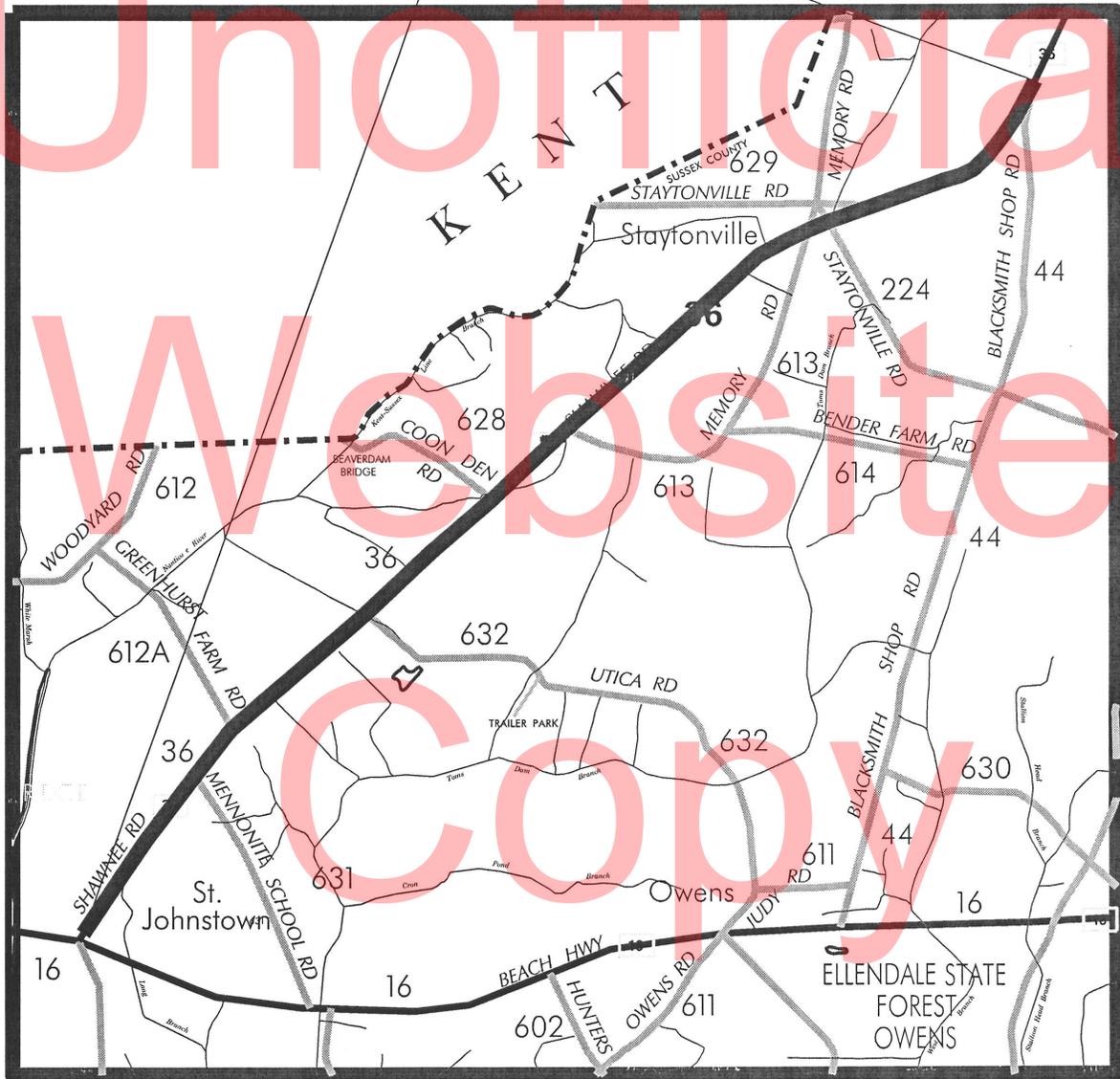


Location 4
Marshall Rd(Milford)
from Johnson Rd
to Elks Lodge Rd

Location 3
Marshall St(Lincoln)
from Clendaniel Pond Rd
to Johnson Rd

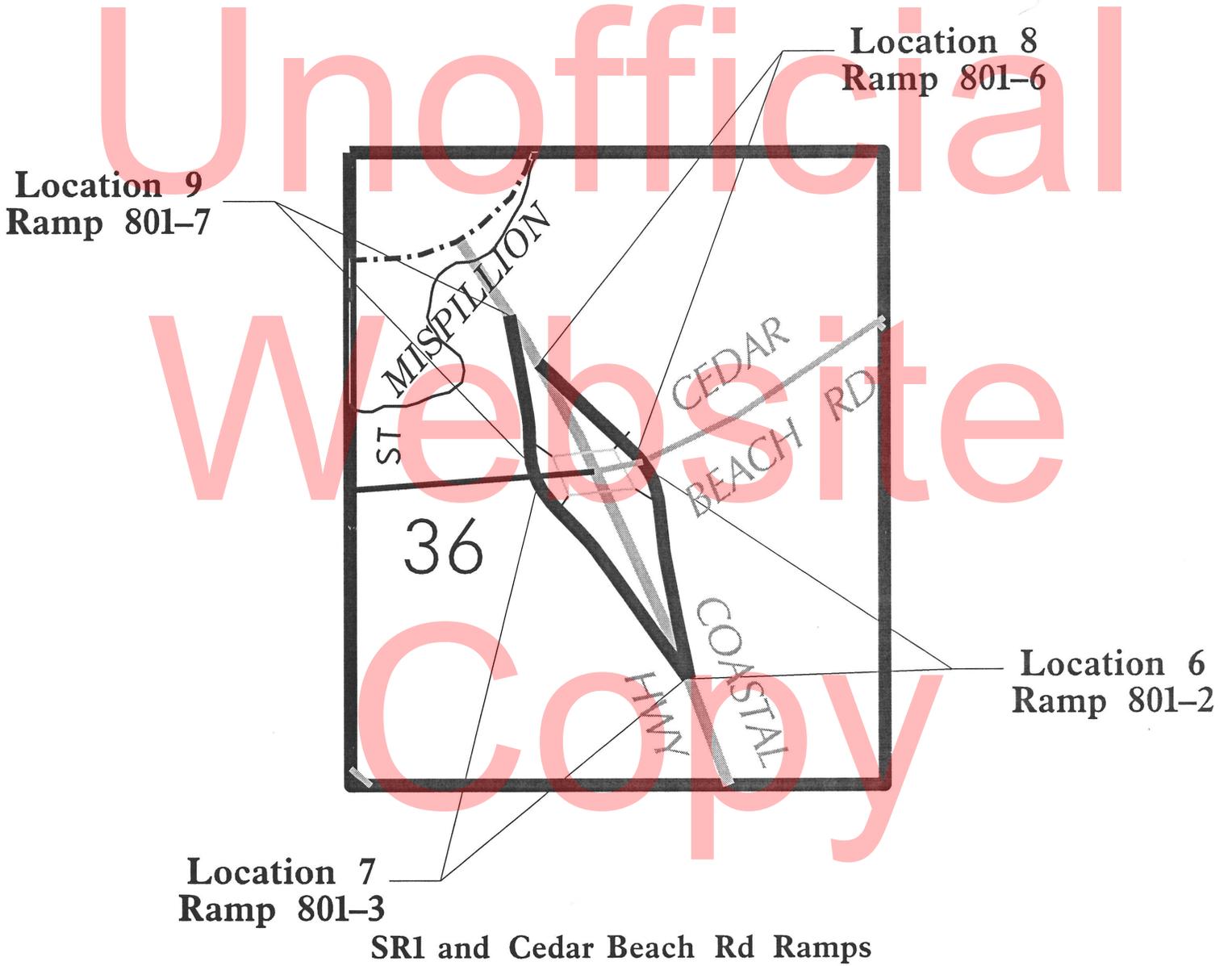
LOCATION MAP

Location 5



Shawnee Rd from DE16, Beach Hwy to Blacksmith Rd

LOCATION MAP



PAVEMENT & REHABILITATION, SUSSEX I, (LINCOLN AREA), 2015

GENERAL CONTRACT PLAN NOTES

REVISED 3/15

GENERAL

1. CONSTRUCTION OF THIS CONTRACT SHALL CONFORM TO DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS DATED AUGUST 2001 AS AMENDED BY THE SUPPLEMENTAL SPECIFICATIONS, THE SPECIAL PROVISIONS, THE MOST RECENT STANDARD CONSTRUCTION DETAILS, DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THESE PLANS.
2. THE PURPOSE OF THIS CONTRACT IS TO MAKE GENERAL IMPROVEMENTS TO ROADS AND/OR STREETS WITHIN THE DISTRICT HEREIN SPECIFIED. DEPENDING ON FUND AVAILABILITY, THE DEPARTMENT RESERVES THE RIGHT TO ADD OR DELETE LOCATIONS AND/OR QUANTITIES FOR THIS CONTRACT. SUCH LOCATION OR QUANTITY ADDITIONS OR DELETIONS SHALL NOT BE CAUSE FOR AN INCREASE OR DECREASE IN ANY CONTRACT UNIT BID PRICES. NO ITEM PRICES ARE TO BE RENEGOTIATED DUE TO EITHER AN INCREASE OR DECREASE IN QUANTITY USAGE RESULTING FROM SAID ADDITIONS OR DELETIONS.
3. THE CONTRACTOR SHALL PROVIDE ALL RESIDENTS AND BUSINESSES WHO LIVE ADJACENT TO THE WORK ZONE A MINIMUM 48 HR. PRIOR WRITTEN NOTICE FOR THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES, CONTRACTOR NAME & ADDRESS, AND DELDOT CONTACT NUMBERS. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND/OR PROPERTY OWNER IN ADVANCE OF THE CLOSURE.
