

GENERAL LOCATION OF CONTRACT

THE STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION

U.S. CUSTOMARY
UNITS



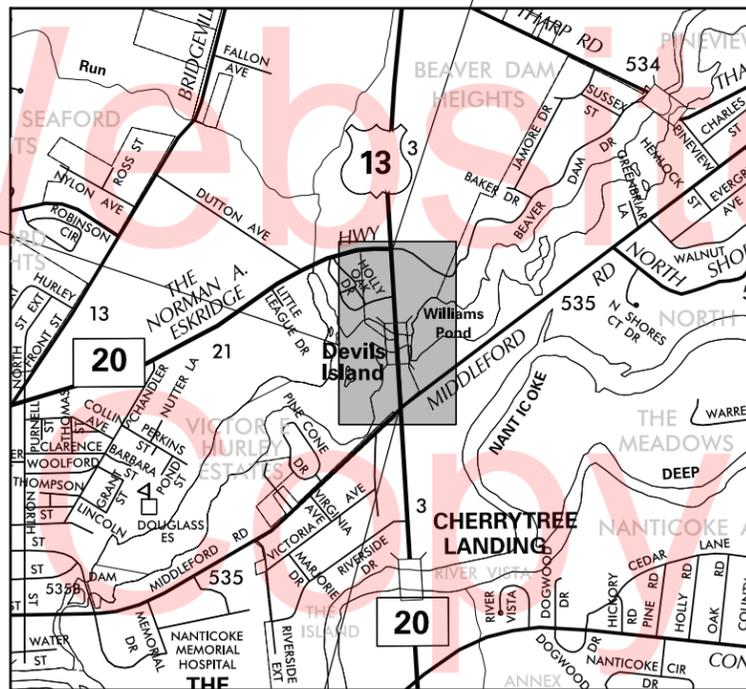
CONSTRUCTION PLANS FOR:

BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT NUMBER: **T201507601**
FEDERAL AID PROJECT NUMBER: **BHN-S003(12)**

COUNTY: **SUSSEX** M.R. #: **03**

BEGIN CONTRACT
STATION 310+70



END CONTRACT
STATION 315+00

PROJECT LOCATION

DESIGN DESIGNATION	
FUNCTIONAL CLASS: URBAN ARTERIAL	D.H.V. PROJECTED: 1950 YEAR: 2040
TYPE OF CONSTRUCTION: BRIDGE REHABILITATION	DESIGN SPEED: 50 MPH
A.A.D.T. CURRENT: 24333	YEAR: 2013 TRUCKS: 10%
A.A.D.T. PROJECTED: 30500	YEAR: 2040 DIRECTION OF DISTRIBUTION: 58%
INDEX OF SHEETS	
SHEET N ^o	TABLE OF CONTENTS
1	TITLE SHEET
2	LEGEND
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9	BRIDGE TYPICAL SECTION
10	MISCELLANEOUS DETAILS
11-14	ABUTMENT REPAIRS
15-18	PIER REPAIRS
19-20	DECK LAUNCH REPAIRS
21	BEARING DETAILS
22	JACKING DETAILS
23	FRAMING PLAN
24	DIAPHRAGM DETAILS
25	LINK SLAB DETAILS
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27	MOT NOTES AND TYPICAL SECTIONS
28-41	CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN
42-49	SIGNING AND STRIPING PLAN

TOTAL SHEETS: 49

APPROVED DESIGN EXCEPTIONS

DESIGN PARAMETER	REQUIRED	PROVIDED	DATE

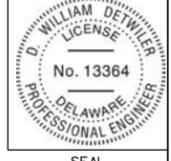
ADDENDA & REVISIONS

DESCRIPTION	NAME & DATE

ASSOCIATED CONTRACTS

CONTRACT NO.	CONTRACT NAME
1105	NANTICOKE RIVER BRIDGE (NORTH FORK)
93-009-02	U.S. 13, SEAFORD
93-076-01	U.S. 13, SEAFORD

PREPARED BY
THE CONSULTING FIRM OF
TYLIN INTERNATIONAL
DOVER, DELAWARE



D. William Detwiler 1/29/2016
RECOMMENDED DATE

RECOMMENDED

[Signature] 02/16/2016
SQUAD MANAGER, CONSTRUCTION DATE

[Signature] 02/17/2016
GROUP ENGINEER, CONSTRUCTION DATE

[Signature] 02/17/2016
ASSISTANT DIRECTOR, CONSTRUCTION DATE

~~RECOMMENDED~~

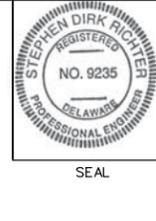
STORMWATER ENGINEER

DATE _____ SEAL _____

RECOMMENDED

[Signature]
SQUAD MANAGER, TRANSPORTATION SOLUTIONS
BRIDGE DESIGN

DATE 02/16/2016 SEAL _____



RECOMMENDED

[Signature]
BRIDGE DESIGN ENGINEER

DATE 02/17/2016 SEAL _____



RECOMMENDED

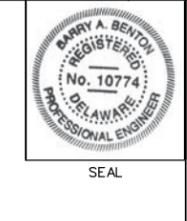
[Signature]
GROUP ENGINEER, PROJECT DEVELOPMENT

DATE _____ SEAL _____

RECOMMENDED

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ASSISTANT DIRECTOR, BRIDGE

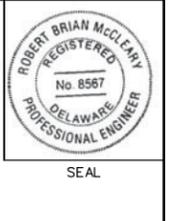
DATE 02/17/2016 SEAL _____



APPROVED

[Signature]
CHIEF ENGINEER

DATE 02/26/2016 SEAL _____



EXISTING SYMBOLS

DRAINAGE	
	DITCH OR STREAM CENTERLINE
	DIRECTIONAL STREAM FLOW ARROW
	DRAINAGE INLET
	DRAINAGE JUNCTION BOX
	DRAINAGE MANHOLE
	DRAINAGE PIPE AND FLOW ARROW
	DRAINAGE PIPE HEADWALL
	RIPRAP - AREA FEATURE
	RIPRAP - LINEAR FEATURE

MANMADE ROADSIDE FEATURES	
	BOLLARD - STEEL POLE
	BOLLARD - WOOD POST
	CURB
	CURB AND GUTTER
	FENCE - CHAINLINK OR STRANDED
	FENCE - STOCKADE OR SPLIT RAIL
	FLAG POLE
	GUARDRAIL - STEEL BEAM
	GUARDRAIL - WIRE ROPE
	LAMP AND POST - RESIDENTIAL
	MAILBOX
	PARKING METER AND POST
	PAVEMENT - FLEXIBLE
	PAVEMENT - RIGID
	PILE - BRIDGE
	PILLAR OR MISCELLANEOUS POST
	TRAFFIC SIGN AND POST
	WALL - BRICK OR BLOCK
	WALL - STONE

NATURAL ROADSIDE FEATURES	
	GRASS LAWN
	HEDGEROW OR THICKET
	MARSH BOUNDARY LINE
	TREE - CONIFEROUS
	TREE - DECIDUOUS
	TREE STUMP
	SHRUBBERY
	DELINEATED WETLAND BOUNDARY LINE
	WOODS LINE BOUNDARY

RIGHT-OF-WAY SYMBOLS	
	PROPERTY MARKER - CONCRETE MON.
	PROPERTY MARKER - IRON PIPE
	HISTORIC RIGHT-OF-WAY BASELINE
	EXISTING RIGHT-OF-WAY
	EXISTING PROPERTY LINE
	EXISTING EASEMENT
	EXISTING DENIAL OF ACCESS
	EXISTING R/W & DENIAL OF ACCESS

SURVEY CONTROL & MONUMENTATION	
	SURVEY BENCHMARK LOCATION
	SURVEY TIE POINT LOCATION
	SURVEY TRAVERSE POINT
	POINT OF CURVATURE OR TANGENCY
	POINT OF INTERSECTING TANGENTS

UTILITY	
	SOIL BORING LOCATION
	UTILITY TEST HOLE LOCATION
	CABLE TV DISTRIBUTION BOX
	ELECTRIC MANHOLE
	ELECTRIC METER
	ELECTRIC TRANSFORMER
	POLE MOUNTED LUMINAIRE
	GAS MANHOLE
	GAS METER
	GAS VALVE
	GAS PUMP - SERVICE STATION
	RAILROAD TRACKS
	SANITARY SEWER MANHOLE
	SANITARY SEWER VALVE
	SANITARY SEWER VENT OR CLEANOUT
	SEPTIC DRAIN FIELD
	TELEPHONE BOOTH
	TELEPHONE MANHOLE
	TELEPHONE TEST POINT
	TRAFFIC - CONDUIT JUNCTION WELL
	TRAFFIC - LIGHT POLE AND BASE
	TRAFFIC - PEDESTRIAN POLE & BASE
	TRAFFIC - SIGNAL CABINET & BASE
	TRAFFIC - SIGNAL POLE AND BASE
	UTILITY BOX
	UTILITY POLE GUY WIRE ANCHOR
	UTILITY POLE
	WATER - FIRE HYDRANT
	WATER METER
	WATER VALVE
	WELL HEAD
	MANHOLE - UNDETERMINED OWNER

UTILITY COMPANY FACILITIES	
	CITY OF SEAFORD (10" WATER)
	CITY OF SEAFORD (5" FORCE MAIN)
	CITY OF SEAFORD (ELECTRIC)
	EAST. SHORE NAT. GAS (6" NAT. GAS)
	EAST. SHORE NAT. GAS (10" NAT. GAS)

CONSTRUCTION	
	CONCRETE SAFETY BARRIER - PERMANENT
	BIOFILTRATION SWALE
	BRICK PATTERNED SURFACE
	BUTT JOINT
	CONSTRUCTION BASELINE
	CONSTRUCTION SAFETY FENCE
	CURB, TYPE 1 & TYPE 3
	CURB, TYPE 2
	CURB & GUTTER, TYPE 1
	CURB & GUTTER, TYPE 2
	CURB & GUTTER, TYPE 3
	CURB & GUTTER, TYPE 4
	CLEAR ZONE
	DRAINAGE INLET
	DITCH
	FENCE - METAL
	FENCE - WOOD
	FLARED END SECTION
	GUARDRAIL, TYPE 1
	GUARDRAIL, TYPE 2
	GUARDRAIL, TYPE 3
	GUARDRAIL END ANCHORAGE
	GUARDRAIL END TREATMENT, TYPE 1
	GUARDRAIL END TREATMENT, TYPE 2
	GUARDRAIL END TREATMENT, TYPE 3
	IMPACT ATTENUATOR
	JUNCTION BOX - DRAINAGE
	LATERAL OFFSET
	LIMIT OF CONSTRUCTION
	MAILBOX
	MANHOLE
	PAVEMENT PATCH
	PAVEMENT REMOVAL - TOPSOIL, SEED AND MULCH
	PIPE & DIRECTIONAL FLOW ARROW
	RIPRAP
	P.C.C. SIDEWALK - 4"
	P.C.C. SIDEWALK - 6" (USE 8" DEPTH FOR CHANNELIZATION ISLANDS.)
	UNDERDRAIN
	UNDERDRAIN OUTLET

RIGHT-OF-WAY SYMBOLS	
	PROPOSED RIGHT-OF-WAY MONUMENT
	PROPOSED DENIAL OF ACCESS
	PROPOSED PERMANENT EASEMENT
	PROPOSED RIGHT-OF-WAY
	PROPOSED R/W & DENIAL OF ACCESS
	TEMPORARY CONSTRUCTION EASEMENT
	PROPOSED RIGHT-OF-WAY BASELINE

PROPOSED SYMBOLS

IDENTIFIERS	
	ADJUST BY CONTRACTOR
	ADJUST BY OTHERS
	CONCRETE SAFETY BARRIER
	CURB OR CURB & GUTTER
	CONVERT TO JUNCTION BOX
	CONVERT TO DRAINAGE MANHOLE
	CURB OPENING
	CURB RAMP / TYPE
	CURB RAMP / TYPE - WITHOUT SIDEWALK SURFACE DETECTABLE WARNING SYSTEM
	CONSTRUCTION SAFETY FENCE
	DRAINAGE INLET
	DO NOT DISTURB
	ENERGY DISSIPATOR
	FENCE
	FLARED END SECTION
	FILL WITH FLOWABLE FILL
	FILTRATION STRUCTURE
	GUARDRAIL
	JUNCTION BOX
	MANHOLE
	MONUMENT - RIGHT-OF-WAY
	PIPE
	RELOCATE BY CONTRACTOR
	RELOCATE BY OTHERS
	REMOVE BY CONTRACTOR
	REMOVE BY OTHERS
	UNDERDRAIN / LENGTH
	UNDERDRAIN OUTLET PIPE

CONSTRUCTION PHASING & M.O.T	
	BARRICADE, TYPE 3
	CONCRETE SAFETY BARRIER - PORTABLE
	CONSTRUCTION SAFETY FENCE / LENGTH
	CONSTRUCTION SAFETY FENCE
	CONSTRUCTION WARNING SIGN LOCATION
	ARROW PANEL
	CONSTRUCTION WARNING SIGN
	CRASH CUSHION ARRAY
	DRUM - TRAFFIC CONTROL
	FLAGGER LOCATION
	PHASING TRAFFIC FLOW ARROW
	TEMPORARY CONSTRUCTION
	TEMPORARY PAVEMENT MARKING ARROW
	TRUCK WITH MOUNTED ATTENUATOR
	WORK AREA - ACTIVE PHASE

PAVEMENT SECTION(S)	
	MILL AND OVERLAY PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS
	UHPC & DECK CONCRETE - SEE LINK SLAB DETAILS AND DIAPHRAGM DETAILS FOR LIMITS AND DEPTHS
	RECONSTRUCTED PAVEMENT - SEE TYPICAL SECTIONS FOR MATERIALS AND DEPTHS

EROSION & SEDIMENT CONTROL	
	DEWATERING BAG
	DEWATERING BASIN
	EARTH DIKE
	INLET SEDIMENT CONTROL
	PERIMETER DIKE/SWALE
	PORTABLE SEDIMENT TANK
	SANDBAG DIKE
	SANDBAG DIVERSION
	STONE CHECK DAM
	STABILIZED CONSTRUCTION ENTRANCE
	SILT FENCE / LENGTH
	SILT FENCE
	SILT FENCE - REINFORCED
	SUMP PIT
	SEDIMENT TRAP / NUMBER
	SEDIMENT TRAP
	SEDIMENT TRAP WITH INLET AS OUTLET
	SEDIMENT TRAP PIPE OUTLET
	STILLING WELL
	TEMPORARY SWALE
	TEMPORARY SLOPE DRAIN
	TURBIDITY CURTAIN / LENGTH
	TURBIDITY CURTAIN

LANDSCAPING	
	LANDSCAPE PLANTINGS
	SHRUBBERY
	CONIFEROUS TREE
	DECIDUOUS TREE

TRAFFIC	
	ITMS CONDUIT
	SIGNAL CONDUIT
	CONDUIT JUNCTION WELL
	LUMINAIRE
	PAVEMENT MARKINGS
	PAVEMENT STRIPING
	TRAFFIC SIGN

MISCELLANEOUS	
	TOP OF BANK

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GENERAL NOTES

1. THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS", DATED AUGUST 2001 AND THE DELAWARE DEPARTMENT OF TRANSPORTATION "STANDARD CONSTRUCTION DETAILS", DATED 2001, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.

EROSION POTENTIAL FOR THIS PROJECT	CONTRACTOR ESC SUPERVISOR REQUIREMENT
(X) INSIGNIFICANT	NONE
() MINOR	CONTRACTOR TRAINING PROGRAM, AS DEFINED IN SECTION 6.2 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MEDIUM	CONTRACTOR TRAINING PROGRAM, AS DEFINED IN SECTION 6.2 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.
() MAJOR	CERTIFIED CONSTRUCTION REVIEWER (CCR), AS DEFINED IN SECTION 6.3 OF THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS.

3. ELECTRONIC PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR, INCLUDE:

()	NONE
()	ASCII DATA FILES WITH COORDINATES AND ELEVATIONS FOR PROPOSED POINTS AS SELECTED BY THE ENGINEER.
(X)	ALL PLAN SHEETS, IN PDF FORMAT.
()	EXISTING DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	PROPOSED DIGITAL TERRAIN MODEL, IN .DTM FILE FORMAT, COMPATIBLE WITH SOFTWARE CURRENTLY USED BY DELDOT.
()	DESIGN FILE, IN .DGN FILE FORMAT, CONTAINING ONLY THE PROPOSED 3D TRIANGLES OF THE PROPOSED DIGITAL TERRAIN MODEL (DTM).

NOTE: THE DOCUMENT ENTITLED "RELEASE FOR DELIVERY OF DOCUMENTS IN ELECTRONIC FORM TO A CONTRACTOR" MUST BE SIGNED BY ALL PARTIES PRIOR TO THE DELIVERY OF ANY ELECTRONIC PROJECT FILES.

4. PROJECT FILES THAT WILL BE MADE AVAILABLE TO THE CONTRACTOR, INCLUDE:

()	CROSS SECTIONS
()	RIGHT-OF-WAY PLANS (WILL BE MADE AVAILABLE TO THE AWARDED CONTRACTOR)

5. AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) CERTIFIED TRAFFIC CONTROL SUPERVISOR REQUIREMENT FOR THIS PROJECT.

()	THE CONTRACTOR SHALL NOT BE REQUIRED TO HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT.
()	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT OR ANOTHER ATSSA CERTIFIED MEMBER OF THE CONTRACTOR'S PROJECT STAFF MAY BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR IS INCIDENTAL TO ITEM 743000.
(X)	THE CONTRACTOR SHALL HAVE AN ATSSA SUPERVISOR ASSIGNED TO THIS PROJECT. THE ATSSA SUPERVISOR'S SOLE JOB SHALL BE SUPERVISION OF THE INSTALLATION, OPERATION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES FOR THIS PROJECT. THE CONTRACTOR'S GENERAL SUPERINTENDENT FOR THIS PROJECT SHALL NOT BE THE ATSSA SUPERVISOR. PAYMENT FOR ATSSA SUPERVISOR SHALL BE PAID FOR UNDER ITEM 743031.

6. THE DISTURBED AREA FOR THIS PROJECT IS 1.48 ACRES. ADDED IMPERVIOUS AREA FOR THIS PROJECT IS 0.00 ACRES.

7. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS HAVE BEEN APPROVED BY DELDOT'S STORMWATER ENGINEER UNDER DELDOT'S DELEGATED AUTHORITY. THE SEDIMENT AND STORMWATER MANAGEMENT PLANS ARE VALID FOR A THREE YEAR PERIOD, BEGINNING ON THE DATE THE STORMWATER ENGINEER SIGNED THE CONSTRUCTION TITLE SHEET. IF THE FINAL ACCEPTANCE OF THE PROJECT IS ANTICIPATED TO EXTEND BEYOND THE THREE YEARS, THE CONTRACTOR WILL INFORM THE ENGINEER THREE MONTHS PRIOR TO THE EXPIRATION OF THE APPROVED SEDIMENT AND STORMWATER MANAGEMENT PLANS. THE STORMWATER ENGINEER WILL REVIEW THE CURRENT SEDIMENT AND STORMWATER MANAGEMENT PLAN AND ISSUE AN EXTENSION WITH ANY APPROPRIATE MODIFICATIONS.

BRIDGE PROJECT NOTES

SECTION 100

1. ANY DAMAGE TO ITEMS NOTED TO BE RELOCATED OR RESET BY THE CONTRACTOR, AT THE DISCRETION OF THE ENGINEER, SHALL BE REPAIRED AND/OR REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.

SECTION 300

2. A. THE CONTRACTOR MAY ELECT TO USE ANY OF THE FOLLOWING MATERIALS TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B':
 a. CRUSHED STONE (PER STANDARD SPECIFICATION 821)
 b. CRUSHED CONCRETE (PER STANDARD SPECIFICATION 821)
 c. HOT-MIX MILLINGS (PER SPECIAL PROVISION 302514 MILLED HOT-MIX BASE COURSE)

THE CONTRACTOR WILL NOT BE ALLOWED TO MIX DIFFERENT MATERIALS (OR SIMILAR MATERIALS FROM DIFFERENT SOURCES) TO MEET THE REQUIREMENTS OF ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

ALL OF THE ABOVE LISTED MATERIALS ARE PERMITTED FOR USE ON THE JOB, PROVIDED THEY ARE SEPARATED INTO APPROVED AREAS. EACH AREA OF BASE COURSE MUST BE CONSTRUCTED USING MATERIALS FROM A SINGULAR SOURCE, FULL DEPTH, IN ORDER THAT PROPER TESTING MAY BE ACCOMPLISHED. THE CONTRACTOR AND ENGINEER SHALL AGREE ON THE LIMITS OF EACH SOURCE OF MATERIAL PRIOR TO PLACEMENT.

B. THE QUANTITY USED FOR BASE OF EACH OF THE ABOVE LISTED MATERIALS WILL BE THE CONTRACTOR'S CHOICE, WITH THE TOTAL BEING EQUAL TO THE ACTUAL QUANTITY USED UNDER ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

C. THE CONTRACTOR MAY ALSO ELECT TO RECYCLE MILLINGS FOR USE IN HOT-MIX AS PERMITTED BY THE STANDARD SPECIFICATIONS. THE CHOICE OF THE QUANTITY OF MILLINGS USED FOR THIS PURPOSE, OR FOR BASE COURSE, LIES WITH THE CONTRACTOR. ALL EXCESS MILLING MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR.

D. HOT-MIX MILLINGS MAY BE GENERATED FROM THE FOLLOWING SOURCES:
 a. MATERIAL MADE AVAILABLE WHEN MILLED ON THIS CONTRACT UNDER ITEM 760006.
 b. MATERIAL MILLED ON THIS CONTRACT AT THE CONTRACTOR'S CHOICE UNDER ITEM 202000.
 c. MILLED MATERIAL FURNISHED ON THE JOB FROM THE CONTRACTOR'S YARD OR OTHER OUTSIDE SOURCE.
 ALL MILLED MATERIALS SHALL MEET THE MATERIAL REQUIREMENTS OF ITEM 302514 - MILLED HOT-MIX BASE COURSE.

E. PAYMENT CLARIFICATION:
 a. SHOULD THE CONTRACTOR ELECT TO MILL PORTIONS OF HOT-MIX SHOWN ON THE PLANS TO BE REMOVED UNDER ITEM 202000 - EXCAVATION AND EMBANKMENT THE COST OF MILLING THIS HOT-MIX WILL BE PAID AS ITEM 202000 - EXCAVATION AND EMBANKMENT. THE MILLINGS GENERATED MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE, OR DISPOSED OF TO AN APPROVED SITE. HAULING COSTS FOR DISPOSAL AND/OR RECYCLING ARE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.

b. MILLINGS GENERATED UNDER ITEM 760006 - PAVEMENT - MILLING, HOT-MIX, 2" DEPTH MAY BE RECYCLED INTO HOT-MIX, UTILIZED FOR BASE COURSE OR DISPOSED OF BY THE CONTRACTOR TO AN APPROVED SITE. NO SEPARATE PAYMENT WILL BE MADE FOR TRANSPORTING MILLINGS ON SITE OR TO AN APPROVED DISPOSAL SITE.

c. SHOULD THE CONTRACTOR ELECT TO TEMPORARILY STOCKPILE MILLINGS ON THE JOB SITE FOR LATER USE, ALL COSTS FOR STOCKPILING AND SUBSEQUENT REHANDLING SHALL BE INCIDENTAL TO ITEM 202000 - EXCAVATION AND EMBANKMENT.

d. MILLINGS USED FOR BASE COURSE SHALL BE PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE. NO SEPARATE PAYMENT WILL BE MADE TO FURNISH MILLINGS FROM AN OUTSIDE SOURCE OR TRANSPORT MILLINGS WITHIN THE PROJECT LIMITS. MILLINGS USED FOR BASE COURSE WILL BE PAID FOR AT THE UNIT BID PRICE FOR ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'.

e. ALL COSTS TO UTILIZE MILLINGS IN RECYCLED HOT-MIX WILL BE INCIDENTAL TO THE UNIT PRICE BID FOR THE HOT-MIX ITEM USING THE RECYCLED MATERIAL.

f. SPECIAL PROVISION 302514 - MILLED HOT-MIX BASE COURSE IS PROVIDED TO SPECIFY THE MEANS OF LAY DOWN AND COMPACTION AS WELL AS THE MATERIAL REQUIREMENTS FOR MILLINGS USED AS BASE COURSE. ALL COSTS TO BRING THE MILLINGS INTO COMPLIANCE WITH THE REQUIREMENTS OF ITEM - 302514 MILLED HOT-MIX BASE COURSE ARE INCIDENTAL TO ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE 'B'. NO PAYMENT WILL BE MADE FOR ITEM 302514 - MILLED HOT-MIX BASE COURSE. THE QUANTITY OF MILLINGS USED FOR BASE COURSE WILL BE PAID FOR UNDER ITEM 302007 - GRADED AGGREGATE BASE COURSE.

SECTION 600

3. PORTLAND CEMENT CONCRETE:
 USE PORTLAND CEMENT CONCRETE FOR CAST-IN-PLACE ELEMENTS AS FOLLOWS:
 (f'c = 28-DAY COMPRESSIVE STRENGTH)

CLASS A - PARAPET (f'c = 4.5 ksi)
 CLASS D - DECK AND END DIAPHRAGM (f'c = 4.5 ksi)
 UHPC - DECK LINK SLAB (f'c = 22.0 ksi)

A HIGHER CLASS CONCRETE MAY BE SUBSTITUTED FOR A LOWER CLASS CONCRETE AT NO ADDITIONAL COST TO DELDOT WITH APPROVAL OF THE BRIDGE DESIGN ENGINEER.

ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

4. BAR REINFORCEMENT:
 - REINFORCING STEEL SHALL CONFORM TO AASHTO M31 (ASTM A615), GRADE 60.
 - REINFORCING STEEL SHALL HAVE A 3" CLEAR COVER IF CAST AGAINST EARTH OR A 2" CLEAR COVER ELSEWHERE, UNLESS OTHERWISE SPECIFIED ON THE PLANS.
 - ANY FIELD CUTTING OR FIELD BENDING MUST BE APPROVED BY THE ENGINEER. PAYMENT SHALL BE INCIDENTAL TO THE BAR REINFORCEMENT ITEM.
 - WELDING OF REINFORCEMENT DURING FABRICATION OR CONSTRUCTION IS NOT PERMITTED UNLESS SPECIFIED.

SECTION 600 (CONT.)

5. CONCRETE SEALER:
 REFER TO DETAILS CONTAINING CONCRETE SEALER LIMITS ON SHEET 9 PERTAINING TO THE DECK AND BARRIER, AND SHEETS 11 TO 18 PERTAINING TO THE SUBSTRUCTURE. TO BE PAID UNDER ITEM #602546 - WATERPROOFING P.C.C. MASONRY AND ITEM #602646 - SILICONE ACRYLIC CONCRETE SEALER.

THE CONCRETE SEALER COLOR SHALL BE WHITE (*37722) AS PER FEDERAL STANDARD 595C. THE COLOR SHALL BE SUBMITTED FOR APPROVAL.

6. STRUCTURAL STEEL:
 PROVIDE STRUCTURAL STEEL CONFORMING TO AASHTO M270, GRADE 50 DESIGNATION, EXCEPT WHEN NOTED OTHERWISE.

ALL FASTENERS ARE 1" DIAMETER ASTM A325 HIGH STRENGTH BOLTS, TYPE I, UNLESS OTHERWISE NOTED.

PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH AASHTO/AWS D1.5M/D.15 **BRIDGE WELDING CODE, AND CONTRACT DOCUMENTS. MAKE TACK WELDS WITH THE SAME TYPE OF ELECTRODE AND INCORPORATE IN THE FINAL WELD. NO OTHER TACK WELDING WILL BE PERMITTED.

USE OVERSIZED HOLES ON DIAPHRAGM CONNECTORS. ALL BOLTS ON DIAPHRAGMS MUST BE FINGER TIGHT AT ERECTION. BOLTS ARE TO BE TORQUED PRIOR TO JACKING OPERATIONS.

REMOVE EXISTING ANCHOR BOLTS BY CORE-DRILLING TO THE DEPTH INDICATED IN THE PLANS. SET ANCHOR BOLTS IN CORE-DRILLED HOLES AND FILL WITH NON-SHRINK GROUT. PAYMENT SHALL BE INCIDENTAL TO ITEM #605651 - REPLACING BRIDGE BEARINGS, SPECIAL.

7. BEARINGS:
 ALL ELASTOMERIC BEARINGS SHALL BE STEEL LAMINATED ELASTOMERIC DESIGNED AS PER AASHTO 14.7.5, METHOD B, AND SHALL CONFORM TO SECTION 605 OF THE STANDARD SPECIFICATIONS. TO BE PAID UNDER ITEM #605651 - REPLACING BRIDGE BEARINGS, SPECIAL.

8. COATINGS:
 ALL STRUCTURAL STEEL SHALL BE CLEANED AND PAINTED IN ACCORDANCE WITH SPECIAL PROVISIONS 605522 AND 605533. TO BE PAID UNDER ITEM #605522 - URETHANE PAINT SYSTEM, EXISTING STEEL AND ITEM #605533 - CLEANING EXISTING STEEL STRUCTURES, HAZARDOUS BASE, (L.S.).

THE STRUCTURAL STEEL PAINT COLOR SHALL BE FOREST GREEN (*24172) AS PER FEDERAL STANDARD 595C. THE COLOR SHALL BE SUBMITTED FOR APPROVAL.

TOP FLANGE SHALL BE CLEANED AND PRIMED WHEN EXPOSED AT THE ABUTMENT LOCATIONS.

CONTAINMENT SYSTEMS ARE REQUIRED FOR ALL ABRASIVE BLASTING. THE DESIGN OF THE CONTAINMENT SYSTEM MUST BE SUBMITTED AND APPROVED BY THE DEPARTMENT PRIOR TO ANY WORK BEING PERFORMED.

ALL DOWNSPOUTS SHALL BE VACUUM POWER TOOL CLEANED OR ABRASIVE BLAST CLEANED, AND WHERE NECESSARY GALVANIZED DOWNSPOUTS SHALL BE PAINTED WITH A MOISTURE CURED ALUMINUM PAINT SYSTEM. IF THE DOWNSPOUTS ARE NON-GALVANIZED, THEN THEY SHALL BE PAINTED WITH THE REGULAR URETHANE PAINT SYSTEM USED TO PAINT THE BRIDGE AS INDICATED IN ITEM #605522. TO BE PAID UNDER ITEM #605522 - URETHANE PAINT SYSTEM, EXISTING STEEL AND ITEM #605533 - CLEANING EXISTING STEEL STRUCTURES, HAZARDOUS BASE, (L.S.). EXISTING CORRUGATED METAL DRAINAGE PIPES AT SOUTH ABUTMENTS SHALL NOT BE CLEANED AND PAINTED.

SECTION 700

9. MAINTENANCE OF TRAFFIC:
 MAINTENANCE OF TRAFFIC WILL BE IN ACCORDANCE WITH TYPICAL APPLICATION 33 AND 34 OF THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE MOT PLANS.

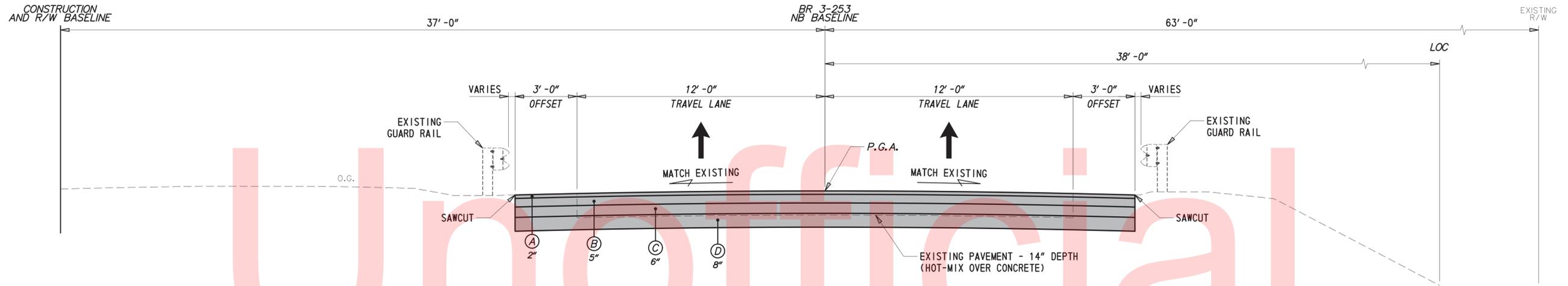
10. TEMPORARY MOUNTED BARRIERS:
 THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR APPROVAL A WORKING DRAWING SUBMITTAL DETAILING THE PROPOSED MEANS AND METHODS TO ANCHOR THE PORTABLE P.C.C. STRUCTURE MOUNTED BARRIER AND TO REPAIR THE BOLT HOLES IN THE DECK AND ROADWAY. THE CONTRACTOR SHALL ADJUST THE LOCATION OF ANCHORS TO AVOID CONFLICT WITH BAR REINFORCEMENT IN THE DECK. PAYMENT INCIDENTAL TO ITEM #743013 - FURNISH PORTABLE PCC STRUCTURE MOUNTED SAFETY BARRIER AND ITEM #743015 - FURNISH AND MAINTAIN PORTABLE PCC SAFETY BARRIER (FOR ROADWAY USE).

11. SAWCUTTING:
 ALL PAVED AREAS TO BE MILL AND OVERLAY OR RECONSTRUCTED SHALL BE SAWCUT AT THE POINT WHERE THE NEW PAVEMENT IS TO TIE INTO THE EXISTING PAVEMENT. ALL P.C.C. SAWCUTTING SHALL BE FULL DEPTH, UNLESS OTHERWISE NOTED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. SAWCUTTING OF THE HOT-MIX AT THE BEGIN AND END LIMITS OF THE MILL AND OVERLAY SHALL BE PAID UNDER ITEM #762001 - SAWCUTTING, HOT MIX. SAWCUTTING OF THE APPROACH ROADWAY AT THE BRIDGES SHALL BE PAID UNDER ITEM #762002 - SAWCUTTING, CONCRETE, FULL DEPTH.

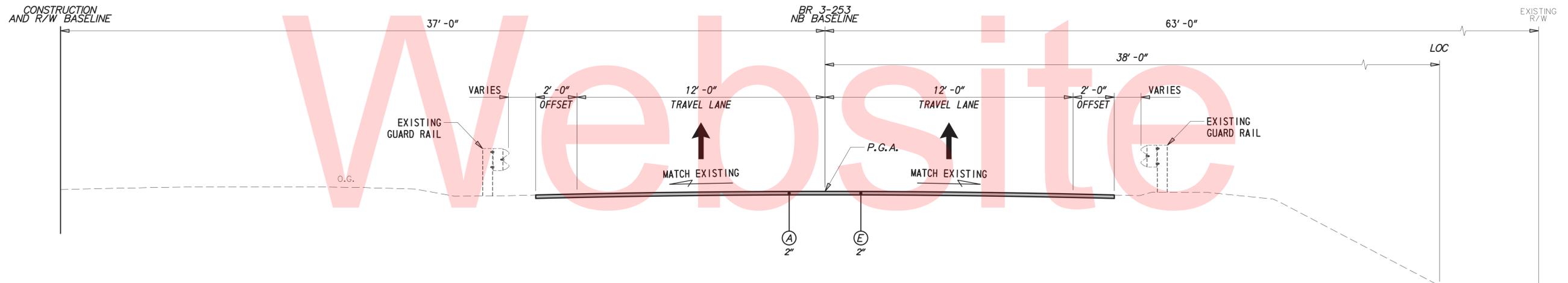
12. SIGNING:
 TO AVOID DAMAGE, SIGNS WITHIN PROJECT LIMITS MAY BE REMOVED DURING CONSTRUCTION IF NEEDED, BUT MUST BE REPLACED TO MATCH EXISTING CONDITIONS BEFORE REOPENING THE ROADWAY. ALL WORK RELATED TO MOVING AND REINSTALLING THE SIGN SHALL BE INCIDENTAL TO ITEM #743000 - MAINTENANCE OF TRAFFIC. IF THE SIGN IS DAMAGED DURING CONSTRUCTION, THE SIGN MUST BE REPLACED AT THE CONTRACTOR'S EXPENSE.

MISCELLANEOUS

MISCELLANEOUS NOTES CAN BE FOUND ON SHEET 10.



TYPICAL SECTION - US 13 NORTHBOUND
STATION 310+96.63 TO STATION 311+46.63
STATION 312+53.38 TO STATION 313+03.38
(NORTHBOUND SHOWN, SOUTHBOUND SIMILAR)

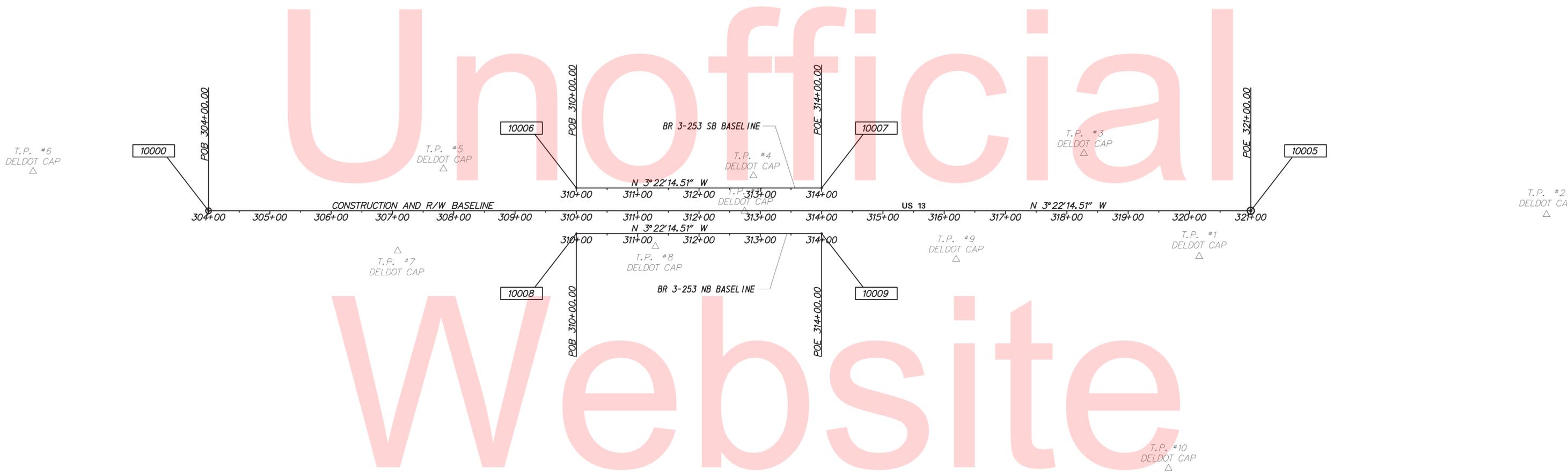


TYPICAL SECTION - US 13 NORTHBOUND
MILLING AND OVERLAY, ALL OTHER SECTIONS
REFER TO SHEETS 42-48 FOR LIMITS OF MILL AND OVERLAY
(NORTHBOUND SHOWN, SOUTHBOUND SIMILAR)

MATERIAL	LIFT THICKNESS	
	MINIMUM	MAXIMUM
BITUMINOUS CONCRETE, TYPE 'C'	1.25"	2"
BITUMINOUS CONCRETE, TYPE 'B'	2.25"	4"
BITUMINOUS CONCRETE BASE COURSE	3"	6"
GRADED AGGREGATE BASE COURSE	-	8"

LEGEND	
(A)	ITEM 401830 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE C, 160 GYRATIONS, PG 70-22 (NON-CARBONATE STONE)
(B)	ITEM 401813 - BITUMINOUS CONCRETE, SUPERPAVE, TYPE B, 160 GYRATIONS, PG 70-22 (PLACE IN 2 EQUAL LIFTS)
(C)	ITEM 401819 - BITUMINOUS CONCRETE, SUPERPAVE, BITUMINOUS CONCRETE BASE COURSE, 160 GYRATIONS, PG 64-22
(D)	ITEM 302007 - GRADED AGGREGATE BASE COURSE, TYPE B
(E)	ITEM 760006 - PAVEMENT - MILLING, HOT-MIX, 2" DEPTH

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CONSTRUCTION ALIGNMENT CONTROL				
POINT	STATION	OFFSET	NORTHING	EASTING
10000	304+00.00	0.00	236045.9972	605512.9172
10002	309+00.00	0.00	236545.1322	605483.5193
10003	314+00.00	0.00	237044.2673	605454.1214
10004	319+00.00	0.00	237543.4023	605424.7235
10005	321+00.00	0.00	237743.0563	605412.9643
10006	310+00.00	-37.00	236642.7838	605440.7037
10007	314+00.00	-37.00	237042.0918	605417.1854
10008	310+00.00	37.00	236647.1347	605514.5757
10009	314+00.00	37.00	237046.4427	605491.0574

NOTE: OFFSETS ARE BASED ON THE CONSTRUCTION AND R/W BASELINE.

HORIZONTAL / VERTICAL CONTROL DATA					
POINT	STATION	OFFSET	NORTHING	EASTING	ELEVATION
TP #1	320+15.90	74.33	237663.4726	605492.1102	20.53
TP #2	325+82.31	6.25	238224.8953	605390.8499	22.04
TP #3	318+27.92	-93.37	237465.9567	605335.7543	19.54
TP #4	312+88.78	-56.99	236929.8915	605403.7697	17.65
TP #5	307+83.18	-68.16	236424.5071	605422.3469	15.39
TP #6	301+13.80	-64.06	235756.5258	605465.7913	13.06
TP #7	307+08.43	65.29	236357.7341	605559.9601	15.59
TP #8	311+28.90	57.98	236777.0411	605527.9363	17.14
TP #9	316+19.40	79.17	237267.9384	605520.2561	16.74
TP #10	319+65.71	419.08	237633.6417	605839.2143	19.01
TP #11	312+74.40	0.65	236918.9230	605462.1524	17.72

NOTE: OFFSETS ARE BASED ON THE CONSTRUCTION AND R/W BASELINE.