4. THE CONTRACTOR SHALL SUBMIT THE REQUIRED COPIES OF A DETAILED PROGRESS SCHEDULE (PSC) AS OUTLINED IN STANDARD SPECIFICATION 108.04 PRIOR TO OR AT THE PRECONSTRUCTION MEETING. DETAILS SHALL INCLUDE A DESCRIPTION OF EACH WORK ACTIVITY, THE PLANNED DAYS OF WORK, MULTIPLE CREWS OR SHIFTS, AND SCHEDULED WORKING HOURS. DURING THE CONTRACT, THE CONTRACTOR SHALL SUBMIT TWO-WEEK (OR AS REQUIRED) "LOOK AHEAD" SCHEDULES TO THE AREA ENGINEER/MANAGER NO LATER THAN NOON OF EACH THURSDAY. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.
- 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE REQUIRED CLEARANCES AND INSURANCES FOR EACH RAILWAY COMPANY INVOLVED IF THE RAILROAD PROPERTY AND/OR FACILITIES ARE LOCATED WITHIN OR NEAR THE PROPOSED LIMITS OF WORK AND/OR IF THE PROPOSED WORK ENCROACHES UPON THE RAILROAD'S RIGHT-OF-WAY, IN ACCORDANCE WITH DELDOT MAINTENANCE OF RAILROAD TRAFFIC ITEM NO. 763502. THE CONTRACTOR MUST CONTACT DELDOT'S RAILROAD SECTION MANAGER AT (302) 760-2183 PRIOR TO ANY WORK STARTED TO COORDINATE THE EXECUTION OF THE APPROPRIATE AGREEMENTS AND AUTHORIZATIONS REQUIRED FROM ANY RAILWAY COMPANY INVOLVED, INCLUDING COORDINATION FOR RAILROAD FLAGGING, IF NECESSARY.**

MAINTENANCE OF TRAFFIC

6. THE CONTRACTOR SHALL BE REQUIRED TO SHOW PROOF THAT HE HAS SUFFICIENT APPROVED STRIPING MATERIALS ON HAND TO ENSURE STRIPING IS COMPLETED PRIOR TO FULL OPENING ROADWAY TO TRAFFIC.