DATUM REFERENCE:

HORIZONTAL - THIS PROJECT IS REFERENCED TO THE DELAWARE STATE PLANE COORDINATE SYSTEM (NAD 83/91).

VERTICAL - THIS PROJECT IS REFERENCED TO NAVD 88.

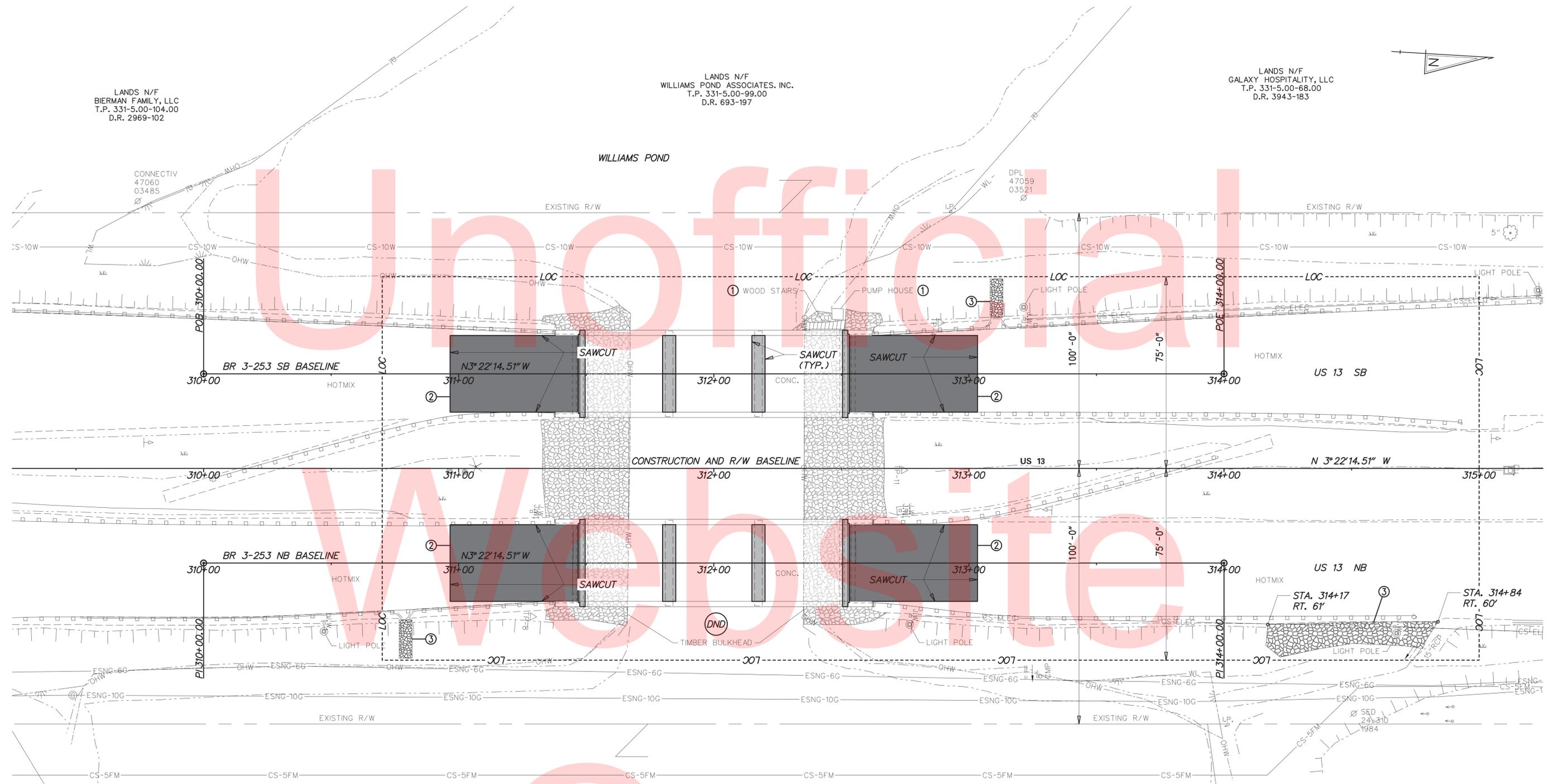
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DELAWARE DEPARTMENT OF TRANSPORTATION	ADDENDUMS / REVISIONS		BR 3-253 N&S ON US 13 OVER WILLIAMS POND	CONTRACT T201507601 COUNTY SUSSEX	BRIDGE NO. BR 3-253 N&S DESIGNED BY: JWS CHECKED BY: DEF	HORIZONTAL AND VERTICAL CONTROL	SHEET NO. 5 TOTAL SHTS. 49	

LANDS N/F
BIERMAN FAMILY, LLC
T.P. 331-5.00-104.00
D.R. 2969-102

LANDS N/F
WILLIAMS POND ASSOCIATES, INC.
T.P. 331-5.00-99.00
D.R. 693-197

LANDS N/F
GALAXY HOSPITALITY, LLC
T.P. 331-5.00-68.00
D.R. 3943-183

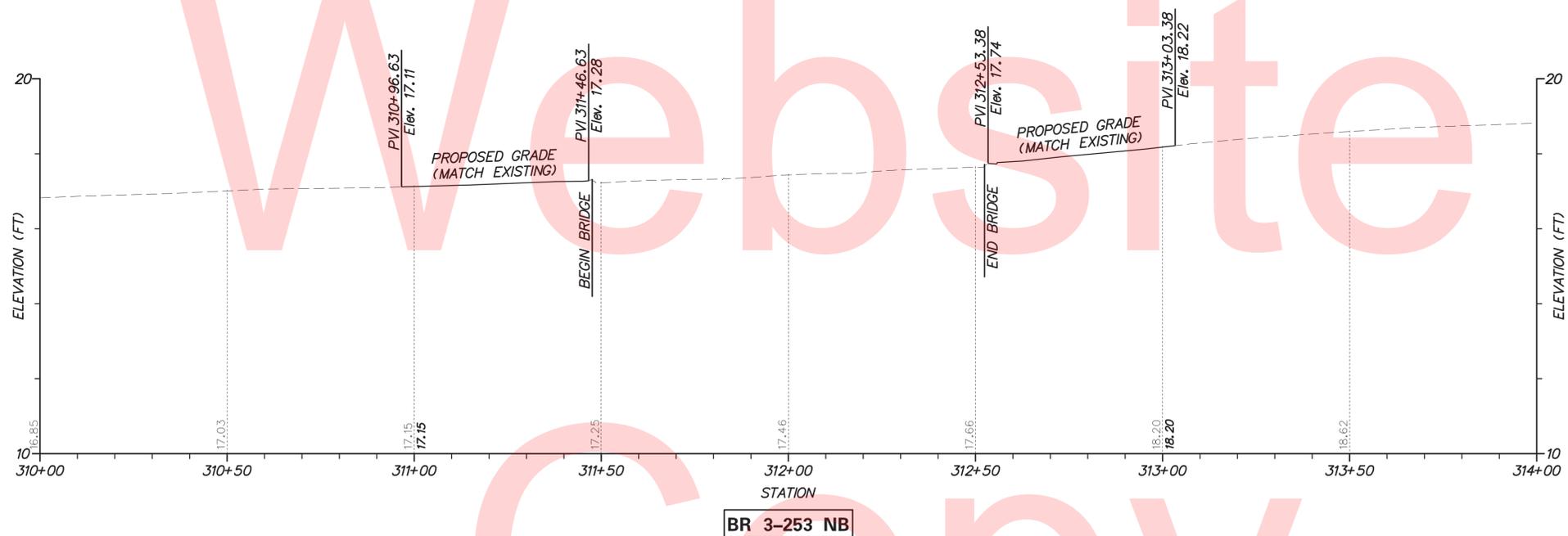
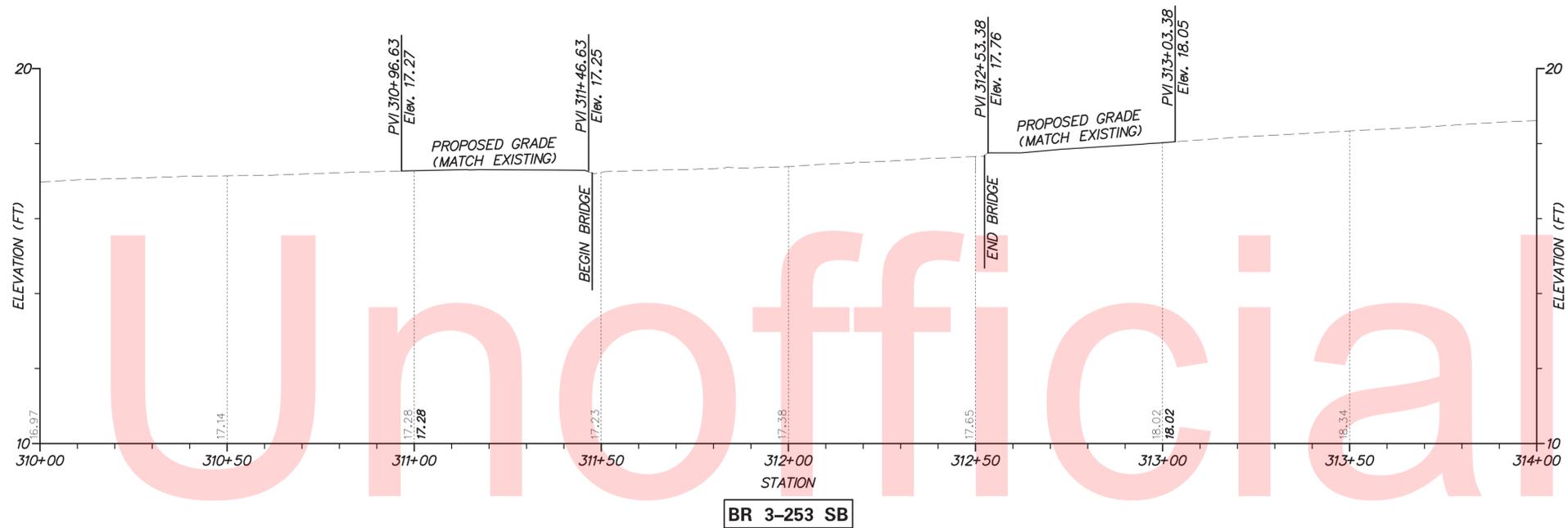


NOTES:

- ① THE EXISTING TIMBER WALKWAY, STAIRS, PUMP HOUSE, AND ATTACHED WATER LINES SHALL REMAIN IN PLACE DURING ALL CONSTRUCTION PHASES.
- ② THE CONCRETE APPROACH ROADWAY SHALL BE REMOVED 50' BEYOND EACH END OF THE NORTHBOUND AND SOUTHBOUND BRIDGES. AFTER REMOVAL OF THE P.C.C. MASONRY, THE EXISTING FILL MATERIAL SHALL BE REMOVED TO ACCOMMODATE THE PROPOSED ROADWAY SECTION. SEE SHEET NO. 4 FOR DETAILS. REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT AND STRUCTURAL EXCAVATION TO BE PAID UNDER ITEM NOS. 758000 AND 207000. REMOVAL OF HOT MIX PAVEMENT SHALL BE INCLUDED IN ITEM NO. 202000. ROADWAY MATERIALS TO BE PAID UNDER ITEM NOS. 302007, 401813, 401819 AND 401830. THE CLEARING AND GRUBBING NEEDED IN THE PROPOSED RIPRAP AREA ON THE NORTHEAST EMBANKMENT OF THE NORTHBOUND BRIDGE TO BE PAID UNDER ITEM NO. 201000. REFER TO SECTION A-A OF DELDOT CONSTRUCTION DETAIL, E-21 FOR CONSTRUCTION OF EMBANKMENT. R-4 RIPRAP, DELAWARE NO. 57 STONE AND GEOTEXTILE RIPRAP FOR THE EMBANKMENT AND STONE OUTLETS TO BE PAID UNDER ITEMS NOS. 712020, 302012 AND 713003. REFER TO DELDOT STANDARD CONSTRUCTION DETAIL, E-21 FOR CONSTRUCTION OF STONE OUTLETS.
- ③

LANDS N/F
WILLIAMS POND ASSOCIATES, INC.
T.P. 331-5.00-99.00
D.R. 693-197

LANDS N/F
SAIRMAN LLC
T.P. 331-5.00-65.00
D.R. 4344-145
P.B. 118-285



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ADDENDUMS / REVISIONS



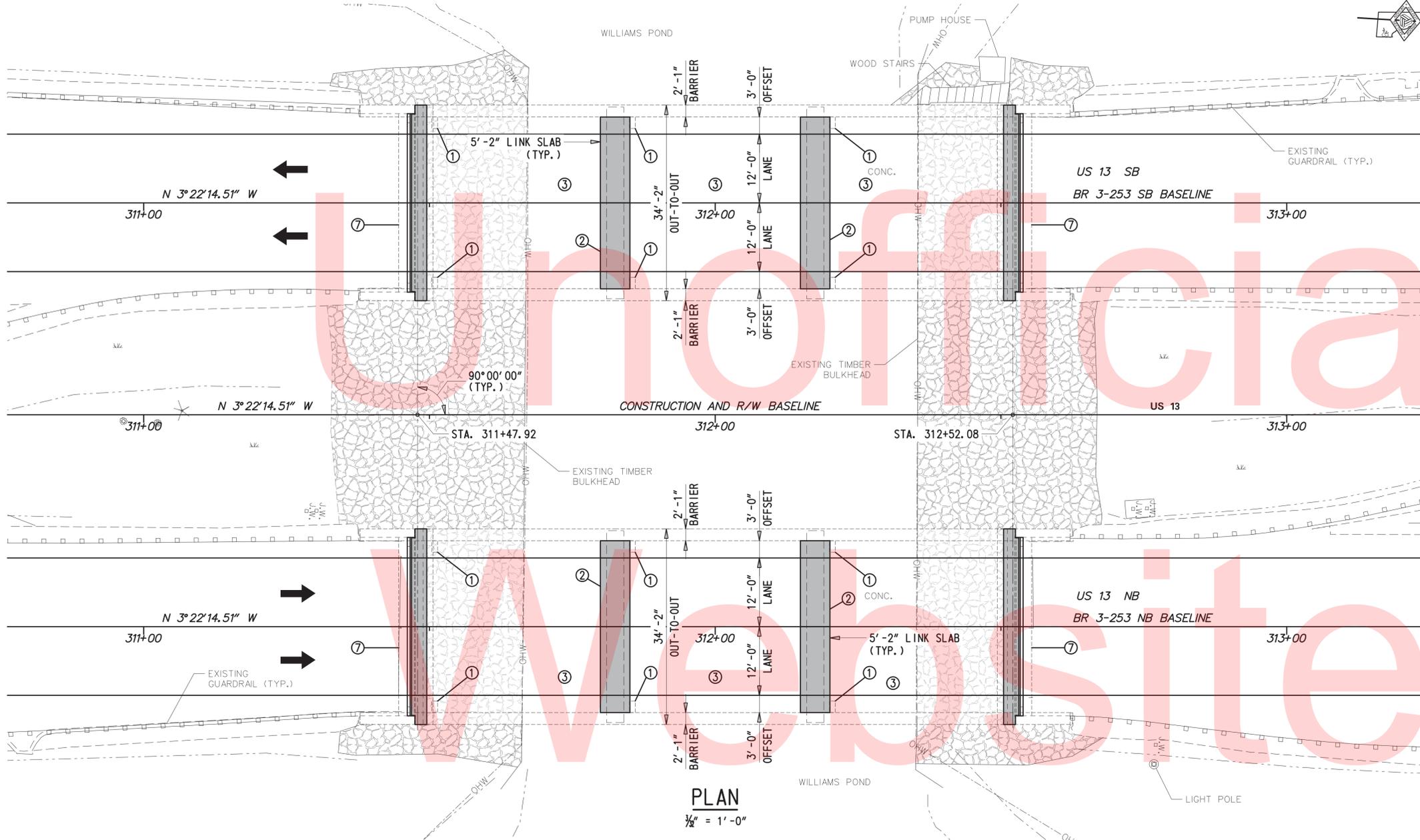
**BR 3-253 N&S ON US 13
OVER WILLIAMS POND**

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

PROFILE

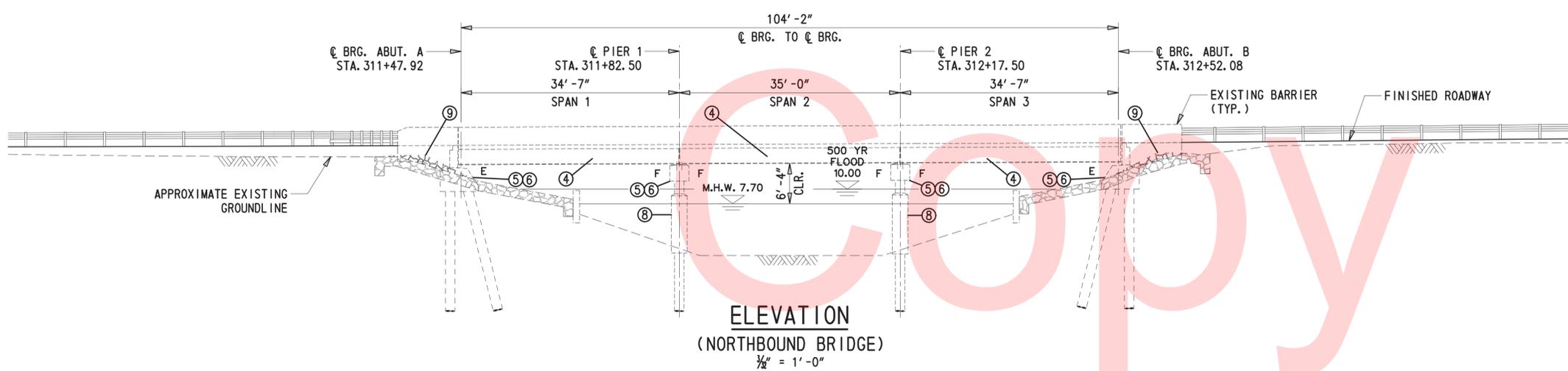


SHEET NO.	7
TOTAL SHTS.	49



PLAN

1/8" = 1'-0"



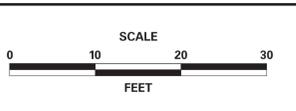
ELEVATION
(NORTHBOUND BRIDGE)

1/8" = 1'-0"

- REPAIRS:**
- ① THE EXISTING BRIDGE SCUPPERS AND DRAINAGE PIPES ON THE NORTHBOUND AND SOUTHBOUND BRIDGES SHALL REMAIN. BRIDGE SCUPPERS SHALL BE CLEARED OF ALL DEBRIS AND OBSTRUCTIONS. TO BE PAID UNDER ITEM NO. 605582.
 - ② THE EXISTING JOINTS AT PIER 1 AND 2 SHALL BE REMOVED IN FULL. THE CONCRETE DECK SHALL BE REMOVED TO A DEPTH OF 4" FOR THE LIMITS INDICATED ON SHEET NO. 25. ULTRA HIGH PERFORMANCE CONCRETE SHALL BE USED FOR THE LINK SLABS OVER THE PIERS. SEE SHEET NO. 25 FOR DETAILS. TO BE PAID UNDER ITEM NOS. 602580 AND 618516.
 - ③ A NEW WATERPROOFING CONCRETE DECK SEALER SHALL BE APPLIED OVER THE FULL AREA OF THE BRIDGE DECKS. TO BE PAID UNDER ITEM NO. 602546.
 - ④ THE NORTHBOUND AND SOUTHBOUND BRIDGE SUPERSTRUCTURES SHALL BE CLEANED AND PAINTED. TO BE PAID UNDER ITEM NOS. 605522 AND 605533. THE EXISTING PVC MATERIAL MAY BE REMOVED DURING PAINTING AND REINSTALLED WHEN PAINTING IS COMPLETED. THE COST IS INCIDENTAL TO ITEM NO. 605522.
 - ⑤ THE EXISTING PIER CAPS, ABUTMENTS AND ALL PEDESTALS THAT ARE SPALLED OR DETERIORATED SHALL BE REPAIRED AT THE DISCRETION OF THE FIELD ENGINEER. ALL DETERIORATED CONCRETE SHALL BE REMOVED TO A MINIMUM OF 2" AND FORMED UP TO THEIR ORIGINAL SHAPE. FOR LOCATIONS OF EXISTING DETERIORATED AREAS, SEE SHEET NOS. 11 TO 20. TO BE PAID FOR UNDER ITEM NOS. 602611, 602572 AND 602586. A SILICONE ACRYLIC CONCRETE SEALER SHALL BE APPLIED TO THE ALL EXPOSED FACES OF THE BARRIERS, PIER CAPS, ABUTMENTS AND WINGWALLS. TO BE PAID FOR UNDER ITEM NO. 602646.
 - ⑥ ROLLED CHANNEL SECTIONS SHALL BE INSTALLED AT BEARING LOCATIONS TO ASSIST IN THE JACKING SEQUENCE. SEE SHEET NO. 22 FOR DETAILS. INSTALLATION OF PERMANENT CHANNELS AT PIERS SHALL BE PAID UNDER ITEM NO. 605001. TEMPORARY CHANNELS AT ABUTMENTS SHALL BE INCIDENTAL TO ITEM NO. 605607. ALL EXISTING SOLE PLATES SHALL BE REMOVED. ALL EXISTING ANCHOR BOLTS SHALL BE CUT FLUSH WITH THE TOP OF THE PEDESTALS AND EXISTING SOLE PLATES SHALL BE CUT FROM THE BOTTOM OF THE STRINGERS AND GROUND SMOOTH. ANCHOR BOLTS SHALL BE REMOVED BY CORE-DRILLING TO THE DEPTH DIRECTED ON SHEET NO. 21. TO BE PAID UNDER ITEM NO. 605651. AT THE EXISTING BEARING LOCATIONS, PLACE PROPOSED FIXED AND EXPANSION BEARINGS. BEARINGS SHALL BE ANCHORED WITH 1" DIA. SWEDGED ANCHOR BOLTS. AT THE EXISTING LOCATION OF THE OLD SOLE PLATE, PLACE A NEW SOLE PLATE WELDED TO THE BOTTOM OF THE EXISTING BRIDGE STRINGER FLANGES. A STEEL-REINFORCED ELASTOMERIC BEARING PAD SHALL BE USED AT ALL LOCATIONS. SEE SHEET NO. 21 FOR DETAILS. TO BE PAID FOR UNDER ITEM NO. 605651. ALL ABUTMENT BEARINGS SHALL BE EXPANSION BEARINGS AND ALL PIER BEARINGS SHALL BE FIXED BEARINGS.
 - ⑦ THE ABUTMENT BACKWALL SHALL BE REMOVED FLUSH WITH THE TOP OF THE ABUTMENT CAP. A PORTION OF THE CONCRETE DECK AND BARRIER SHALL BE REMOVED TO THE LIMITS SHOWN ON SHEET NO. 24. CONSTRUCT A FULL-DEPTH P. C. C. MASONRY DIAPHRAGM AT EACH ABUTMENT. SEE SHEET NO. 24 FOR DETAILS. TO BE PAID FOR UNDER ITEM NO. 602017 AND 602013. AN APPROVED MEMBRANE WATERPROOFING SHALL BE BONDED ACROSS THE ABUTMENT CAP AND DIAPHRAGM. WATERPROOFING MEMBRANE SHALL BE IN ACCORDANCE WITH SPECIAL PROVISION 602616. TO BE PAID UNDER ITEM NO. 602616. A 1" THICK PREFORMED CELLULAR POLYSTYRENE SHALL BE PLACED BEHIND THE DIAPHRAGM AND ON TOP OF THE ABUTMENT CAP. PREFORMED CELLULAR POLYSTYRENE MUST MEET ASTM C578, TYPE 1, MATERIAL REQUIREMENTS EXCEPT THE MAXIMUM ALLOWABLE WATER ABSORPTION TO BE 2%. PAYMENT SHALL BE INCIDENTAL TO ITEM NO. 602013.
 - ⑧ THE PILE JACKETS SHALL BE REMOVED AT ALL LOCATIONS. TO BE PAID UNDER ITEM NO. 210000. CONCRETE ABOVE THE WATER LINE SHALL BE SOUNDED AND INSPECTED FOR DETERIORATION. ALL DETERIORATED CONCRETE AS DETERMINED BY THE ENGINEER SHALL BE REMOVED A MINIMUM OF 2". THE AREA SHALL BE CLEANED AND AN APPROVED BONDING AGENT AND P. C. C. MASONRY CLASS A APPLIED TO THE AREA. TO BE PAID UNDER ITEM NOS. 602572 AND 602586. TOP OF PILES TO BE CLEANED AND PAINTED. TO BE PAID UNDER ITEM NOS. 605522 AND 605533.
 - ⑨ WHERE UNDERMINING OF THE WINGWALLS IS PRESENT, BACKFILL WITH BORROW, TYPE C TO MATCH GRADE AND PLACE R4 RIPRAP UP TO 12" ABOVE THE BASE OF WINGWALL FOR PROTECTION. BACKFILL AND RIPRAP PROTECTION TO BE PAID UNDER ITEM NOS. 210000 AND 712020.

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ADDENDUMS / REVISIONS

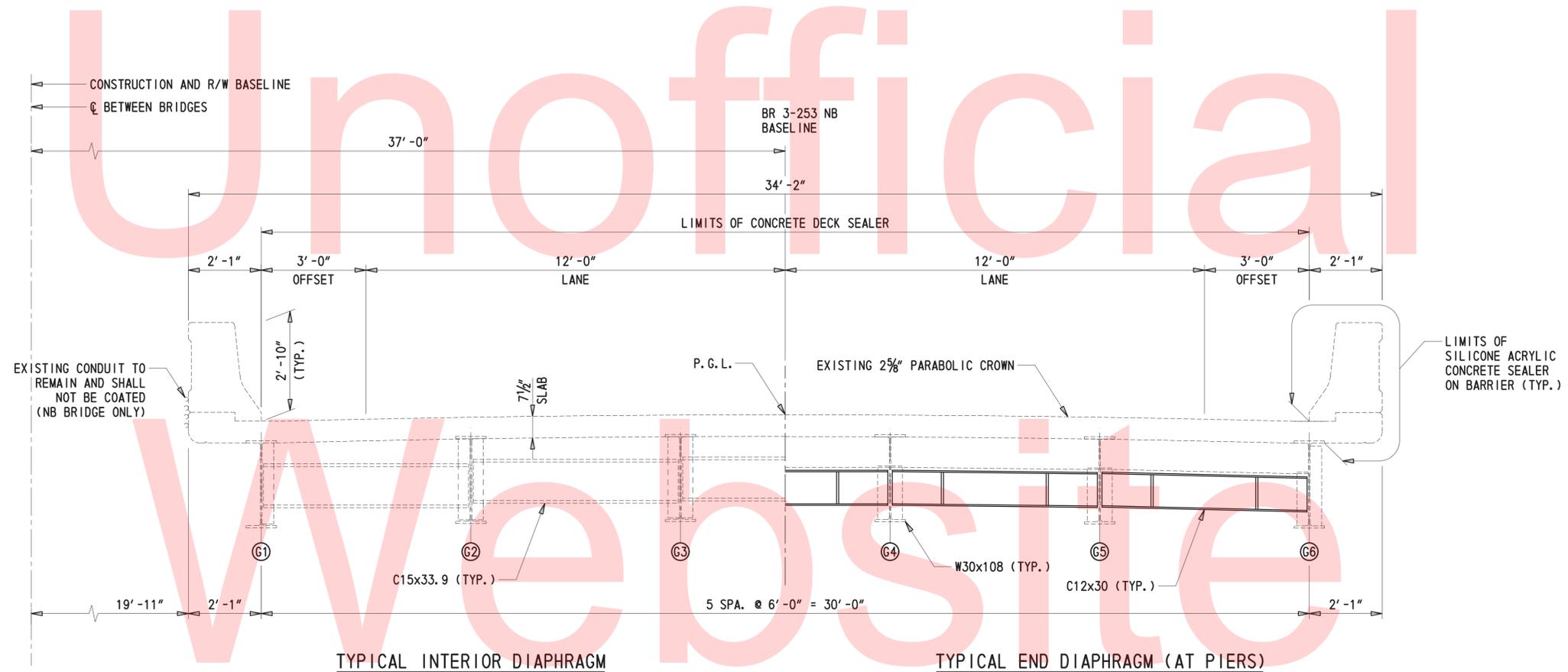


BR 3-253 N&S ON US 13
OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

GENERAL BRIDGE PLAN
AND ELEVATION

SHEET NO. 8
TOTAL SHTS. 49



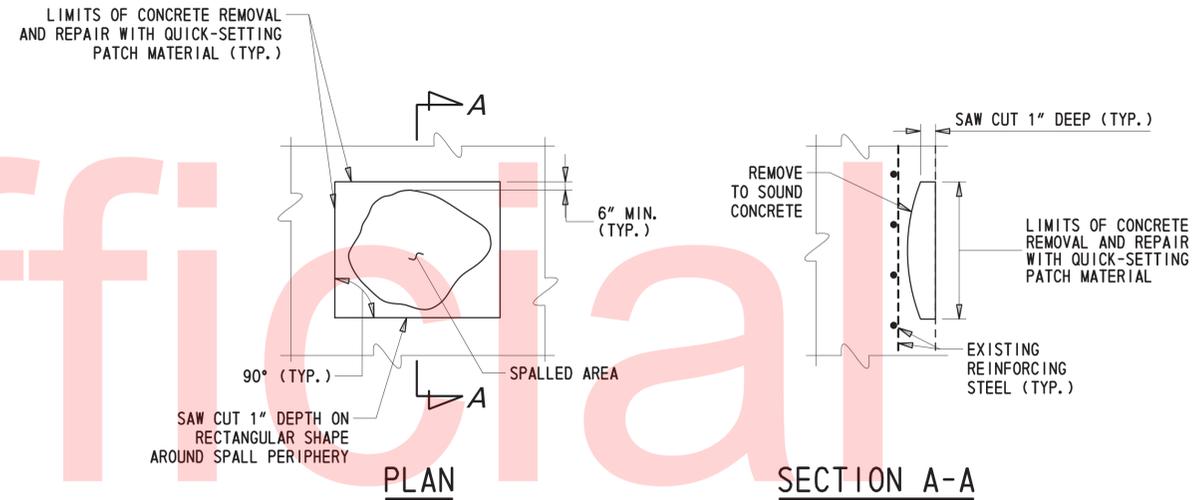
TYPICAL SECTION - NORTHBOUND BRIDGE (SOUTHBOUND SIMILAR)

1/2" = 1' -0"

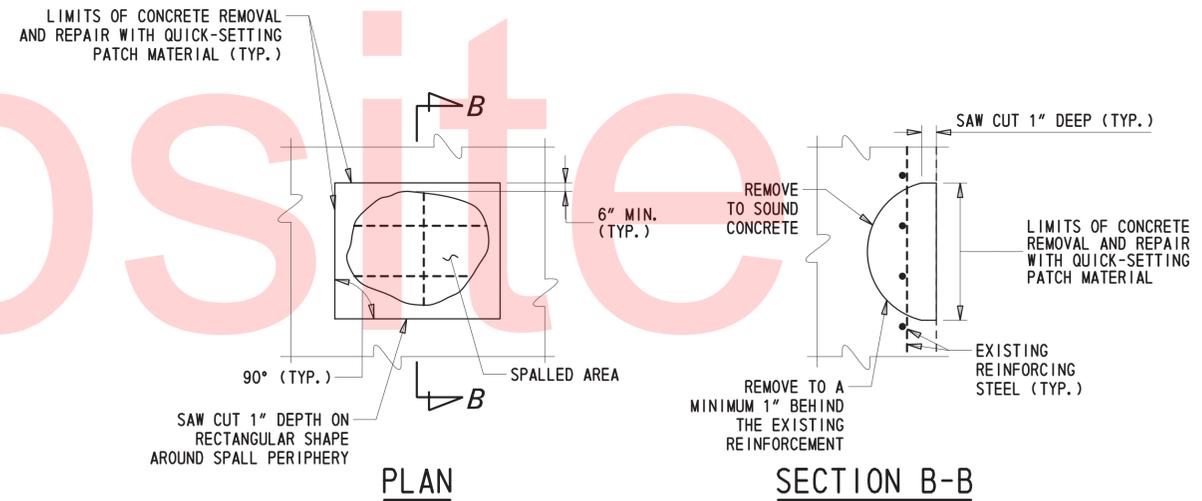
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MISCELLANEOUS

13. DESIGN SPECIFICATIONS:
 (A) 2015 DELDOT BRIDGE DESIGN MANUAL
 (B) 2014 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION, CUSTOMARY U.S. UNITS.
 (C) PROVIDE MATERIALS AND PERFORM WORK IN ACCORDANCE WITH THE DELDOT STANDARD SPECIFICATIONS.
14. LOADING:
 - DEAD LOADS INCLUDE BARRIER (APPLIED AS A SUPERIMPOSED DEAD LOAD AND DISTRIBUTED EQUALLY TO ALL GIRDERS).
 - DESIGN LIVE LOADS INCLUDE HL-93 LOADING.
 - FATIGUE DESIGN IS BASED ON THE FOLLOWING ONE DIRECTIONAL TRAFFIC VOLUMES: ADTT = 3050.
 - LIVE LOAD DISTRIBUTION TO THE GIRDERS IS BASED ON THE AASHTO SIMPLIFIED METHOD.
 - THERMAL LOADS AND MOVEMENTS ARE BASED ON THE MODERATE TEMPERATURE RANGE AS STIPULATED IN THE AASHTO LRFD DESIGN SPECIFICATIONS AS 0°F TO 120°F FOR STEEL AND 10°F TO 90°F FOR CONCRETE. THE NORMAL TEMPERATURE SHALL BE CONSIDERED TO BE 68°F.
 - LIVE LOAD DEFLECTION SHALL BE LIMITED TO L/800.
 - FOR SEISMIC LOADS, CONSIDER SEISMIC PERFORMANCE ZONE I, WITH A SITE CLASS = E AND OPERATIONAL CATEGORY = ESSENTIAL.
 - FOR REINFORCEMENT DISTRIBUTION REQUIREMENTS, CONSIDER CLASS 2 EXPOSURE CRITERIA FOR DECKS.
15. EXISTING CONDITIONS:
 - ALL EXISTING DIMENSIONS SHOWN ARE BASED ON THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING DIMENSIONS, GEOMETRY, AND ELEVATIONS AS NECESSARY PRIOR TO ORDERING ANY MATERIALS AND COMMENCING CONSTRUCTION TO ENSURE PROPER FIT OF THE PROPOSED CONSTRUCTION. PAYMENT SHALL BE INCIDENTAL TO ITEM *763501 - CONSTRUCTION ENGINEERING.
 - THE CONTRACTOR SHALL NOT CONSIDER ANY OF THE DATA ON THE EXISTING STRUCTURE SUPPLIED IN THE ORIGINAL DESIGN DRAWINGS OR MADE AVAILABLE BY THE DEPARTMENT OR ITS AUTHORIZED AGENTS AS POSITIVE REPRESENTATIONS OF ANY OF THE CONDITIONS THAT WILL BE ENCOUNTERED IN THE FIELD.
 - THE INFORMATION SHOWN ON THE PLANS FOR THE EXISTING BRIDGES ARE NOT PART OF THE PLANS, PROPOSAL, OR CONTRACT AND IS NOT TO BE CONSIDERED A BASIS FOR COMPUTATION OF UNIT PRICES USED FOR BIDDING PURPOSES. THERE IS NO EXPRESSED OR IMPLIED AGREEMENT THAT INFORMATION IS CORRECTLY SHOWN. THE BIDDER IS NOT TO RELY ON THIS INFORMATION, BUT IS TO ASSUME THE POSSIBILITY THAT CONDITIONS AFFECTING THE COST AND/OR QUANTITIES OF THE WORK TO BE PERFORMED MAY DIFFER FROM THOSE INDICATED.
16. CONTINGENT QUANTITIES:
 THESE CONTRACT DRAWINGS HAVE BEEN PREPARED BASED ON ORIGINAL CONTRACT PLANS AND FIELD INSPECTION NOTES. ACTUAL CONDITIONS MAY REQUIRE MODIFICATION IN CONSTRUCTION DETAILS AND WORK QUANTITIES. ALL DIMENSIONS AND DETAILS SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ORDERING ANY MATERIALS. THE ESTIMATED QUANTITIES FOR THE FOLLOWING PAY ITEMS INCLUDE CONTINGENCY FACTORS TO ACCOUNT FOR THE ANTICIPATED VARIATIONS IN THE ACTUAL QUANTITY.
 - ITEM *602572 - REPAIRING EXISTING P.C.C. STRUCTURES, 50%
 - ITEM *602586 - REHABILITATION OF CONCRETE STRUCTURES, 50%
 - ITEM *602611 - REPAIR OF CONCRETE STRUCTURES BY EPOXY INJECTION, 50%
17. UTILITIES:
 SEE UTILITY STATEMENT FOR FURTHER INFORMATION ON UTILITY COORDINATION.
18. RIGHT-OF-WAY:
 ALL WORK SHALL BE PERFORMED WITHIN THE EXISTING RIGHT-OF-WAY.
19. LOAD RATING:
 THIS PROJECT DOES NOT CHANGE THE LOAD RATING OF THE BRIDGE. A CURRENT LOAD RATING OF THE BRIDGE IS ON FILE AND MAY BE OBTAINED FROM THE BRIDGE MANAGEMENT SECTION.
20. ENVIRONMENTAL IMPACTS:
 NO ENVIRONMENTAL PERMITS ARE REQUIRED FOR THIS WORK PROVIDED NO JURISDICTIONAL WETLANDS OR WATERS ARE IMPACTED. IF THERE IS ANY QUESTION AS TO WHETHER OR NOT A WATER OR WETLANDS IS JURISDICTIONAL, CONTACT THE DELDOT ENVIRONMENTAL STUDIES SECTION AT 302-760-2264.
- MIGRATORY BIRDS - APRIL 15 TO AUGUST 1 (INCLUSIVE) EITHER BEGIN WORK ON THE UNDERSIDE OF THE STRUCTURE PRIOR TO 4/15 (TO PREVENT BIRDS FROM NESTING) OR WAIT UNTIL AFTER 8/1 (AFTER HATCHLINGS HAVE LEFT) TO BEGIN WORK TO UNDERSIDE. IF NEITHER OF THESE OPTIONS IS PRACTICABLE, THEN DETERRENT NETTING OR SIMILAR DEVICE SHALL BE INSTALLED PRIOR TO 4/15.
21. CONSTRUCTION TIMING:
 FOR THE DETERMINATION OF CONSTRUCTION TIME, IT WAS ASSUMED THAT THE CONTRACTOR WOULD SUPPLY MULTIPLE WORK CREWS, WORKING SIX DAYS PER WEEK FOR EXTENDED HOURS.



SHALLOW SPALL REPAIR



DEEP SPALL REPAIR

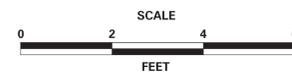
SHALLOW SPALL REPAIR NOTES

- SHALLOW SPALLS ARE DEFINED AS PATCHES THAT DO NOT EXTEND BELOW THE TOP MAT OF REBAR.
- ALL WORK INVOLVING METHODS OF CONCRETE REMOVAL; SAWCUTTING; CLEANING OF CONCRETE SURFACE; SURFACE PREPARATION AND CONCRETE PLACEMENT SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS OF THE STANDARD SPECIFICATIONS. PAYMENT INCIDENTAL TO 602572 - REPAIRING EXISTING P.C.C. STRUCTURES.

DEEP SPALL REPAIR NOTES

- DEEP SPALLS ARE DEFINED AS PATCHES THAT EXTEND TO, OR BELOW THE TOP MAT OF REINFORCEMENT.
- ALL WORK INVOLVING METHODS OF CONCRETE REMOVAL; SAWCUTTING; CLEANING OF CONCRETE SURFACE AND EXISTING REINFORCEMENT; REPAIRING OR REPLACING DAMAGED REINFORCEMENT AS RESULT OF CONSTRUCTION ACTIVITIES OR SECTION LOSS; PRESENCE OF CONTRACTION OR EXPANSION JOINTS; SURFACE PREPARATION AND CONCRETE PLACEMENT SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS OF THE STANDARD SPECIFICATIONS. PAYMENT INCIDENTAL TO 602586 - REHABILITATION OF CONCRETE STRUCTURES.

ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13
OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

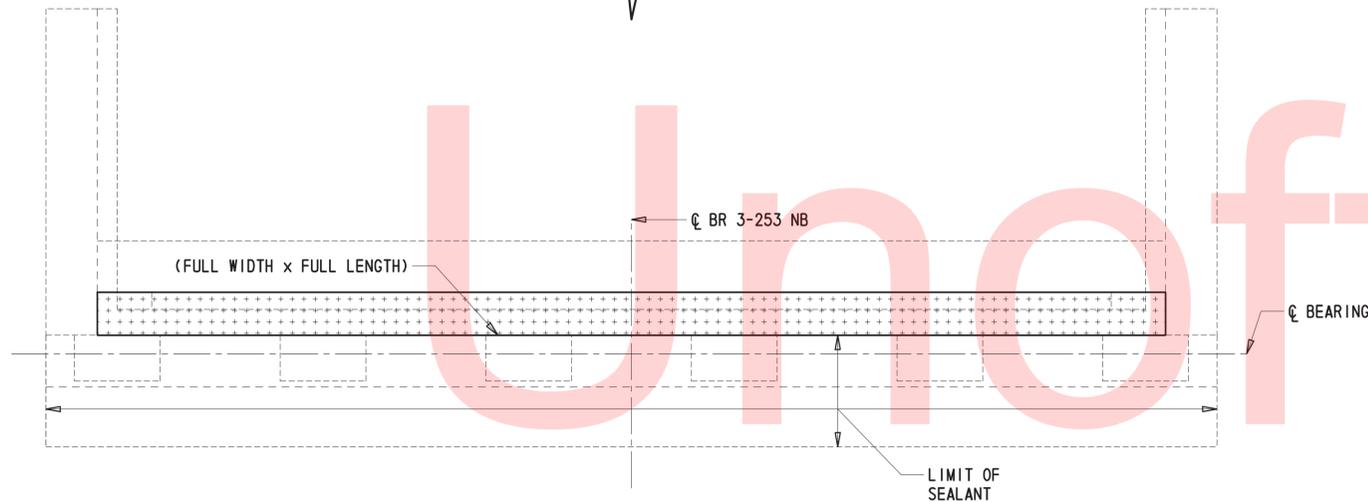
MISCELLANEOUS DETAILS

SHEET NO.	10
TOTAL SHTS.	49



CONCRETE REPAIR QUANTITIES		
ABUTMENT A		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	340 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	0 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

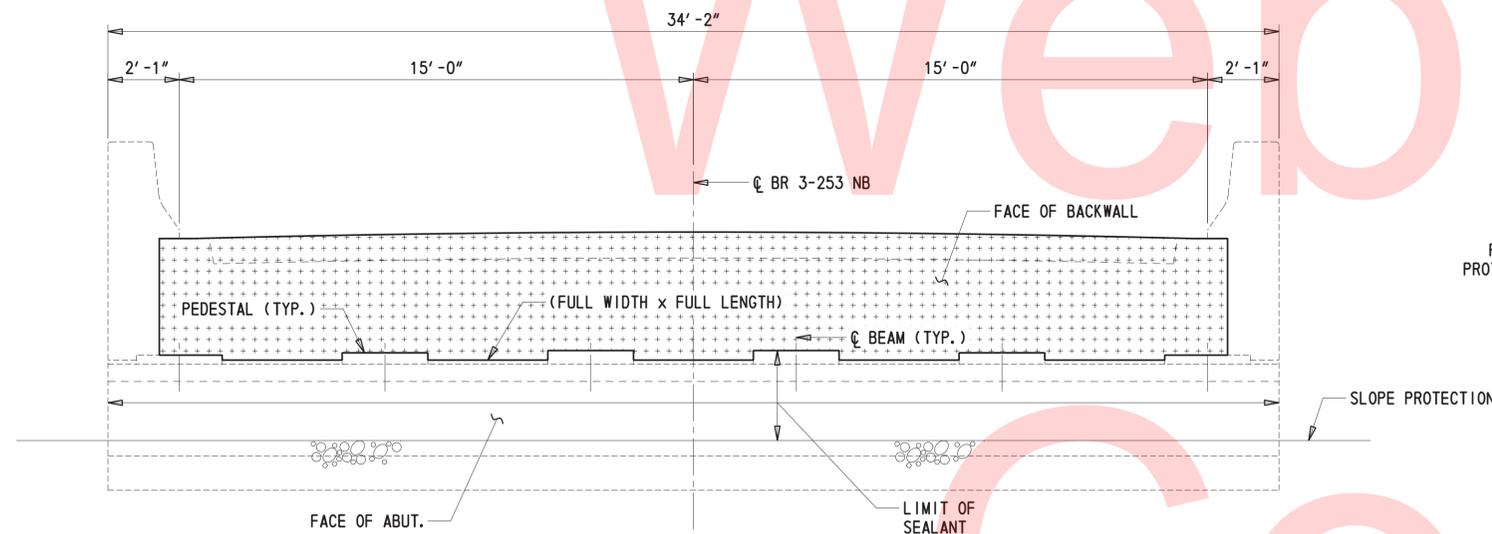
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



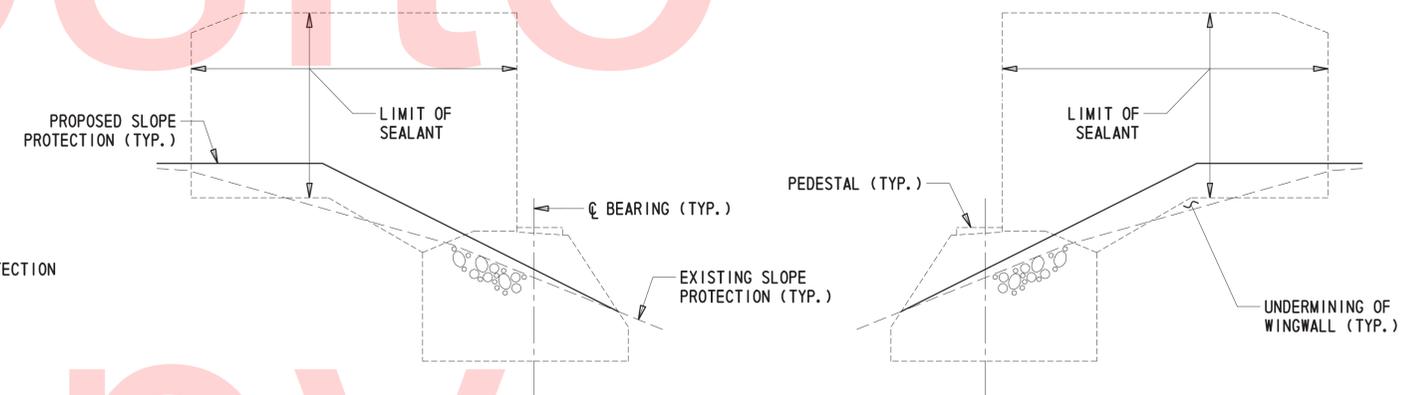
PLAN - ABUTMENT A, NORTHBOUND
3/8" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
5. EXCAVATE AS REQUIRED TO LOCATE THE EXTENT OF THE CRACK OR SPALL. EXCAVATION, REPLACEMENT AND COMPACTION OF SOIL IS INCIDENTAL TO THE ASSOCIATED REPAIR ITEMS: 602572, 602586 AND 602611.
6. CONTRACTOR TO REMOVE DEBRIS FROM ABUTMENT SEAT PRIOR TO APPLICATION OF SEALANT. PAYMENT FOR REMOVAL OF DEBRIS SHALL BE PAID FOR UNDER ITEM NO. 602646.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.
8. WHERE UNDERMINING OF THE WINGWALLS IS PRESENT, BACKFILL WITH BORROW, TYPE C TO MATCH GRADE. PLACE R4 RIPRAP TO A MINIMUM OF 12" ABOVE THE BASE OF WINGWALL FOR PROTECTION AT ALL LOCATIONS. BACKFILL AND RIPRAP PROTECTION TO BE PAID UNDER ITEM NOS. 210000 AND 712020.
9. REMOVAL OF BACKWALL TO BE PAID UNDER ITEM NO. 602580.



ELEVATION - ABUTMENT A, NORTHBOUND
3/8" = 1'-0"



ELEVATION - WINGWALL A
3/8" = 1'-0"

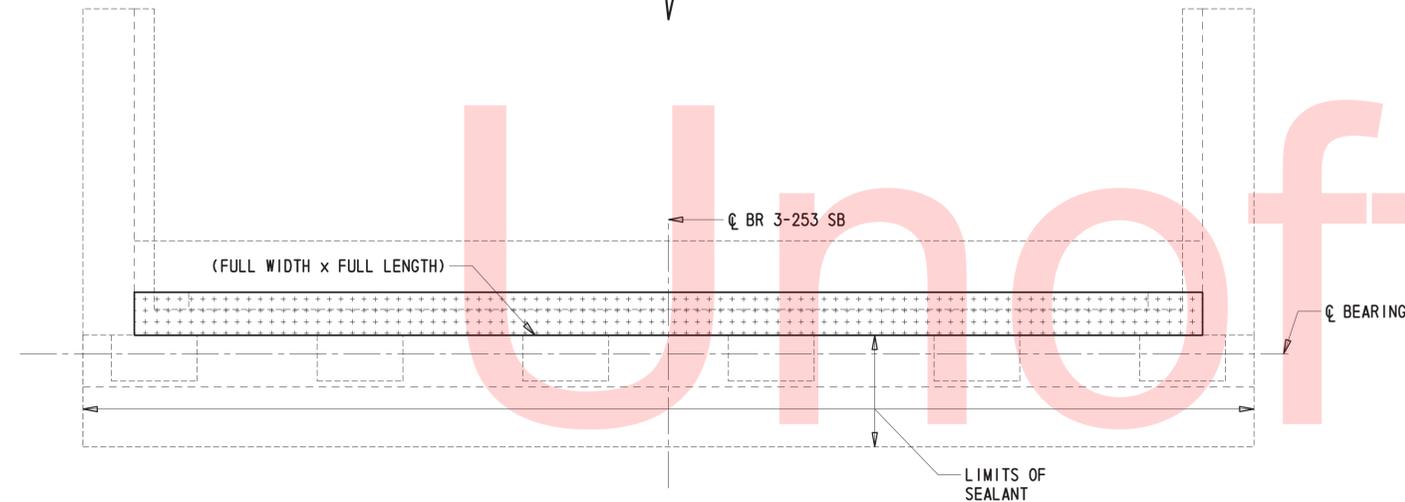
ELEVATION - WINGWALL B
3/8" = 1'-0"

3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans\AB02S-NB_BR3-253.dgn



CONCRETE REPAIR QUANTITIES		
ABUTMENT A		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	340 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	0 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

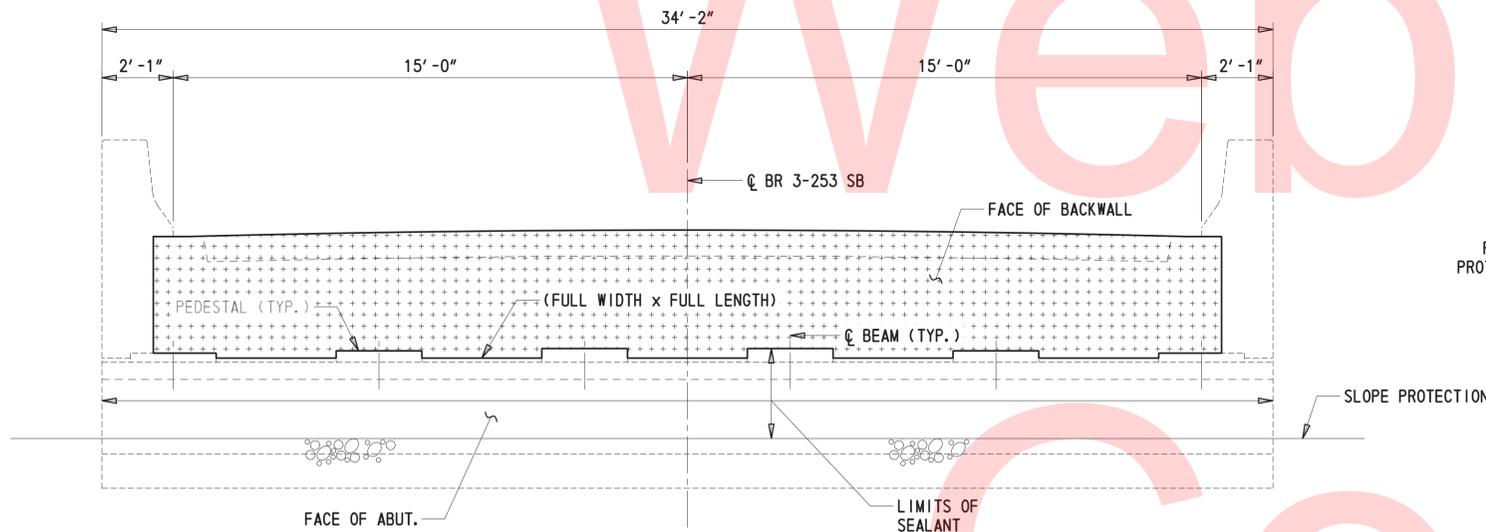
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



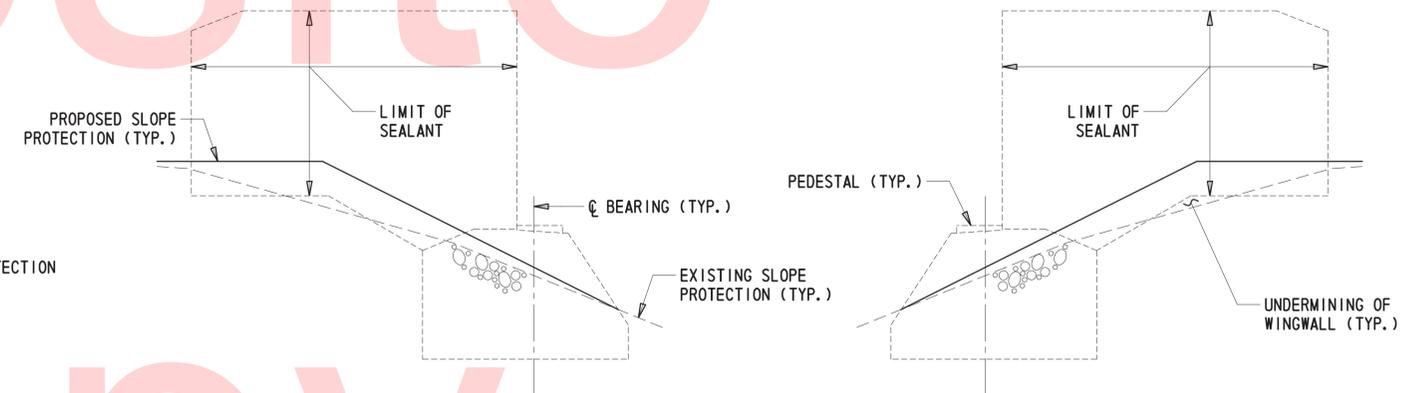
PLAN - ABUTMENT A, SOUTHBOUND
3/8" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
5. EXCAVATE AS REQUIRED TO LOCATE THE EXTENT OF THE CRACK OR SPALL. EXCAVATION, REPLACEMENT AND COMPACTION OF SOIL IS INCIDENTAL TO THE ASSOCIATED REPAIR ITEMS: 602572, 602586 AND 602611.
6. CONTRACTOR TO REMOVE DEBRIS FROM ABUTMENT SEAT PRIOR TO APPLICATION OF SEALANT. PAYMENT FOR REMOVAL OF DEBRIS SHALL BE PAID FOR UNDER ITEM NO. 602646.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.
8. WHERE UNDERMINING OF THE WINGWALLS IS PRESENT, BACKFILL WITH BORROW, TYPE C TO MATCH GRADE. PLACE R4 RIPRAP TO A MINIMUM OF 12" ABOVE THE BASE OF WINGWALL FOR PROTECTION AT ALL LOCATIONS. BACKFILL AND RIPRAP PROTECTION TO BE PAID UNDER ITEM NOS. 210000 AND 712020.
9. REMOVAL OF BACKWALL TO BE PAID UNDER ITEM NO. 602580.



ELEVATION - ABUTMENT A, SOUTHBOUND
3/8" = 1'-0"



ELEVATION - WINGWALL A
3/8" = 1'-0"

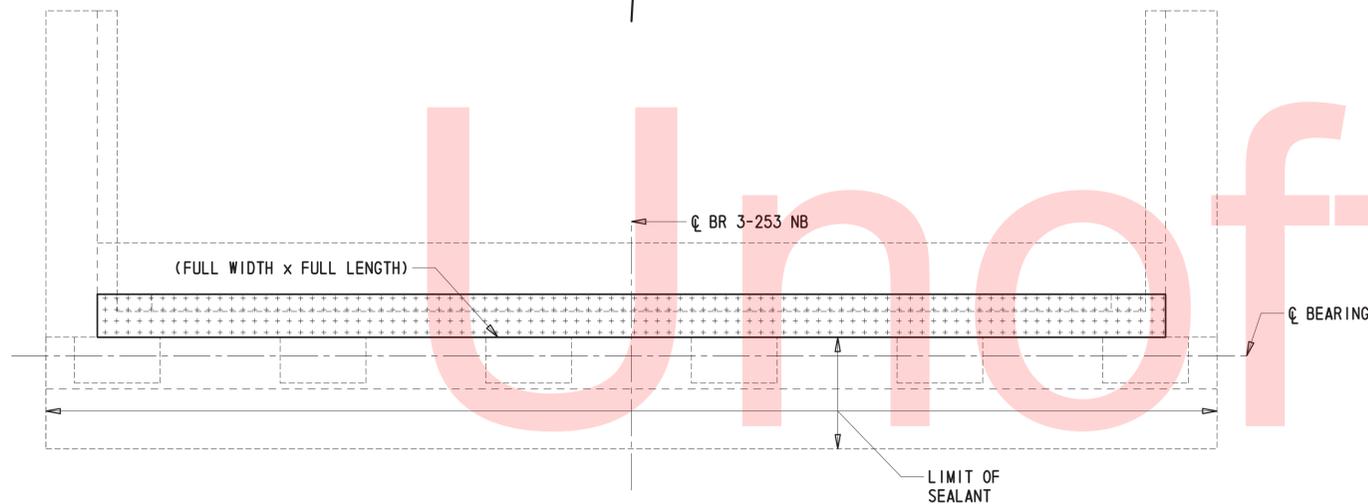
ELEVATION - WINGWALL B
3/8" = 1'-0"

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CONCRETE REPAIR QUANTITIES		
ABUTMENT B		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	340 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	226 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

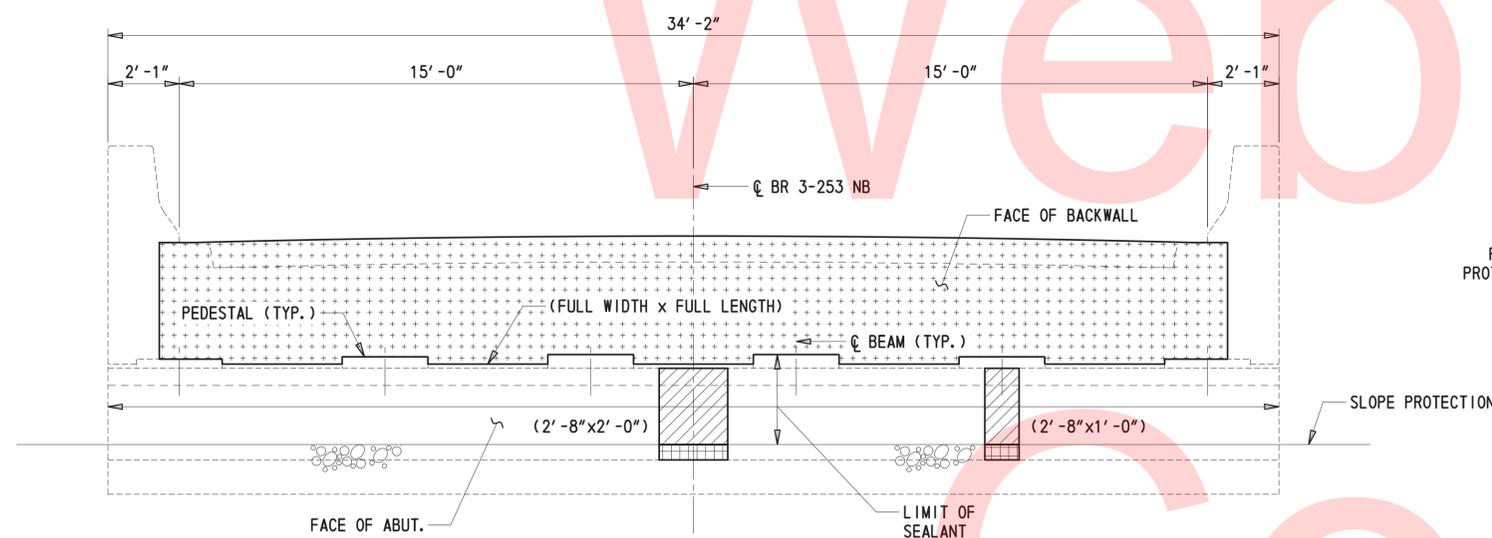
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



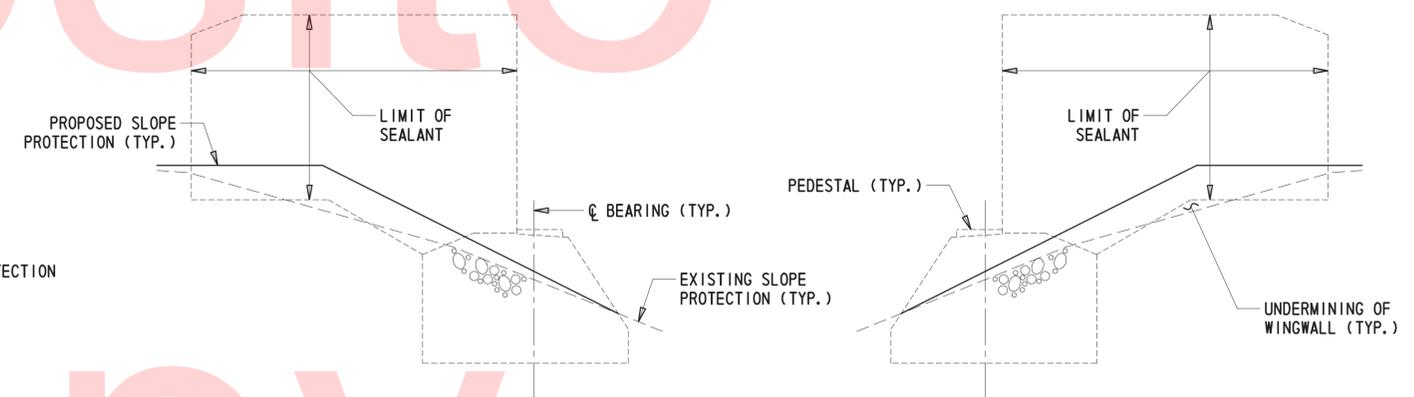
PLAN - ABUTMENT B, NORTHBOUND
3/8" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
5. EXCAVATE AS REQUIRED TO LOCATE THE EXTENT OF THE CRACK OR SPALL. EXCAVATION, REPLACEMENT AND COMPACTION OF SOIL IS INCIDENTAL TO THE ASSOCIATED REPAIR ITEMS: 602572, 602586 AND 602611.
6. CONTRACTOR TO REMOVE DEBRIS FROM ABUTMENT SEAT PRIOR TO APPLICATION OF SEALANT. PAYMENT FOR REMOVAL OF DEBRIS SHALL BE PAID FOR UNDER ITEM NO. 602646.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.
8. WHERE UNDERMINING OF THE WINGWALLS IS PRESENT, BACKFILL WITH BORROW, TYPE C TO MATCH GRADE. PLACE R4 RIPRAP TO A MINIMUM OF 12" ABOVE THE BASE OF WINGWALL FOR PROTECTION AT ALL LOCATIONS. BACKFILL AND RIPRAP PROTECTION TO BE PAID UNDER ITEM NOS. 210000 AND 712020.
9. REMOVAL OF BACKWALL TO BE PAID UNDER ITEM NO. 602580.



ELEVATION - ABUTMENT B, NORTHBOUND
3/8" = 1'-0"



ELEVATION - WINGWALL C
3/8" = 1'-0"

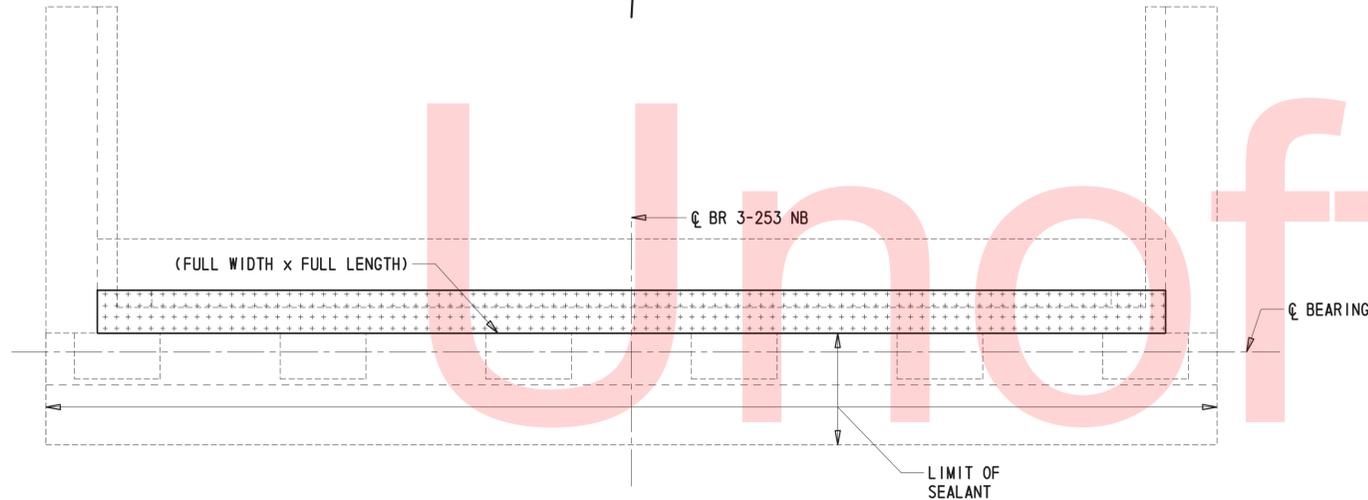
ELEVATION - WINGWALL D
3/8" = 1'-0"

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CONCRETE REPAIR QUANTITIES		
ABUTMENT B		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	340 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	0 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

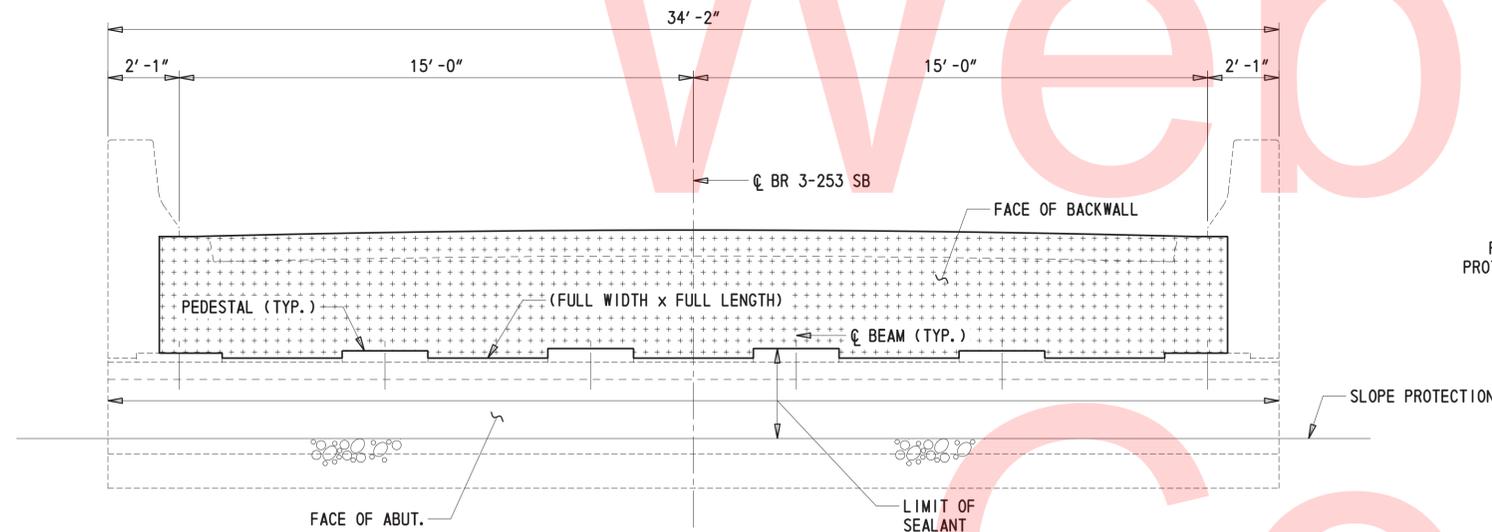
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



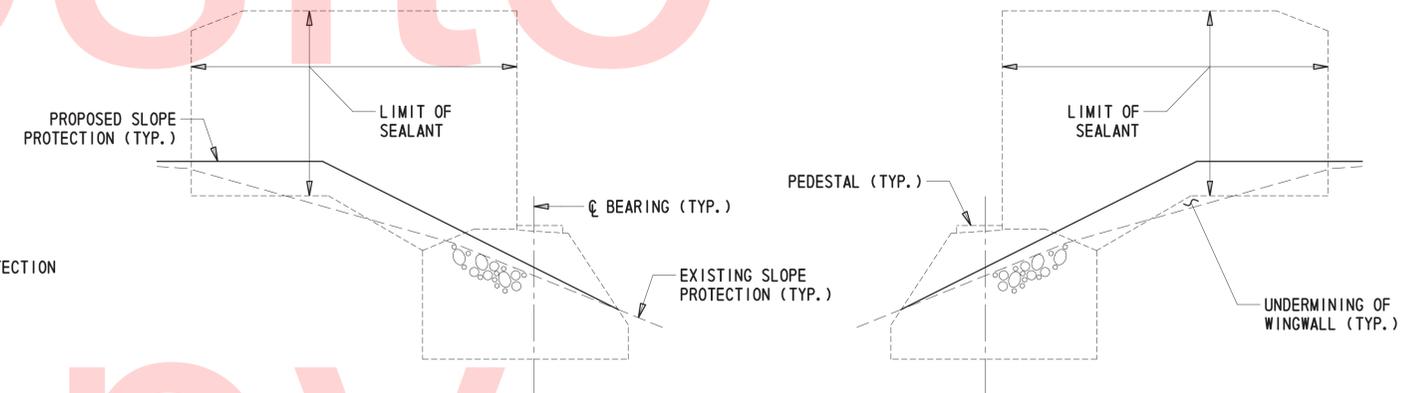
PLAN - ABUTMENT B, SOUTHBOUND
3/8" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
5. EXCAVATE AS REQUIRED TO LOCATE THE EXTENT OF THE CRACK OR SPALL. EXCAVATION, REPLACEMENT AND COMPACTION OF SOIL IS INCIDENTAL TO THE ASSOCIATED REPAIR ITEMS: 602572, 602586 AND 602611.
6. CONTRACTOR TO REMOVE DEBRIS FROM ABUTMENT SEAT PRIOR TO APPLICATION OF SEALANT. PAYMENT FOR REMOVAL OF DEBRIS SHALL BE PAID FOR UNDER ITEM NO. 602646.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.
8. WHERE UNDERMINING OF THE WINGWALLS IS PRESENT, BACKFILL WITH BORROW, TYPE C TO MATCH GRADE. PLACE R4 RIPRAP TO A MINIMUM OF 12" ABOVE THE BASE OF WINGWALL FOR PROTECTION AT ALL LOCATIONS. BACKFILL AND RIPRAP PROTECTION TO BE PAID UNDER ITEM NOS. 210000 AND 712020.
9. REMOVAL OF BACKWALL TO BE PAID UNDER ITEM NO. 602580.



ELEVATION - ABUTMENT B, SOUTHBOUND
3/8" = 1'-0"



ELEVATION - WINGWALL C
3/8" = 1'-0"

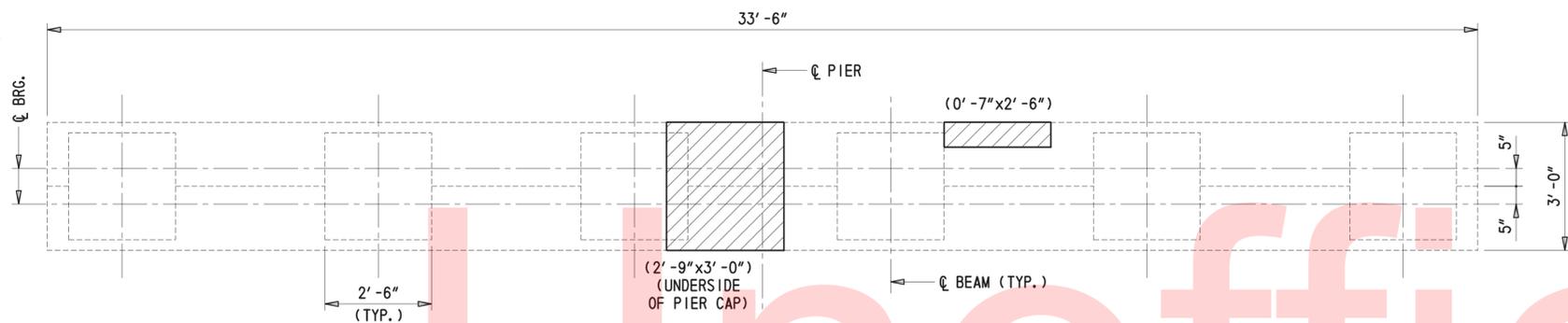
ELEVATION - WINGWALL D
3/8" = 1'-0"

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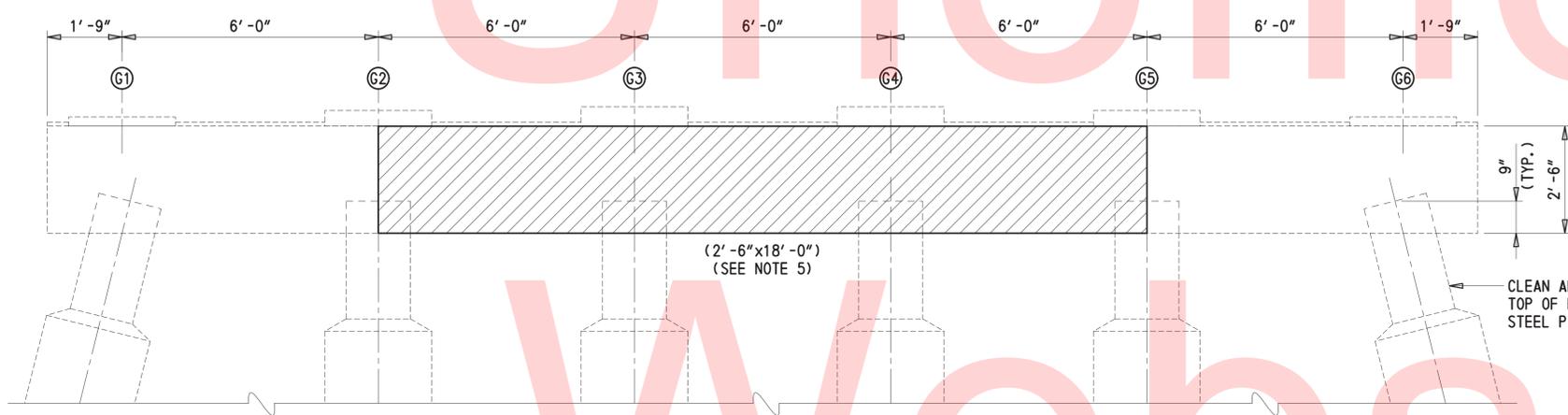


CONCRETE REPAIR QUANTITIES		
PIER 1		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	384 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	1467 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

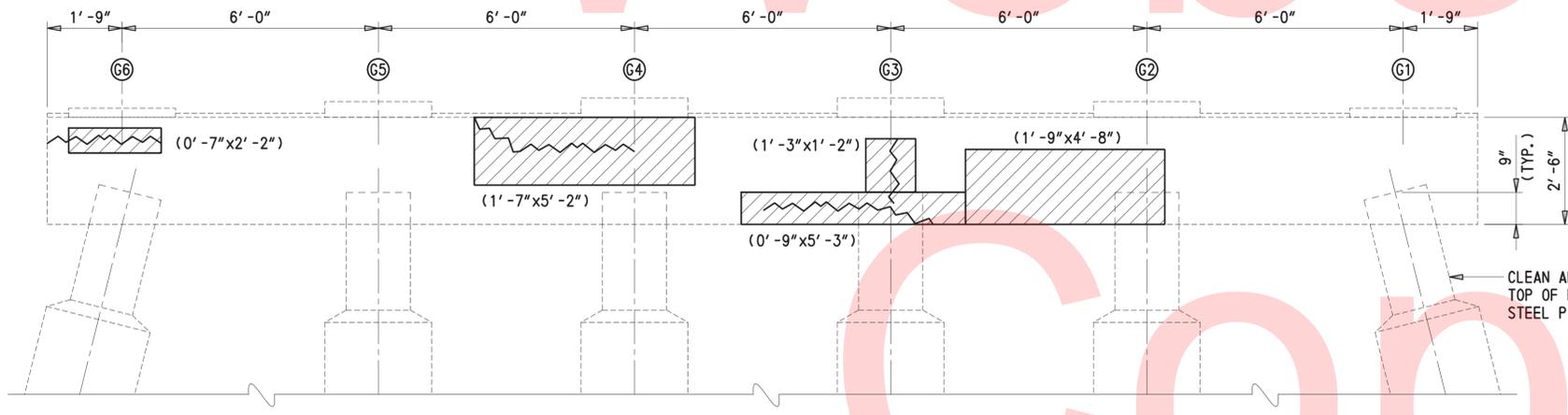
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



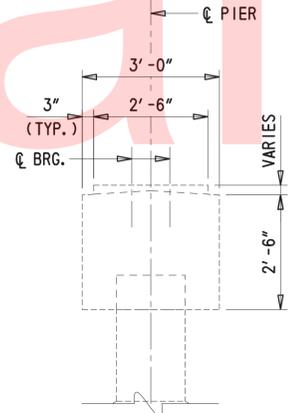
PLAN
1/2" = 1'-0"



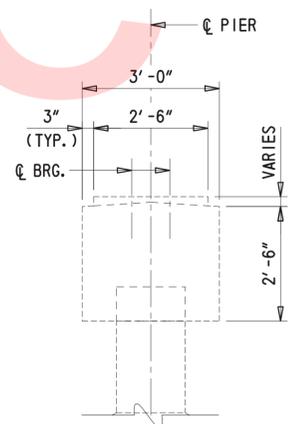
**ELEVATION
(LOOKING NORTH)**
1/2" = 1'-0"



**ELEVATION
(LOOKING SOUTH)**
1/2" = 1'-0"



**END VIEW
(LOOKING WEST)**
1/2" = 1'-0"

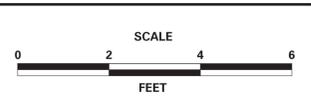


**END VIEW
(LOOKING EAST)**
1/2" = 1'-0"

- NOTES:**
1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
 2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
 3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
 4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
 5. PREVIOUSLY NOTED CRACKS AND DELAMINATIONS COULD NOT BE VERIFIED DUE TO ACCESS ISSUES AT TIME OF INSPECTION.
 6. SEALANT TO BE APPLIED TO ALL PIER FACES AFTER REPAIRS HAVE BEEN COMPLETED.
 7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.

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ADDENDUMS / REVISIONS

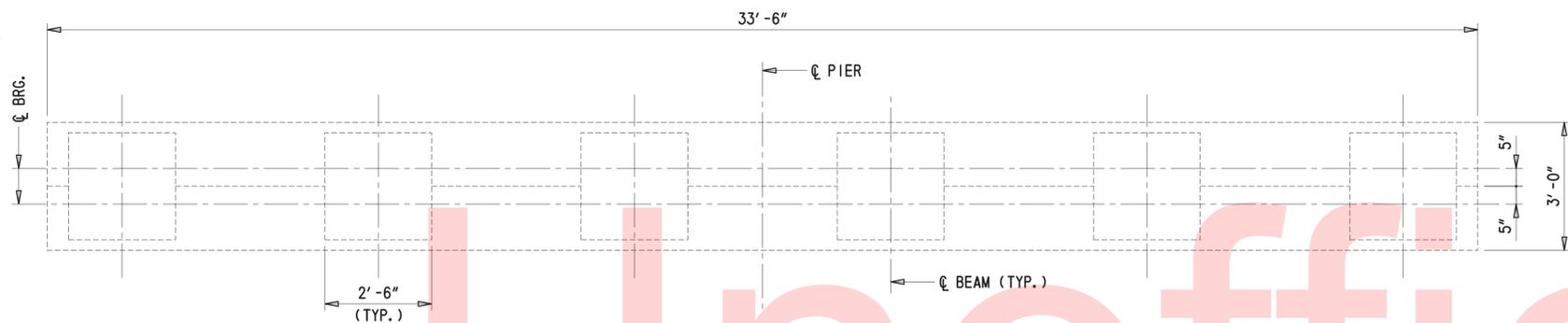


CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		



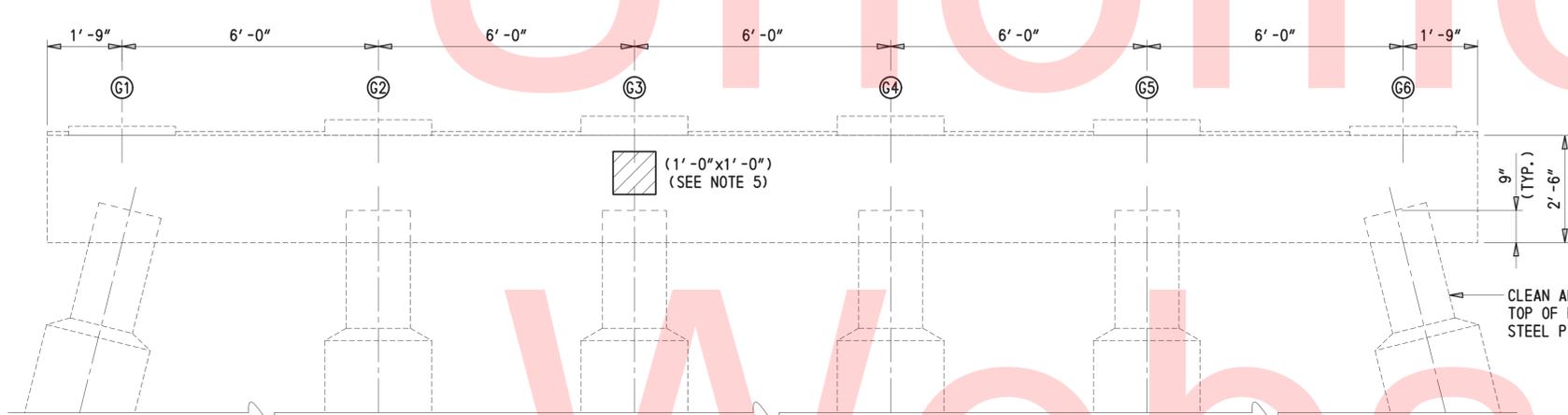
CONCRETE REPAIR QUANTITIES		
PIER 1		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	384 SF	602646
CRACK REPAIR	3 LF	602611
REPAIR OF EXISTING P. C. C.	113 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



PLAN

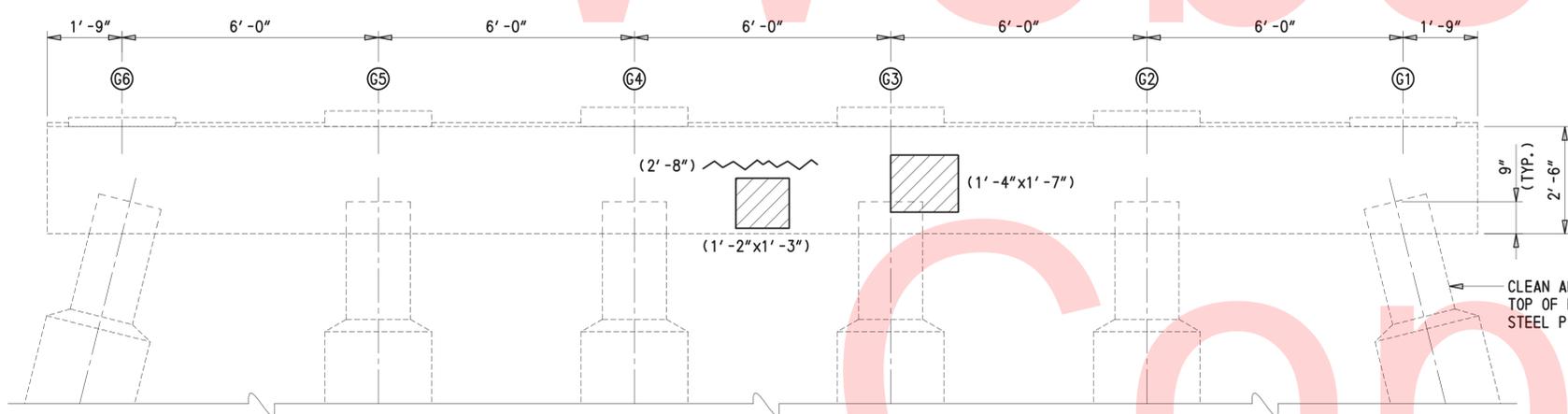
1/2" = 1'-0"



ELEVATION

(LOOKING NORTH)

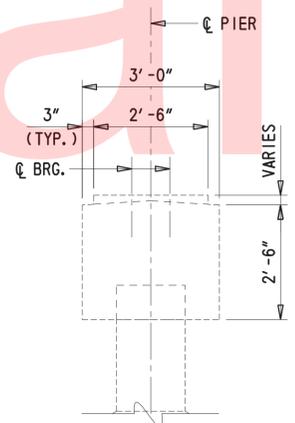
1/2" = 1'-0"



ELEVATION

(LOOKING SOUTH)

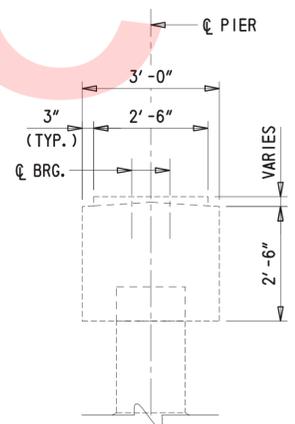
1/2" = 1'-0"



END VIEW

(LOOKING WEST)

1/2" = 1'-0"



END VIEW

(LOOKING EAST)

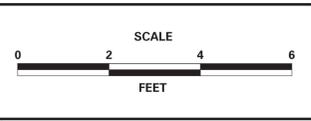
1/2" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
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5. PREVIOUSLY NOTED CRACKS AND DELAMINATIONS COULD NOT BE VERIFIED DUE TO ACCESS ISSUES AT TIME OF INSPECTION.
6. SEALANT TO BE APPLIED TO ALL PIER FACES AFTER REPAIRS HAVE BEEN COMPLETED.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.

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ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

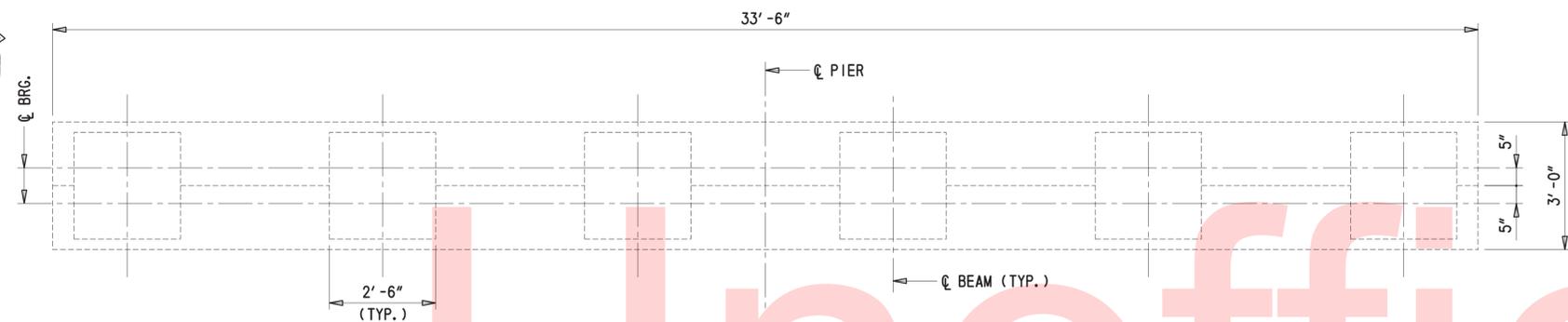
SOUTHBOUND (BR 3-253S) PIER 1 REPAIRS

SHEET NO.	16
TOTAL SHTS.	49



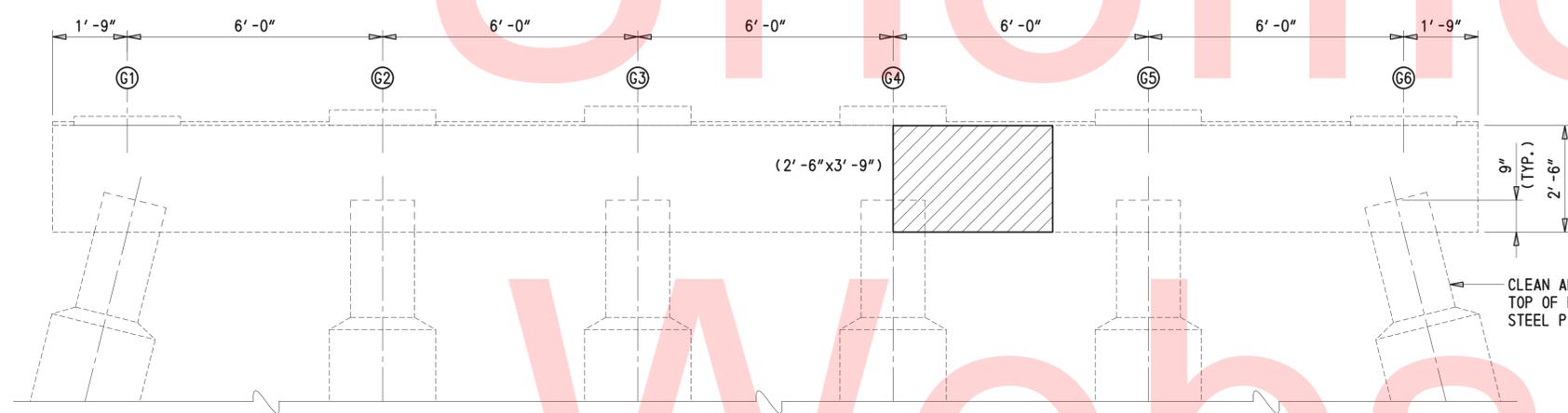
CONCRETE REPAIR QUANTITIES		
PIER 2		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	384 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	226 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



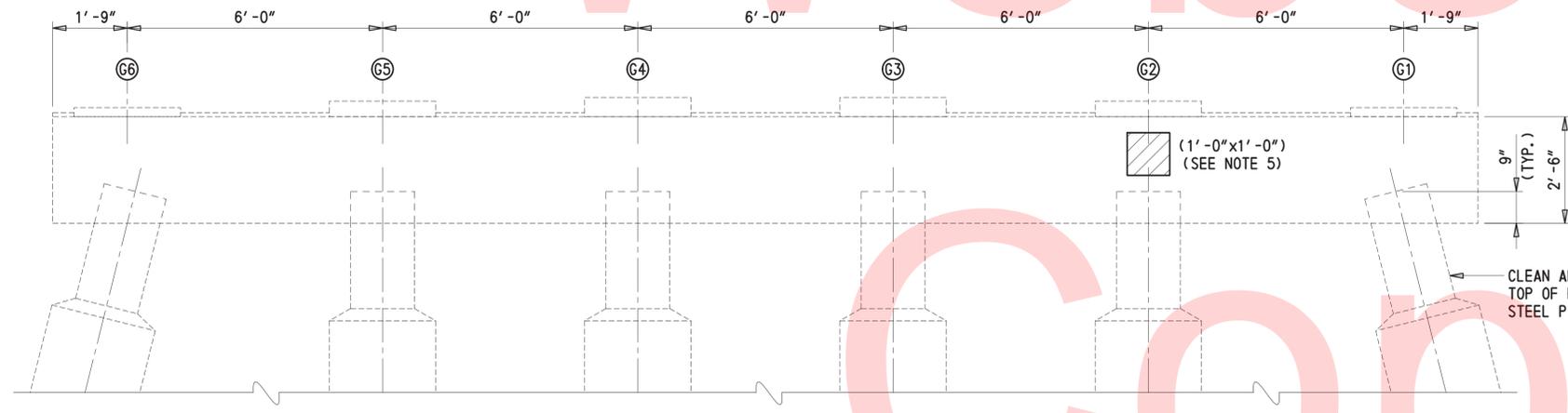
PLAN

1/2" = 1'-0"



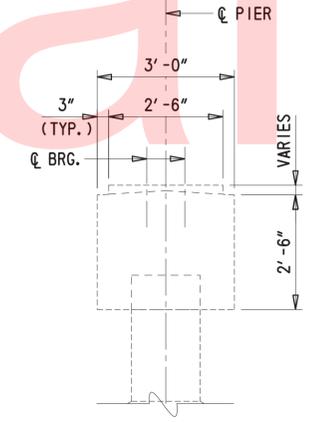
**ELEVATION
(LOOKING NORTH)**

1/2" = 1'-0"



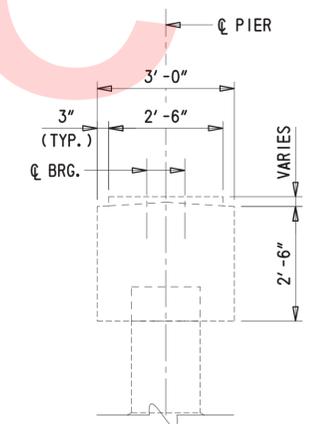
**ELEVATION
(LOOKING SOUTH)**

1/2" = 1'-0"



**END VIEW
(LOOKING WEST)**

1/2" = 1'-0"



**END VIEW
(LOOKING EAST)**

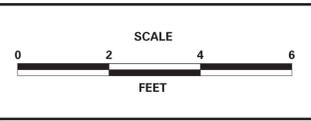
1/2" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
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6. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\PRO2-NB_BR3-253.dgn

ADDENDUMS / REVISIONS

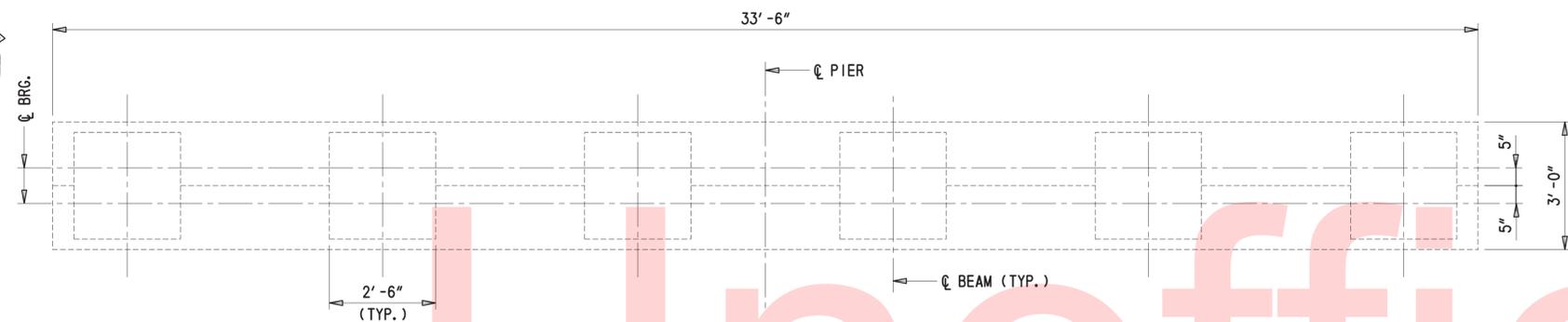


CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		



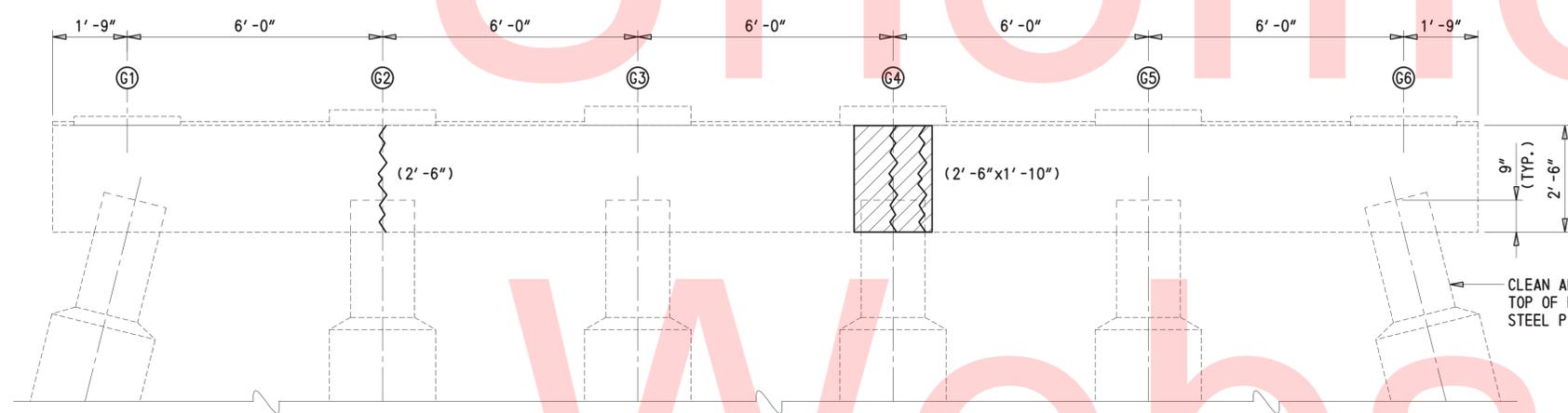
CONCRETE REPAIR QUANTITIES		
PIER 2		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	384 SF	602646
CRACK REPAIR	3 LF	602611
REPAIR OF EXISTING P. C. C.	226 LB	602572
REHABILITATION OF CONCRETE	0 CY	602586

LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE



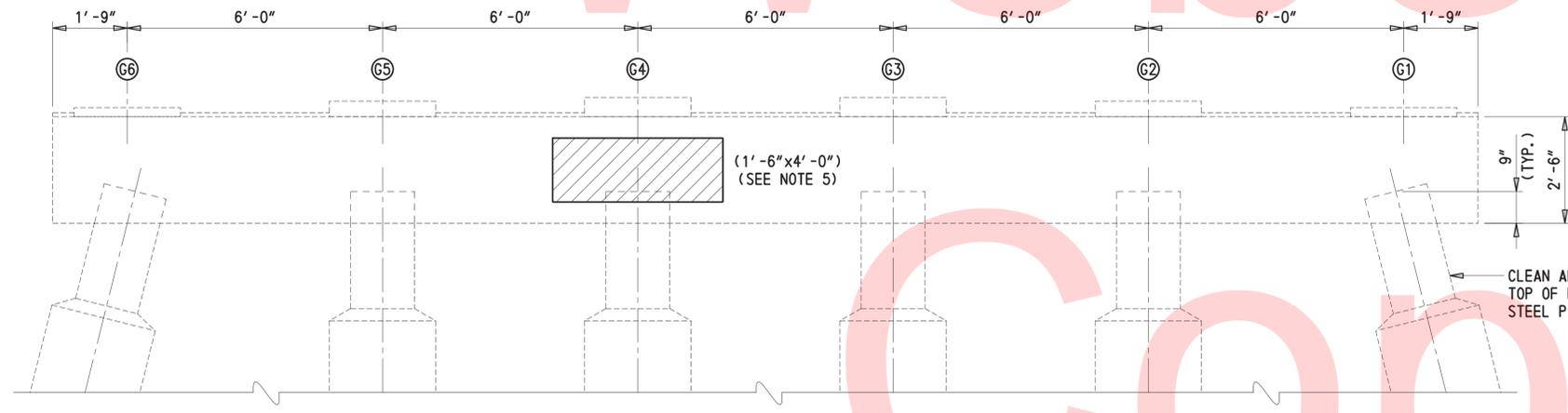
PLAN

1/2" = 1'-0"



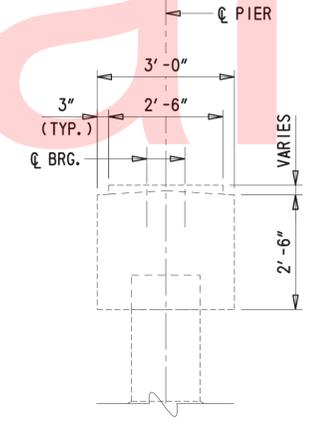
**ELEVATION
(LOOKING NORTH)**

1/2" = 1'-0"



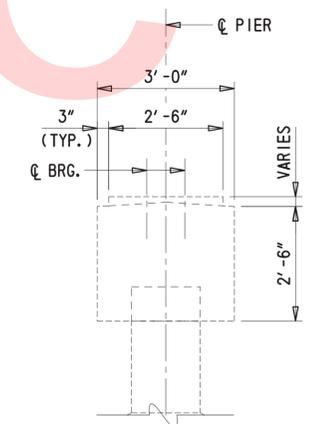
**ELEVATION
(LOOKING SOUTH)**

1/2" = 1'-0"



**END VIEW
(LOOKING WEST)**

1/2" = 1'-0"



**END VIEW
(LOOKING EAST)**

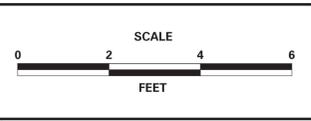
1/2" = 1'-0"

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.
5. PREVIOUSLY NOTED CRACKS AND DELAMINATIONS COULD NOT BE VERIFIED DUE TO ACCESS ISSUES AT TIME OF INSPECTION.
6. SEALANT TO BE APPLIED TO ALL PIER FACES AFTER REPAIRS HAVE BEEN COMPLETED.
7. PROTECT BEARINGS DURING CLEANING AND APPLICATION OF SEALANT. BEARING PROTECTION COST IS INCIDENTAL TO ITEM NO. 602646.

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ADDENDUMS / REVISIONS



CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

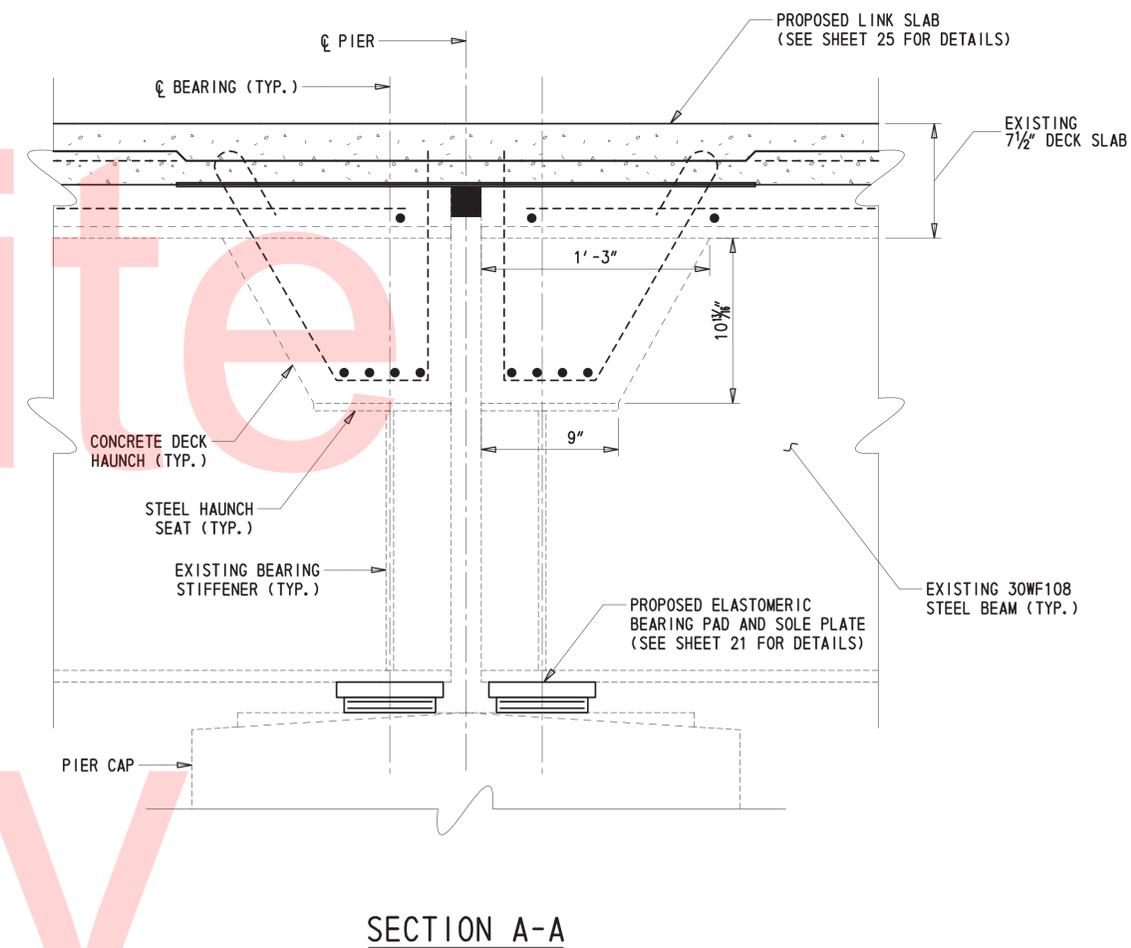
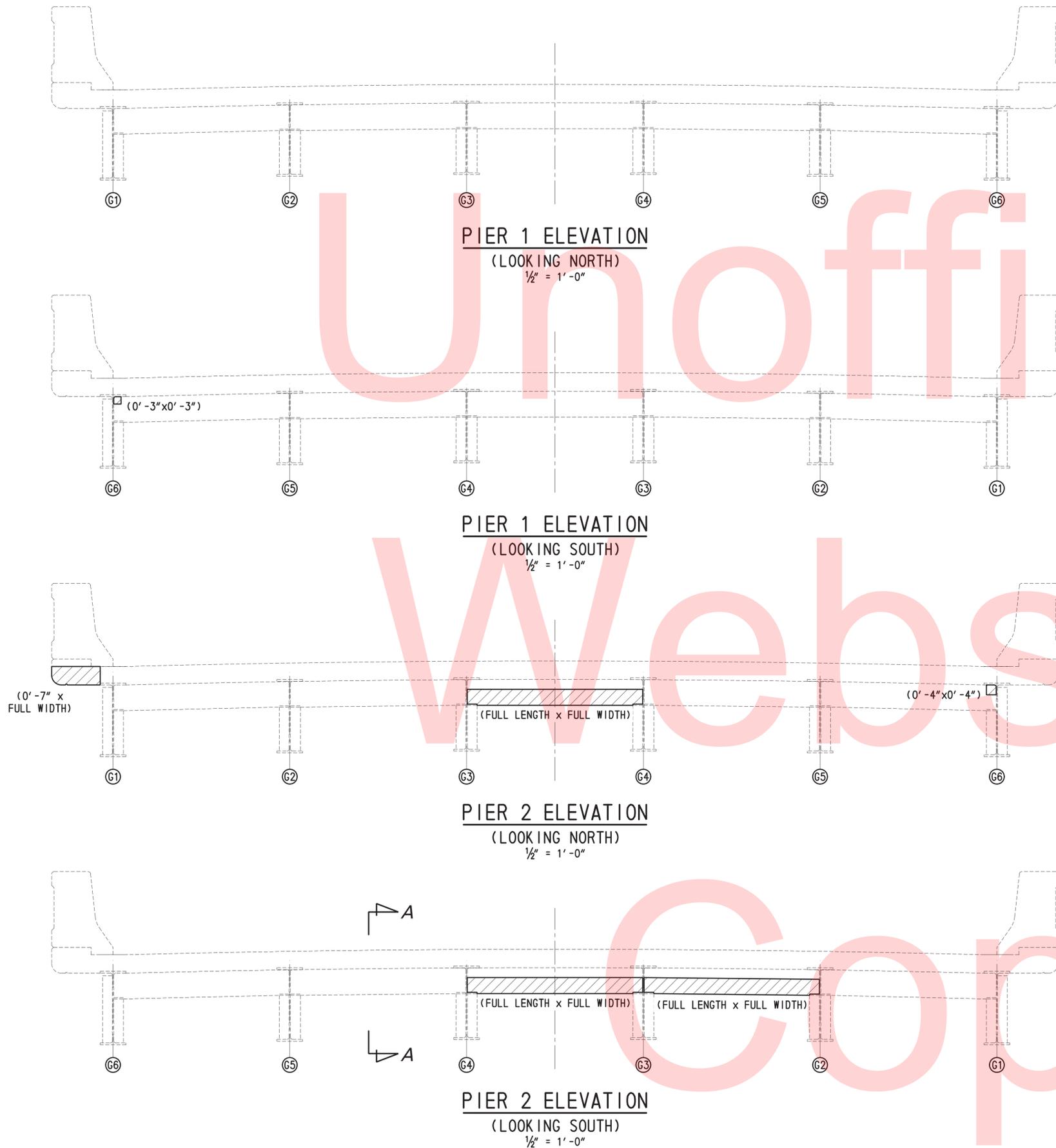
SHEET NO.	18
TOTAL SHTS.	49

CONCRETE REPAIR QUANTITIES		
DECK HAUNCHES (NORTHBOUND)	PAID FOR UNDER ITEM NO.	
CONCRETE SEALANT	0 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	113 LB	602572
REHABILITATION OF CONCRETE	1 CY	602586

LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE

NOTES:

1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.

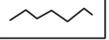


NOTE: PROPOSED END DIAPHRAGMS NOT SHOWN FOR CLARITY. SEE SHEET 22 FOR DETAILS.

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	ADDENDUMS / REVISIONS	SCALE AS NOTED	BR 3-253 N&S ON US 13 OVER WILLIAMS POND	CONTRACT	BRIDGE NO.	BR 3-253 N&S	SHEET NO. 19
				T201507601	DESIGNED BY: JWS	TOTAL SHTS. 49	
				COUNTY	CHECKED BY: DEF		
				SUSSEX			
DELAWARE DEPARTMENT OF TRANSPORTATION			NORTHBOUND (BR 3-253N) DECK HAUNCH REPAIRS				

CONCRETE REPAIR QUANTITIES		
DECK HAUNCHES (NORTHBOUND)		PAID FOR UNDER ITEM NO.
CONCRETE SEALANT	0 SF	602646
CRACK REPAIR	0 LF	602611
REPAIR OF EXISTING P. C. C.	0 LB	602572
REHABILITATION OF CONCRETE	1 CY	602586

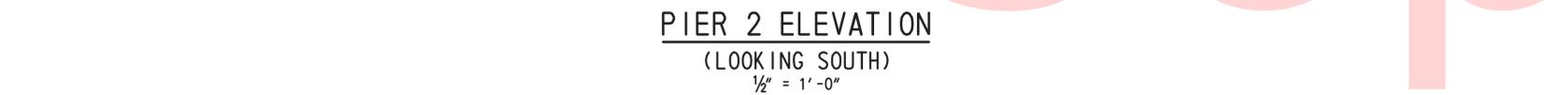
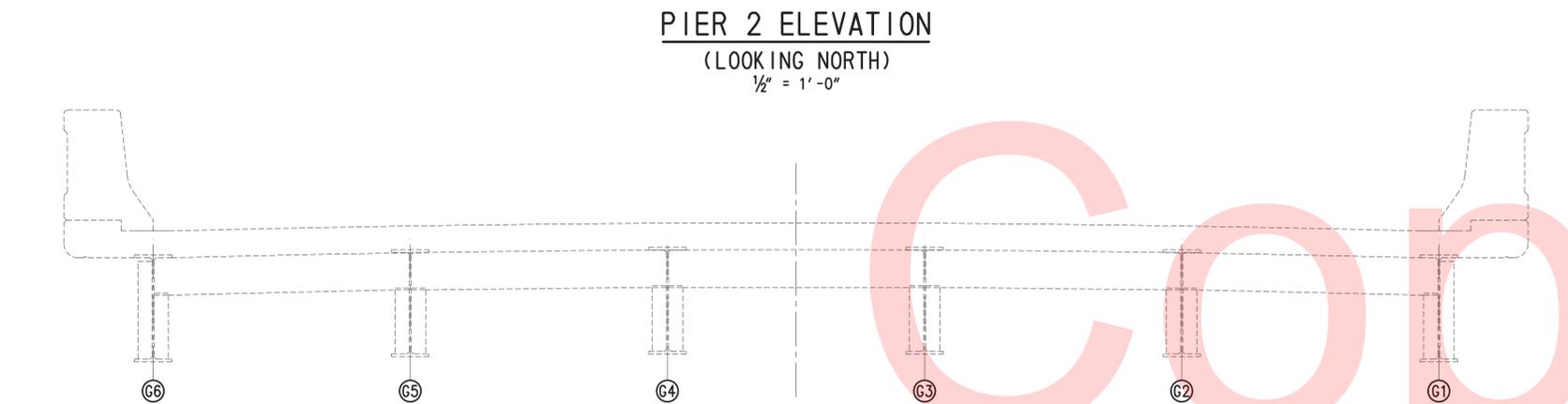
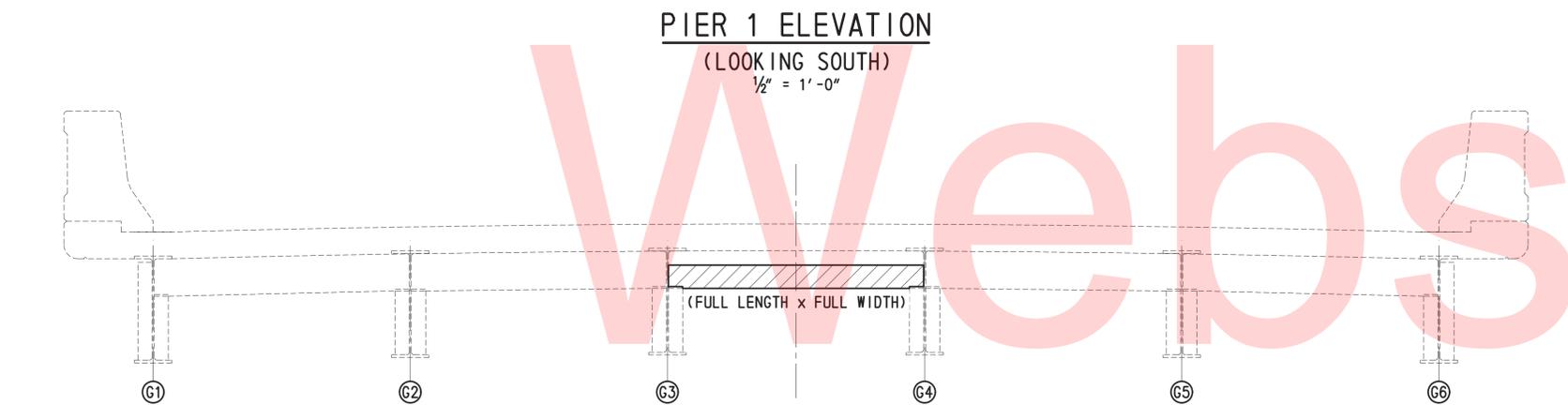
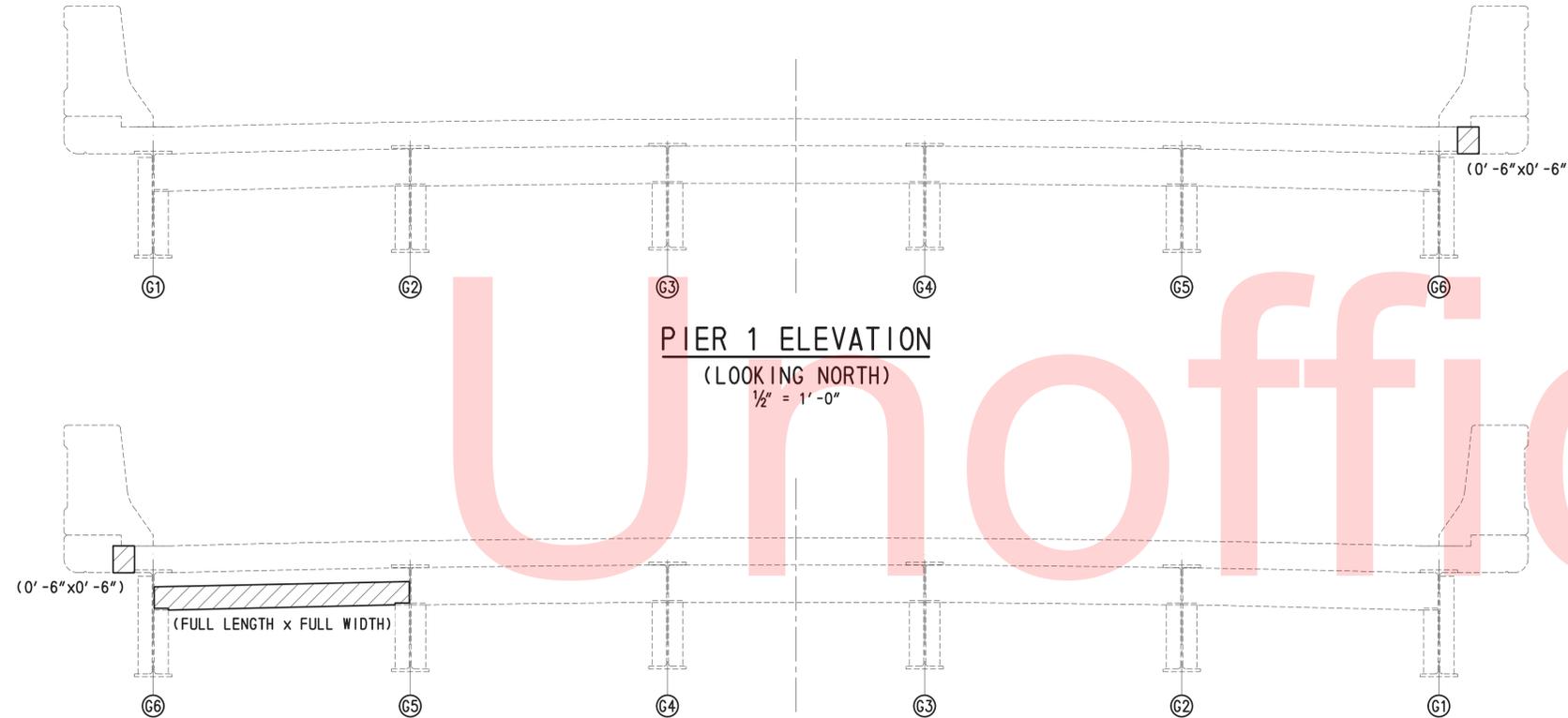
LEGEND	
	SPALL REPAIR (LENGTH x WIDTH)
	CRACK REPAIR (LENGTH)
	EXCAVATION AREA
	REMOVAL OF EXISTING CONCRETE

NOTES:

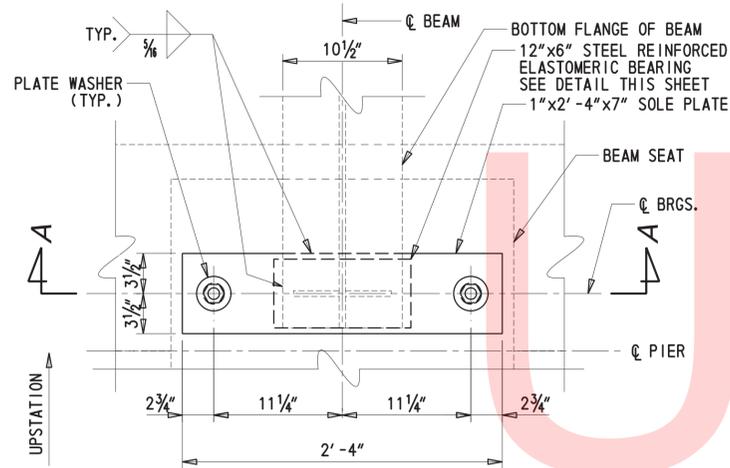
1. CONCRETE REPAIR DIMENSIONS IN FEET AND INCHES.
2. CONTRACTOR TO FIELD VERIFY ALL REPAIR DIMENSIONS.
3. ENGINEER SHALL CONFIRM CONTRACTOR DELINEATION OF CRACK AND SPALL AREAS PRIOR TO COMMENCEMENT OF REPAIRS.
4. WHERE CRACKS AND SPALLS ARE SHOWN CONCURRENTLY, COMPLETE THE SPALL REPAIR AND THEN COMPLETE THE CRACK REPAIR TO ALL CRACKS THAT EXTEND BEYOND THE LIMITS OF THE SPALL REPAIR. IF A CRACK WITHIN THE SPALL LIMITS EXTENDS DEEPER THAN THE LIMITS OF CONCRETE REMOVAL, THE CRACK SHALL BE EPOXY INJECTED PRIOR TO PLACEMENT OF THE SPALL REPAIR MATERIAL.

CROSS-REFERENCE NOTES:

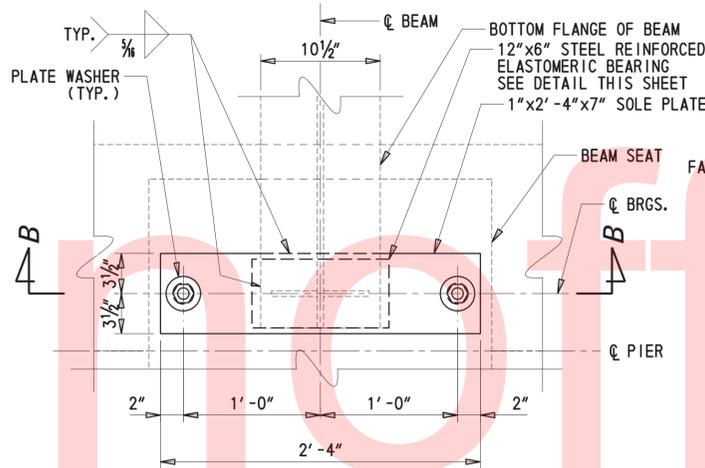
1. FOR SECTION-VIEW OF DECK HAUNCH, SEE SHEET 19.