PAVEMENT & REHABILITATION, SUSSEX I, (LINCOLN AREA), 2015

GENERAL CONTRACT PLAN NOTES

7. AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY STRIPING SHALL BE UTILIZED AT LOCATIONS THAT REQUIRE PERMANENT STRIPING, SHALL BE INSTALLED IN ACCORDANCE WITH THE TEMPORARY STRIPING POLICY. TEMPORARY PAVEMENT STRIPING MUST MATCH PERMANENT PAVEMENT STRIPING IN ALL REGARDS. TEMPORARY PAVEMENT MARKINGS SHALL BE PAID AT THE APPLICABLE CONTRACT UNIT PRICE. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING THE TEMPORARY MARKINGS IN GOOD CONDITION SUCH THAT THE PAVEMENT IS PROPERLY DELINEATED AT ALL TIMES. ANY REFRESHING OF THE TEMPORARY MARKINGS WILL BE AT THE CONTRACTOR'S EXPENSE.

8. ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ALL ROAD USERS, INCLUDING VEHICULAR, PEDESTRIAN AND BICYCLE TRAFFIC, AND SHALL CONFORM TO THE REQUIREMENTS OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, PART 6, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT FOR BIDS.

9. THE CONTRACTOR SHALL MAINTAIN ACCESS TO AND ALONG PEDESTRIAN FACILITIES AT ALL TIMES DURING CONSTRUCTION. EARTH, STONE AND GRAVEL SURFACES ARE NOT ACCEPTABLE FOR PROVIDING PEDESTRIAN ACCESS. WHERE PEDESTRIAN ROUTES ARE CLOSED OR BLOCKED, ALTERNATE PEDESTRIAN ACCESS SHALL BE PROVIDED USING TA-28 AND/OR TA-29 OF THE 2011 DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES OR TO THE SATISFACTION OF THE ENGINEER. PEDESTRIAN DETOUR ROUTES OR ALTERNATE PEDESTRIAN FACILITIES SHALL CONSIST OF ADA COMPLIANT FACILITIES TO THE LEVEL OF WHICH EXISTS ON THE PEDESTRIAN ROUTE. THE COST FOR ANY PROVISIONS SHALL BE INCIDENTAL TO ITEM 763XXX - MAINTENANCE OF TRAFFIC.

PEDESTRIAN MAINTENANCE OF TRAFFIC: THIS WORK SHALL CONSIST OF PROVIDING AND MAINTAINING AN ACCESSIBLE PEDESTRIAN ROUTE THROUGHOUT THE PROJECT'S LIMITS IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT OF 1990 (ADA) TITLE II, PARAGRAPH 35.130.

THE CONTRACTOR SHALL BE REQUIRED TO REVIEW EACH CURB RAMP LOCATION AND SUBMIT THE APPROPRIATE MAINTENANCE OF TRAFFIC DETAIL AND DEVICES TO THE ENGINEER FOR EACH LOCATION AT LEAST 2 WEEKS BEFORE CONSTRUCTION FOR REVIEW, COMMENT, AND APPROVAL. THE FOLLOWING CONSIDERATIONS SHALL BE TAKEN INTO ACCOUNT WHEN ADDRESSING ACCESSIBLE PEDESTRIAN MAINTENANCE OF TRAFFIC:

- ALL PEDESTRIANS, INCLUDING PERSONS WITH DISABILITIES, SHALL BE PROVIDED WITH A REASONABLY SAFE, CONVENIENT AND ACCESSIBLE PATH THAT REPLICATES AS MUCH AS PRACTICABLE THE EXISTING PEDESTRIAN FACILITIES.
- MAINTAIN ACCESS TO ALL BUSINESSES AND RESIDENCES AT ALL TIMES.
- PROVIDE PEDESTRIAN ACCESS THROUGH OR AROUND THE WORK ZONE. IF A DETOUR IS CHOSEN THE CONTRACTOR MUST SUBMIT THE DETOUR ROUTE TO THE ENGINEER FOR REVIEW AND APPROVAL. THE DETOUR ROUTE MUST MEET OR EXCEED THE CURRENT CONDITIONS.
- WORK SHOULD BE PHASED SO THAT ALL AREAS OF AN INTERSECTION/SIDEWALK PATH ARE NOT UNDER CONSTRUCTION AT THE SAME TIME UNLESS A CLEAR PATH IS PROVIDED AND IDENTIFIED.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY PATHWAY OR OTHER PEDESTRIAN FACILITY.
- SIGNS AND OTHER DEVICES MOUNTED LOWER THAN 7FT ABOVE THE TEMPORARY PEDESTRIAN PATHWAY SHALL NOT PROJECT MORE THAN 4 IN. INTO ACCESSIBLE PEDESTRIAN ROUTE.

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GENERAL CONTRACT PLAN NOTES

A SMOOTH, CONTINUOUS HARD SURFACE SHALL BE PROVIDED THROUGHOUT THE ENTIRE LENGTH AND WIDTH OF THE PEDESTRIAN ROUTE THROUGHOUT CONSTRUCTION. THERE SHALL BE NO CURBS OR VERTICAL ELEVATION CHANGES GREATER THAN ¼ IN. IN GRADE OR TERRAIN THAT COULD CAUSE TRIPPING OR BE A BARRIER TO WHEELCHAIR USE.

10. AN AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR SHALL BE REQUIRED FOR ALL LOCATIONS SPECIFIED IN THE CONTRACT PLANS. A COPY OF THE CERTIFICATION CARD FOR THE ATSSA TRAFFIC CONTROL SUPERVISOR SHALL BE SUBMITTED AT THE PRECONSTRUCTION MEETING. THE PAPER CERTIFICATE IS NOT ACCEPTABLE.

PAVEMENT MARKINGS

11. AT THE PRECONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT DETAILED DRAWINGS (INCLUDING BUT NOT LIMITED TO EXISTING STRIPING LENGTHS, LANE AND SHOULDER WIDTHS, TURN LANE LENGTHS, LOCATIONS OF STOP BARS, TURN ARROWS, CROSSWALKS AND RAILROAD CROSSINGS) THAT DEPICT THE EXISTING PAVEMENT MARKINGS FOR EACH PROJECT LOCATION. THESE DRAWINGS WILL BE REVIEWED BY THE DEPARTMENT'S TRAFFIC SECTION TO DETERMINE IF ANY CHANGES TO THE FINAL PAVEMENT MARKINGS ARE REQUIRED. FINAL PAVEMENT MARKINGS SHALL CONFORM TO ALL EXISTING PATTERNS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE DEPARTMENT WILL PROVIDE STRIPING LAYOUT AT LOCATIONS WHERE NO PAVEMENT MARKINGS PREVIOUSLY EXISTED. IF DEPARTMENT FORCES PROVIDE STRIPING LAYOUT ON LOCATIONS WHERE MARKINGS PREVIOUSLY EXISTED, THEN THE CONTRACTOR SHALL REIMBURSE THE DEPARTMENT FOR THE LAYOUT COSTS.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, WHITE EDGE LINES SHALL WRAP AROUND THE RADIUS OF ALL SIDE STREETS AND MAJOR COMMERCIAL ENTRANCES TO A TANGENT POINT. YELLOW CENTERLINES SHALL BE CONTINUOUS AROUND MEDIAN ISLANDS. ALL DOUBLE YELLOW CENTERLINES SHALL BE PLACED IN A 5-6-5 CONFIGURATION (2 EACH, 5" YELLOW STRIPES WITH A 6" GAP BETWEEN).