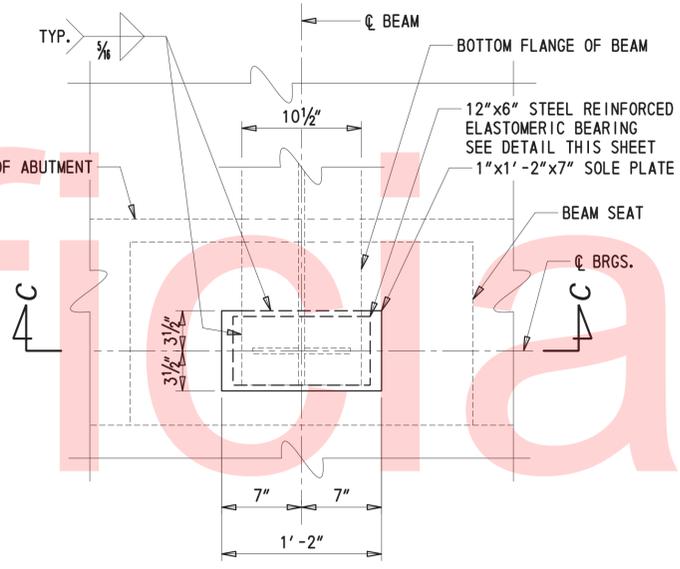
3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans\DK07SB_BR3-253.dgn



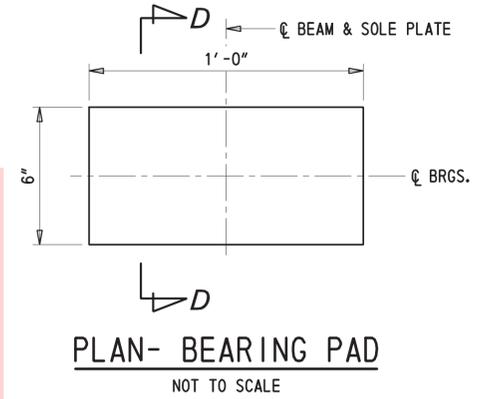
PLAN - FIXED BEARING F1
1 1/2" = 1'-0"



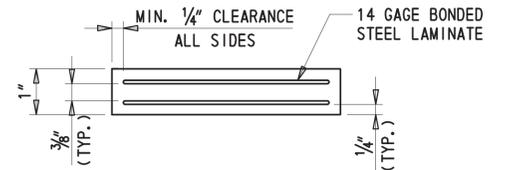
PLAN - FIXED BEARING F2
1 1/2" = 1'-0"



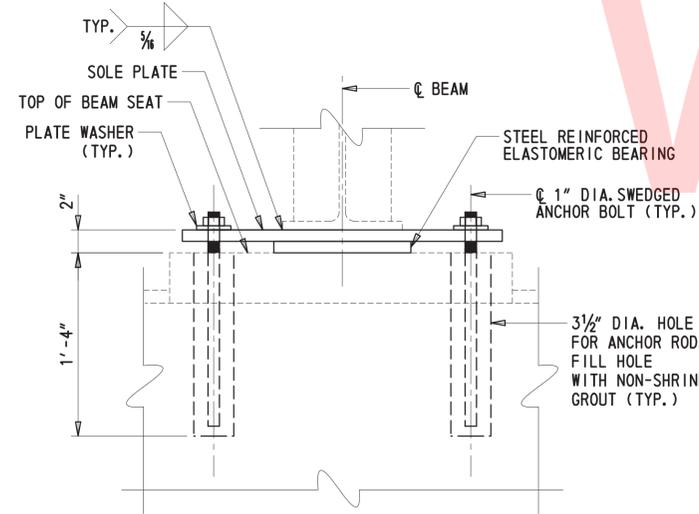
PLAN - EXP. BEARING E1
1 1/2" = 1'-0"



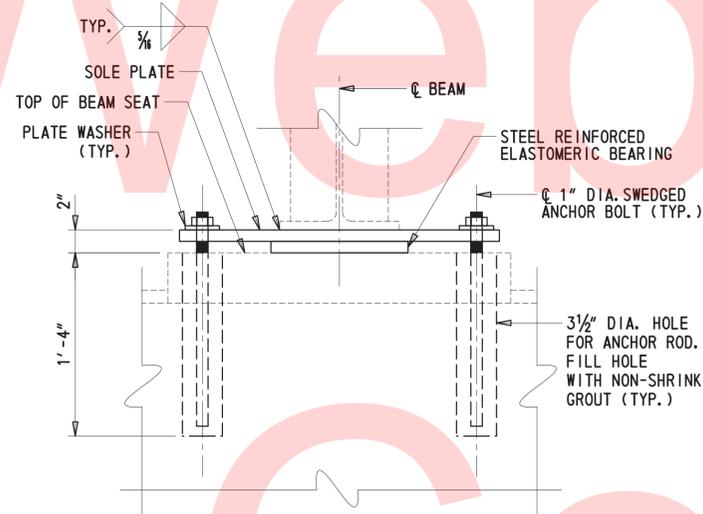
PLAN- BEARING PAD
NOT TO SCALE



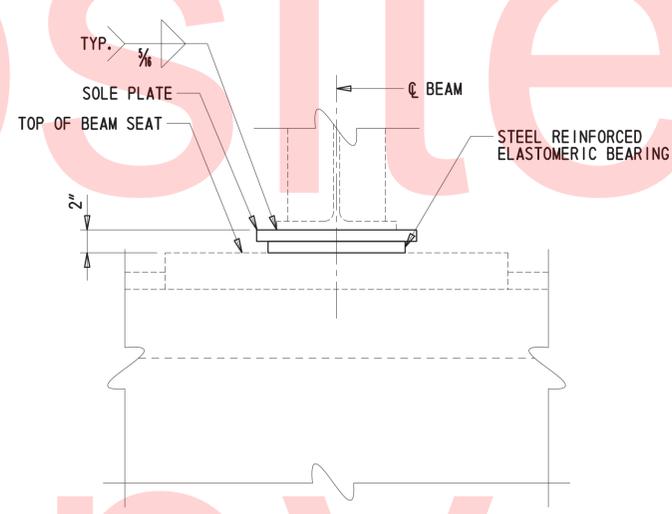
SECTION D-D
NOT TO SCALE



SECTION A-A
1 1/2" = 1'-0"



SECTION B-B
1 1/2" = 1'-0"



SECTION C-C
1 1/2" = 1'-0"

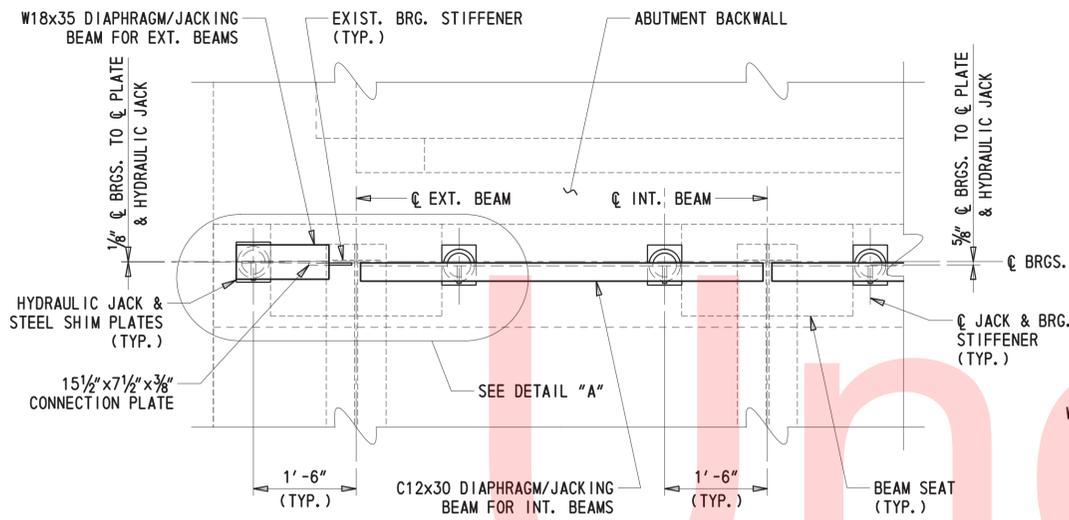
CROSS-REFERENCE NOTES:

- FOR LOCATION OF BEARINGS F1, F2 & E1, SEE SHEET 23.

NOTES:

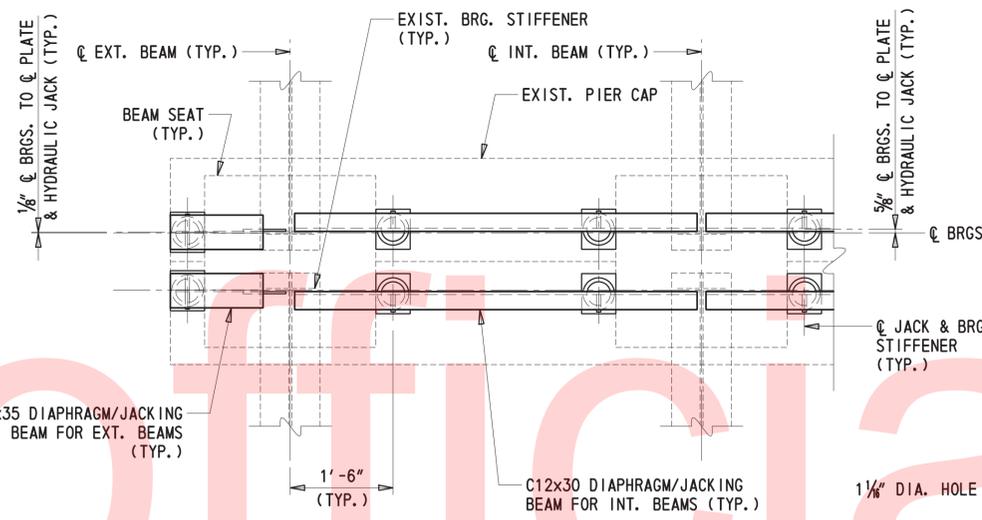
- PROVIDE ALL STEEL REINFORCED ELASTOMERIC BEARINGS IN ACCORDANCE WITH THE SPECIAL PROVISION 605651.
- ANCHOR BOLTS SHALL BE UNPAINTED AASHTO M 314 (GRADE 55) STEEL. WASHERS SHALL BE UNPAINTED A709 GRADE 36 GALVANIZED STEEL. ALL NUTS SHALL BE UNPAINTED A307 GALVANIZED STEEL. SET NUTS 1/4" CLEAR OF SOLE PLATES AND BURR THREADS ABOVE AND BELOW NUTS.
- 1" SWEDGED ANCHOR BOLTS SHALL BE USED. BOLTS SHALL BE GROUTED IN PREFORMED DRILLED HOLES. WHEN DRILLING HOLES, DO NOT COME IN CONTACT WITH THE REINFORCING BARS.
- SOLE PLATES SHALL BE ASTM A709 GRADE 50.
- SOLE PLATES SHALL MEET A FLATNESS REQUIREMENT OF 0.5 PERCENT IN THE DIRECTION BEING MEASURED (LENGTH, WIDTH, DIAGONAL) MAXIMUM, BUT NOT TO EXCEED 1/8".
- SOLE PLATES SHALL BE PAINTED IN ACCORDANCE WITH SPECIAL PROVISION 605522.
- BEARING SHALL BE PLACED NORMAL TO CENTERLINE OF BEAM.
- FILL SLOTS AND HOLES AROUND ANCHOR BOLTS WITH AN APPROVED NON-HARDENING CAULKING COMPOUND OR ELASTIC JOINT SEALER.
- FOR FIXED BEARINGS: USE 1 3/8" DIA. HOLES IN SOLE PLATE AND 3/8" x 3" DIA. WASHER WITH 1 1/4" DIA. HOLE IN WASHER.
- STEEL REINFORCED ELASTOMERIC BEARING PADS SHALL BE ATTACHED TO THE TOP OF THE CONCRETE PEDESTAL WITH AN APPROVED EPOXY ADHESIVE IN ACCORDANCE WITH SECTION 826.06(E) OF THE STANDARD SPECIFICATIONS IN SUCH A WAY THAT VISIBLE CONCRETE SURFACES WILL NOT BE STAINED. ENSURE THE EPOXY ADHESIVE HAS SET PRIOR TO PLACEMENT OF BEAMS. CONTRACTOR TO FIELD VERIFY ALL MEASUREMENTS.
- PAYMENT FOR FABRICATION AND INSTALLATION OF STEEL REINFORCED ELASTOMERIC BEARINGS, ANCHOR BOLTS, NUTS, WASHERS, AND SOLE PLATES SHALL BE INCIDENTAL TO ITEM NO. 605651.

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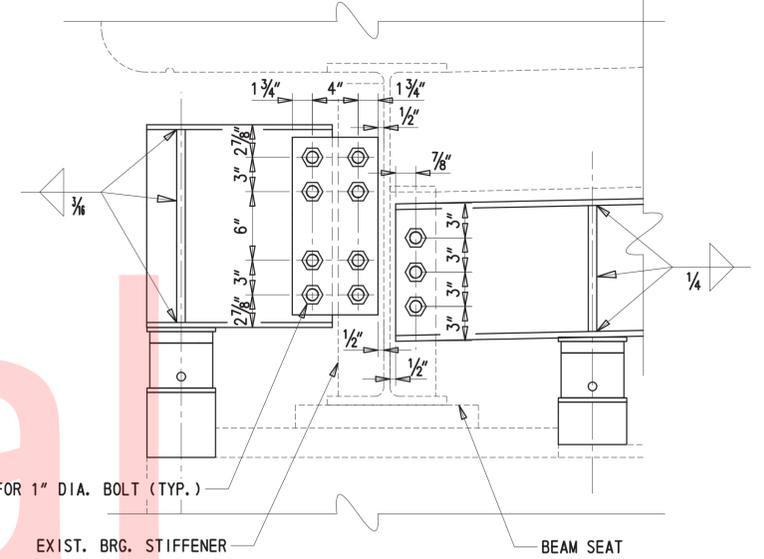
PLAN - JACKING AT ABUTMENT

3/4" = 1'-0"



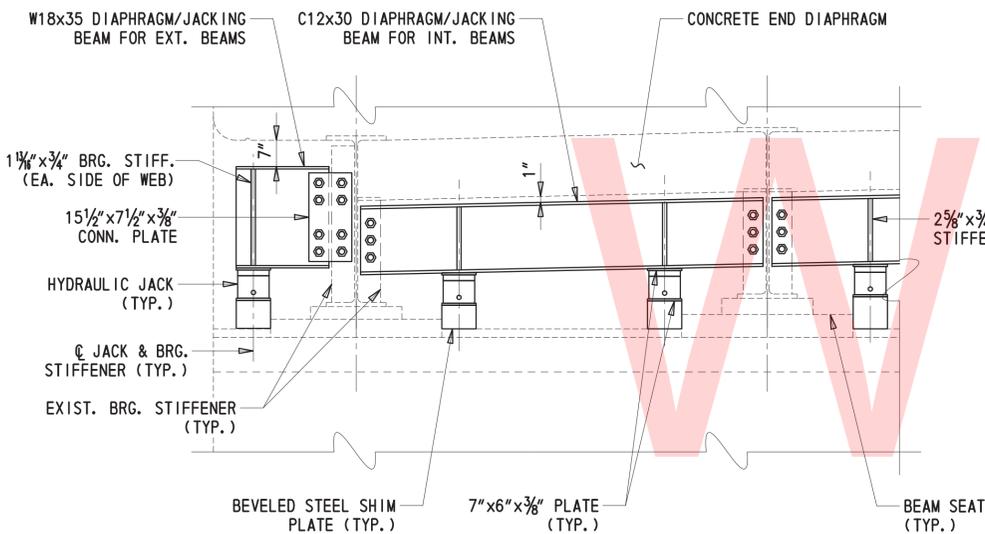
PLAN - JACKING AT PIER

3/4" = 1'-0"



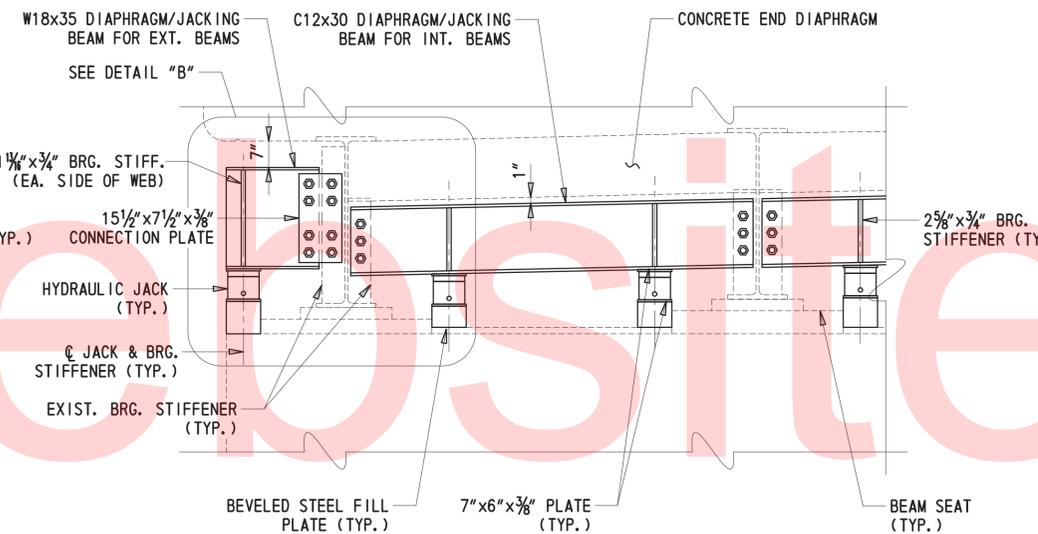
DETAIL "B"

NOT TO SCALE



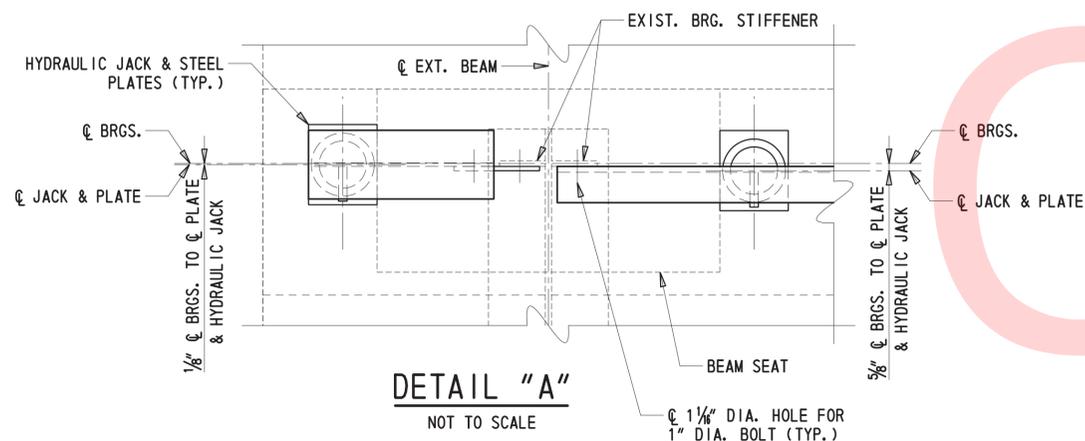
ELEVATION - JACKING AT ABUTMENT

3/4" = 1'-0"



ELEVATION - JACKING AT PIER

3/4" = 1'-0"



DETAIL "A"

NOT TO SCALE

DIAPHRAGM NOTES:

- BEARING STIFFENERS SHALL BE SHOP WELDED TO THE DIAPHRAGMS, FIELD WELDING WILL NOT BE PERMITTED. BEARING STIFFENERS SHALL BE INSTALLED ON BOTH SIDES OF THE WEB FOR THE EXTERIOR DIAPHRAGMS. FOR THE EXTERIOR DIAPHRAGMS THE CONTRACTOR SHALL CUT THE INSIDE CORNERS OF STIFFENER PLATES 3/4" x 3/4". BEARING STIFFENERS SHALL BE INSTALLED ON ONE SIDE OF THE WEB FOR INTERIOR DIAPHRAGMS, BETWEEN THE CHANNEL FLANGES. FOR THE INTERIOR DIAPHRAGMS THE CONTRACTOR SHALL CUT THE INSIDE CORNERS OF STIFFENER PLATES 1/2" x 1/2". ALL WORK ASSOCIATED WITH JACKING SHALL BE PAID FOR UNDER PAY ITEM NO. 605607. ALL WORK ASSOCIATED WITH FURNISHING AND INSTALLING PERMANENT INTERIOR DIAPHRAGMS SHALL BE PAID FOR UNDER PAY ITEM NO. 605001. AT COMPLETION OF BEARING REPLACEMENT, END DIAPHRAGMS SHALL BE REMOVED. HOLES IN EXTERIOR BEARING STIFFENER OF FASCIA BEAMS SHALL BE LEFT FILLED WITH BOLTS. NUTS SHALL BE TORQUED.

JACKING NOTES:

- CONTRACTOR SHALL SUBMIT A JACKING PLAN TO THE ENGINEER FOR APPROVAL. JACKING PLAN SHALL INCLUDE AT A MINIMUM: MATERIALS, ERECTION SCHEME, DESIGN CALCULATIONS, SUPPORTING DATA, AND EQUIPMENT. PAYMENT IS INCIDENTAL TO ITEM NO. 605607.
- TOTAL VERTICAL DISPLACEMENT FOR ALL GIRDERS SHALL BE 1/2" MAX.
- CONTRACTOR SHALL VERIFY ALL EXISTING FIELD DIMENSIONS AND PRESENT ANY DISCREPANCIES TO THE ENGINEER IN WRITING PRIOR TO BEGINNING OF JACKING OPERATIONS.
- THE MAXIMUM DIFFERENTIAL DISPLACEMENT BETWEEN ANY TWO ADJACENT GIRDERS IS NOT TO EXCEED 1/8" AT ANY TIME. THE MAXIMUM DIFFERENTIAL DISPLACEMENT BETWEEN ANY TWO GIRDERS IS NOT TO EXCEED 1/4" AT ANY TIME.
- THE CONTRACTOR SHALL NOT PERMIT THE MAXIMUM SPACE BETWEEN THE TOP OF ANY JACK AND THE TOP OF ADJACENT BLOCKING OR TEMPORARY SUPPORTS TO EXCEED 1/4" AT ANY TIME DURING THE JACKING PROCEDURE.
- THE CONTRACTOR SHALL USE ONLY THOSE JACKS WITH LOCK-NUTS CAPABLE OF SUPPORTING A LOAD EQUAL TO THE RATED CAPACITY OF THE JACK IN THE EVENT THE JACK LOSES HYDRAULIC PRESSURE. THE CONTRACTOR SHALL ADVANCE THE LOCK-NUTS ON ALL JACKS SUCH THAT THE MAXIMUM DISTANCE BETWEEN THE TOP OF A JACK AND THE LOCK-NUT SHALL NOT EXCEED 1/4" AT ANY TIME DURING THE JACKING PROCEDURE.
- THE DEADLOAD HAS BEEN INCREASED BY 15% TO CONSIDER ANY ADDITIONAL FORCE THAT MAY BE REQUIRED TO SEPARATE THE SUPERSTRUCTURE FROM THE SUBSTRUCTURE. THE LIVE LOAD HAS NOT BEEN INCREASED.
- THE UNFACTORED DEADLOAD AND LIVELOAD SHALL BE ADJUSTED BY A SAFETY FACTOR OF 1.65 TO ACCOUNT FOR "STICKY FORCE" AS DESCRIBED IN THE DELDOT BRIDGE DESIGN MANUAL 2015 EDITION.
- ALL JACKS SHALL BE SET Laterally 1'-6" FROM THE CENTERLINE OF THE NEAREST GIRDER, SEE DETAILS THIS SHEET. JACKS FOR EXTERIOR DIAPHRAGMS SHALL BE SET ALONG THE CENTERLINE OF THE WEB OF THE EXTERIOR DIAPHRAGM, SEE DETAILS THIS SHEET. JACKS FOR THE INTERIOR DIAPHRAGMS SHALL BE SET ALONG THE PLASTIC NEUTRAL AXIS OF THE CHANNEL DIAPHRAGM, SEE DETAILS THIS SHEET.

UNFACTORED LOADS FOR JACKING

LOCATION	GIRDER	DL+15%	DL+LL+Imp
ABUT. A	G1/G6	22.1 KIPS	60.9 KIPS
	G2/G5	17.7 KIPS	68.8 KIPS
ABUT. B	G3/G4	14.4 KIPS	65.9 KIPS
PIER 1	G1/G6	22.1 KIPS	60.9 KIPS
	G2/G5	17.7 KIPS	68.8 KIPS
PIER 2	G3/G4	14.4 KIPS	65.9 KIPS

FACTORED LOADS FOR JACKING

LOCATION	GIRDER	DL+15%	DL+LL+Imp
ABUT. A	G1/G6	27.6 KIPS	100.5 KIPS
	G2/G5	22.1 KIPS	113.5 KIPS
ABUT. B	G3/G4	18.0 KIPS	108.7 KIPS
PIER 1	G1/G6	27.6 KIPS	100.5 KIPS
	G2/G5	22.1 KIPS	113.5 KIPS
PIER 2	G3/G4	18.0 KIPS	108.7 KIPS

ADDENDUMS / REVISIONS

SCALE AS NOTED

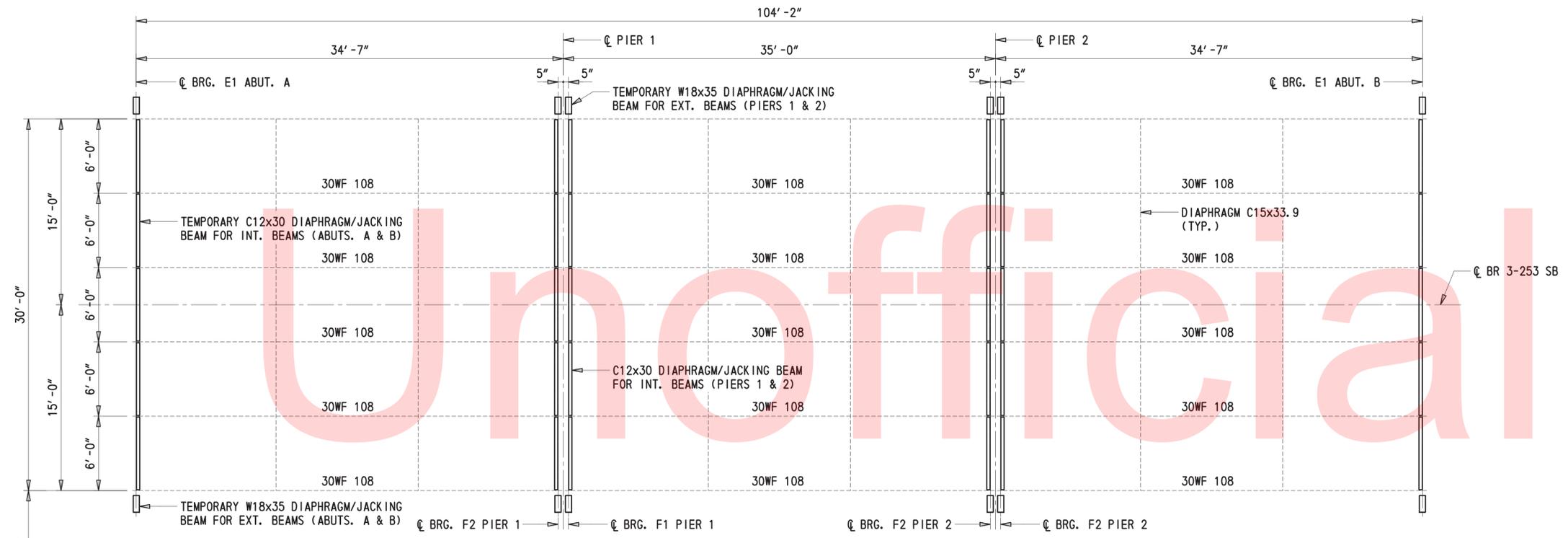
BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT
T201507601
COUNTY
SUSSEX

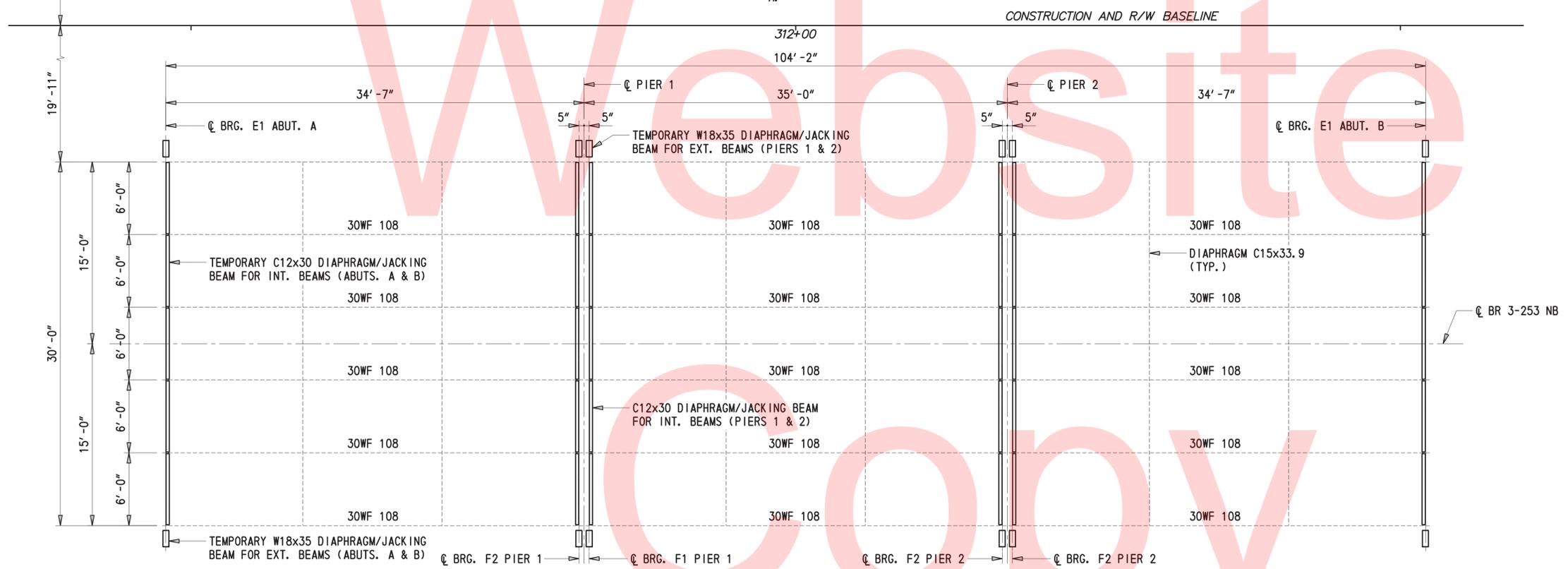
BRIDGE NO. BR 3-253 N&S
DESIGNED BY: JWS
CHECKED BY: DEF

JACKING DETAILS

SHEET NO.
22
TOTAL SHTS.
49



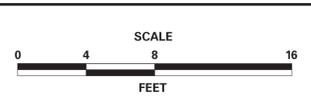
FRAMING PLAN
(SOUTHBOUND BRIDGE)
3/8" = 1'-0"



FRAMING PLAN
(NORTHBOUND BRIDGE)
3/8" = 1'-0"

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ADDENDUMS / REVISIONS	

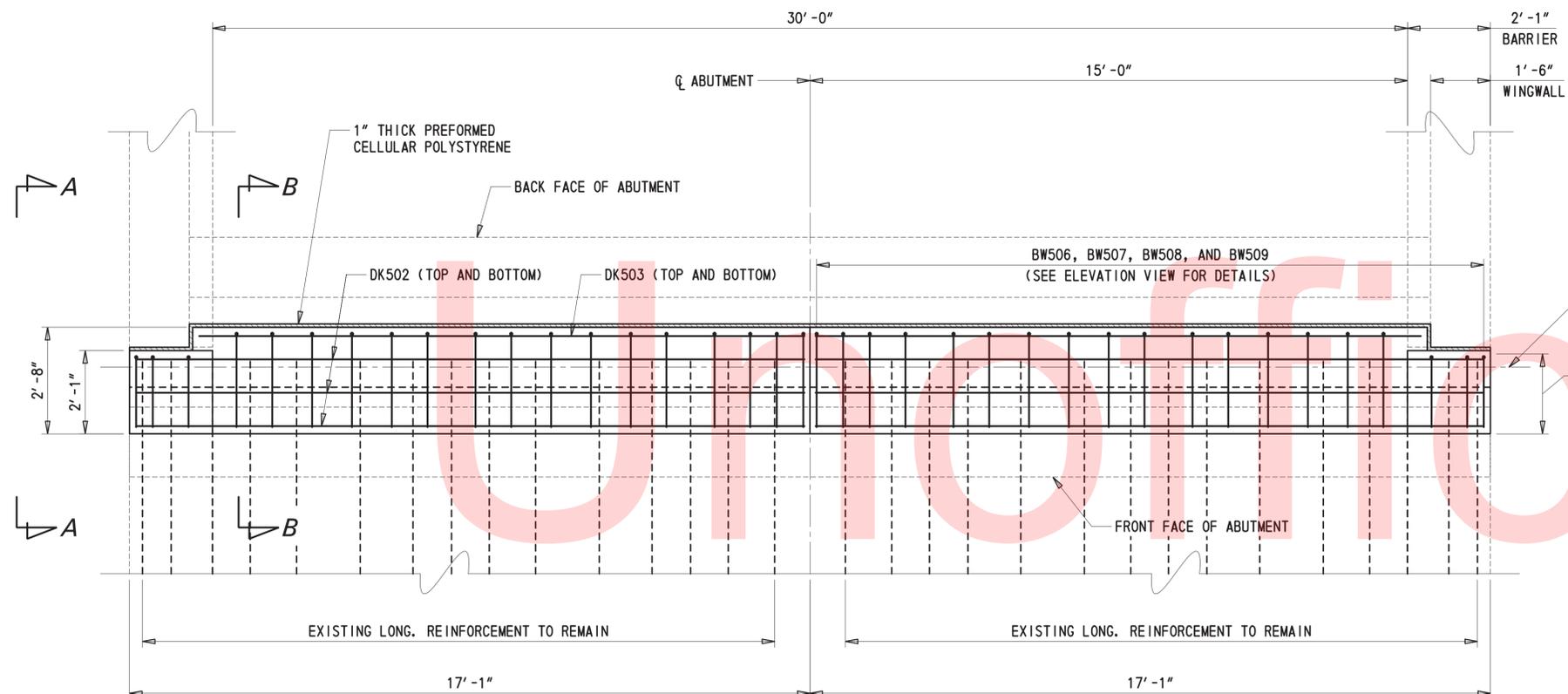


BR 3-253 N&S ON US 13
OVER WILLIAMS POND

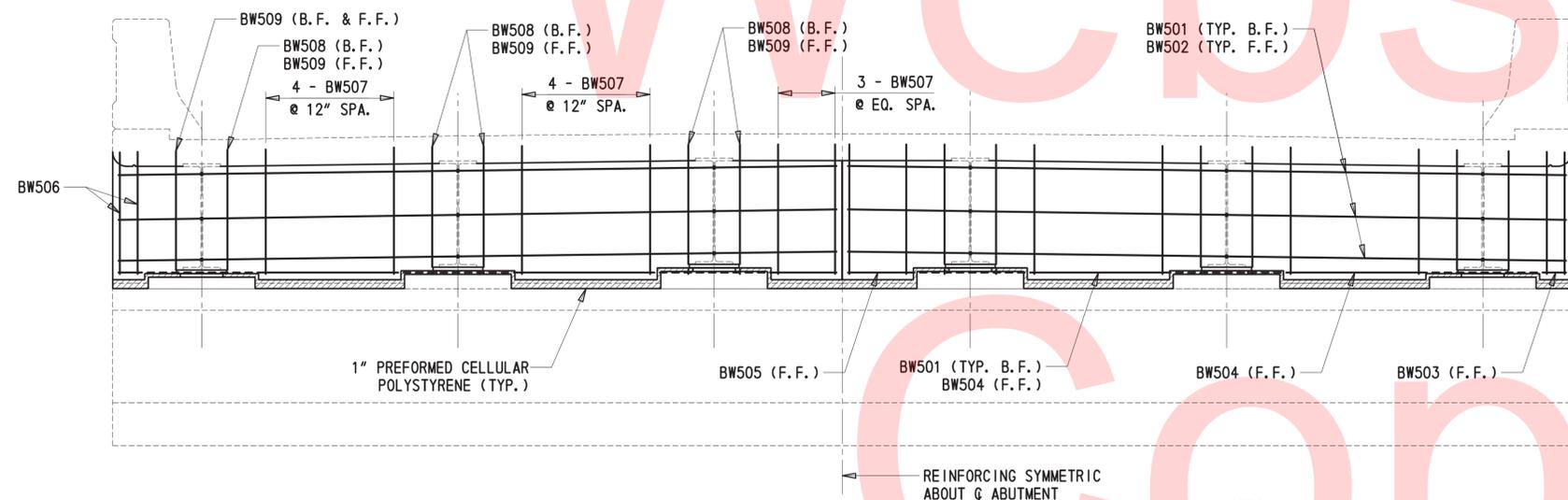
CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

FRAMING PLAN

SHEET NO.	23
TOTAL SHTS.	49

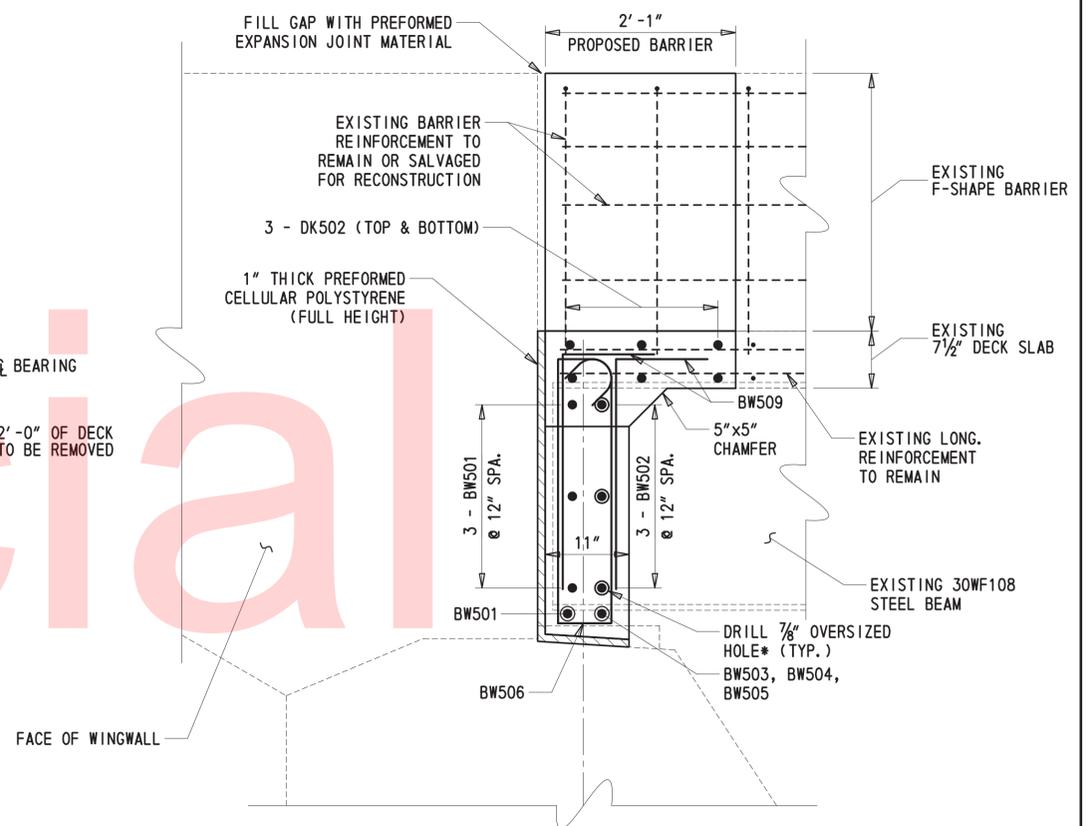


DECK - PLAN (TYP.)
1/2" = 1'-0"

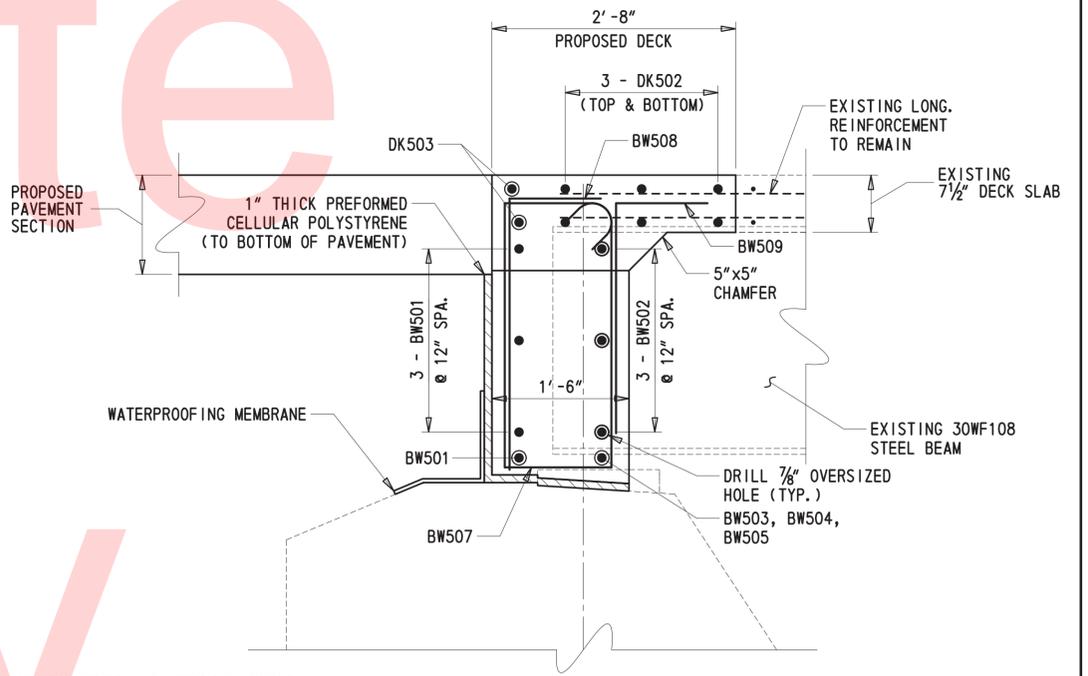


END DIAPHRAGM - ELEVATION (TYP.)
1/2" = 1'-0"

- NOTES:**
1. PAYMENT OF 1" PREFORMED CELLULAR POLYSTYRENE SHALL BE INCIDENTAL TO ITEM NO. 602013.
 2. PAYMENT OF PREFORMED EXPANSION JOINT MATERIAL SHALL BE INCIDENTAL TO ITEM NO. 602013.
 3. PAYMENT OF 7/8" DRILLED HOLES SHALL BE INCIDENTAL TO ITEM NO. 602013.
 4. DECK AND BARRIER REMOVAL TO BE PAID FOR UNDER ITEM NO. 602580.
 5. APPROVED WATERPROOFING MEMBRANE SHALL BE BONDED ACROSS ABUTMENT CAP AND DIAPHRAGM. TO BE PAID UNDER ITEM NO. 602616.
 6. SHEETING AND SHORING SHALL BE PLACED ALONG CENTERLINE OF ROADWAY DURING THE EXCAVATION AND REMOVAL OF BACKWALL. TO BE PAID UNDER ITEM NO. 207501. PROTECTION AND SUPPORT OF EXPOSED WINGWALLS SHALL BE AT THE DISCRETION OF THE ENGINEER. TO BE INCIDENTAL TO ITEM NO. 207501.

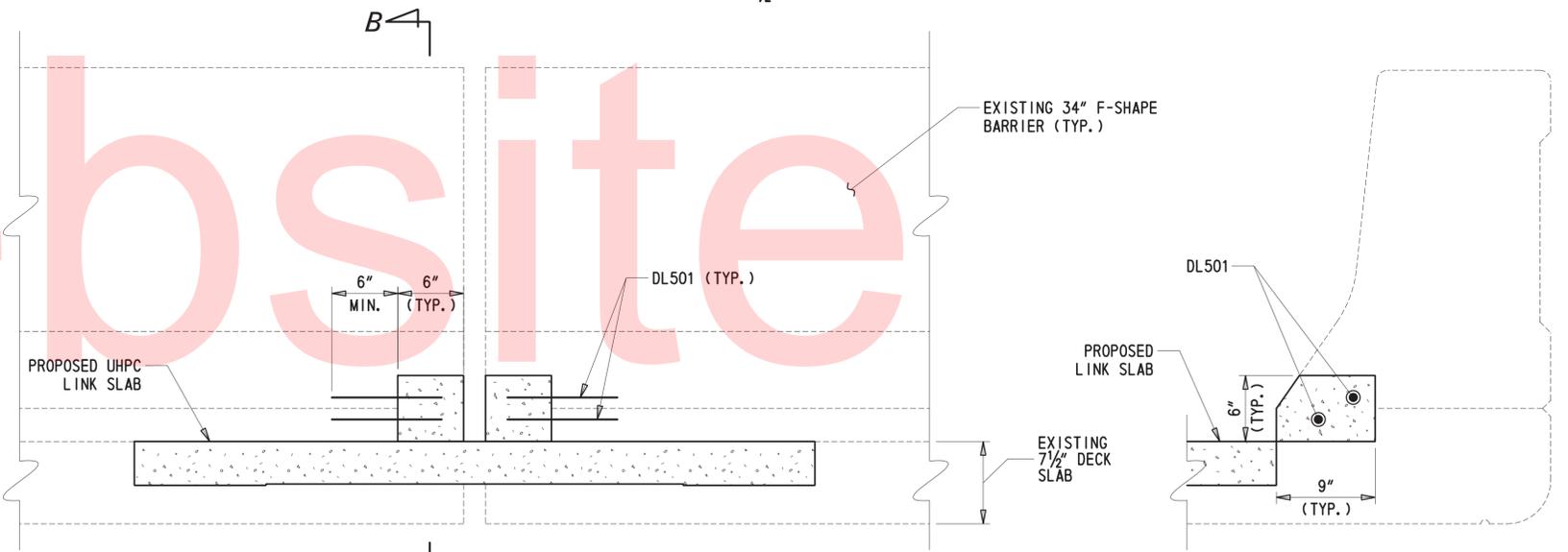
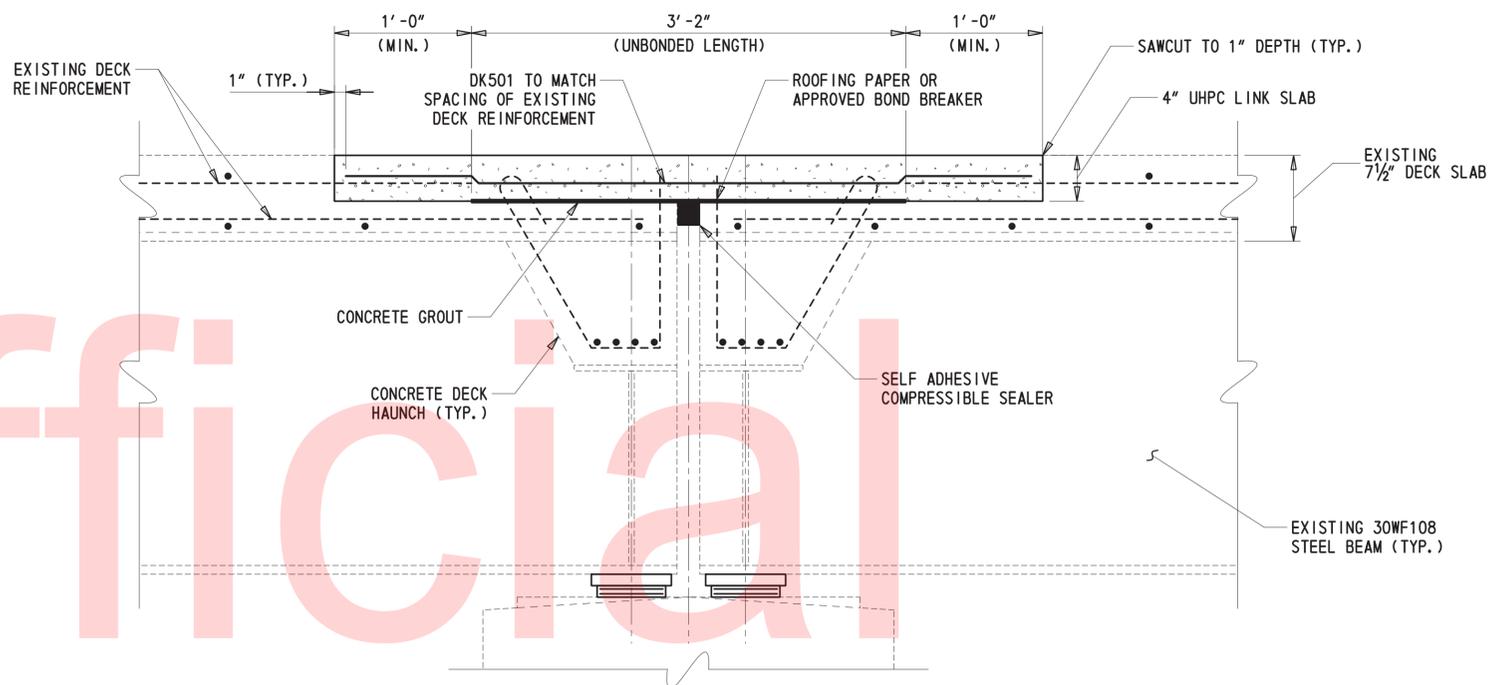
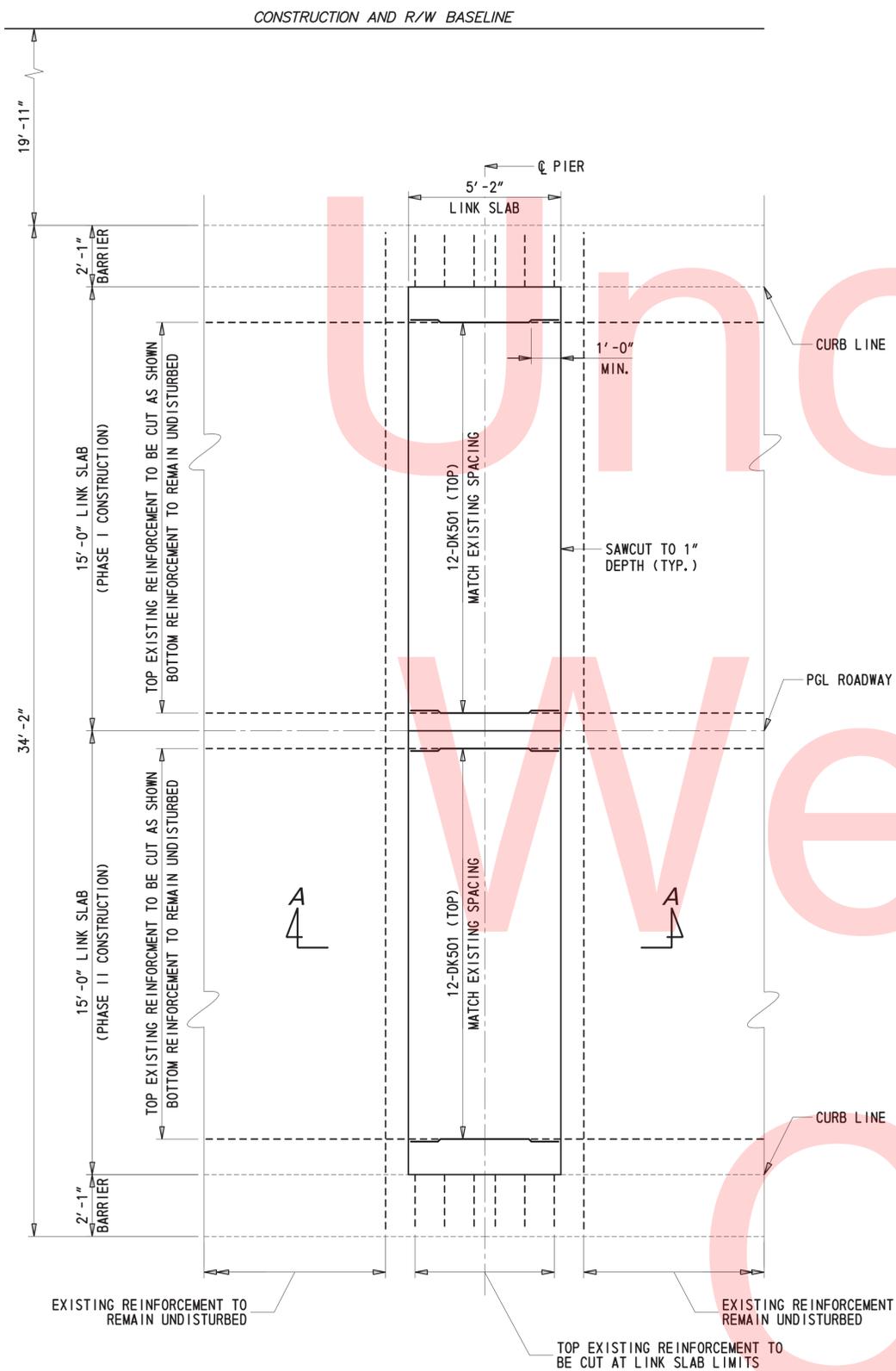


SECTION A-A
1" = 1'-0"



SECTION B-B
1" = 1'-0"

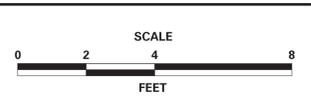
3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\BM02_BR3-253.dgn



- NOTES:
- PARAPET JOINT REPAIR TO OCCUR AT THE FOLLOWING LOCATIONS:
 - ABUTMENT A: NORTHBOUND ONLY
 - PIER 1: NORTHBOUND AND SOUTHBOUND
 - PIER 2: NORTHBOUND AND SOUTHBOUND
 - ABUTMENT B: NORTHBOUND ONLY
 - EXISTING REINFORCEMENT IN PARAPET JOINT REPAIR DETAIL NOT SHOWN FOR CLARITY.
 - CLASS A CONCRETE SHALL BE USED FOR PARAPET JOINT REPAIR DETAIL. TO BE PAID UNDER ITEM NO. 602586.
 - DRILLING FOR PARAPET JOINT REPAIR DETAIL TO BE PAID UNDER ITEM NO. 602579.
 - ALL WORK INVOLVING METHODS OF CONCRETE REMOVAL SHALL BE PERFORMED IN ACCORDANCE WITH SPECIAL PROVISION 602580. SAWCUTTING AND DECK REMOVAL AT LINK SLAB LOCATIONS TO BE PAID FOR UNDER ITEM NO. 602580.
 - CONCRETE GROUT SHALL MEET ASTM C1107, GRADE C. A THIN LAYER OF CONCRETE GROUT SHALL BE APPLIED AFTER REMOVAL OF CONCRETE DECK TO FORM A SMOOTH LEVEL SURFACE. ROOFING PAPER OR APPROVED BOND BREAKER WILL BE LAID ON TOP OF GROUT SURFACE. CONCRETE GROUT SHALL BE INCIDENTAL TO ITEM NO. 618516.
 - PAYMENT FOR ROOFING PAPER (OR APPROVED BOND BREAKER) AND SELF ADHESIVE COMPRESSIBLE SEALER SHALL BE INCIDENTAL TO ITEM NO. 618516.

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ADDENDUMS / REVISIONS



CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

① ANY MARK NUMBER WITH SUFFIX 'E' DENOTES EPOXY COATED REINFORCING STEEL.

② ALL MARK 'LOCATION PREFIXES' SHALL CONSIST OF TWO LETTERS AND ARE AS FOLLOWS: AB = ABUTMENT, AS = APPROACH SLAB, BC = BOX CULVERT, BW = BACKWALL, CL = COLUMN, DK = DECK, DL = DOWEL, FT = FOOTING, HW = HEADWALL, MS = MISC. BARS, PA = PARAPET, PR = PIER, SC = SHEETPILE CAP, SL = SLAB, TW = TOEWALL, WL = WALL (UNIQUE LOCATION), WW = WINGWALL

SPECIFICATIONS					BENDING DIMENSIONS (FEET-INCHES /QUARTER INCH)										
QTY.	SIZE	LENGTH	MARK	TYPE	A	B	C	D	E	F/R	G	H	J	K	O
96	5	5-00	DK501	STR		5-00									
48	5	16-90	DK502	STR		16-90									
16	5	15-20	DK503	STR		15-20									
32	5	17-40	BW501	20		1-70	0-70	15-20							
24	5	16-90	BW502	STR		16-90									
8	5	0-50	BW503	STR		0-50									
16	5	3-00	BW504	STR		3-00									
8	5	1-40	BW505	STR		1-40									
16	5	7-110	BW506	T1	0-52	0-70	2-110	0-70	2-110		0-52				
88	5	9-10	BW507	T1	0-52	1-20	2-110	1-20	2-110		0-52				
40	5	4-00	BW508	20		1-00	3-00								
56	5	3-70	BW509	20		1-00	2-70								
48	5	0-100	DL501	STR		0-100									

SPECIFICATIONS					BENDING DIMENSIONS (FEET-INCHES /QUARTER INCH)										
QTY.	SIZE	LENGTH	MARK	TYPE	A	B	C	D	E	F/R	G	H	J	K	O

SPECIFICATIONS					BENDING DIMENSIONS (FEET-INCHES /QUARTER INCH)										
QTY.	SIZE	LENGTH	MARK	TYPE	A	B	C	D	E	F/R	G	H	J	K	O

ASTM STANDARD ENGLISH REINFORCING BARS

RECOMMENDED END HOOKS, APPLICABLE TO ALL GRADES

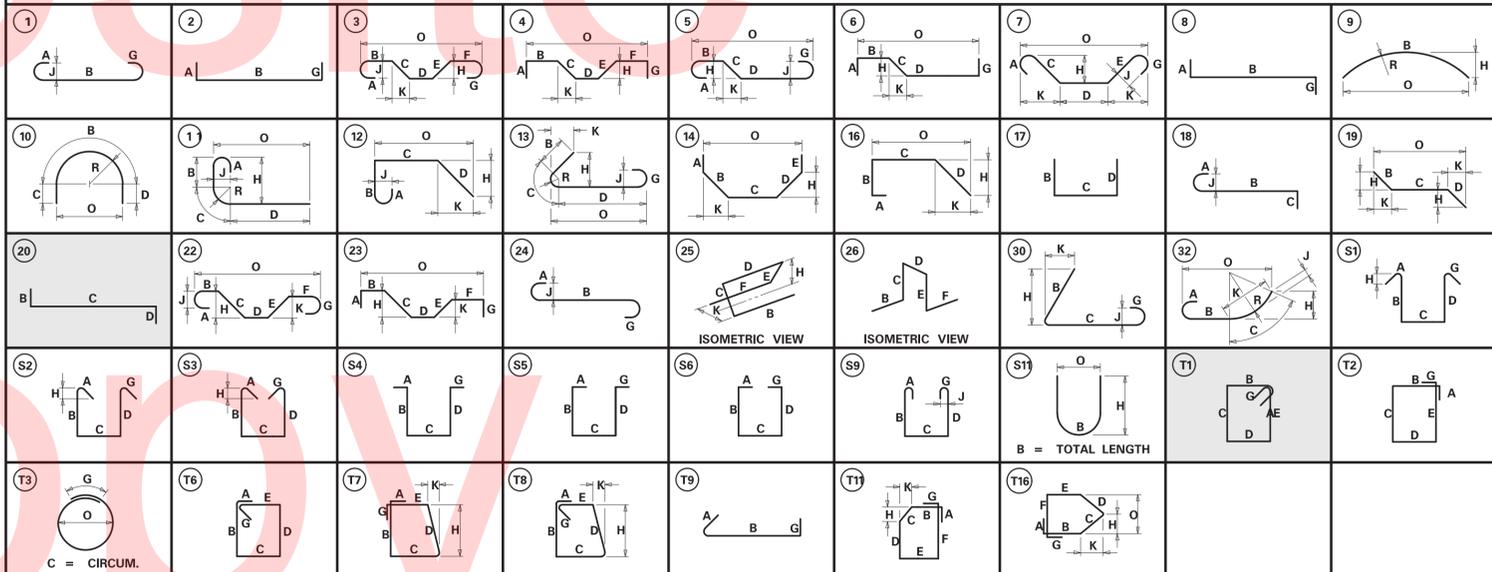
STIRRUP AND TIE HOOKS, APPLICABLE TO ALL GRADES

BAR SIZE	NOMINAL DIMENSIONS			180° HOOKS		90° HOOKS		90° HOOK		135° HOOK	
	DIAMETER (INCHES)	AREA (INCHES ²)	WEIGHT (LBS./FT.)	D	A OR G	J	A OR G	D	A OR G	A OR G	A OR G
3	0.375	0.110	0.376	2 1/4"	5"	3"	6"	1 1/2"	4"	4"	2 1/2"
4	0.500	0.200	0.668	3"	6"	4"	8"	2"	4 1/2"	4 1/2"	3"
5	0.625	0.310	1.043	3 3/4"	7"	5"	10"	2 1/2"	6"	5 1/2"	3 3/4"
6	0.750	0.440	1.502	4 1/2"	8"	6"	1-0"	4 1/2"	1-0"	8"	4 1/2"
7	0.875	0.600	2.044	5 1/4"	10"	7"	1-2"	5 1/4"	1-2"	9"	5 1/4"
8	1.000	0.790	2.670	6"	11"	8"	1-4"	6"	1-4"	10 1/2"	6"
9	1.128	1.000	3.400	9 1/2"	1-3"	11 3/4"	1-7"				
10	1.270	1.270	4.303	10 3/4"	1-5"	1-1 1/4"	1-10"				
11	1.410	1.560	5.313	1-0"	1-7"	1-2 3/4"	2-0"				
14	1.693	2.250	7.650	1-6 1/4"	2-3"	1-9 3/4"	2-7"				
18	2.257	4.000	13.600	2-0"	3-0"	2-4 1/2"	3-5"				

NOTES:

- FIGURES SHOWN IN CIRCLES REPRESENT BAR BEND TYPES.
- STANDARD BAR BENDS INCLUDE ONLY THOSE TYPES BELOW, INDICATED AS SUCH.
- ALL DIMENSIONS OUT-TO-OUT, EXCEPT "A" AND "C" ON STD. 180° AND 135° HOOKS.
- "J" DIMENSIONS ON 180° HOOKS TO BE SHOWN ONLY WHERE NECESSARY TO RESTRICT HOOK SIZE, OTHERWISE STANDARD 'ACI' HOOKS ARE TO BE USED.
- WHERE "J" IS NOT SHOWN, "J" WILL BE KEPT EQUAL TO OR LESS THAN "H", IT SHALL BE SHOWN.
- "H" DIMENSIONS OF STIRRUPS TO BE SHOWN AS NEEDED TO FIT WITHIN THE CONCRETE.
- UNLESS OTHERWISE NOTED, DIAMETER "D" IS THE SAME FOR ALL BENDS AND HOOKS ON A BAR (EXCEPT FOR BEND TYPES 11 AND 13).
- WHERE SLOPE DIFFERS FROM 45° OFFSET, "H" AND "K" MUST BE SHOWN.
- WHERE BARS ARE TO BE BENT MORE ACCURATELY THAN STANDARD BENDING TOLERANCES, BENDING DIMENSIONS REQUIRING CLOSER FABRICATION SHOULD HAVE LIMITS INDICATED.
- FOR RECOMMENDED DIAMETER "D", OF BENDS, HOOKS, ETC., REFER TO TABLE ABOVE, 'CRS1' OR 'ACI' TABLES WHERE APPLICABLE AND REQUIRED.
- TYPE S1-S6, S11, T1-T3 AND T6-T9 APPLICABLE TO BAR SIZES #3 THROUGH #8.

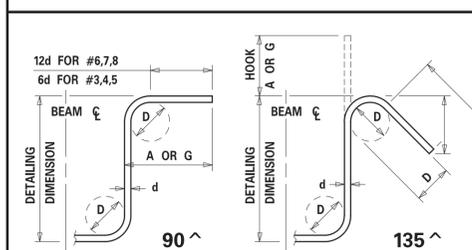
STANDARD BAR BENDS



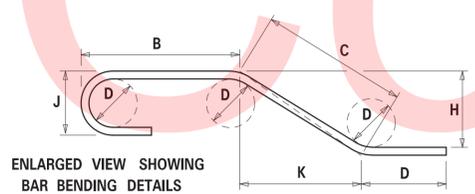
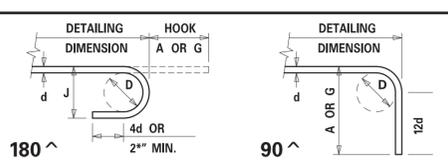
SPECIAL BAR BENDS



STIRRUP AND TIE HOOKS



180° AND 90° END HOOKS



ADDENDUMS / REVISIONS

NOT TO SCALE

BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT

T201507601

COUNTY

SUSSEX

BRIDGE NO.

BR 3-253 N&S

DESIGNED BY: JWS

CHECKED BY: DEF

SUPERSTRUCTURE REINFORCEMENT LIST

SHEET NO.

26

TOTAL SHTS.

49

GENERAL MOT NOTES

1. MAINTENANCE OF TRAFFIC DURING LANE CLOSURES SHALL CONFORM TO TYPICAL APPLICATIONS TA-33 AND TA-34 OF THE DELAWARE MUTCD.
2. ON ALL INTERSECTING STREETS APPROACHING THE WORK AREA, "ROAD WORK AHEAD." PERMANENT SIGNS SHALL BE PLACED AS SHOWN ON THESE PLANS OR AS DIRECTED BY THE ENGINEER. AN "END ROAD WORK" SIGN SHALL BE PLACED ACROSS THE STREET FROM THE "ROAD WORK AHEAD" SIGN, VISIBLE TO TRAFFIC EXITING THE WORK ZONE. THE CONTRACTOR SHALL SUPPLY TWO REMOTE PROGRAMMABLE BOARDS TO BE INSTALLED SOUTH OF CONCORD RD FOR NORTHBOUND TRAFFIC AND NORTH OF HERRING RUN RD FOR SOUTHBOUND TRAFFIC. DELDOT WILL REMOTELY PROGRAM THE MESSAGES AS NECESSARY IN COORDINATION WITH THE CONTRACTOR.
3. THIS PROJECT IS CONSIDERED A SIGNIFICANT PROJECT AS DEFINED BY DELDOT'S WORK ZONE MOBILITY PROCEDURES AND GUIDELINES. A TYPE B TRANSPORTATION MANAGEMENT PLAN (TMP) HAS BEEN PREPARED AND IS AVAILABLE FOR VIEWING BY CONTACTING THE DEPARTMENT'S SAFETY PROGRAMS MANAGER AT (302) 659-4060. ALL MONITORING REQUIREMENTS OF THE TMP SHALL BE CONDUCTED AS PER THE TMP UNLESS OTHERWISE DIRECTED BY THE ENGINEER. MODIFICATION TO THE TMP SHALL BE COMPLETED BY THE CONTRACTOR IF CHANGES TO THE TIME RESTRICTIONS OR THE TRAFFIC CONTROL PLAN ARE DESIRED. THE MODIFIED TMP SHALL BE PREPARED BY A PROFESSIONAL ENGINEER, REGISTERED IN THE STATE OF DELAWARE, AND APPROVED BY DELDOT.
4. A TYPE II TRUCK MOUNTED ATTENUATOR (TMA) SHALL BE REQUIRED ON THIS PROJECT DURING THE FOLLOWING PAVEMENT OPERATIONS:
 - TEMPORARY AND PERMANENT PAVEMENT MARKING INSTALLATION AND REMOVAL
 - PLACEMENT AND REMOVAL OF TEMPORARY CONCRETE TRAFFIC BARRIER
 - PLACEMENT AND REMOVAL OF TEMPORARY TRAFFIC CONTROL DEVICES FOR LANE CLOSURES
 - MILLING OPERATIONS
 - PAVING OPERATIONS
 - AS DIRECTED BY THE ENGINEER
 THE ROLL AHEAD DISTANCE SHALL BE AS PER THE MANUFACTURER'S RECOMMENDATIONS. THE TMA SHALL CONFORM TO THE REQUIREMENTS OF SECTION 6F OF THE DELAWARE MUTCD.
5. NO WIDE LOAD PERMIT VEHICLES SHALL BE PERMITTED ON US-13 DURING CONSTRUCTION PHASE 2 AND CONSTRUCTION PHASE 3. THE CONTRACTOR SHALL CONTACT THE DISTRICT SAFETY OFFICER AND DELDOT HAULING PERMITS OFFICE AT LEAST 14 DAYS PRIOR TO ANY PHASE.
6. THE REFLECTIVE LENSES ONLY OF ANY EXISTING PLOWABLE RAISED PAVEMENT MARKERS IN CONFLICT WITH PROPOSED TEMPORARY STRIPING SHALL BE REMOVED. PAYMENT FOR REMOVAL OF THE REFLECTIVE LENSES WILL BE INCIDENTAL TO ITEM NO. 743000 - MAINTENANCE OF TRAFFIC.

SEQUENCE OF CONSTRUCTION

CONSTRUCTION PHASE 1:

1. STAGING AND STORAGE OF EQUIPMENT AND MATERIALS SHALL BE IN THE MEDIAN, A MINIMUM OF 4' BEHIND THE EXISTING BARRIER. FOR DELIVERY AND REMOVAL OF EQUIPMENT AND MATERIALS TO THE STORAGE AREA, SET UP TRAFFIC CONTROLS IN ACCORDANCE WITH TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. ANY TEMPORARY LANE CLOSURES ASSOCIATED WITH THIS PHASE SHALL BE RESTRICTED TO 8 AM TO 3 PM, TUESDAY THROUGH THURSDAY.
2. INSTALL PERMANENT AND TEMPORARY CHANNEL DIAPHRAGMS AT PIERS AND ABUTMENTS.
3. JACK BRIDGE AND REPLACE BEARINGS, SOLE PLATES AND ANCHOR BOLTS. REMOVE TEMPORARY CHANNEL DIAPHRAGMS AT ABUTMENTS.
4. CLEAN AND PAINT STEEL GIRDERS, DIAPHRAGMS, DOWNSPOUTS AND TOP OF PILES.
5. REPAIR DETERIORATED CONCRETE ON PIERS AND ABUTMENTS (EXCLUDING BACKWALL) AS SHOWN IN PLANS.
6. REMOVE ALL FIBERGLASS PILE JACKETS AT PIER LOCATIONS.

CONSTRUCTION PHASE 2:

1. SET UP TRAFFIC CONTROLS ALONG NORTHBOUND AND SOUTHBOUND US-13 FOR A LEFT LANE CLOSURE AS PER THE PLANS. FOR INSTALLATION OF TRAFFIC CONTROL DEVICES, USE TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. INSTALL EROSION AND SEDIMENT CONTROL DEVICES.
2. REMOVE EXISTING STRIPING, PLACE TEMPORARY STRIPING AND PIN PORTABLE CONCRETE BARRIER TO DECK AND ROADWAY AS SHOWN IN PLANS.
3. EXCAVATE AND REMOVE 50' OF APPROACH ROADWAY AND ABUTMENT BACKWALL FOR THE SIDE OF BRIDGE CLOSED TO TRAFFIC. REMOVE THE JOINTS AND PORTIONS OF THE DECK AND BARRIER AS INDICATED IN THE PLANS.
4. INSTALL LINK SLAB AT PIER JOINT LOCATIONS, CONSTRUCT FULL-DEPTH PCC END DIAPHRAGM AT ABUTMENTS, REPLACE PORTION OF DECK AND BARRIER AT ABUTMENTS, AND REPLACE APPROACH ROADWAY AS SHOWN IN PLANS.
5. APPLY SILICONE BASED ACRYLIC CONCRETE SEALER TO ALL EXPOSED FACES OF THE SUBSTRUCTURE AND BARRIER, AS SHOWN IN PLANS.
6. CLEAN AND REMOVE DEBRIS FROM EXISTING BRIDGE SCUPPERS.
7. APPLY SILANE BASED CONCRETE DECK SEALER TO DECK SURFACE.
8. REMOVE TRAFFIC CONTROLS PERTAINING TO THIS PHASE. FOR REMOVAL OF TRAFFIC CONTROLS, USE TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. FILL BARRIER PIN HOLES AT THIS TIME.

CONSTRUCTION PHASE 3:

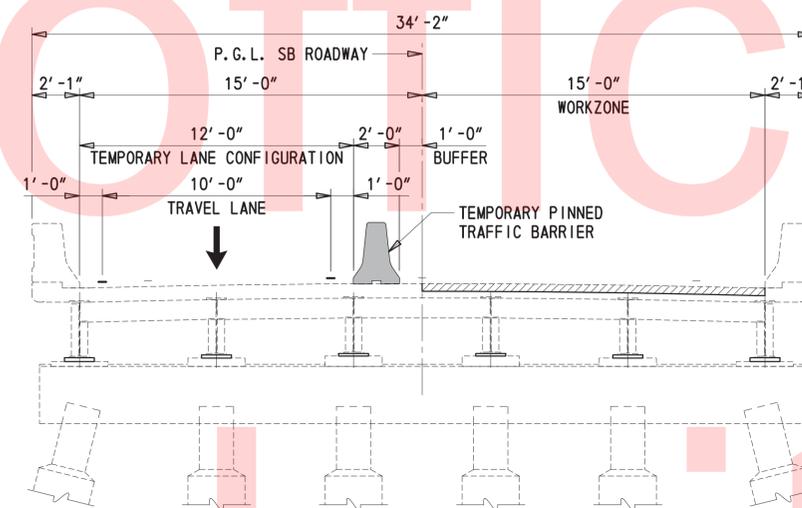
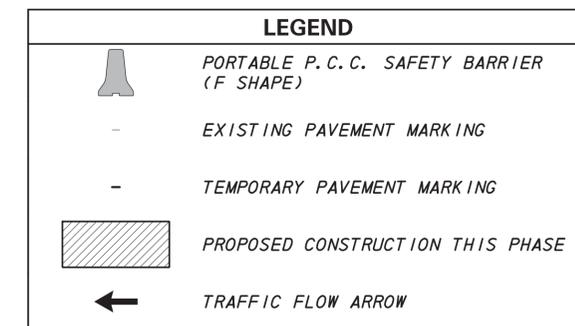
1. SET UP TRAFFIC CONTROLS ALONG NORTHBOUND AND SOUTHBOUND US-13 FOR A RIGHT LANE CLOSURE AS PER THE PLANS. FOR INSTALLATION OF TRAFFIC CONTROL DEVICES, USE TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. INSTALL EROSION AND SEDIMENT CONTROL DEVICES.
2. REMOVE EXISTING STRIPING, PLACE TEMPORARY STRIPING AND PIN PORTABLE CONCRETE BARRIER TO DECK AND ROADWAY AS SHOWN IN PLANS.
3. REPEAT STEPS 3-7 OF CONSTRUCTION PHASE 2.
4. INSTALL STONE OUTLETS ON NORTHBOUND AND SOUTHBOUND US-13, AS SHOWN IN PLANS. REGRADE AND INSTALL R-4 RIPRAP ON NORTHBOUND US-13, EAST EMBANKMENT, NORTH OF THE BRIDGE.
5. REMOVE TRAFFIC CONTROLS PERTAINING TO THIS PHASE. FOR REMOVAL OF TRAFFIC CONTROLS, USE TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. FILL BARRIER PIN HOLES AT THIS TIME.

CONSTRUCTION PHASE 4:

1. SET UP TRAFFIC CONTROLS IN ACCORDANCE WITH TYPICAL APPLICATION 33 OF THE DELAWARE MUTCD. ANY TEMPORARY LANE CLOSURES ASSOCIATED WITH THIS PHASE SHALL BE RESTRICTED TO 8 AM TO 3 PM, MONDAY THROUGH FRIDAY.
2. MILL PAVEMENT AT A 2" DEPTH FOR A TOTAL WIDTH OF 28' TO THE LIMITS SHOWN IN THE PLANS AND OVERLAY WITH A 2" BITUMINOUS WEARING COURSE.
3. INSTALL PERMANENT PAVEMENT STRIPING AS SHOWN IN PLANS.

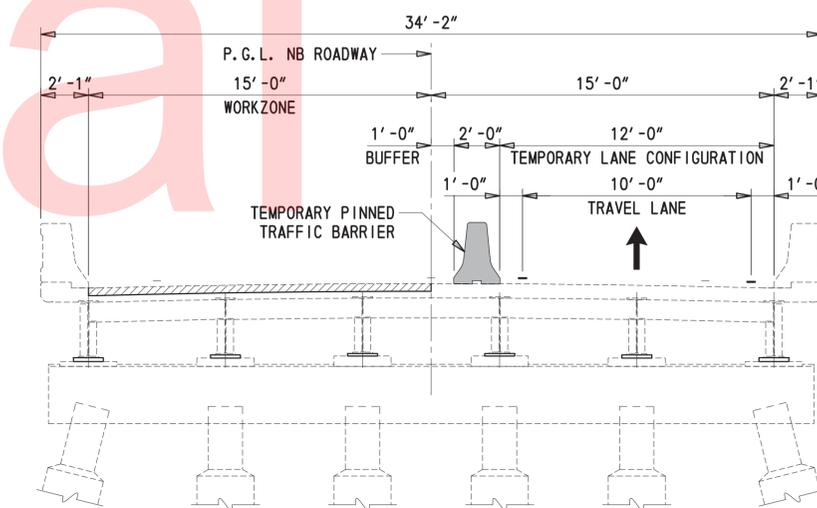
GENERAL EROSION AND SEDIMENT CONTROL NOTES:

1. EROSION AND SEDIMENT CONTROL DEVICES WILL BE MAINTAINED IN PLACE AND WILL ONLY BE REMOVED WHEN IT IS NO LONGER NECESSARY TO PROVIDE FOR THEIR DESIGNATED FUNCTIONS AS APPROVED BY THE ENGINEER.



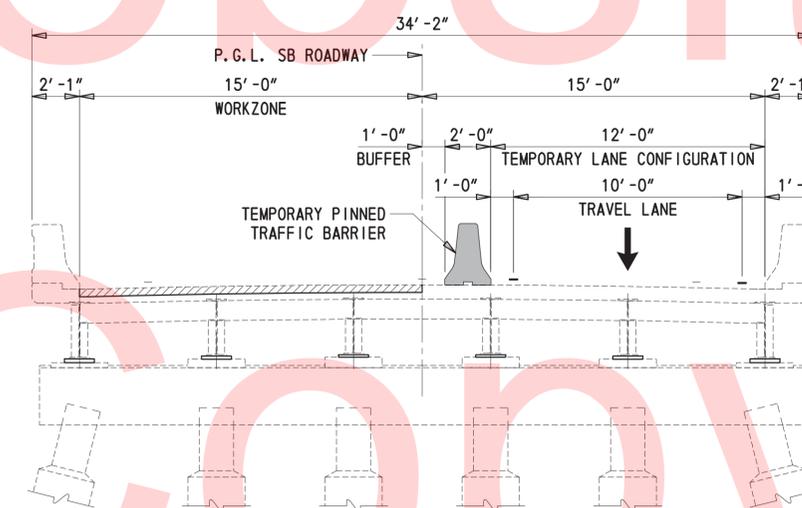
SECTION - SB BRIDGE PHASE 2

1/4" = 1'-0"



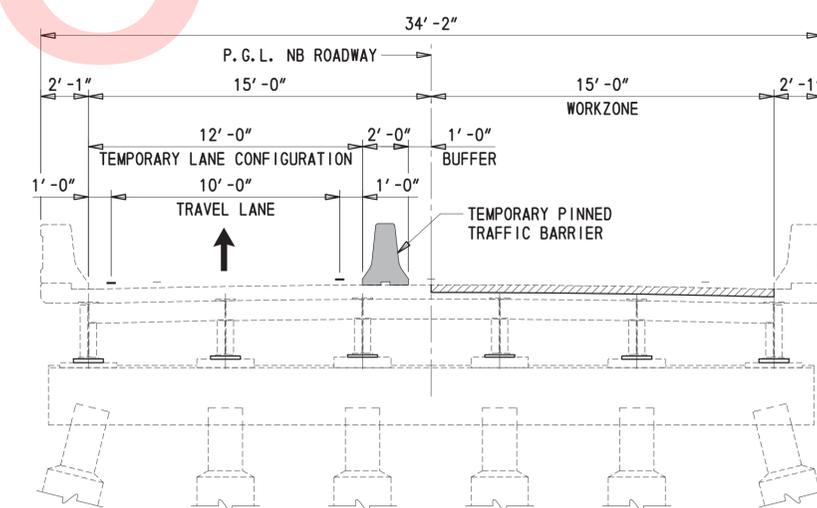
SECTION - NB BRIDGE PHASE 2

1/4" = 1'-0"



SECTION - SB BRIDGE PHASE 3

1/4" = 1'-0"



SECTION - NB BRIDGE PHASE 3

1/4" = 1'-0"

ADDENDUMS / REVISIONS

SCALE AS NOTED

BR 3-253 N&S ON US 13
OVER WILLIAMS POND

CONTRACT
T201507601
COUNTY
SUSSEX

BRIDGE NO. **BR 3-253 N&S**
DESIGNED BY: JWS
CHECKED BY: DEF

MOT NOTES
AND
TYPICAL SECTIONS

SHEET NO.
27
TOTAL SHTS.
49

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO LANE CLOSURE
(10 DAYS PRIOR TO BEGINNING OF LANE CLOSURE)

DURING LANE CLOSURE
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF LANE CLOSURE)

PCMS-1

PCMS-1

**ROADWORK
STARTING
XXXXXX**

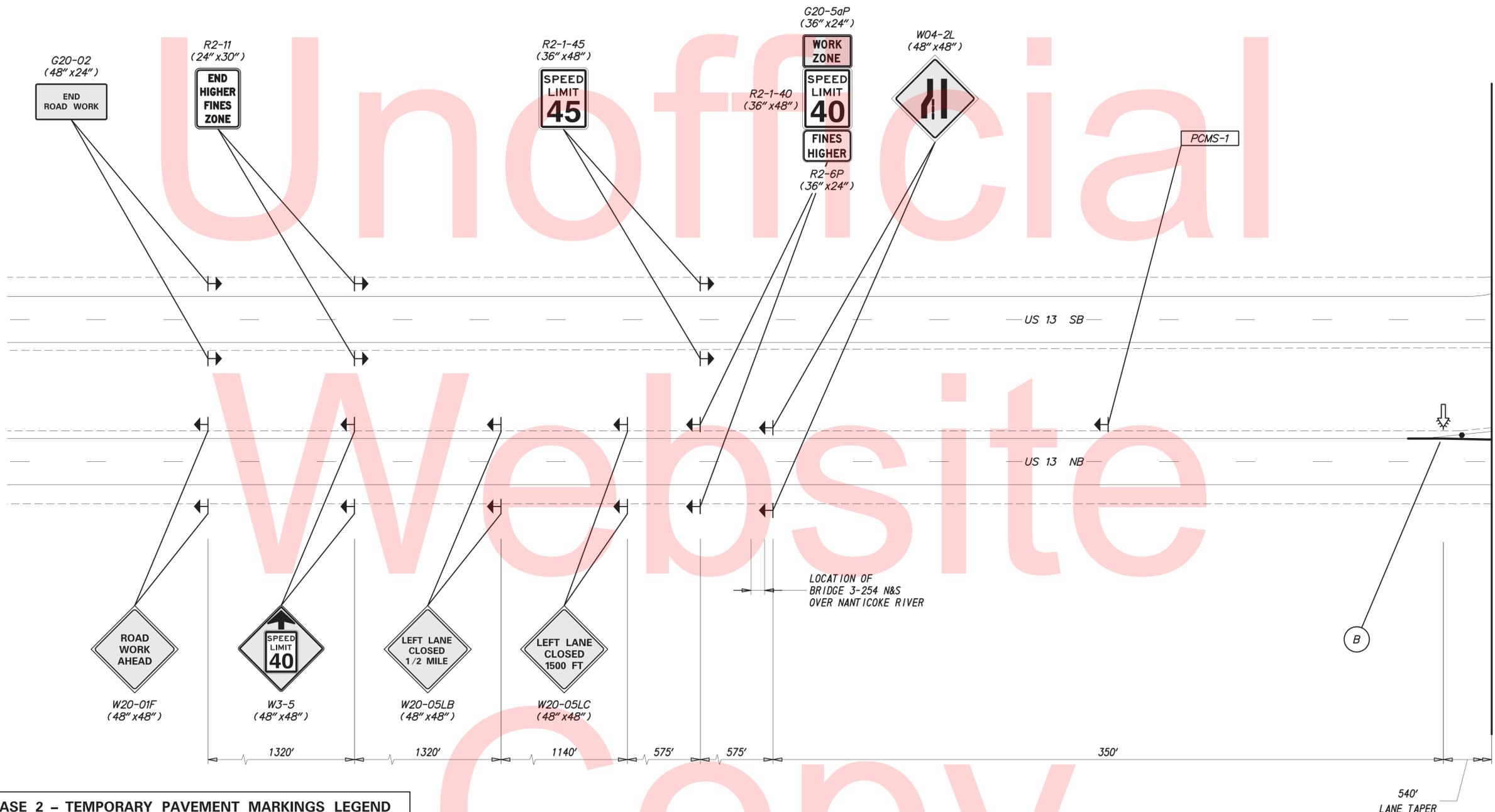
**EXPECT
DELAYS**

**LEFT
LANE
CLOSED**

**MERGE
RIGHT**

NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. MESSAGE AND LOCATIONS OF PCMS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
- 4.