12. ANY ERRONEOUS MARKING WILL NOT BE PAID FOR AND SHALL BE CORRECTED IMMEDIATELY AT THE CONTRACTORS EXPENSE. ERRONEOUS MARKINGS OR SHADOWS THAT EXCEED ONE (1) INCH IN WIDTH SHALL BE REMOVED BY EITHER SAND OR WATER BLASTING AS DIRECTED BY THE ENGINEER. NO OTHER REMOVAL METHODS WILL BE ALLOWED. A FLAT BLACK PAINT OR DRIVEWAY SEALER SHALL BE APPLIED IN THE AREA OF THE REMOVED MARKING TO MASK THE REPAIR. ANY DAMAGE TO THE PAVEMENT CAUSED BY REMOVAL OF ERRONEOUS MARKINGS SHALL BE REPAIRED / REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTORS EXPENSE.

13. STOP LINES "BARS" SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.16 OF THE 2011 DELAWARE MUTCD.

14. ALL CROSSWALKS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 3B.18 OF THE 2011 DELAWARE MUTCD.

TRAFFIC SIGNAL NOTES

15. TRAFFIC DETECTION LOOPS SHALL BE PLACED IN THE FINAL WEARING SURFACE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

16. TRAFFIC DETECTION LOOPS THAT ARE CURRENTLY 5'x7' SHALL BE REPLACED WITH LOOPS THAT ARE 6'x6' AT THE SAME LOCATION. STOP BAR DETECTION LOOPS SHALL BE THE SAME SIZE AS CURRENTLY

PAVEMENT & REHABILITATION, SUSSEX I, (LINCOLN AREA), 2015

GENERAL CONTRACT PLAN NOTES

EXISTING, AND SHOULD BE PLACED STARTING TWO FEET BEHIND THE STOP BAR, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF SEPARATE SIGNAL PLANS HAVE BEEN DEVELOPED, THE PLANS SUPERSEDE THIS NOTE.

PROJECT

17. UNLESS SPECIFIED, OVERLAY WIDTHS SHALL MATCH EXISTING WIDTHS.

18. THE COST OF CLIPPING BACK THE EDGES OF THE ROADWAY, SHOULDERS, AND THE FIRST 3' OF UNPAVED DRIVEWAY ENTRANCES, PICKING UP AND DISPOSING OF WASTE AND EXCESS MATERIAL, AND CLEANING THE EXISTING PAVEMENT PRIOR TO OVERLAY SHALL BE INCIDENTAL TO SECTION 401(.). PREPARING UNPAVED DRIVEWAY ENTRANCES BEYOND THE FIRST 3' SHALL BE PAID UNDER THE APPROPRIATE CONTRACT ITEMS: 202000 AND/OR 30200X.

19. BUTT JOINTS SHALL BE PLACED AT ALL INTERSECTING HOT MIX ROADS AND ANY OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. BUTT JOINTS AT DRAINAGE STRUCTURES NOT TO BE RESURFACED SHALL BE AS PER THE INCLUDED PLAN DETAIL OR LOCATION NOTES. BUTT JOINTS CUT PRIOR TO THE DAY OF THE PAVEMENT OVERLAY SHALL BE RAMPED WITH HOT MIX TRM. IN MILLING AREAS, THERE WILL BE NO SEPARATE PAYMENT FOR BUTT JOINTS. AFTER PAVEMENT MILLING, ALL TRANSVERSE VERTICAL DIFFERENCES, RAISED EDGES OF MANHOLES, CATCH BASINS, WATER VALVE BOXES, ETC...SHALL BE RAMPED WITH HOT-MIX TRM AT A 20:1 OR FLATTER SLOPE PRIOR TO OPENING THE ROADWAY TO TRAFFIC. PAVEMENT MILLINGS WILL NOT BE ALLOWED FOR USE AS RAMPING MATERIAL.

20. TAPER MILLING IS INTENDED FOR MILLING IN THE AREA OF FIXED STRUCTURES (I.E. CURBS GUARDRAIL, ETC.) THE MILLED DEPTH AT THE STRUCTURE SHALL BE THE DEPTH OF THE PROPOSED OVERLAY AND 0" AT A DISTANCE OF +/- 6½' FROM THE STRUCTURE. THE COMPUTED DEPTH FOR PAYMENT PURPOSES IS THE AVERAGE OF THE TWO DEPTHS.

21. ALL MILLED MATERIAL SHALL REMAIN THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE STATED.

22. EXCAVATION FOR P.C.C. CURBS, SIDEWALK OR VALLEY GUTTER SHALL BE INCIDENTAL TO THE BID PRICE FOR THESE RESPECTIVE ITEMS. EXCAVATED MATERIAL, NOT NEEDED ON THE PROJECT, SHALL BE REMOVED FROM THE CONTRACT AT THE CONTRACTORS EXPENSE. ITEM 732XXX, TOPSOIL, SHALL BE USED AS BACKFILL MATERIAL FOR CURB AND SIDEWALK AS DIRECTED BY THE ENGINEER. TOPSOIL SHALL BE SCREENED/SIFTED SO AS NO MATERIAL GREATER THAN .5" IS PRESENT IN MATERIAL USED FOR BACKFILL. AT EACH LOCATION, BACKFILLING SHALL BE PERFORMED IMMEDIATELY UPON REMOVAL OF FORMS FOR CURB OR SIDEWALK WORK. COMPLETE RESTORATION, TOPSOIL, SEEDING, AND REMOVAL OF ALL MATERIALS FOR CURB & GUTTER OR SIDEWALK; SEALING OF VALLEY GUTTERS AND PARGING OF CATCH BASINS, SHALL BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS OF COMPLETION OF THE ITEM OF WORK. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