PHASE 2 - TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	2,512 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	4,978 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	70 LF
	REMOVAL OF PAVEMENT STRIPING (ITEM 748530)	1,186 SF

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS001_BR3-253.dgn

NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.



3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans\CS002_BR3-253.dgn

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO LANE CLOSURE
(10 DAYS PRIOR TO BEGINNING OF LANE CLOSURE)

DURING LANE CLOSURE
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF LANE CLOSURE)

PCMS-1

PCMS-1

**ROADWORK
STARTING
XXXXXX**

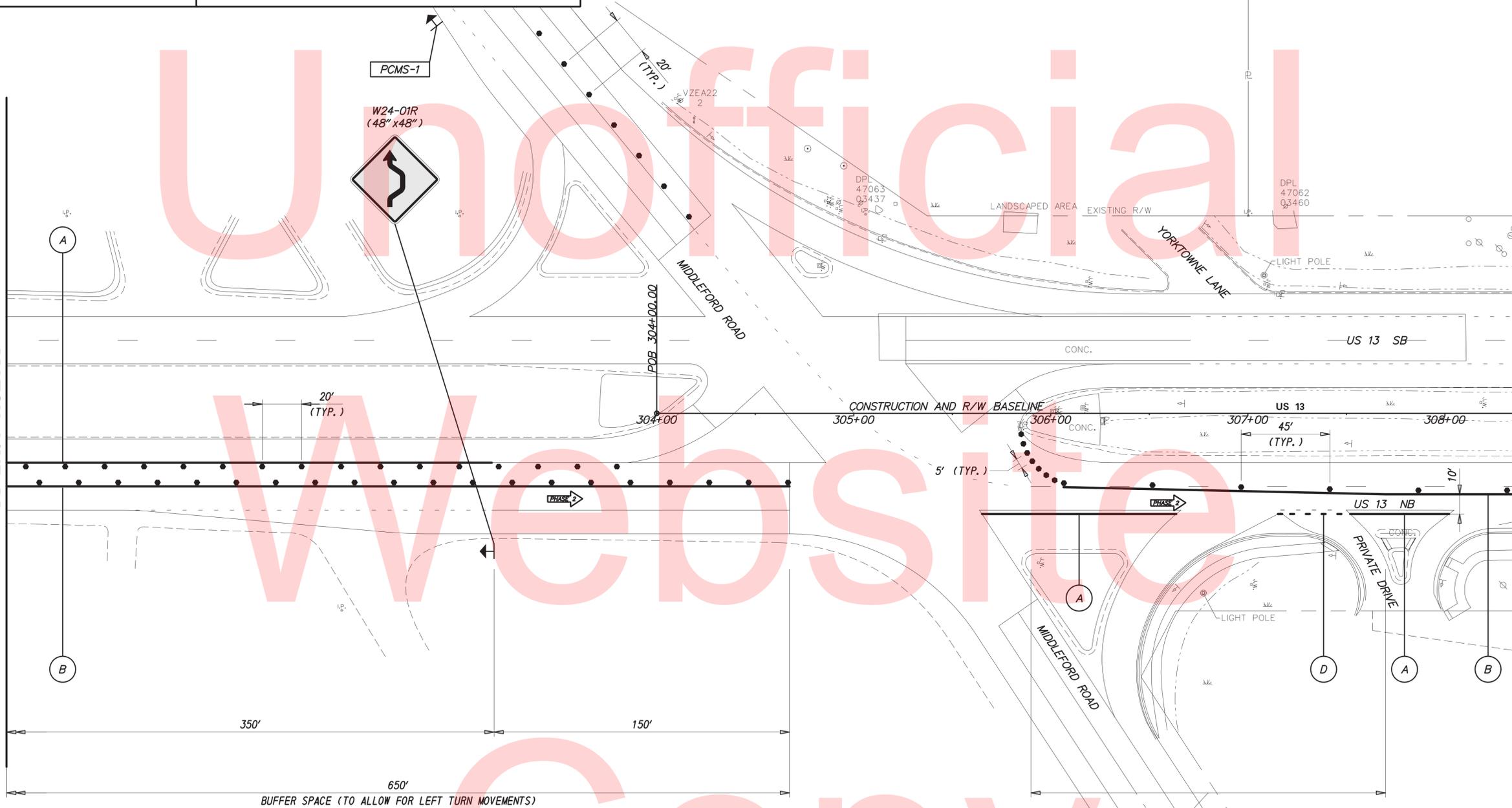
**EXPECT
DELAYS**

**LEFT
LANE
CLOSED**

**MERGE
RIGHT**

MATCH LINE BB

MATCH LINE CC



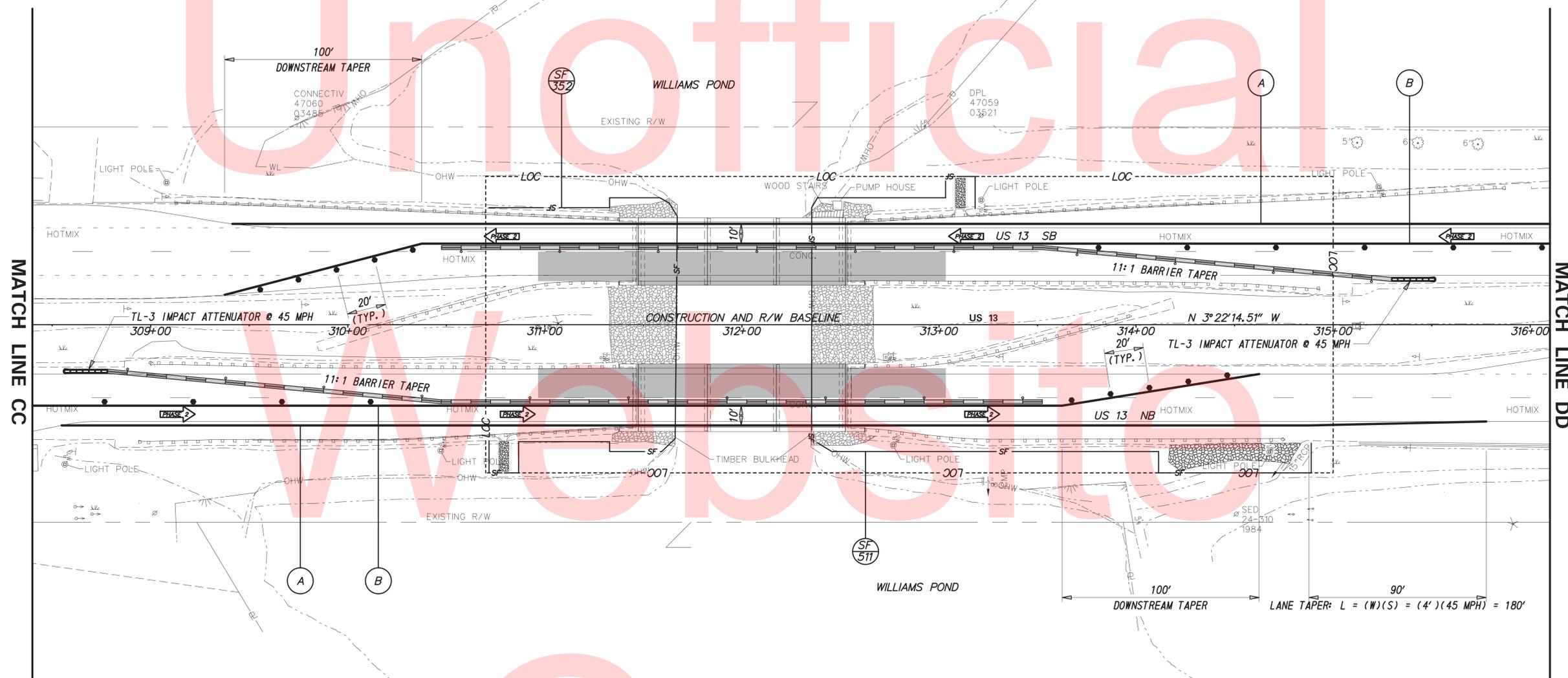
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. SIGNAL CHANGES AT INTERSECTION SHALL BE COORDINATED WITH TRAFFIC SIGNAL CONSTRUCTION AND TMC.
4. MESSAGE AND LOCATIONS OF PCMS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.
5. LEFT-MOST LEFT TURN LANE ON EASTBOUND MIDDLEFORD ROAD WILL BE CLOSED DURING THIS PHASE OF CONSTRUCTION. (DRUMS WILL CONTINUE FULL LENGTH OF TURN LANE, FOR A TOTAL LENGTH OF 400', AT 10-FOOT SPACING.) LEFT TURN SIGNAL HEAD ASSOCIATED WITH THIS MOVEMENT SHALL BE COVERED. THE CONTRACTOR SHALL COORDINATE WITH DELDOT SIGNAL MAINTENANCE TO COMPLETE THIS WORK.
6. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.

LANE TAPER: $L = (N)(S) = (4')(45 \text{ MPH}) = 180'$

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS003_BR3-253.dgn

Unofficial



MATCH LINE CC

MATCH LINE DD

Copy

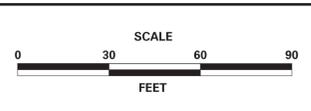
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. STAGING AND STORAGE OF EQUIPMENT AND MATERIALS SHALL BE IN THE MEDIAN, A MINIMUM OF 4' BEHIND THE EXISTING GUARDRAIL.
4. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.

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DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 2

SHEET NO. 31
TOTAL SHTS. 49

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO LANE CLOSURE
(10 DAYS PRIOR TO BEGINNING OF LANE CLOSURE)

DURING LANE CLOSURE
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF LANE CLOSURE)

PCMS-1

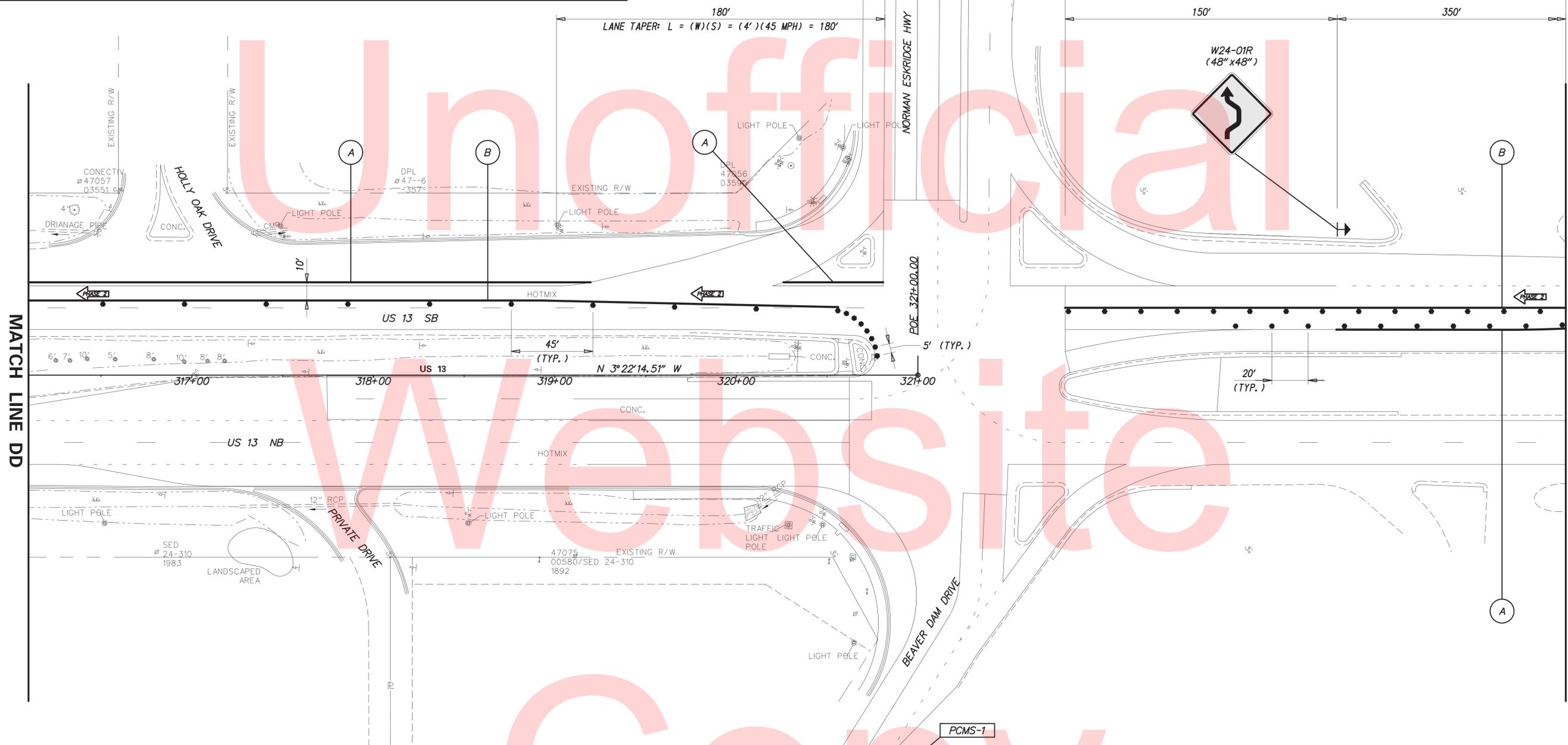
PCMS-1

ROADWORK STARTING
XXXXXX

EXPECT DELAYS

LEFT LANE CLOSED

MERGE RIGHT



MATCH LINE DD

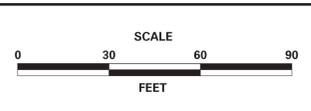
MATCH LINE EE

NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. SIGNAL CHANGES AT INTERSECTION SHALL BE COORDINATED WITH TRAFFIC SIGNAL CONSTRUCTION AND TMC.
4. MESSAGE AND LOCATIONS OF PCMS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.
5. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.

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ADDENDUMS / REVISIONS

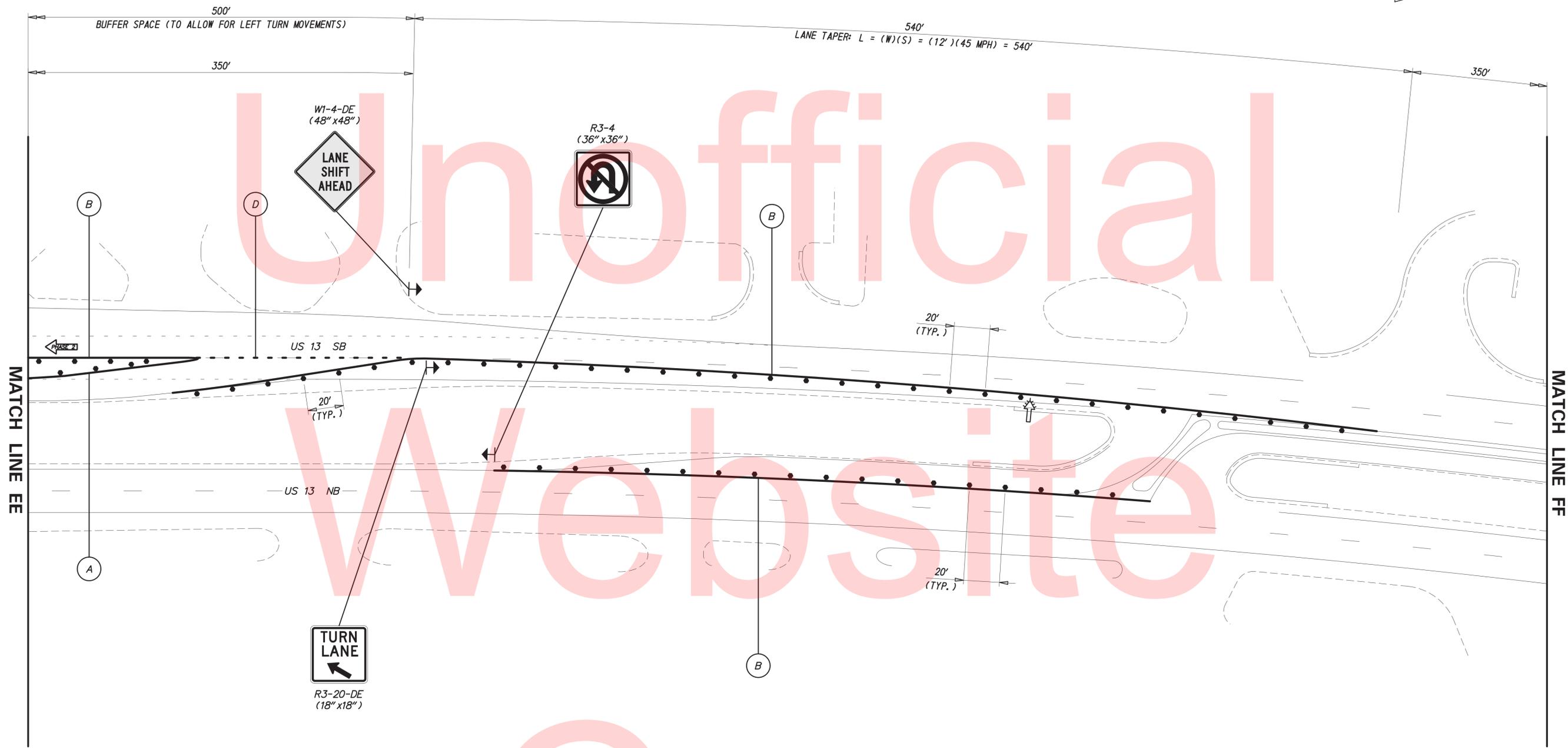


BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 2

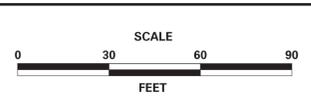
SHEET NO.	32
TOTAL SHTS.	49



- NOTES:
1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
 2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
 3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.

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ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 2

SHEET NO.	33
TOTAL SHTS.	49

PORTABLE CHANGEABLE MESSAGE SIGNS

PRIOR TO LANE CLOSURE
(10 DAYS PRIOR TO BEGINNING OF LANE CLOSURE)

DURING LANE CLOSURE
(DISPLAY FOR 5 DAYS AFTER IMPLEMENTATION OF LANE CLOSURE)

PCMS-1

PCMS-1

**ROADWORK
STARTING
XXXXXX**

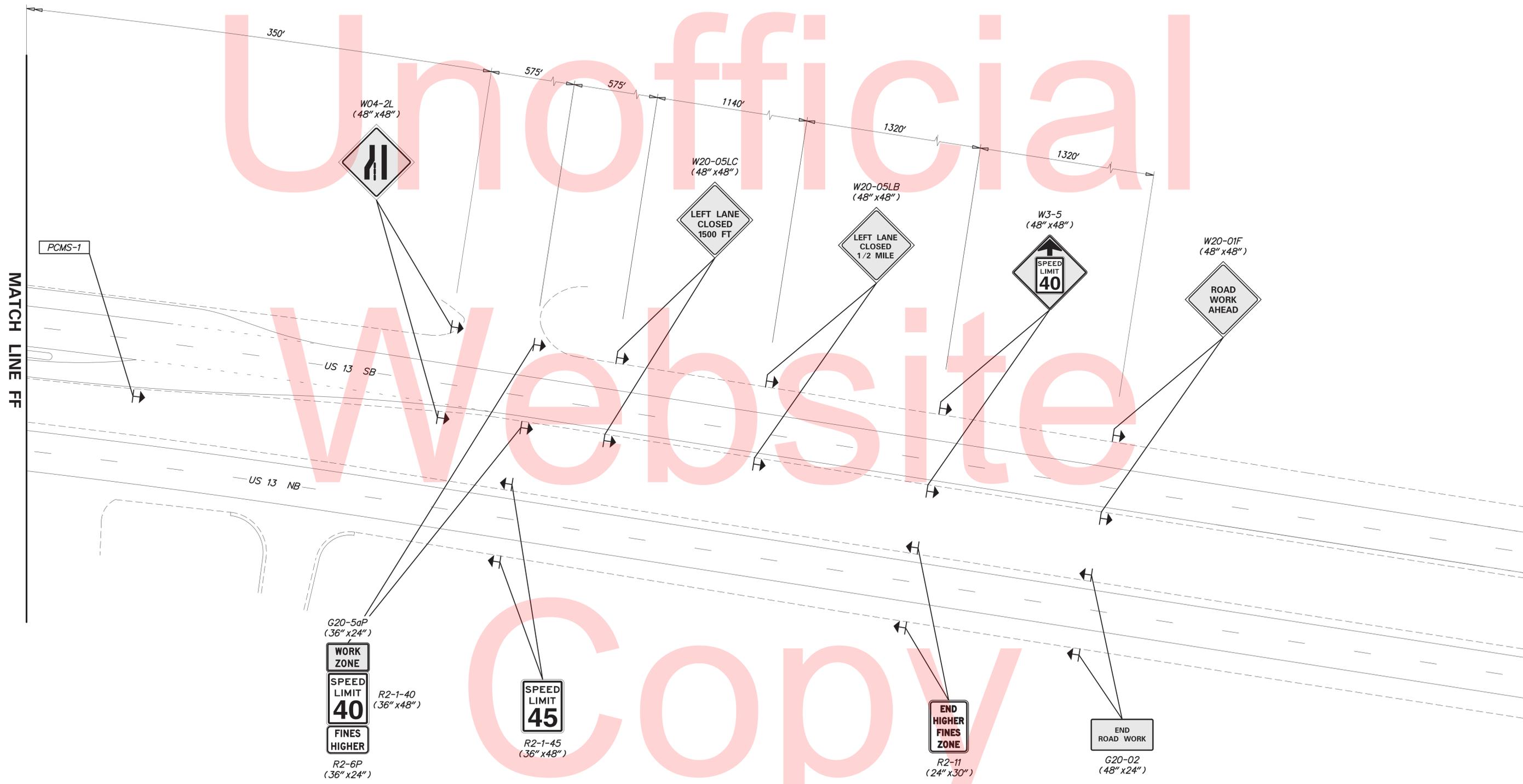
**EXPECT
DELAYS**

**LEFT
LANE
CLOSED**

**MERGE
RIGHT**

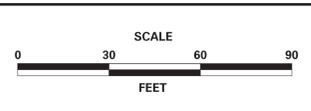
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. MESSAGE AND LOCATIONS OF PCMS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.
4. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.



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ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

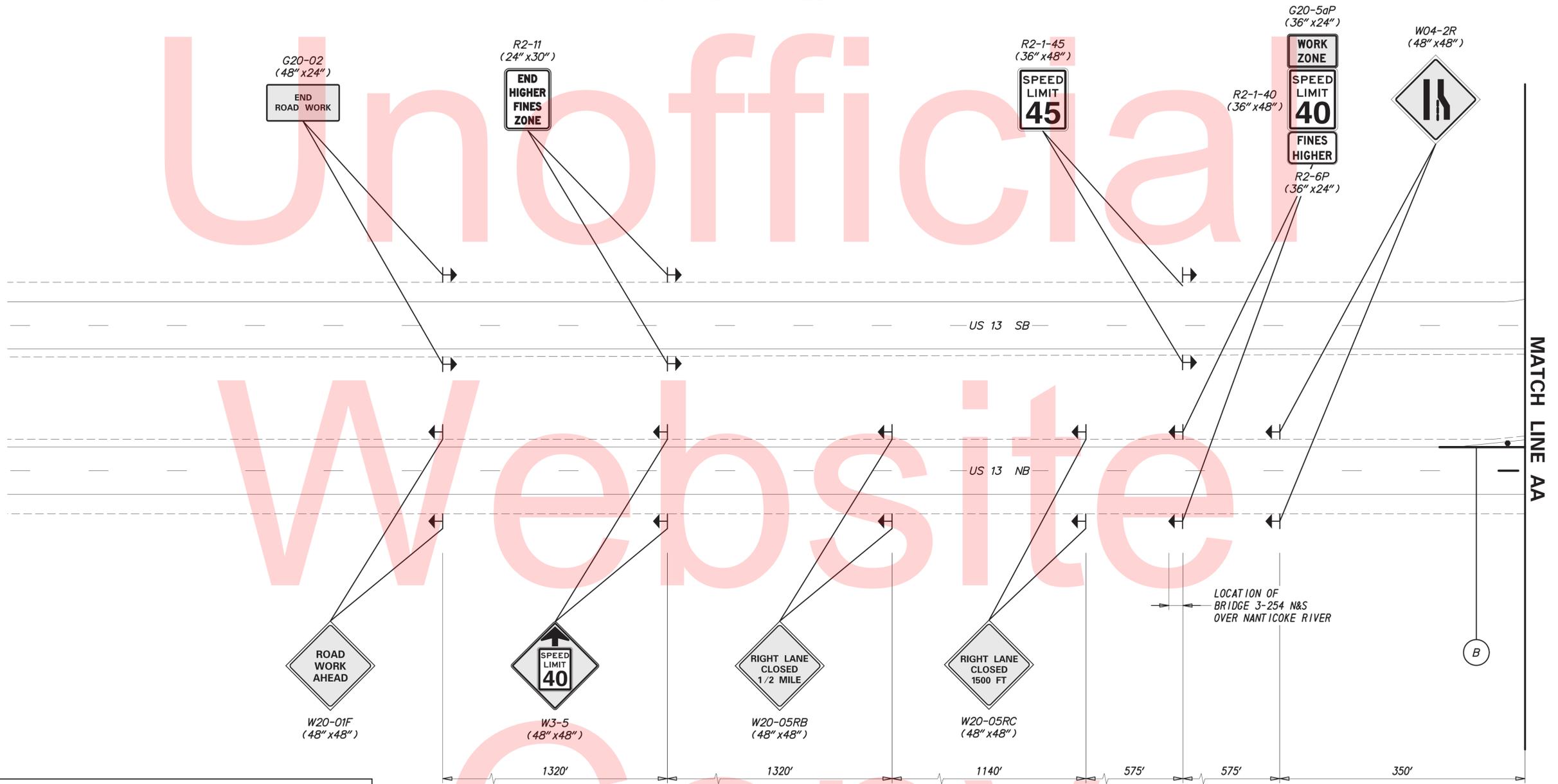
CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 2

SHEET NO. 34
TOTAL SHTS. 49

NOTES:

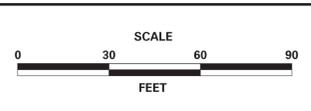
1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
4. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.



PHASE 3 - TEMPORARY PAVEMENT MARKINGS LEGEND		
SYMBOL	ITEM	QUANTITY
(A)	4" SOLID WHITE TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	4, 670 LF
(B)	4" SOLID YELLOW TEMPORARY PAINT PAVEMENT STRIPING (ITEM 748019)	2, 302 LF
(D)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 2' LINE & 6' GAP (ITEM 748019)	228 LF
(E)	4" DASHED WHITE TEMPORARY PAINT PAVEMENT STRIPING, 10' LINE & 30' GAP (ITEM 748019)	60 LF
	REMOVAL OF PAVEMENT STRIPING (ITEM 748530)	2, 347 SF

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS001_BR3-253_PHASE-2.dgn

ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

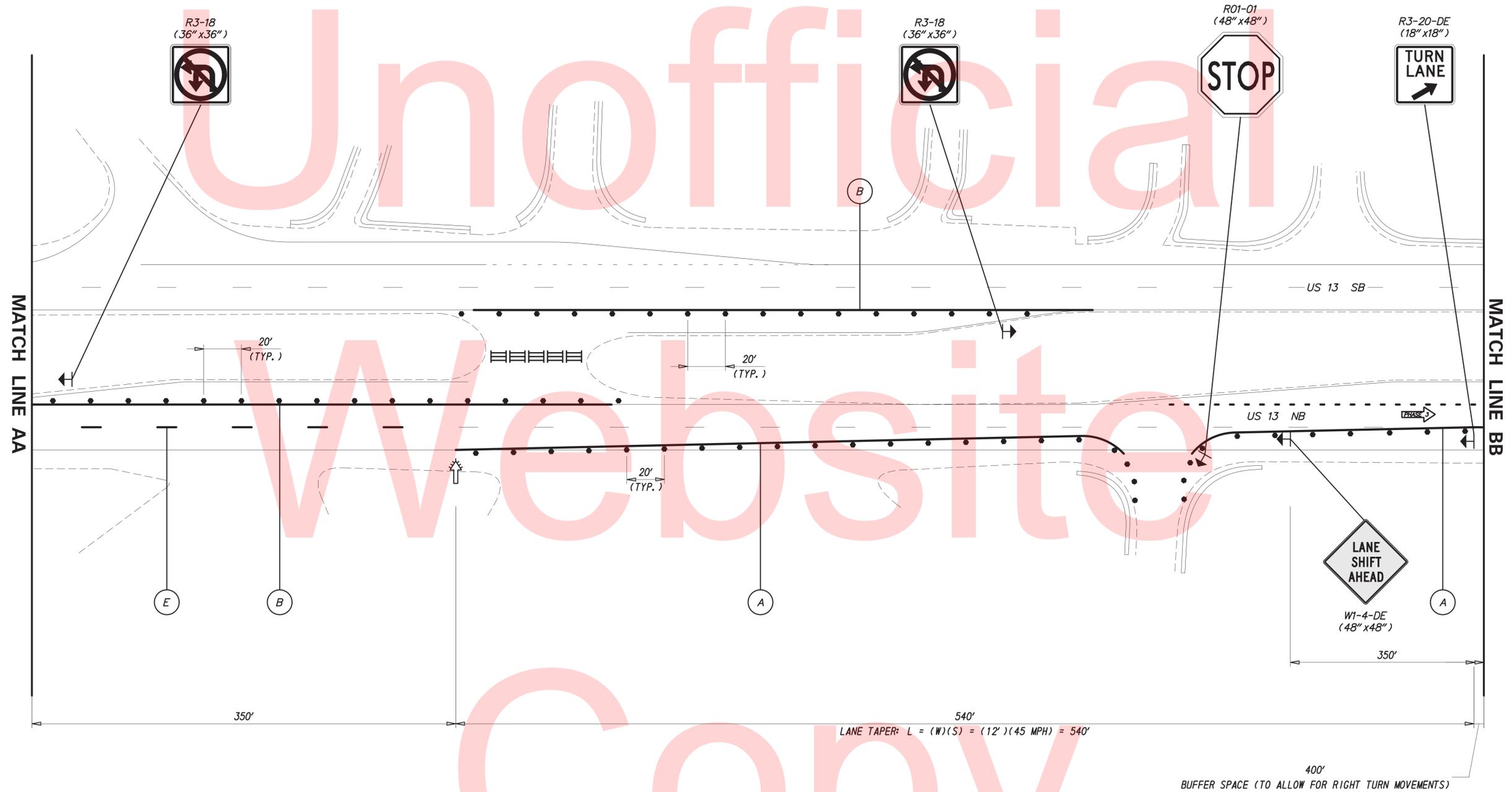
CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 3

SHEET NO.	35
TOTAL SHTS.	49

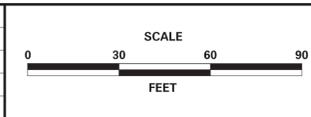
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
4. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.

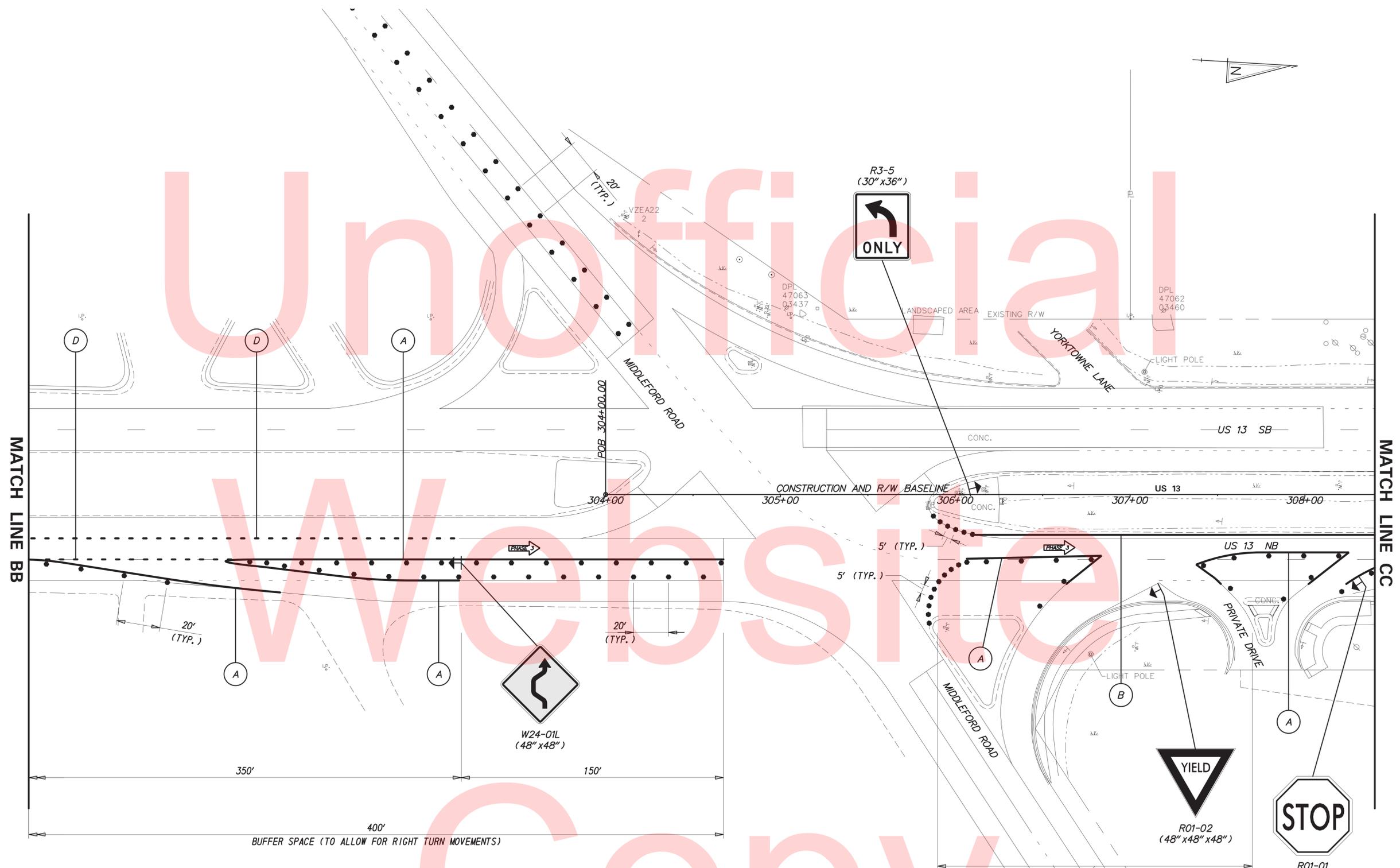


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ADDENDUMS / REVISIONS	



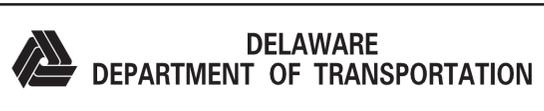
CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF



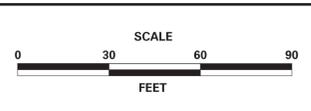
- NOTES:
1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
 2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
 3. SIGNAL CHANGES AT INTERSECTION SHALL BE COORDINATED WITH TRAFFIC SIGNAL CONSTRUCTION AND TMC.
 4. RIGHT-MOST LEFT TURN LANE ON EASTBOUND MIDDLEFORD ROAD WILL BE CLOSED DURING THIS PHASE OF CONSTRUCTION. (DRUMS WILL CONTINUE FULL LENGTH OF TURN LANE, FOR A TOTAL LENGTH OF 400', AT 10-FOOT SPACING.) LEFT TURN SIGNAL HEAD ASSOCIATED WITH THIS MOVEMENT SHALL BE COVERED. THE CONTRACTOR SHALL COORDINATE WITH DELDOT SIGNAL MAINTENANCE TO COMPLETE THIS WORK.
 5. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
 6. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.

LANE TAPER: $L = (W)(S) = (4')(45 \text{ MPH}) = 180'$

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS003_BR3-253_PHASE-2.dgn



ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

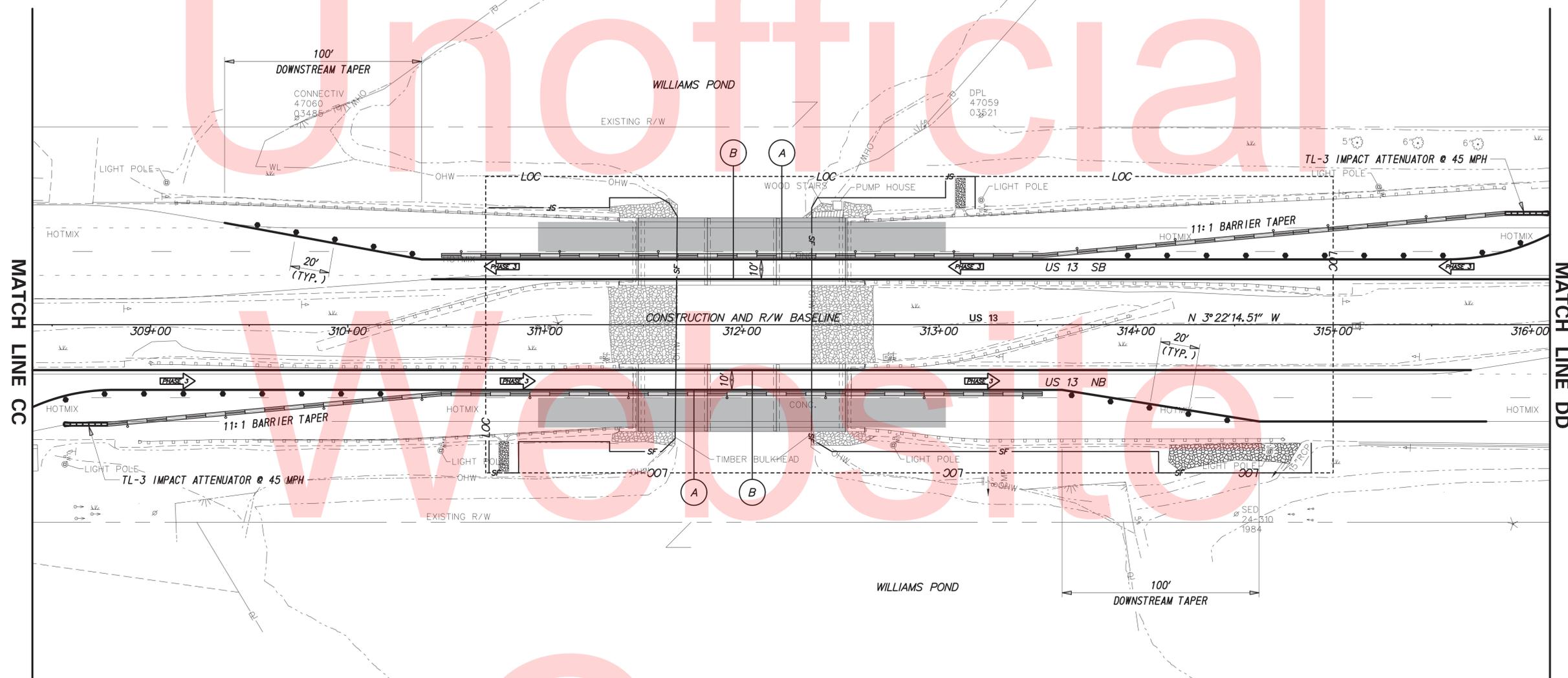
CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 3

SHEET NO. 37
TOTAL SHTS. 49

Unofficial

Workfiles

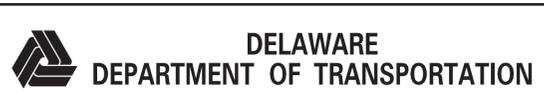
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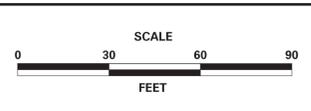
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. STAGING AND STORAGE OF EQUIPMENT AND MATERIALS SHALL BE IN THE MEDIAN, A MINIMUM OF 4' BEHIND THE EXISTING GUARDRAIL.
4. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
5. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.