23. ALL ADJUSTMENTS SHALL BE MADE NO MORE THAN SEVEN (7) CALENDAR DAYS PRIOR TO THE PLACEMENT OF THE FINAL COURSE OF HOT MIX UNLESS OTHERWISE APPROVED BY THE ENGINEER. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

24. ALL PAVING, INCLUDING TURN LANES, SHOULDERS AND INTERSECTIONS, IS TO BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS FROM THE TIME THE AREA WAS MILLED. FAILURE TO COMPLY WILL RESULT IN A SUSPENSION OF ALL OTHER CONTRACT WORK WITH TIME CHARGES CONTINUING TO BE ASSESSED.

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GENERAL CONTRACT PLAN NOTES

25. THE CONTRACTOR SHALL TAKE CARE IN REMOVING PAVEMENT AROUND UTILITIES, BUTT JOINTS, CURBING, ETC. SO THAT EXISTING PAVEMENT BEYOND THE SPECIFIED DEPTHS IS NOT DAMAGED. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS MAY RESULT IN PLACING LEVELING COURSES AT THE CONTRACTOR'S EXPENSE. THE REMOVAL AND CLEAN UP OF THE HOT MIX RESIDUE WEDGE REMAINING AFTER MILLING OPERATIONS SHALL BE INCIDENTAL TO THE MILLING ITEM. THE REMOVAL OF EXISTING RAISED PAVEMENT MARKERS (RPM'S) SHALL BE INCIDENTAL TO THE APPLICABLE MILLING AND/OR RECLAMATION ITEMS.

26. ALL PORTLAND CEMENT CONCRETE MUST BE RECEIVED FROM AN APPROVED PRODUCTION PLANT UNLESS OTHERWISE APPROVED BY THE ENGINEER. ONLY CALIBRATED VOLUMETRIC CONCRETE TRUCKS WILL BE PERMITTED TO PROVIDE ON-SITE MIXTURES. THIS PERTAINS TO BOTH WORK OCCURRING DURING NORMAL WORKING HOURS AND NIGHTTIME RESTRICTED WORK.

27. DRAINAGE INLETS, WHICH ARE 4' GREATER IN DEPTH AND ARE SLATED FOR REPAIR WILL HAVE STEPS INSTALLED IN ACCORDANCE WITH SECTION 708.07 OF THE STANDARD SPECIFICATIONS. PAYMENT SHALL BE INCIDENTAL TO THE UNIT PRICE BID FOR ITEM 710001.

28. THE PORTLAND CEMENT CONCRETE USED WITHIN ITEM 701002 - ADJUST AND REPAIR EXISTING MAN HOLES AND ITEM 750000 - ADJUST WATER VALVE BOX SHALL CONFORM TO THE REQUIREMENTS OF CLASS A CONCRETE AS STATED WITHIN SECTION 503.02 OF THE DEPARTMENT'S STANDARD SPECIFICATIONS.

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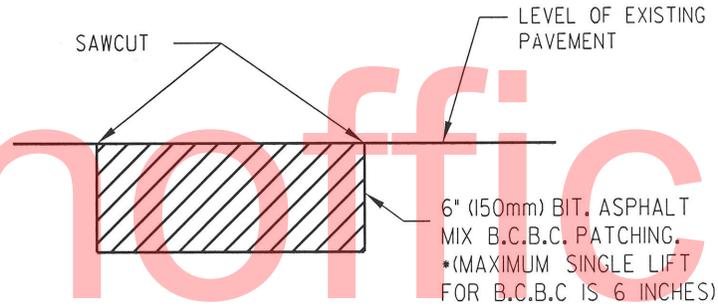
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SOUTH MAINTENANCE LINCOLN & SOUTH MILFORD AREA

DETAILS SHEET



BITUMINOUS CONCRETE MIX PATCH DETAIL

N.T.S.

NOTE: ALL EDGES TO BE SAWCUT. CUTTING BY USE OF AIR HAMMERS MAY BE SUBSTITUTED FOR SAWCUTTING IF THE AREA IS TO BE OVERLAID UNDER THE SAME CONTRACT. THIS DETAIL SHALL BE USED FOR ALL HOT-MIX PATCHES UNLESS OTHERWISE SHOWN ON THE PLANS.

WHEN PCC PATCHING IS REQUIRED ON A COMPOSITE PAVEMENT (HOT-MIX OVER CONCRETE), THE CONTRACTOR MAY ELECT TO ELIMINATE ANY GRADE DIFFERENCE BETWEEN THE TOP OF THE CONCRETE PATCH AND THE TOP OF THE EXISTING HOT-MIX PAVEMENT BY EITHER PLACING HOT-MIX TRM MATERIAL (PAYMENT UNDER ITEM 402000) OR PLACING CONCRETE FLUSH WITH EXISTING HOT-MIX PAVEMENT (ADDITIONAL CONCRETE PAID AS ITEM 503503). IF CONCRETE IS PLACED FLUSH WITH EXISTING HOT-MIX, THE MILLING OF THE CONCRETE WILL BE PAID FOR AS MILLING HOT-MIX.

P.C.C. PATCHING NOTES

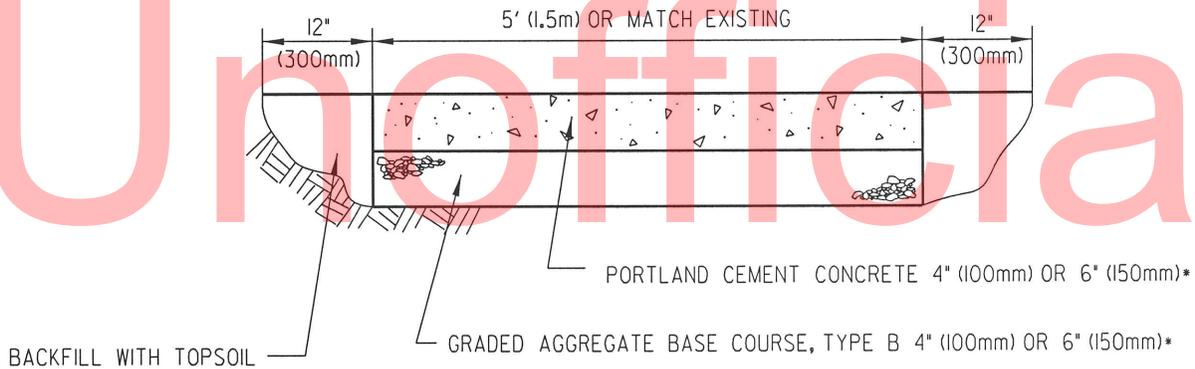
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SOUTH MAINTENANCE LINCOLN & SOUTH MILFORD AREA

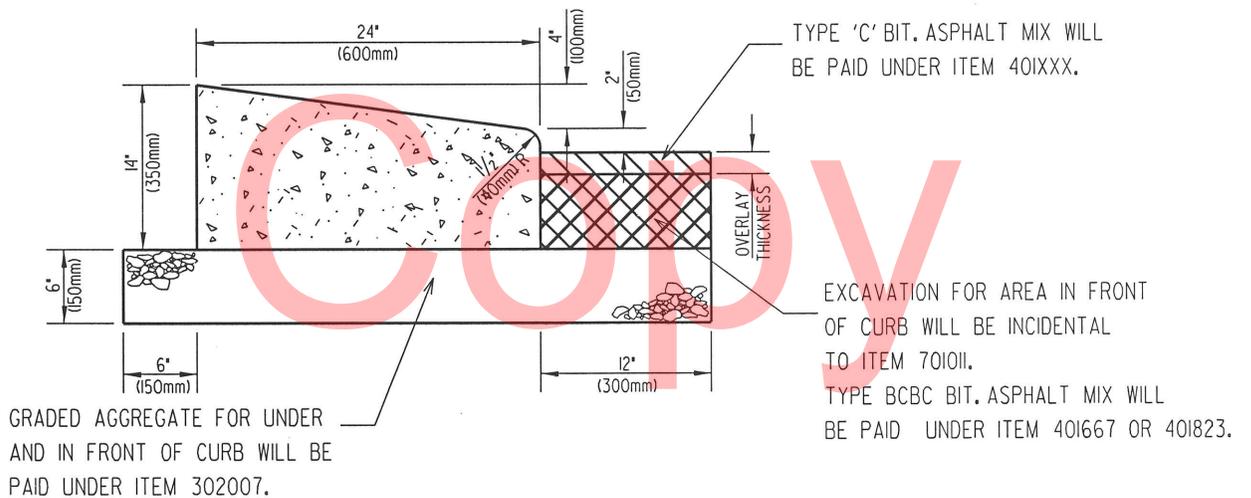
DETAILS SHEET



P.C.C. SIDEWALK

N.T.S.

- NOTE: 6" (150mm) CONCRETE OVER 6" (150mm) GRADED AGGREGATE TO BE USED FOR DRIVEWAY PLACEMENT



P.C.C. CURB TYPE 2

N.T.S.

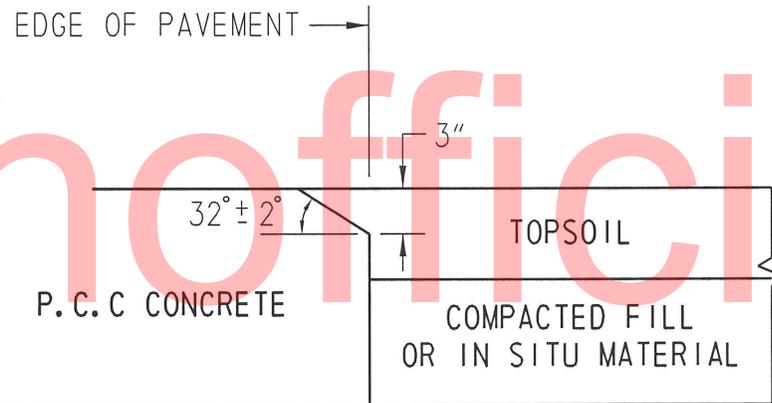
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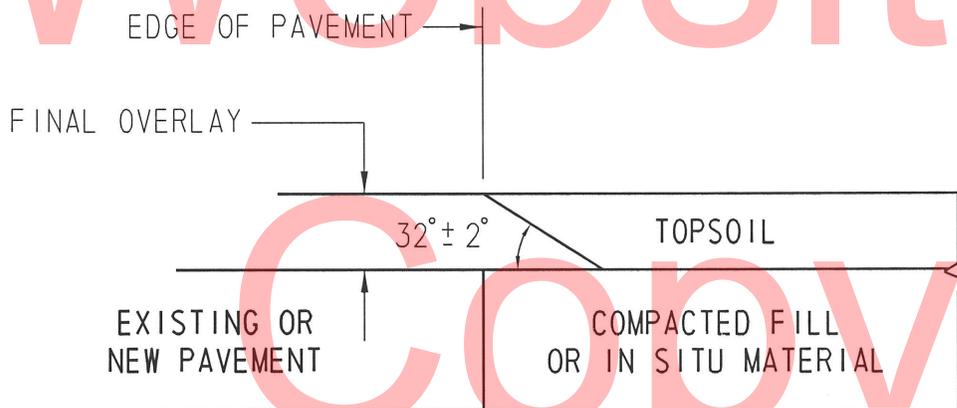
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SOUTH MAINTENANCE LINCOLN & SOUTH MILFORD AREA