3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans\CS004_BR3-253_PHASE-2.dgn



ADDENDUMS / REVISIONS	



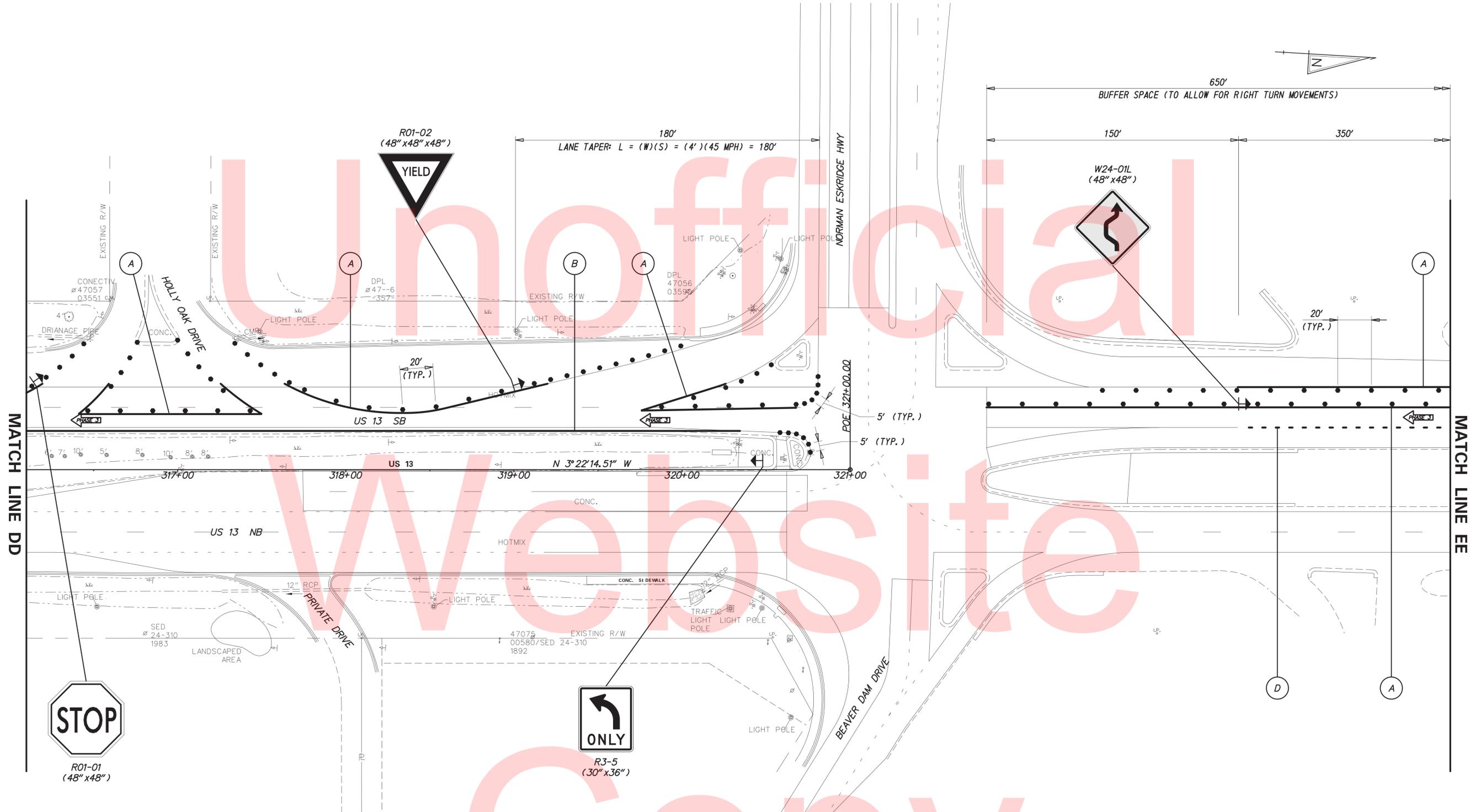
BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 3

SHEET NO. 38
TOTAL SHTS. 49

3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS005_BR3-253_PHASE-2.dgn

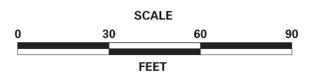


NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION. ALL LOCATIONS SHOWN ARE APPROXIMATE.
2. SIGNAL CHANGES AT INTERSECTION SHALL BE COORDINATED WITH TRAFFIC SIGNAL CONSTRUCTION AND TMC.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
4. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.



ADDENDUMS / REVISIONS	

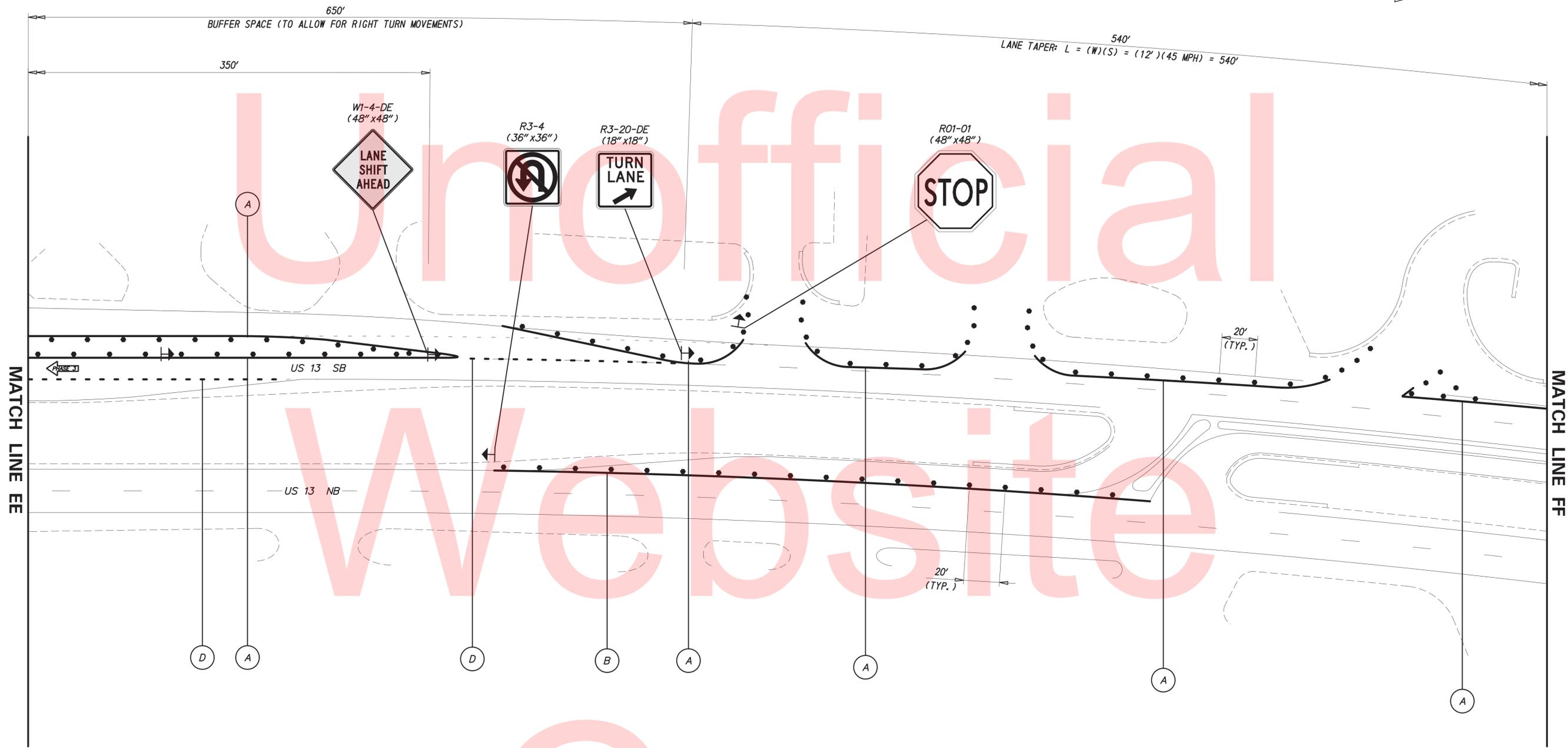


**BR 3-253 N&S ON US 13
OVER WILLIAMS POND**

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

**CONSTRUCTION PHASING,
M.O.T. AND EROSION
CONTROL PLAN - PHASE 3**

SHEET NO. 39
TOTAL SHTS. 49

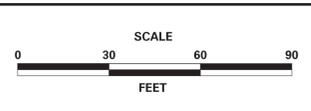


NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
4. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE AND LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.

3/2/2016 T:\Dover\Projects\4250009\10\600_CADD\Plans\CS006_BR3-253_PHASE-2.dgn

ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

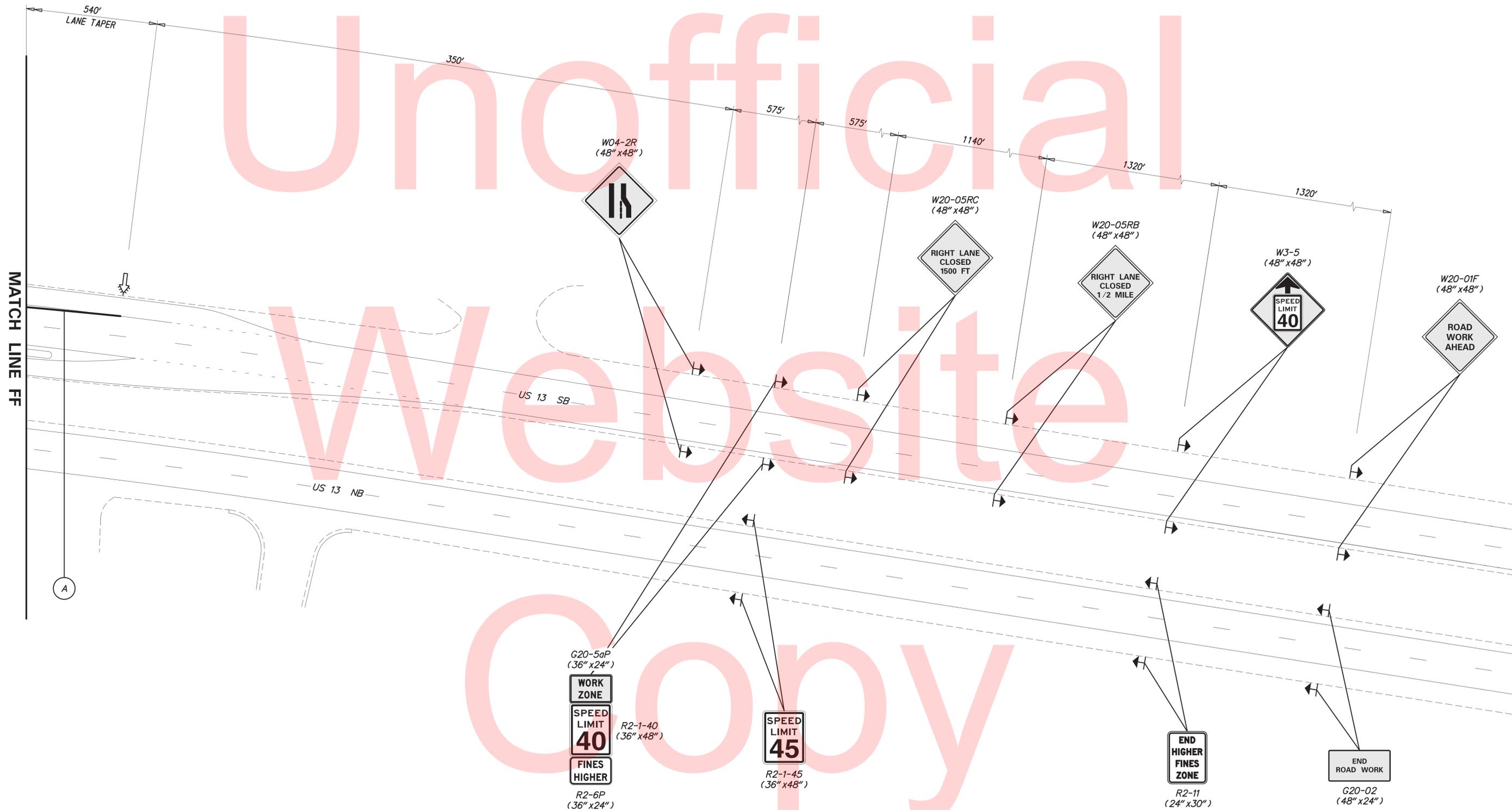
CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 3

SHEET NO. 40
TOTAL SHTS. 49

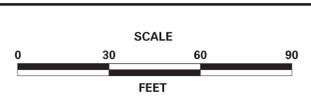
NOTES:

1. PLACE ALL PERMANENT WARNING SIGNS (ITEM NO. 743024) AS SHOWN ON THIS SHEET PRIOR TO THE BEGINNING OF CONSTRUCTION.
2. ALL LOCATIONS SHOWN ARE APPROXIMATE.
3. PERMANENT REGULATORY SIGNS THAT ARE BEING REPLACED BY TEMPORARY REGULATORY SIGNS SHALL BE COVERED OR REMOVED FOR THE DURATION OF THE CONSTRUCTION PHASE. SIGNS SHALL BE RESTORED TO THE ORIGINAL LOCATION PRIOR TO REMOVING TRAFFIC CONTROL.
4. MESSAGE BOARDS SHALL BE PLACED IN BOTH DIRECTIONS OF US-13, 10 DAYS PRIOR TO ANY CHANGE IN THE TRAFFIC PATTERN. MESSAGE BOARD LOCATIONS OF MESSAGE BOARDS SHALL BE COORDINATED WITH DISTRICT SAFETY OFFICER PRIOR TO IMPLEMENTATION.



3/2/2016 T:\Dover\Projects\425009\10_600_CADD\Plans\CS007_BR3-253_PHASE-2.dgn

ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

CONSTRUCTION PHASING, M.O.T. AND EROSION CONTROL PLAN - PHASE 3

SHEET NO. 41
TOTAL SHTS. 49



END MILL AND OVERLAY.
SAWCUT TO 2" DEPTH.
SAWCUTTING TO BE PAID
UNDER ITEM NO. 762001.

113'

RIVERSIDE DRIVE

(A)

(B)

(A)

(E)

(D)

(A)

(D)

(A)

MATCH LINE AA

BEGIN MILL AND OVERLAY.
SAWCUT TO 2" DEPTH.
SAWCUTTING TO BE PAID
UNDER ITEM NO. 762001.

141'

SEAFORD ICE &
COLD STORAGE ENTRANCE

(B)

(D)

(A)

(A)

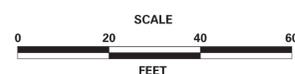
(E)

(B)

PAVEMENT MARKINGS LEGEND

SYMBOL	ITEM	QUANTITY
(A)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" SOLID (ITEM 748548)	7,435 LF
(B)	EPOXY RESIN PAINT PAVEMENT STRIPING, YELLOW 5" SOLID (ITEM 748548)	4,602 LF
(D)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" DOTTED - 2' LINE & 6' GAP (ITEM 748548)	820 LF
(E)	EPOXY RESIN PAINT PAVEMENT STRIPING, WHITE 5" BROKEN - 10' LINE & 30' GAP (ITEM 748548)	1,790 LF
(C)	16" SOLID WHITE ALKYD THERMOPLASTIC PAVEMENT STRIPING (ITEM 748015)	107 SF

ADDENDUMS / REVISIONS



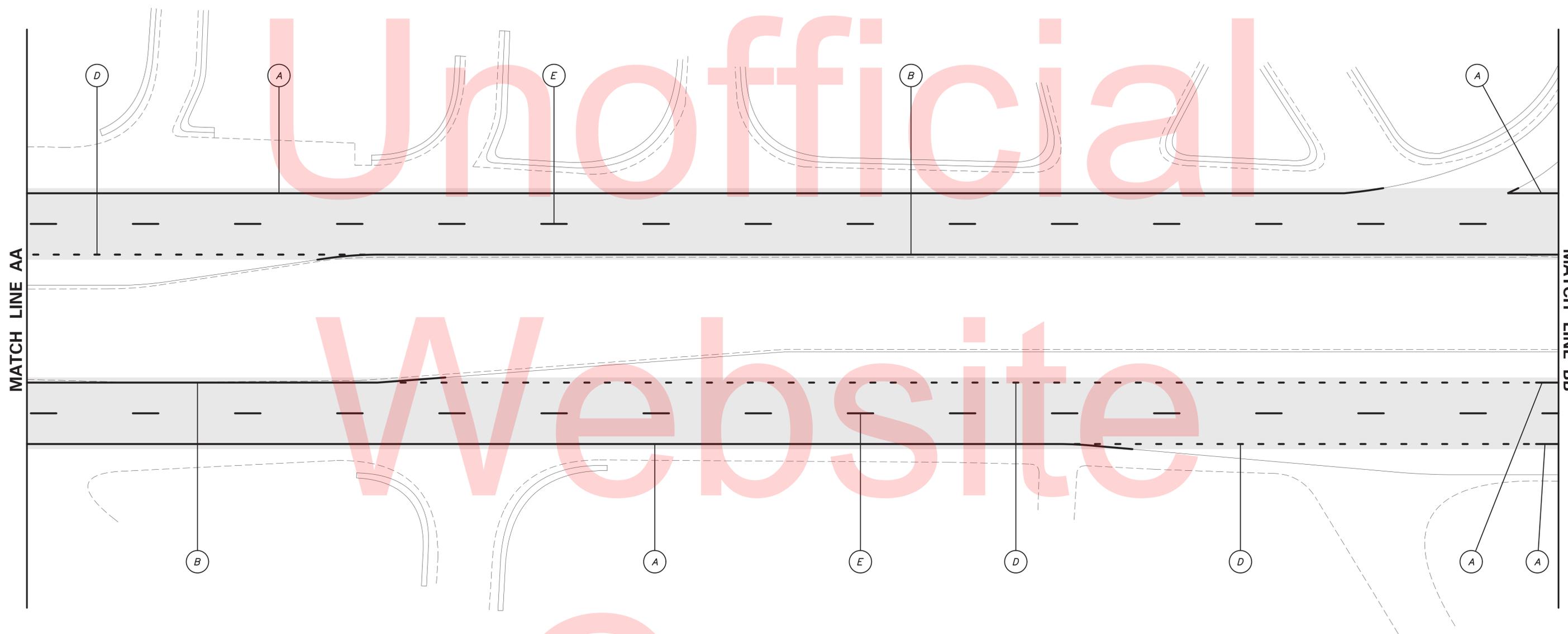
**BR 3-253 N&S ON US 13
OVER WILLIAMS POND**

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

**SIGNING & STRIPING
PLAN**

SS-01
SHEET NO.
42
TOTAL SHTS.
49

3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans\SS01_BR3-253.dgn

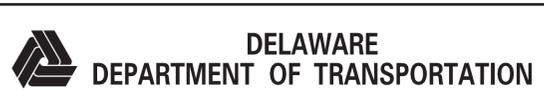


MATCH LINE AA

MATCH LINE BB

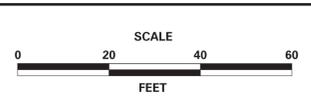
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3/2/2016 T:\Dover\Projects\4250009\10\600_CADD\Plans_SS002_BR3-253.dgn



DELAWARE DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

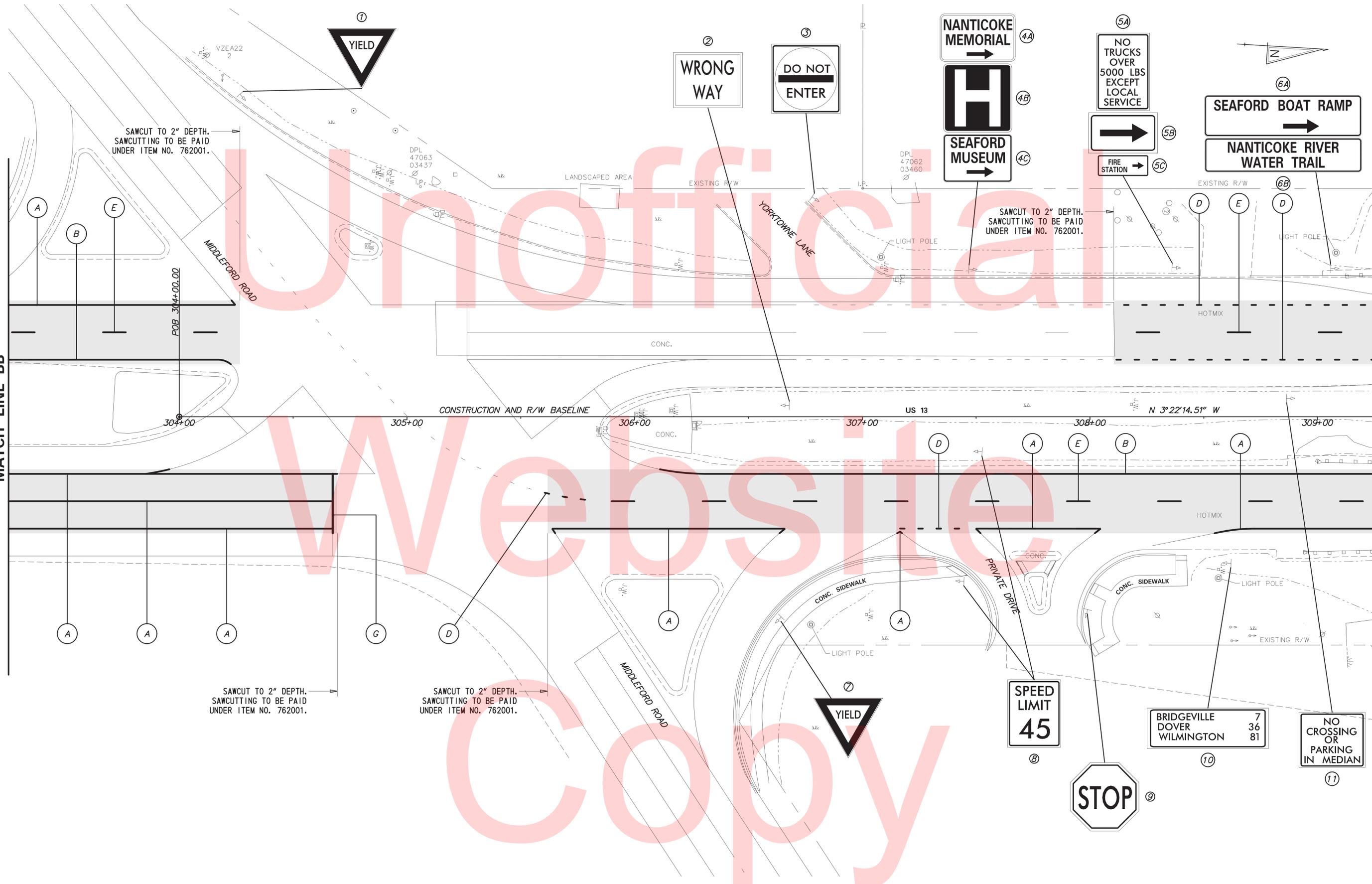
SIGNING & STRIPING PLAN

SS-02
SHEET NO.
43
TOTAL SHTS.
49

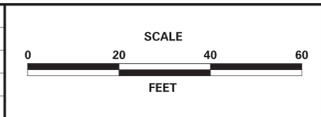
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MATCH LINE BB

MATCH LINE CC



ADDENDUMS / REVISIONS	



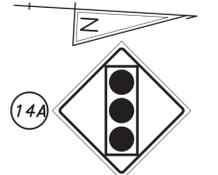
**BR 3-253 N&S ON US 13
OVER WILLIAMS POND**

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

**SIGNING & STRIPING
PLAN**

SS-03
SHEET NO.
44
TOTAL SHTS.
49

MIDDLEFORD 12

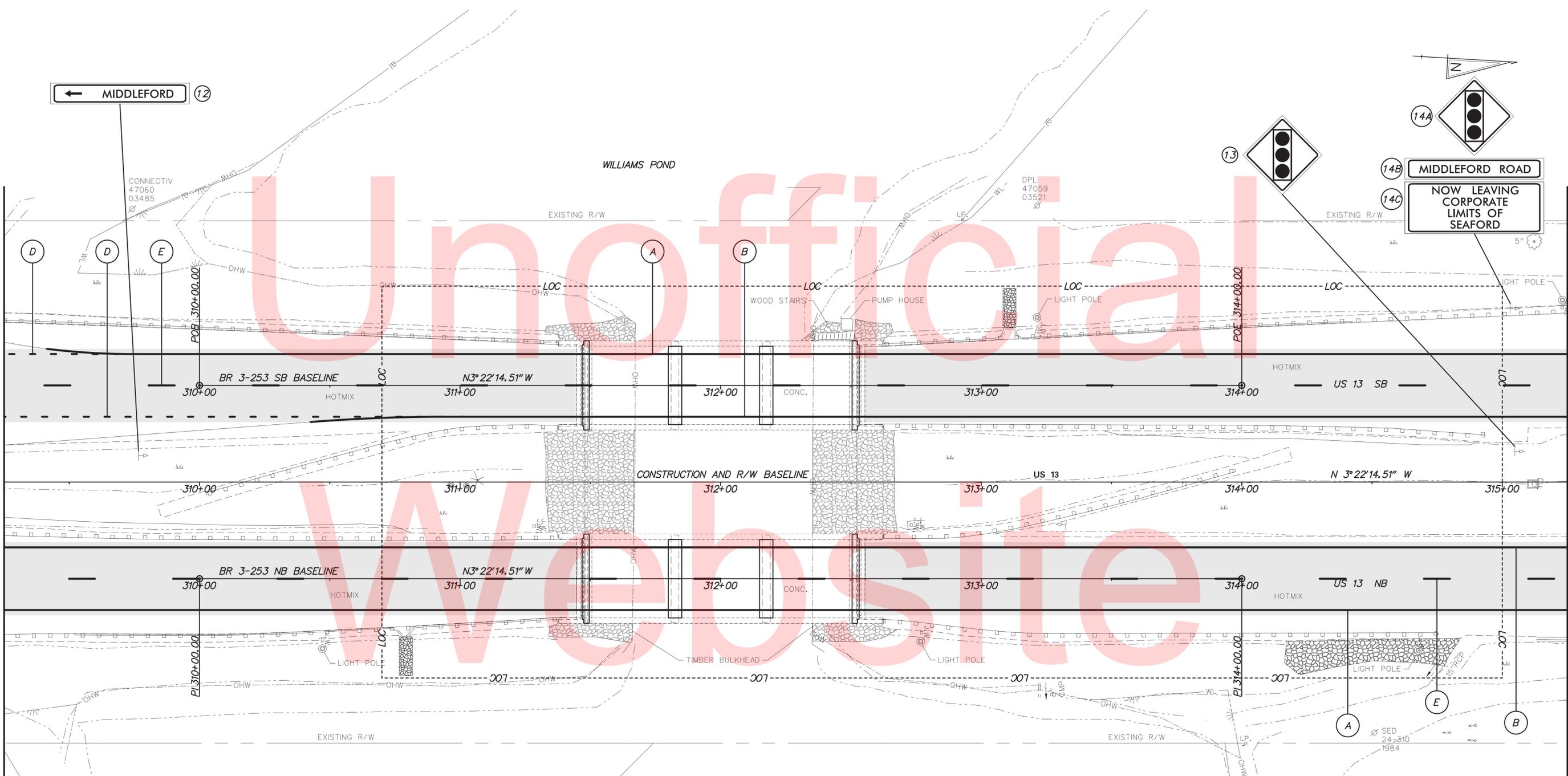


14A
14B MIDDLEFORD ROAD
14C NOW LEAVING CORPORATE LIMITS OF SEAFORD

WILLIAMS POND

MATCH LINE CC

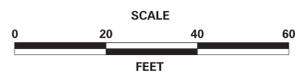
MATCH LINE DD



NOTES:

1. SAWCUT AT EACH BRIDGE ABUTMENT TO 2" DEPTH. SAWCUTTING TO BE PAID UNDER ITEM NO. 762001.

ADDENDUMS / REVISIONS



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT	BRIDGE NO.	BR 3-253 N&S
T201507601	DESIGNED BY:	JWS
COUNTY	CHECKED BY:	DEF
SUSSEX		

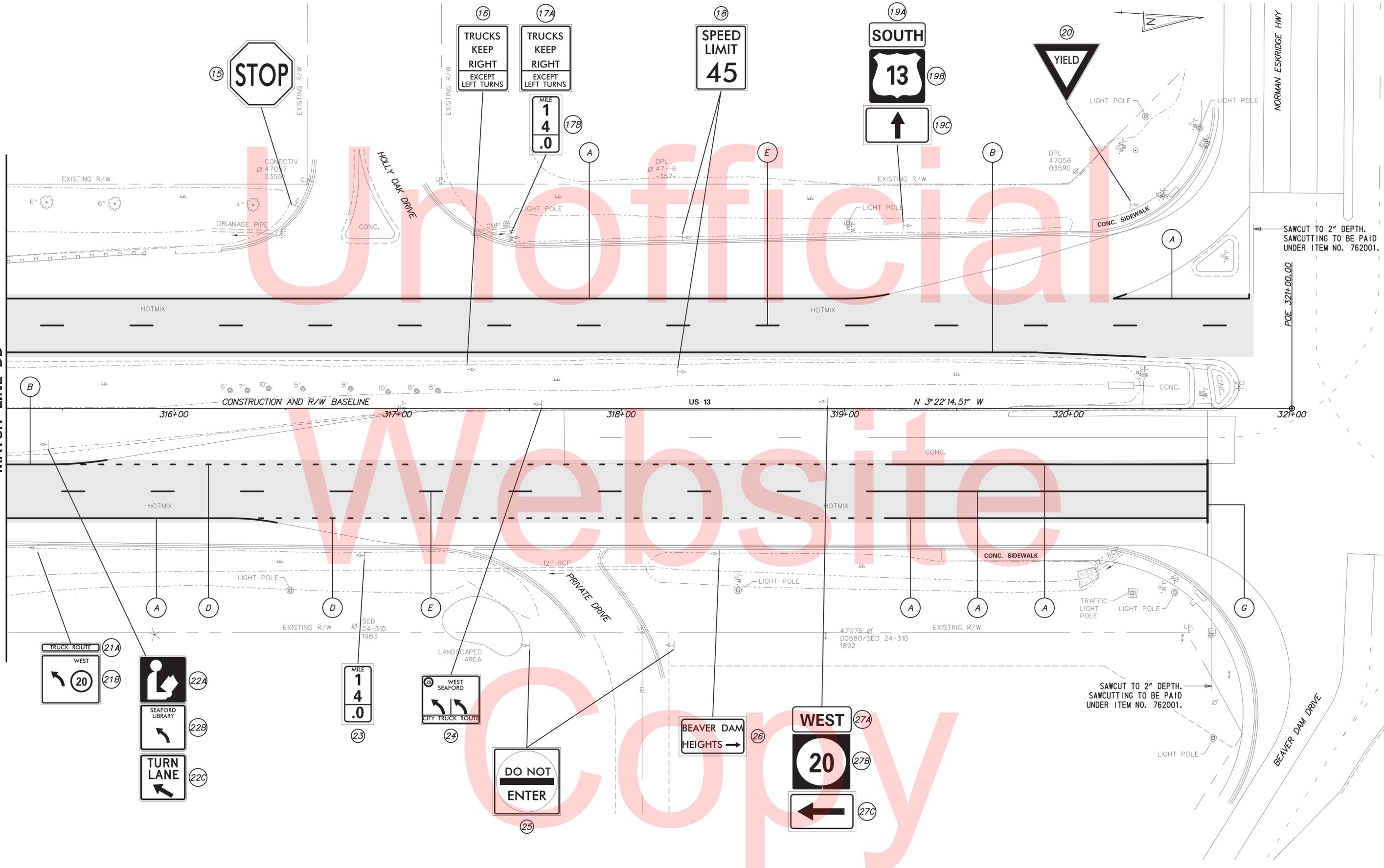
SIGNING & STRIPING PLAN

SS-04
SHEET NO.
45
TOTAL SHTS.
49

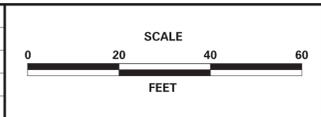
3/2/2016 T:\Dover\Projects\4250009\10\600_CADD\Plans_SS005_BR3-253.dgn

MATCH LINE DD

MATCH LINE EE



ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

SIGNING & STRIPING PLAN

SS-05
SHEET NO. 46
TOTAL SHTS. 49



SAWCUT TO 2" DEPTH.
SAWCUTTING TO BE PAID
UNDER ITEM NO. 762001.

A

A

A

D

E

D

B

G

MATCH LINE EE

MATCH LINE FF

SAWCUT TO 2" DEPTH.
SAWCUTTING TO BE PAID
UNDER ITEM NO. 762001.

B

E

A

D

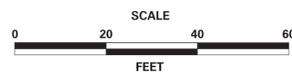
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DELAWARE
DEPARTMENT OF TRANSPORTATION

ADDENDUMS / REVISIONS



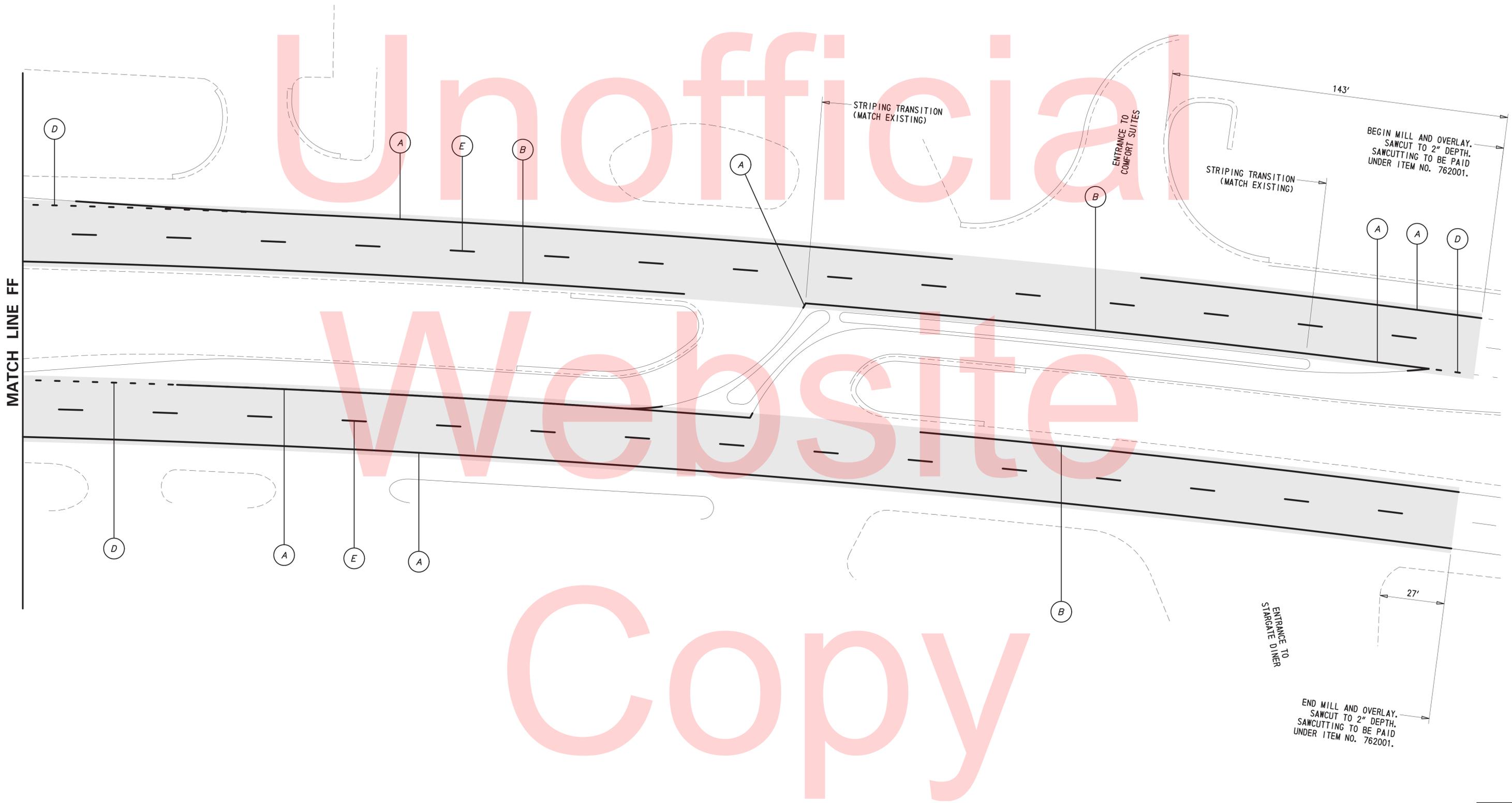
BR 3-253 N&S ON US 13
OVER WILLIAMS POND

CONTRACT
T201507601
COUNTY
SUSSEX

BRIDGE NO. BR 3-253 N&S
DESIGNED BY: JWS
CHECKED BY: DEF

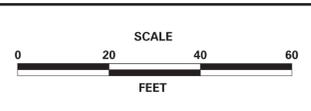
SIGNING & STRIPING
PLAN

SS-06
SHEET NO.
47
TOTAL SHTS.
49



3/2/2016 T:\Dover\Projects\425009\10\600_CADD\Plans_SS007_BR3-253.dgn

ADDENDUMS / REVISIONS	



BR 3-253 N&S ON US 13 OVER WILLIAMS POND

CONTRACT T201507601	BRIDGE NO. BR 3-253 N&S
COUNTY SUSSEX	DESIGNED BY: JWS
	CHECKED BY: DEF

SIGNING & STRIPING PLAN

SS-07
SHEET NO. 48
TOTAL SHTS. 49

SIGN SCHEDULE

#	SHEET NO.	PLAN INDICATOR	CODE	QTY.	DESCRIPTION	ASSEMBLY NO.	SIGN WIDTH (IN)	SIGN HEIGHT (IN)	SIGN AREA (SF)	ITEM 749687 SINGLE POST (EACH)			ITEM 749690 MULTI POST (POST NEEDED)			POST INSTALLATION TYPE	Code X11 12' Post (W/ Basepost)	ITEM 749688 4" HOLE, 0-6" (EACH)	ITEM 749689 4" HOLE, >6" (EACH)	REMARKS
										SIGN DISPOSTION	REMOVE	INSTALL	SIGN DISPOSTION	REMOVE	INSTALL					
SS-03		1	R1-2 (36)	1	"YIELD"	1				REMAIN						0	0	0		
		2	R5-1A (24)	1	"WRONG WAY"	2				REMAIN						0	0	0		
		3	R5-1 (30)	1	"DO NOT ENTER"	3				REMAIN						0	0	0		
		4A	D9-13aP	1	HOSPITAL	4				REMAIN						0	0	0		
		4B	D9-2 (24)	1	HOSPITAL	4				REMAIN						0	0	0		
		4C		1	DESTINATION GUIDE SIGN (BROWN)	4				REMAIN						0	0	0	SEAFORD MUSEUM	
		5A	R12-3-DE (36)	1	WEIGHT LIMIT	5				REMAIN						0	0	0		
		5B	M6-1 (15)	1	DIRECTIONAL ARROW	5				REMAIN						0	0	0		
		5C		1	DESTINATION GUIDE SIGN (GREEN)	5				REMAIN						0	0	0	FIRE STATION	
		6A		1	DESTINATION GUIDE SIGN (BROWN)	6				REMAIN						0	0	0	SEAFORD BOAT RAMP	
		6B		1	DESTINATION GUIDE SIGN (BROWN)	6				REMAIN						0	0	0	NANTICOOKE RIVER WATER TRAIL	
		7	R1-2 (36)	1	"YIELD"	7				REMAIN						0	0	0		
		8	R2-1 (36)	2	SPEED LIMIT (45 MPH)	8				REMAIN						0	0	0		
		9	R1-1 (36)	1	"STOP"	9				REMAIN						0	0	0		
		10	D2-3 (42)	1	DISTANCE (3 LINES)	10				REMAIN						0	0	0		
		11	R11-1	1	KEEP OFF MEDIAN	11				REMAIN						0	0	0	NO CROSSING OR PARKING IN MEDIAN	
SS-04		12	D1-1 (18)	1	DESTINATION (1 LINE)	12				REMAIN						0	0	0		
		13	W3-3 (30)	1	SIGNAL AHEAD	13				REMAIN						0	0	0		
		14A	W3-3 (30)	1	SIGNAL AHEAD	14				REMAIN						0	0	0		
		14B	D3-1	1	STREET NAME (1 LINE)	14				REMAIN						0	0	0		
		14C	I-2-DE2 (24)	1	NOW LEAVING CORPORATE LIMITS	14				REMAIN						0	0	0		
SS-05		15	R1-1 (36)	1	"STOP"	15				REMAIN						0	0	0		
		16	R4-3-DE (36)	1	TRUCKS KEEP RIGHT EXCEPT LEFT TURNS	16				REMAIN						0	0	0		
		17A	R4-3-DE (36)	1	TRUCKS KEEP RIGHT EXCEPT LEFT TURNS	17				REMAIN						0	0	0		
		17B	D10-2a (36)	1	INTERMEDIATE REFERENCE LOCATION (3 DIGITS)	17				REMAIN						0	0	0		
		18	R2-1 (36)	2	SPEED LIMIT (45 MPH)	18				REMAIN						0	0	0		
		19A	M3-3 (12)	1	CARDINAL DIRECTION	19				REMAIN						0	0	0		
		19B	M1-4 (24)	1	U.S. ROUTE SIGN	19				REMAIN						0	0	0		
		19C	M6-3 (15)	1	DIRECTIONAL ARROW	19				REMAIN						0	0	0		
		20	R1-2 (36)	1	"YIELD"	20				REMAIN						0	0	0		
		21A	R14-1	1	TRUCK ROUTE	21				REMAIN						0	0	0		
		21B		1	DESTINATION GUIDE SIGN (GREEN)	21				REMAIN						0	0	0	STATE ROUTE 20, WEST	
		22A	I-8 (24)	1	LIBRARY	22				REMAIN						0	0	0		
		22B		1	DESTINATION GUIDE SIGN (GREEN)	22				REMAIN						0	0	0	SEAFORD LIBRARY	
		22C	R3-20-DE (18)	1	TURN LANE	22				REMAIN						0	0	0		
		23	D10-2a (36)	1	INTERMEDIATE REFERENCE LOCATION (3 DIGITS)	23				REMAIN						0	0	0		
		24		1	DESTINATION GUIDE SIGN (GREEN)	24				REMAIN						0	0	0	STATE ROUTE 20, WEST SEAFORD	
		25	R5-1 (30)	2	"DO NOT ENTER"	25				REMAIN						0	0	0		
		26	D3-2-DE3	1	DEVELOPMENT NAME	26				REMAIN						0	0	0		
		27A	M3-4 (12)	1	CARDINAL DIRECTION	27				REMAIN						0	0	0		
		27B	M1-5 (24)	1	STATE ROUTE SIGN	27				REMAIN						0	0	0		
		27C	M6-1 (15)	1	DIRECTIONAL ARROW	27				REMAIN						0	0	0		
JOB TOTALS									0		0	0			0	0	0			

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ADDENDUMS / REVISIONS

NOT TO SCALE

**BR 3-253 N&S ON US 13
OVER WILLIAMS POND**

CONTRACT
T201507601
COUNTY
SUSSEX

BRIDGE NO. **BR 3-253 N&S**
DESIGNED BY: JWS
CHECKED BY: DEF

**SIGNING & STRIPING
PLAN**

SS-08
SHEET NO.
49
TOTAL SHTS.
49