DETAILS SHEET



CONCRETE PAVEMENTS
NOT TO SCALE



BIT. ASPHALT MIX PAVEMENTS AND OVERLAYS
NOT TO SCALE

SAFETY EDGE DETAIL
NOT TO SCALE

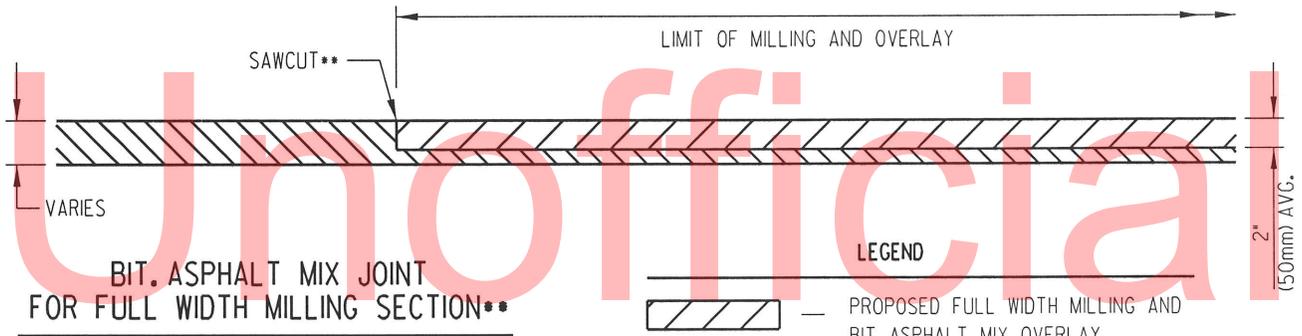
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DETAILS SHEET

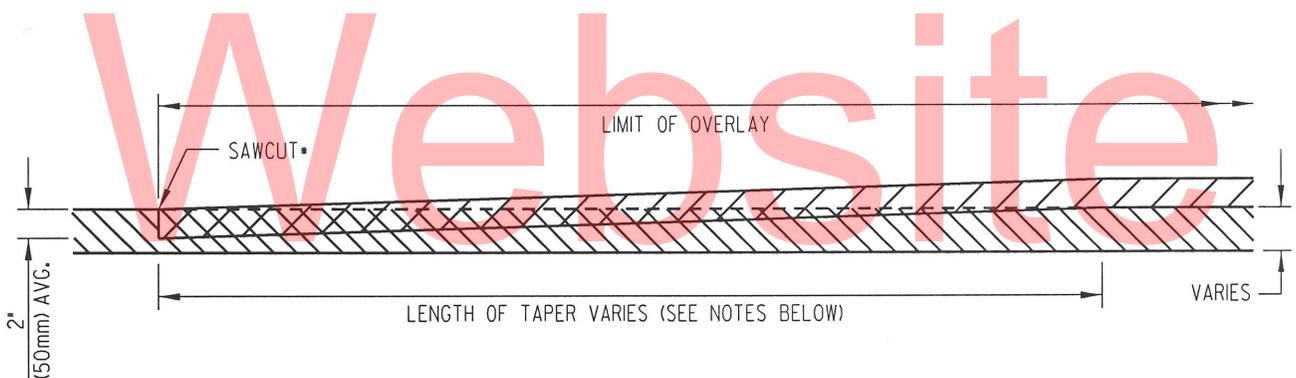


BIT. ASPHALT MIX JOINT FOR FULL WIDTH MILLING SECTION**

PAYMENT INCIDENTAL TO ITEM 760XXX, MILLING.
 **SAWCUT SHALL BE PAID UNDER ITEM 76200I.

LEGEND

	— PROPOSED FULL WIDTH MILLING AND BIT. ASPHALT MIX OVERLAY
	— EXISTING PAVEMENT (TO REMAIN)



BIT. ASPHALT MIX JOINT FOR OVERLAY SECTION*

PAYMENT UNDER ITEM 76100I.
 *SAWCUT IS INCIDENTAL TO ITEM 76100I.

LEGEND

	— PROPOSED OVERLAY
	— EXISTING PAVEMENT
	— EXISTING PAVEMENT TO BE REMOVED

NOTES:

- THE FOLLOWING LENGTHS ARE BASED ON THE POSTED SPEED OF THE ROADWAY:
1. EQUAL TO OR GREATER THAN 55 MPH = 40 FT (9.1m) PER 1" (25mm) OF OVERLAY DEPTH.
 2. LESS THAN 55 MPH = 30 FT (6m) PER 1" (25mm) OF OVERLAY DEPTH.
 3. AT STOP SIGNS = 15 FT (4.6m) PER 1" (25mm) OF OVERLAY DEPTH.

BITUMINOUS ASPHALT MIX JOINT DETAILS

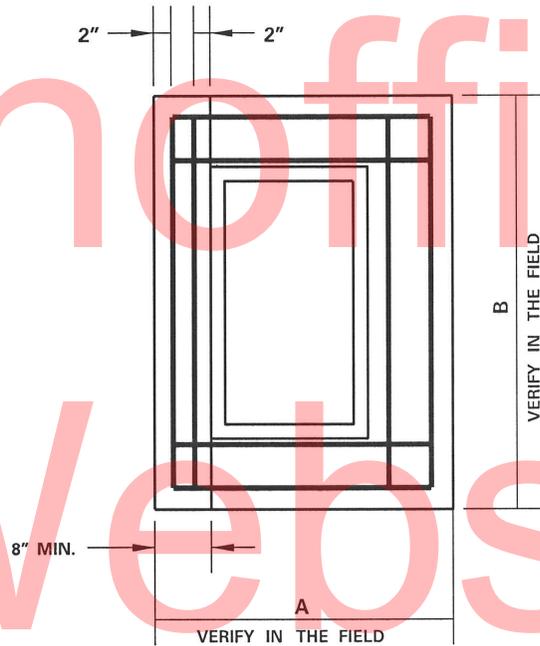
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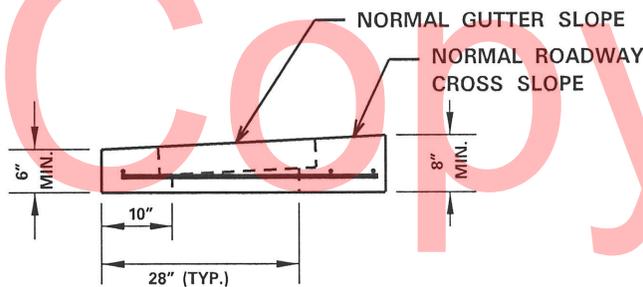
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SOUTH MAINTENANCE LINCOLN & SOUTH MILFORD AREA

DETAILS SHEET



	A	B
PWBD-1	44"	60"
PWBD-2	52"	70"



PRECAST INLET TOP CONVERSION
WITHOUT CURB OPENING

N.T.S.

LOCATION 1 ARGOS CORNER RD (K14E) FROM SR1 COASTAL HWY TO SR1 COASTAL HWY.

LENGTH 1773.936 m 5820 L.F. AADT 565

GENERAL IMPROVEMENTS

BIT. ASPHALT PATCHING AND 1" THIN OVERLAY.

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 1

302005	GRADED AGGREGATE BASE COURSE, TYPE B	36.00 TON
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	134.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	84.00 TON
401836	THIN BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 64-22	1,396.00 TON
406001	BITUMINOUS CONCRETE PATCHING	1,467.00 SY-IN
743003	ARROW PANELS, TYPE C	8.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	14.00 EA-DY
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	8.00 EA-DY
743052	FLAGGER, SUSSEX COUNTY, STATE	254.00 HOUR
743064	FLAGGER, SUSSEX COUNTY, STATE, OVERTIME	38.00 HOUR
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	100.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	20,800.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	100.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	20,800.00 L.F.
761001	BUTT JOINTS, HOT MIX	1,183.00 S.Y.
762001	SAW CUTTING, BITUMINOUS CONCRETE	1,660.00 L.F.
908001	TOPSOIL (TON)	120.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	1,940.00 S.Y.

SOUTH
MAINTENANCE

PAVEMENT & REHABILITATION, SUSSEX I, (LINCOLN AREA), 2015

LOCATION 1 ARGOS CORNER RD (K14E) FROM SR1 COASTAL HWY TO SR1 COASTAL HWY.

COMMENTS:

1. RECOMMENDED M.O.T. TYPICAL APPLICATION, TA-10 (TRAVEL LANES).
2. DAYTIME WORK HOURS: 8:00A - 6:00P.
3. ON THE MAIN CONSTRUCTION ROAD AND ALL INTERSECTING ROADS, "ROAD WORK AHEAD AND END" SIGNS FACING THE TRAFFIC SHALL BE PLACED AT 1500', 1000' AND 500' APPROACHING THE WORK ZONE. IN THOSE CASES, WHERE IT IS DIFFICULT TO MEET THE ABOVE CRITERIA BECAUSE OF DISTANCE LIMITATIONS, THE SIGN SHALL BE PLACED AS DIRECTED BY THE FIELD ENGINEER
4. GABC ITEM 302005 AND QUANTITY IS FOR UNIMPROVED DRIVEWAYS.

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LOCATION 2 WILKINS RD (S206) FROM JOHNSON RD (S207) TO CEDAR CREEK RD (S212)

LENGTH 2788.92 m 9150 L.F. AADT 809

GENERAL IMPROVEMENTS

2" TYPE C (64-22,C) FULL WIDTH OVERLAY.

COMMENTS:

Please see attached sheets for comments

MATERIAL ESTIMATE FOR LOCATION 2

302005	GRADED AGGREGATE BASE COURSE, TYPE B	35.00 TON
302007	GRADED AGGREGATE BASE COURSE, TYPE B	1.00 C.Y.
401696	ENTRANCE, DRIVEWAY AND INTERSECTING STREET PAVING SURCHARGE	145.00 TON
401801	BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS PG 64-22 (CARBONATE STONE)	2,911.00 TON
401823	BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22, PATCHING	46.00 TON
406001	BITUMINOUS CONCRETE PATCHING	800.00 SY-IN
705002	P.C.C. SIDEWALK, 6"	63.00 S.F.
743003	ARROW PANELS, TYPE C	12.00 EA-DY
743004	FURNISH AND MAINTAIN PORTABLE CHANGEABLE MESSAGE SIGN	14.00 EA-DY
743010	FURNISH AND MAINTAIN TRUCK MOUNTED ATTENUATOR, TYPE II	12.00 EA-DY
743052	FLAGGER, SUSSEX COUNTY, STATE	354.00 HOUR
743064	FLAGGER, SUSSEX COUNTY, STATE, OVERTIME	53.00 HOUR
748015	PERMANENT PAVEMENT STRIPING, SYMBOL/LEGEND ALKYD-THERMOPLASTIC	170.00 S.F.
748019	TEMPORARY MARKINGS, PAINT, 4"	10,150.00 L.F.
748026	TEMPORARY MARKINGS, PAINT SYMBOL/LEGEND	170.00 S.F.
748548	PERMANENT PAVEMENT STRIPING, EPOXY RESIN PAINT, WHITE/YELLOW, 5"	10,150.00 L.F.
758000	REMOVAL OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT, CURB, SIDEWALK, ETC.	14.00 S.Y.
761001	BUTT JOINTS, HOT MIX	1,283.00 S.Y.
762001	SAW CUTTING, BITUMINOUS CONCRETE	1,170.00 L.F.
762002	SAW CUTTING, CONCRETE, FULL DEPTH	20.00 L.F.
908001	TOPSOIL (TON)	252.00 TON
908014	PERMANENT GRASS SEEDING, DRY GROUND	3,050.00 S.Y.

LOCATION 2 WILKINS RD (S206) FROM JOHNSON RD (S207) TO CEDAR CREEK RD (S212)

COMMENTS:

1. RECOMMENDED M.O.T. TYPICAL APPLICATION, TA-10 (TRAVEL LANES) .
TYPICAL APPLICATION, TA-3 (SHOULDERS) .
2. DAYTIME WORK HOURS: 8:00A - 6:00P.
3. ON THE MAIN CONSTRUCTION ROAD AND ALL INTERSECTING ROADS, "ROAD WORK AHEAD AND END" SIGNS FACING THE TRAFFIC SHALL BE PLACED AT 1500', 1000' AND 500' APPROACHING THE WORK ZONE. IN THOSE CASES, WHERE IT IS DIFFICULT TO MEET THE ABOVE CRITERIA BECAUSE OF DISTANCE LIMITATIONS, THE SIGN SHALL BE PLACED AS DIRECTED BY THE FIELD ENGINEER
4. GABC ITEM 302005 AND QUANTITY IS FOR UNIMPROVED DRIVEWAYS.
5. THE P.C.C. REMOVAL AND 6" SIDEWALK ITEMS ARE FOR CONCRETE DRIVEWAYS.
6. PROJECT LIMITS SKIP OVER CEDAR CREEK RD, DO NOT OVERLAY.

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